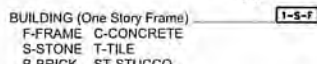
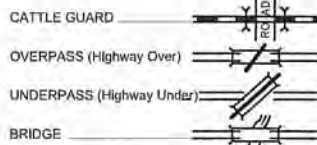
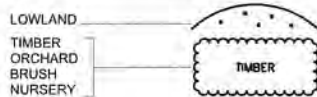


PLAN SYMBOLS

- COUNTY LINE
- TOWNSHIP OR RANGE LINE
- SECTION LINE
- QUARTER LINE
- SIXTEENTH LINE
- RIGHT OF WAY LINE
- SLOPE EASEMENT
- EXISTING RIGHT OF WAY
- PROPERTY LINE
- CORPORATE OR CITY LIMITS
- RETAINING WALL
- RAILROAD
- RAILROAD RIGHT OF WAY
- RIVER OR CREEK
- DRAINAGE DITCH
- CULVERT
- DROP INLET
- GUARD RAIL
- BARBED WIRE FENCE
- WOVEN WIRE FENCE
- CHAIN LINK FENCE
- WOOD FENCE
- STONE WALL OR FENCE
- HEDGE

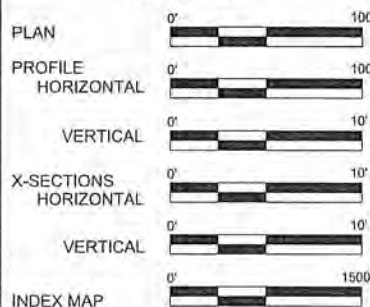


- BUILDING (One Story Frame)
- F-FRAME C-CONCRETE
- S-STONE T-TILE
- B-BRICK ST-STUCCO
- RAILROAD CROSSING BELL
- RAILROAD CROSSING GATE
- MANHOLE
- CATCH BASIN
- FIRE HYDRANT
- CAST IRON MONUMENT
- IRON PIN
- GRAVEL PIT
- SAND PIT
- BORROW PIT
- ROCK QUARRY

UTILITY SYMBOLS

- POWER POLE LINE
- TELEPHONE-OR TELEGRAPH POLE LINE
- JOINT TELEPHONE & POWER ON POWER POLES
- ON TELEPHONE POLES
- ANCHOR
- STEEL TOWER
- STREET LIGHT
- PEDESTAL (Cable Terminal)
- GAS MAIN
- WATERMAIN
- TELEPHONE CABLE IN CONDUIT
- ELECTRIC CABLE IN CONDUIT
- TELEPHONE MANHOLE
- ELECTRIC MANHOLE
- BURIED TELEPHONE CABLE
- BURIED ELECTRIC CABLE
- SEWER (Sanitary or Storm)
- SEWER MANHOLE

SCALES



MINNESOTA DEPARTMENT OF TRANSPORTATION

ANOKA COUNTY

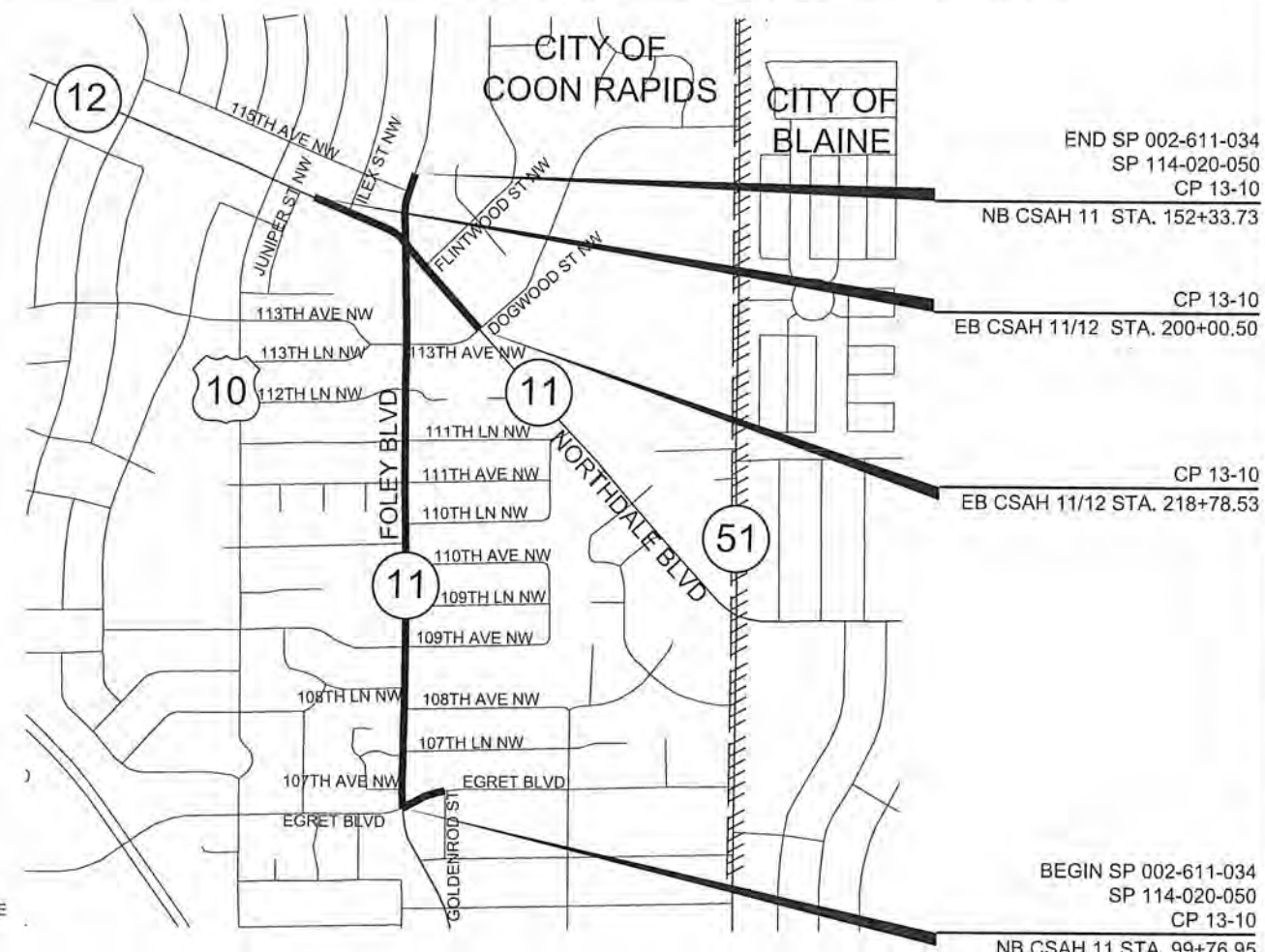
CONSTRUCTION PLAN FOR GRADING, BITUMINOUS SURFACING, DRAINAGE, SIGNAL SYSTEM AND SIGNING

LOCATED ON CSAH 11 BETWEEN EGRET BLVD NW AND 115TH AVE
 LOCATED ON CSAH 11/12 BETWEEN JUNIPER ST AND 113TH AVE/DOGWOOD ST

STATE PROJ. NO. 002-611-034

<table border="0" style="width: 100%;"> <tr> <td style="width: 30%;">GROSS LENGTH</td> <td style="width: 20%; text-align: right;">5256.78</td> <td style="width: 10%;">FEET</td> <td style="width: 10%; text-align: right;">0.996</td> <td style="width: 10%;">MILES</td> <td style="width: 10%;"></td> </tr> <tr> <td>BRIDGES-LENGTH</td> <td style="text-align: right;">0.00</td> <td>FEET</td> <td style="text-align: right;">0.000</td> <td>MILES</td> <td></td> </tr> <tr> <td>EXCEPTIONS-LENGTH</td> <td style="text-align: right;">0.00</td> <td>FEET</td> <td style="text-align: right;">0.000</td> <td>MILES</td> <td></td> </tr> <tr> <td>NET LENGTH</td> <td style="text-align: right;">5256.78</td> <td>FEET</td> <td style="text-align: right;">0.996</td> <td>MILES</td> <td></td> </tr> </table>	GROSS LENGTH	5256.78	FEET	0.996	MILES		BRIDGES-LENGTH	0.00	FEET	0.000	MILES		EXCEPTIONS-LENGTH	0.00	FEET	0.000	MILES		NET LENGTH	5256.78	FEET	0.996	MILES		<table border="0" style="width: 100%;"> <tr> <td style="width: 30%;">GROSS LENGTH</td> <td style="width: 20%; text-align: right;">1878.03</td> <td style="width: 10%;">FEET</td> <td style="width: 10%; text-align: right;">0.356</td> <td style="width: 10%;">MILES</td> <td style="width: 10%;"></td> </tr> <tr> <td>BRIDGES-LENGTH</td> <td style="text-align: right;">0.00</td> <td>FEET</td> <td style="text-align: right;">0.000</td> <td>MILES</td> <td></td> </tr> <tr> <td>EXCEPTIONS-LENGTH</td> <td style="text-align: right;">0.00</td> <td>FEET</td> <td style="text-align: right;">0.000</td> <td>MILES</td> <td></td> </tr> <tr> <td>NET LENGTH</td> <td style="text-align: right;">1878.03</td> <td>FEET</td> <td style="text-align: right;">0.356</td> <td>MILES</td> <td></td> </tr> </table>	GROSS LENGTH	1878.03	FEET	0.356	MILES		BRIDGES-LENGTH	0.00	FEET	0.000	MILES		EXCEPTIONS-LENGTH	0.00	FEET	0.000	MILES		NET LENGTH	1878.03	FEET	0.356	MILES	
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PROJECT LOCATION
 CITY OF COON RAPIDS
 ANOKA COUNTY
 MN/DOT DISTRICT - METRO
 SECTION 13, 24
 TOWNSHIP 31 NORTH
 RANGE 24 WEST



UTILITY QUALITY LEVEL NOTE:

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS UTILITY QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CII/ASCE 38-02, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA".

DESIGN DESIGNATION (CSAH 11)	
ESAL 20	1,381,000
R VALUE	40
ADT (2016)	12200
PROJ. ADT (2036)	12200
PROJ. HCADT (2036)	436
SOIL FACTOR	NA
10 TON DESIGN	
FUNCTIONAL CLASSIFICATION	A MINOR EXPANDER
NO. OF TRAFFIC LANES	4
NO. OF PARKING LANES	0
DESIGN SPEED	40 MPH
STOPPING SIGHT DISTANCE BASED ON:	
HEIGHT OF EYE	3.5'
HEIGHT OF OBJECT	2.0'
DESIGN SPEED NOT ACHIEVED AT:	
STA. _____ TO STA. _____	MPH _____

DESIGN DESIGNATION (CSAH 11/12)	
ESAL 20	1,291,000
R VALUE	40
ADT (2016)	11400
PROJ. ADT (2036)	11400
PROJ. HCADT (2036)	407
SOIL FACTOR	NA
10 TON DESIGN	
FUNCTIONAL CLASSIFICATION	A MINOR EXPANDER
NO. OF TRAFFIC LANES	2
NO. OF PARKING LANES	0
DESIGN SPEED	35 MPH
STOPPING SIGHT DISTANCE BASED ON:	
HEIGHT OF EYE	3.5'
HEIGHT OF OBJECT	2.0'
DESIGN SPEED NOT ACHIEVED AT:	
STA. _____ TO STA. _____	MPH _____

MINN. PROJ. NO. _____ STPM 0216 (004)

GOVERNING SPECIFICATIONS

THE 2014 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" AND THE 2014 EDITION OF THE "MATERIALS LAB SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN.

ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING THE LATEST FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS.

INDEX

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8	SOILS AND CONSTRUCTION NOTES
9 - 19	STANDARD PLATES, BASIS OF QUANTITIES, TABULATIONS
20 - 22	EARTHWORK TABULATION & BALANCE
23 - 25	TYPICAL SECTIONS
26	MISCELLANEOUS DETAILS
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110 - 116	RETAINING WALL STANDARD PLANS AND DETAILS
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134 - 137	EXISTING UTILITY PLAN
138 - 139	SWPPP NARRATIVE
140 - 149	SIGNING & STRIPING PLAN, TAB., & DETAILS
150 - 153	EROSION CONTROL PLAN
154 - 157	TURF ESTABLISHMENT PLAN
158 - 162	TEMPORARY & PERMANENT EROSION CONTROL DETAILS
163 - 191	TRAFFIC SIGNAL PLANS
192 - 197	WATERMAIN PLAN
198 - 298	CROSS SECTIONS

THIS PLAN CONTAINS 298 SHEETS

APPROVED DATE 5/27/16
 ANOKA COUNTY ENGINEER

APPROVED DATE 6/1/16
 CITY OF COON RAPIDS ENGINEER

Elisa Battar DATE 6/1/16
 DISTRICT STATE AID ENGINEER: REVIEWED FOR COMPLIANCE WITH STATE AID AND FEDERAL AID RULES/POLICY

Elisa Battar DATE 6/1/16
 APPROVED FOR STATE AID AND FEDERAL AID FUNDING: STATE AID ENGINEER

NO	DATE	BY	CKD	APPR	REVISION

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE:
 DATE: 5-19-16 LICENSE NO. 22713

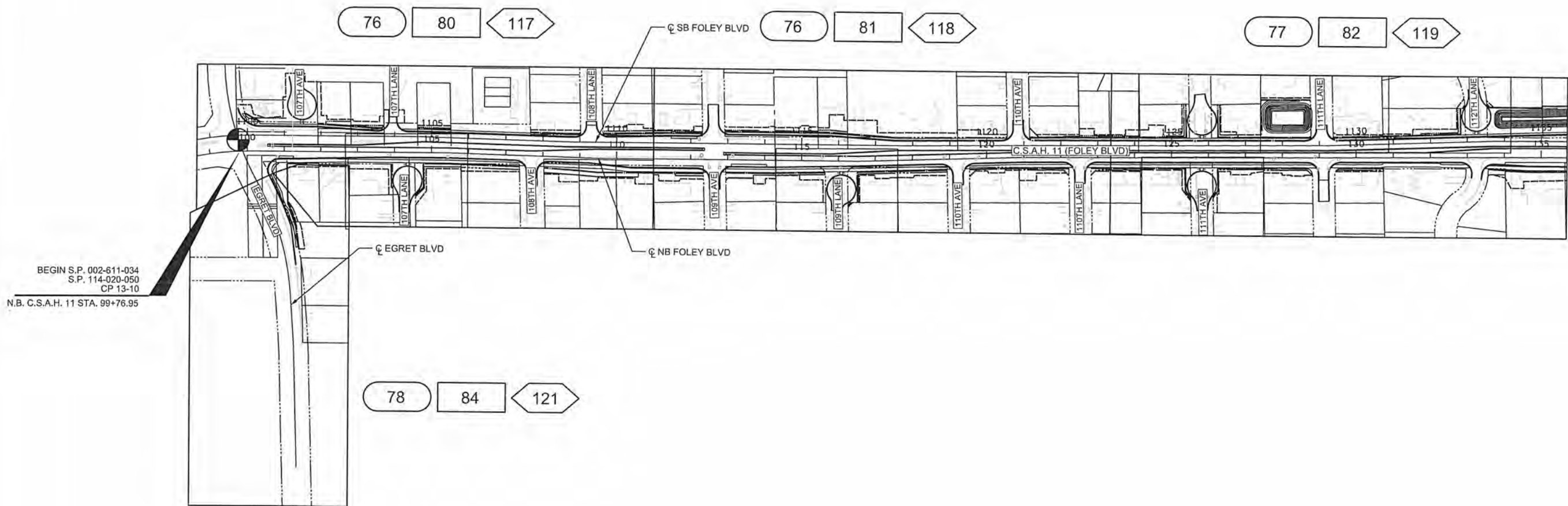
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 DESIGN BY: OFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

TITLE SHEET

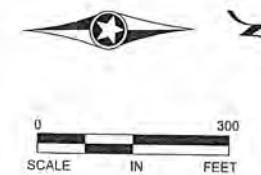
Sheet 1 of 298 Sheets



LEGEND

- INPLACE TOPOGRAPHY AND REMOVAL PLAN SHEET NUMBER
- CONSTRUCTION PLAN SHEET NUMBER

- STORM DRAINAGE PLAN SHEET NUMBER
- INPLACE SIGNAL SYSTEM



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_GL_P1.dgn
 05/13/2016 12:54:23 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

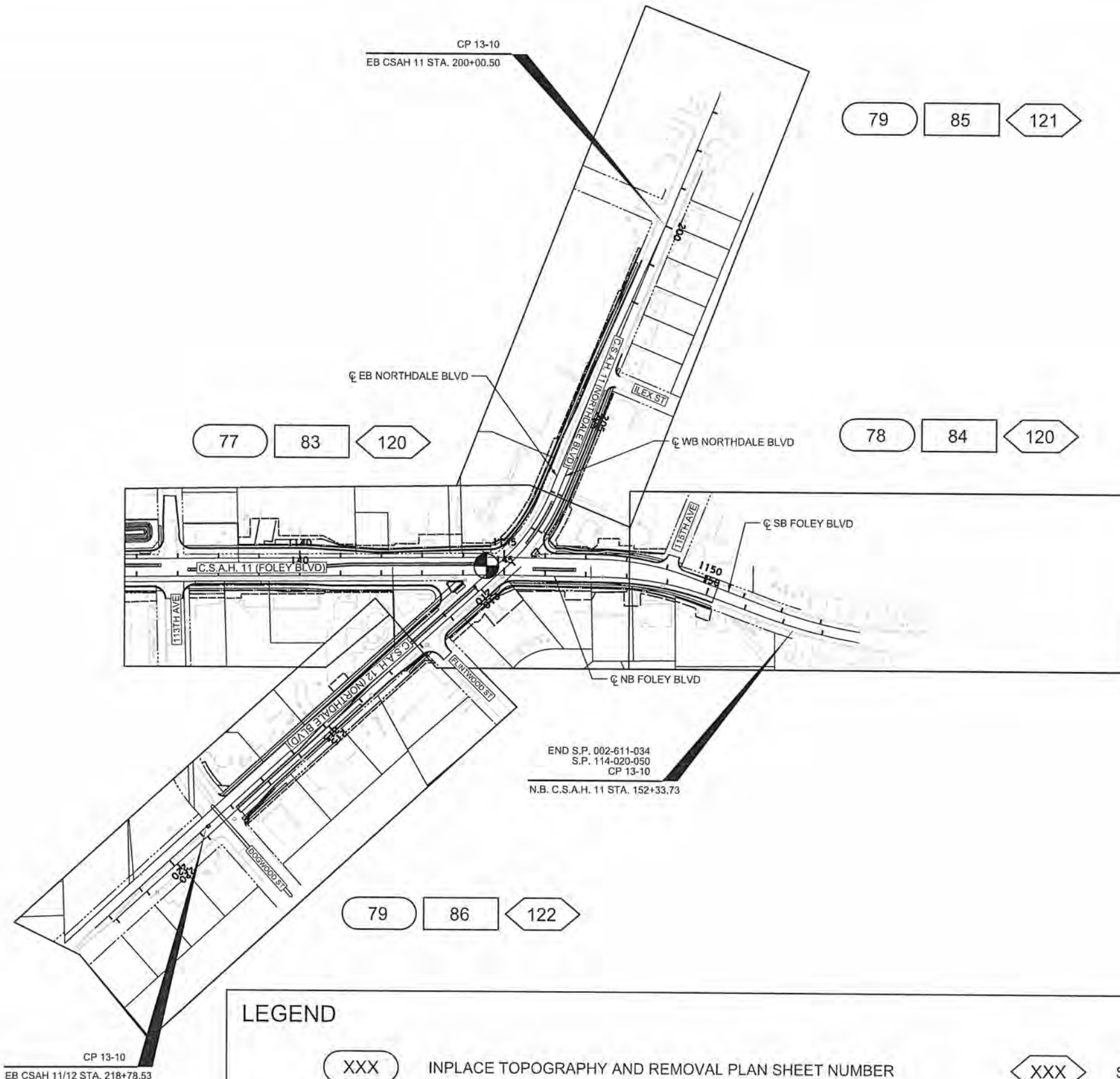
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 CHECKED BY: JEO DATE: 02/27/16



**ANOKA COUNTY
 HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

GENERAL LAYOUT



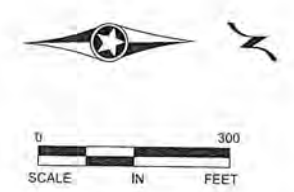
LEGEND

XXX INPLACE TOPOGRAPHY AND REMOVAL PLAN SHEET NUMBER

XXX CONSTRUCTION PLAN SHEET NUMBER

XXX STORM DRAINAGE PLAN SHEET NUMBER

INPLACE SIGNAL SYSTEM



2 OF 2

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_GL_P2.dgn 05/13/2016 12:54:24 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

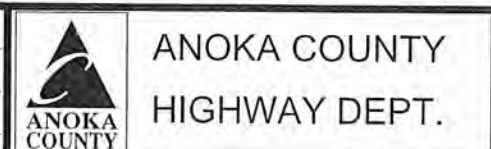
SIGNATURE: *Gina M. Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16



SP 002-611-034
SP 114-020-050
CP 13-10

GENERAL LAYOUT

Sheet 3 of 298 Sheets

TAB/ NOTE	ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL	PARTICIPATING- FEDERAL FUNDS SP 002-611-034			NON-PARTICIPATING- LOCAL FUNDS
					ANOKA COUNTY ROADWAY QUANTITIES ESTIMATED	CITY OF COON RAPIDS SP 114-020-050 ROADWAY QUANTITIES ESTIMATED	DRAINAGE QUANTITIES ESTIMATED	CITY OF COON RAPIDS CP 13-10 ROADWAY QUANTITIES ESTIMATED
	2011.601	VIBRATION MONITORING						
	2021.501	MOBILIZATION	LUMP SUM	1	1			
	2031.501	FIELD OFFICE TYPE D	LUMP SUM	1	0.48	0.07	0.15	0.3
	2041.610	TRAINEES	EACH	1	0.48	0.07	0.15	0.3
A	2101.502	CLEARING	HOUR	1000	1000			
A	2101.507	GRUBBING	TREE	337	337			
G	2102.502	PAVEMENT MARKING REMOVAL	TREE	299	299			
W	2103.507	DISCONNECT WATER SERVICE	LIN FT	11360	11360			
W	2104.501	REMOVE WATER MAIN	EACH	48	4			44
D	2104.501	REMOVE SEWER PIPE (STORM)	LIN FT	9825				9825
B	2104.501	REMOVE CURB & GUTTER	LIN FT	4716	4716			
B	2104.501	REMOVE RETAINING WALL	LIN FT	17324	17324			
B	2104.501	REMOVE SANITARY SERVICE PIPE	LIN FT	652	652			
W	2104.501	REMOVE WATER SERVICE PIPE	LIN FT	270	270			
B	2104.505	REMOVE CONCRETE WALK	LIN FT	2465				2465
B	2104.505	REMOVE CONCRETE DRIVEWAY PAVEMENT	SQ YD	4379	4379			
B	2104.505	REMOVE BITUMINOUS DRIVEWAY PAVEMENT	SQ YD	2155	2155			
B/[9]	2104.505	REMOVE BITUMINOUS PAVEMENT	SQ YD	3022	3022			
W	2104.509	REMOVE UTILITY VAULT	SQ YD	42049	41574			
W	2104.509	REMOVE GATE VALVE & BOX	EACH	6				475
W	2104.509	REMOVE HYDRANT	EACH	37				6
D	2104.509	REMOVE DRAINAGE STRUCTURE	EACH	16				37
K	2104.509	REMOVE SIGN TYPE C	EACH	63	63			16
	2104.509	REMOVE SIGNAL SYSTEM	EACH	136	136			
B	2104.511	SAWING CONCRETE PAVEMENT (FULL DEPTH)	EACH	1	1			
B	2104.513	SAWING BIT PAVEMENT (FULL DEPTH)	LIN FT	711	711			
			LIN FT	3373	3303			70
K	2104.523	SALVAGE SIGN TYPE SPECIAL	EACH	22	22			
	2105.501	COMMON EXCAVATION	CU YD	29605	29605			
	2105.507	SUBGRADE EXCAVATION	CU YD	19460	19460			
	2105.522	SELECT GRANULAR BORROW (LV)	CU YD	9997	9997			
	2105.607	COMMON BORROW SPECIAL (CV) (POND A)	CU YD	597				
	2105.607	COMMON BORROW SPECIAL (CV) (POND B)	CU YD	871			597	
							871	
	2130.501	WATER						
E, C/[8]	2211.503	AGGREGATE BASE (CV) CLASS 5 (P)	M GALLONS	500	500			
J	2357.502	BITUMINOUS MATERIAL FOR TACK COAT	CU YD	13635	9754	381		3500
C, J	2360.501	TYPE SP 9.5 WEARING COURSE MIX (2,B)	GALLON	4808	4808			
J/[9]	2360.501	TYPE SP 12.5 WEARING COURSE MIX (3,F)	TON	900	407	493		
J	2360.502	TYPE SP 12.5 NON WEAR COURSE MIX (3,B)	TON	11118	11058			60
F	2411.618	MODULAR BLOCK RETAINING WALL	TON	5748	5748			
			SQ FT	1888	1888			
P	2501.515	15" RC PIPE APRON	EACH	1				
P	2501.515	27" RC PIPE APRON	EACH	1			1	
P	2501.515	30" RC PIPE APRON	EACH	1			1	
[7]	2502.541	4" PERF TP PIPE DRAIN	EACH	1			1	
			LIN FT	720			720	

GENERAL NOTES:

- [1] SEE TRAFFIC CONTROL PLAN SHEETS 54 - 72
- [2] SEE TRAFFIC SIGNAL PLAN SHEETS 163 - 191
- [3] SEE PERMANENT SIGNING TAB SHEET 144 (STREET SIGNS PAID FOR AS ITEM 2564.537)
- [4] SEE TEMPORARY TRAFFIC SIGNAL PLAN SHEETS 168 - 174
- [6] SEE SHEET 132 FOR BAFFLE PANEL DIMENSIONS AND DETAILS
- [7] SEE SHEET 127 (POND B) FOR DETAILS
- [8] SEE NOTE 4 ON WATERMAIN TABULATION (SHEET 14) FOR CITY OF COON RAPIDS LOCAL FUNDS QUANTITY
- [9] SEE SHEET 197 FOR CITY OF COON RAPIDS QUANTITY

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_SEQ.dgn
07/08/2016 9:05:42 AM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 7-8-16 LICENSE NO. 22713

DRAWN BY DFF DATE 02/27/16
 DESIGN BY DFF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

STATEMENT OF
ESTIMATED QUANTITIES

Sheet 4 of 298 Sheets

TAB/ NOTE	ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL	PARTICIPATING- FEDERAL FUNDS SP 002-611-034			NON-PARTICIPATING- LOCAL FUNDS
					ANOKA COUNTY ROADWAY QUANTITIES ESTIMATED	CITY OF COON RAPIDS SP 114-020-050 ROADWAY QUANTITIES ESTIMATED	DRAINAGE QUANTITIES ESTIMATED	CITY OF COON RAPIDS CP 13-10 ROADWAY QUANTITIES ESTIMATED
P	2503.521	28" SPAN RC PIPE-ARCH SEWER CL IIA	LIN FT	660			660	
P	2503.541	15" RC PIPE SEWER DES 3006 CL V	LIN FT	3009			3009	
P	2503.541	18" RC PIPE SEWER DES 3006 CL V	LIN FT	1636			1636	
P	2503.541	21" RC PIPE SEWER DES 3006 CL III	LIN FT	716			716	
P	2503.541	21" RC PIPE SEWER DES 3006 CL V	LIN FT	547			547	
P	2503.541	24" RC PIPE SEWER DES 3006 CL III	LIN FT	530			530	
P	2503.541	24" RC PIPE SEWER DES 3006 CL V	LIN FT	284			284	
P	2503.541	27" RC PIPE SEWER DES 3006 CL III	LIN FT	467			467	
P	2503.541	27" RC PIPE SEWER DES 3006 CL V	LIN FT	381			381	
P	2503.541	30" RC PIPE SEWER DES 3006 CL III	LIN FT	138			138	
P	2503.541	30" RC PIPE SEWER DES 3006 CL V	LIN FT	202			202	
P	2503.541	36" RC PIPE SEWER DES 3006 CL III	LIN FT	567			567	
P	2503.541	36" RC PIPE SEWER DES 3006 CL V	LIN FT	37			37	
W	2504.601	TEMPORARY WATER SERVICE	LUMP SUM	1				
W	2504.602	CONNECT TO EXISTING WATER MAIN	EACH	31				1
W	2504.602	CONNECT TO EXISTING WATER SERVICE	EACH	43				31
W	2504.602	HYDRANT	EACH	30				43
W	2504.602	1" CORPORATION STOP	EACH	42				30
W	2504.602	2" CORPORATION STOP	EACH	1				42
W	2504.602	16" BUTTERFLY VALVE & BOX	EACH	1				1
W	2504.602	18" BUTTERFLY VALVE & BOX	EACH	11				11
W	2504.602	30" BUTTERFLY VALVE & BOX	EACH	3				3
W	2504.602	6" GATE VALVE & BOX	EACH	34				34
W	2504.602	8" GATE VALVE & BOX	EACH	7				7
W	2504.602	12" GATE VALVE & BOX	EACH	2				2
W	2504.602	6" GATE VALVE	EACH	30				30
W	2504.602	CURB STOP & BOX	EACH	43				43
W	2504.603	WATER SERVICE (DIRECTIONAL DRILLED)	LIN FT	1200				1200
W	2504.603	HYDRANT RISER	LIN FT	2				2
W	2504.603	LOWER WATERMAIN	LIN FT	655				655
W	2504.603	TAP SERVICE PIPING 1"	LIN FT	1120				1120
W	2504.603	TAP SERVICE PIPING 2"	LIN FT	40				40
W	2504.603	6" WATERMAIN DUCTILE IRON CL 52	LIN FT	2930				2930
W	2504.603	8" WATERMAIN DUCTILE IRON CL 52	LIN FT	245				245
W	2504.603	16" WATERMAIN DUCTILE IRON CL 52	LIN FT	30				30
W	2504.603	30" WATERMAIN DUCTILE IRON CL 52	LIN FT	260				260
W	2504.603	8" PVC WATERMAIN	LIN FT	465				465
W	2504.603	12" PVC WATERMAIN	LIN FT	435				435
W	2504.603	18" PVC WATERMAIN	LIN FT	5105				5105
W	2504.603	30" PVC WATERMAIN	LIN FT	425				425
W	2504.604	4" POLYSTYRENE INSULATION	SQ YD	186				186
W	2504.608	DUCTILE IRON FITTINGS	POUND	64000				64000
P	2506.501	CONST DRAINAGE STRUCTURE DESIGN G	LIN FT	175			175	
P	2506.501	CONST DRAINAGE STRUCTURE DES 48-4020	LIN FT	246			246	
P	2506.501	CONST DRAINAGE STRUCTURE DES 54-4020	LIN FT	194			194	
P	2506.501	CONST DRAINAGE STRUCTURE DES 60-4020	LIN FT	119			119	

GENERAL NOTES:

- [1] SEE TRAFFIC CONTROL PLAN SHEETS 54 - 72
- [2] SEE TRAFFIC SIGNAL PLAN SHEETS 163 - 191
- [3] SEE PERMANENT SIGNING TAB SHEET 144 (STREET SIGNS PAID FOR AS ITEM 2564.537)
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- [6] SEE SHEET 132 FOR BAFFLE PANEL DIMENSIONS AND DETAILS
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NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_SEQ.dgn					
07/09/2016 9:05:48 AM					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *GINA M. PIZZO*
 DATE: 7-8-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



**ANOKA COUNTY
HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

STATEMENT OF
ESTIMATED QUANTITIES

TAB/ NOTE	ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL	PARTICIPATING- FEDERAL FUNDS SP 002-611-034			NON-PARTICIPATING- LOCAL FUNDS
					ANOKA COUNTY ROADWAY QUANTITIES ESTIMATED	CITY OF COON RAPIDS SP 114-020-050 ROADWAY QUANTITIES ESTIMATED	DRAINAGE QUANTITIES ESTIMATED	CITY OF COON RAPIDS CP 13-10 ROADWAY QUANTITIES ESTIMATED
P	2506.501	CONST DRAINAGE STRUCTURE DES 66-4020	LIN FT	62			62	
P	2506.501	CONST DRAINAGE STRUCTURE DES 72-4020	LIN FT	58			58	
P	2506.501	CONST DRAINAGE STRUCTURE DES 78-4020	LIN FT	9			9	
P	2506.501	CONST DRAINAGE STRUCTURE DES 84-4020	LIN FT	12			12	
P	2506.501	CONST DRAINAGE STRUCTURE DES 120-4020	LIN FT	23			23	
S	2506.503	RECONSTRUCT DRAINAGE STRUCTURE	LIN FT	55		55		
P	2506.516	CASTING ASSEMBLY	EACH	192			192	
S	2506.522	ADJUST FRAME & RING CASTING	EACH	32	32			
P	2506.602	CONNECT INTO EXISTING DRAINAGE STRUCTURE	EACH	2			2	
P	2506.602	CONNECT INTO EXISTING STORM SEWER	EACH	3			3	
[6]	2506.602	BAFFLE PANEL	EACH	5			5	
P	2511.501	RANDOM RIPRAP CLASS II	CU YD	15			15	
P	2511.515	GEOTEXTILE FILTER TYPE III	SQ YD	69			69	
I	2521.501	4" CONCRETE WALK	SQ FT	56833	56833			
I	2521.501	6" CONCRETE WALK	SQ FT	9381	6894	2487		
I	2521.618	SPECIAL SURFACE TREATMENT	SQ FT	25398				25398
I	2531.501	CONCRETE CURB & GUTTER DESIGN B418 (MOD)	LIN FT	10686	10686			
I	2531.501	CONCRETE CURB & GUTTER DESIGN B424	LIN FT	13578	6789	6789		
I	2531.501	CONCRETE CURB & GUTTER DESIGN B612	LIN FT	224	224			
I	2531.501	CONCRETE CURB & GUTTER DESIGN B618 (MOD)	LIN FT	538	269	269		
I	2531.501	CONCRETE CURB & GUTTER DESIGN S518	LIN FT	1476	738	738		
C	2531.507	6" CONCRETE DRIVEWAY PAVEMENT	SQ YD	822	822			
I	2531.618	TRUNCATED DOMES	SQ FT	408	312	96		
	2533.507	PORTABLE PRECAST CONCRETE BARRIER DES 8337	LIN FT	3912	1917	274	587	1134
	2533.508	RELOCATE PORTABLE PRECAST CONCRETE BARRIER DESIGN 8337	LIN FT	260	128	18	39	75
W	2540.602	INSTALL MONUMENT BOX	EACH	6				6
C	2540.602	RELOCATE MAIL BOX SUPPORT	EACH	27	27			
	2554.615	IMPACT ATTENUATOR	ASSEMBLY	7	4		1	2
L	2557.501	WIRE FENCE DESIGN 48V-9322	LIN FT	641	641			
H	2557.517	VEHICULAR GATE-SPECIAL	EACH	2	2			
H	2557.602	INSTALL VEHICULAR GATE	EACH	10	10			
	2563.601	TRAFFIC CONTROL SUPERVISOR	LUMP SUM	1	0.49	0.07	0.15	0.29
[1]	2563.601	TRAFFIC CONTROL	LUMP SUM	1	0.49	0.07	0.15	0.29
O/(1)	2563.602	RAISED PAVEMENT MARKER TEMPORARY	EACH	882	432	62	132	256
	2563.610	POLICE OFFICER	HOURLY	60	30	4	9	17
[1]	2563.613	PORTABLE CHANGEABLE MESSAGE SIGN	UNIT DAY	42	42			
[3]	2564.531	SIGN PANELS TYPE C	SQ FT	1312	1312			
[3]	2564.537	INSTALL SIGN TYPE SPECIAL	EACH	22	22			
[2]	2565.511	TRAFFIC CONTROL SIGNAL SYSTEM B	SIG SYS	1	0.375	0.625		
[2]	2565.601	EMERGENCY VEHICLE PREEMPTION SYSTEM "B"	LUMP SUM	1		1		
[2]	2565.601	TRAFFIC CONTROL INTERCONNECTION	LUMP SUM	1	1			
[2]	2565.602	SIGNAL SERVICE CABINET	EACH	2	0.625	1.375		
[2]	2565.616	REVISE SIGNAL SYSTEM "A"	SYSTEM	1	0.25	0.75		

GENERAL NOTES:

- [1] SEE TRAFFIC CONTROL PLAN SHEETS 54 - 72
- [2] SEE TRAFFIC SIGNAL PLAN SHEETS 163 - 191
- [3] SEE PERMANENT SIGNING TAB SHEET 144 (STREET SIGNS PAID FOR AS ITEM 2564.537)
- [4] SEE TEMPORARY TRAFFIC SIGNAL PLAN SHEETS 168 - 174
- [6] SEE SHEET 132 FOR BAFFLE PANEL DIMENSIONS AND DETAILS
- [7] SEE SHEET 127 (POND B) FOR DETAILS
- [8] SEE NOTE 4 ON WATERMAIN TABULATION (SHEET 14) FOR CITY OF COON RAPIDS LOCAL FUNDS QUANTITY
- [9] SEE SHEET 197 FOR CITY OF COON RAPIDS QUANTITY

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:02-611-34\Plan\002-611-034_SEQ.dgn 07/06/2016 1:34:12 PM

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**ANOKA COUNTY
HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

STATEMENT OF
ESTIMATED QUANTITIES

TAB/ NOTE	ITEM NO.	ITEM DESCRIPTION	UNIT	TOTAL	PARTICIPATING- FEDERAL FUNDS SP 002-611-034			NON-PARTICIPATING- LOCAL FUNDS
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[4]	2565.616	TEMPORARY SIGNAL SYSTEM A	SYSTEM	1	0.49	0.07	0.15	0.29
[4]	2565.616	TEMPORARY SIGNAL SYSTEM B	SYSTEM	1	0.49	0.07	0.15	0.29
N	2573.502	SILT FENCE, TYPE MS	LIN FT	3370	3370			
N	2573.530	STORM DRAIN INLET PROTECTION	EACH	195	195			
N	2574.508	FERTILIZER TYPE 2	POUND	532	532			
N/[9]	2575.501	SEEDING	ACRE	1.03	1			0.03
N	2575.502	SEED MIXTURE 25-131	POUND	151	151			
N	2575.502	SEED MIXTURE 25-151	POUND	30	30			
N	2575.502	SEED MIXTURE 33-261	POUND	19	19			
N, P	2575.505	SODDING TYPE SALT TOLERANT	SQ YD	12871	12871			
N	2575.511	MULCH MATERIAL TYPE 3	TON	3	3			
N	2575.519	DISK ANCHORING	ACRE	1	1			
N	2575.523	EROSION CONTROL BLANKETS CATEGORY 00	SQ YD	40618	40618			
O/(1)	2581.501	REMOVABLE PREFORMED PLASTIC MARKING (BLACK)	LIN FT	60	60			
O/(1)	2581.501	REMOVABLE PREFORMED PLASTIC MARKING (WHITE)	LIN FT	4175	4175			
O/(1)	2581.501	REMOVABLE PREFORMED PLASTIC MARKING (YELLOW)	LIN FT	3648	3648			
O/(1)	2582.501	PAVEMENT MESSAGE (LEFT ARROW) PAINT	EACH	2	2			
O/(1)	2582.501	PAVEMENT MESSAGE (RIGHT ARROW) PAINT	EACH	1	1			
M	2582.501	PAVEMENT MESSAGE (LT ARROW) PREFORMED THERMOPLASTIC	EACH	10	10			
M	2582.501	PAVEMENT MESSAGE (RT ARROW) PREFORMED THERMOPLASTIC	EACH	9	9			
O/(1)	2582.502	4" SOLID LINE WHITE-PAINT	LIN FT	37325	37325			
O/(1)	2582.502	8" SOLID LINE WHITE-PAINT	LIN FT	135	135			
O/(1)	2582.502	24" SOLID LINE WHITE-PAINT	LIN FT	73	73			
O/(1)	2582.502	4" SOLID LINE YELLOW-PAINT	LIN FT	14090	14090			
O/(1)	2582.502	24" SOLID LINE YELLOW-PAINT	LIN FT	340	340			
O/(1)	2582.502	4" DOUBLE SOLID LINE YELLOW-PAINT	LIN FT	14770	14770			
M	2582.502	24" SOLID LINE WHITE - PREFORMED THERMOPLASTIC	LIN FT	254	254			
M	2582.502	24" SOLID LINE YELLOW - PREFORMED THERMOPLASTIC	LIN FT	395	395			
M	2582.502	4" SOLID LINE WHITE-EPOXY	LIN FT	18737	18737			
M	2582.502	4" BROKEN LINE WHITE-EPOXY	LIN FT	1756	1756			
M	2582.502	4" SOLID LINE YELLOW-EPOXY	LIN FT	10040	10040			
M	2582.502	4" DOUBLE SOLID LINE YELLOW-EPOXY	LIN FT	2220	2220			
O/(1)	2582.503	CROSSWALK MARKING-PAINT	SQ FT	342	342			
M	2582.503	CROSSWALK MARKING - PREFORMED THERMOPLASTIC	SQ FT	1782	1782			

GENERAL NOTES:

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**ANOKA COUNTY
HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

STATEMENT OF
ESTIMATED QUANTITIES

SOILS AND CONSTRUCTION NOTES:

1. TOP OF THE GRADING SUBGRADE (GRADING GRADE) IS DEFINED AS THE BOTTOM OF THE CLASS 5 AGGREGATE BASE LAYER.
2. BOTTOM OF SUBBASE GRADE SHALL BE DEFINED AS THE BOTTOM OF THE 1' SUBGRADE EXCAVATION.
3. SUITABLE GRADING MATERIAL ON THIS PROJECT SHALL CONSIST OF ALL GRANULAR AND FINER GRAINED SOILS ENCOUNTERED WITH THE EXCEPTION OF TOPSOIL, DEBRIS, PEAT, MUCK, ORGANIC MATERIAL, AND OTHER UNSTABLE MATERIAL.
4. NO OVER EXCAVATION WILL BE ALLOWED.
5. SELECT GRANULAR MATERIAL SHALL MEET THE REQUIREMENTS OF MN/DOT SPEC. 3149.2B2.
6. ALL TOPSOIL STRIPPING WILL BE CONSIDERED TO BE COMMON EXCAVATION.
7. COMMON BORROW MEETING THE REQUIREMENTS OF SELECT GRANULAR BORROW SHALL BE USED TO BACK FILL THE EMBANKMENT UNDER THE NEW ROADWAY CORE, UP TO THE TOP OF THE GRADING SUBGRADE AS APPROVED BY THE ENGINEER.
8. WHENEVER THE WORD "INCIDENTAL" IS USED IN THIS PLAN, IT SHALL MEAN THIS WORK WILL BE INCIDENTAL FOR WHICH NO DIRECT COMPENSATION WILL BE MADE.
9. TOPSOIL SHALL BE DEFINED AS EXISTING SOILS WHICH MEET MN/DOT SPECIFICATION 3877 THAT WOULD BE SUITABLE FOR REUSE.
10. SUITABLE GRADING MATERIAL OBTAINED FROM COMMON EXCAVATION NOT MEETING THE REQUIREMENTS OF MN/DOT SPEC 3149.2B1, SHALL BE USED AS EMBANKMENT MATERIAL ON THE PROJECT AS APPROVED BY THE ENGINEER.
11. UNSUITABLE MATERIALS ARE TOPSOILS, PAVEMENT OR CONCRETE DEBRIS, PEAT, MUCK AND ORGANIC OR OTHER UNSTABLE SOILS.
12. EXCESS TOPSOIL AND MUCK MATERIAL SHALL BE USED THROUGHOUT THE PROJECT AND AS DIRECTED BY THE ENGINEER.
13. REGULAR EMBANKMENT SHALL BE DEFINED AS ALL GRADING MATERIALS THAT ARE APPROPRIATE FOR REUSE ON THE PROJECT BUT THAT MAY NOT MEET THE REQUIREMENTS OF SUITABLE GRADING MATERIALS. REGULAR EMBANKMENT MAY CONSIST OF GRADING SOILS NOT MEETING GRANULAR SPECIFICATIONS AND THEREFORE NOT SUITABLE FOR REUSE UNDER ROAD CORE. REGULAR EMBANKMENT MAY CONSIST OF TOPSOIL AND ORGANIC SOILS; UNLESS THE ENGINEER DETERMINES THESE SOILS ARE NOT REUSABLE, IN WHICH CASE THE CONTRACTOR SHALL REMOVE THEM FROM THE PROJECT LIMITS. REGULAR EMBANKMENT SHALL NOT CONSIST OF DEBRIS.
14. UNLESS OTHERWISE SPECIFICALLY ALLOWED OR REQUIRED BY THE CONTRACT, BITUMINOUS AND CONCRETE ITEMS DISTURBED BY CONSTRUCTION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE RECYCLED TO THE EXTENT ALLOWED IN BASE AND SURFACING ITEMS OR DISPOSED OF OUTSIDE THE RIGHT-OF-WAY IN ACCORDANCE WITH SPEC. 2104.3C3.
15. UNLESS OTHERWISE REQUIRED, IN ALL TREATMENTS, THE CONTRACTOR SHOULD STRIVE TO SUBSTANTIALLY MATCH THE SOILS INPLACE IN THE UPPER 5.0 FEET OF THE ROADWAY.
16. WHERE CONNECTING NEW SURFACING ADJACENT TO ANY INPLACE PAVEMENTS TO BE WIDENED, CUT VERTICALLY TO THE BOTTOM OF THE INPLACE SURFACING OR TO THE BOTTOM OF THE NEW SURFACING DESIGN, WHICHEVER IS DEEPER, THEN AT 1:2 SLOPE TO THE BOTTOM OF THE RECOMMENDED SUBGRADE EXCAVATION.
17. WHERE CONNECTING TO THE INPLACE ROADWAYS AT THE TERMINI OF PROPOSED NEW CONSTRUCTION, CUT VERTICALLY TO THE BOTTOM OF THE INPLACE SURFACING OR TO THE BOTTOM OF THE NEW SURFACING DESIGN, WHICHEVER IS DEEPER, THEN AT A 1:20 TAPER TO THE BOTTOM OF THE RECOMMENDED SUBGRADE EXCAVATION.
18. WHERE MATCHING INTO INPLACE CROSSROADS, CUT VERTICALLY TO THE BOTTOM OF THE INPLACE SURFACING OR TO THE BOTTOM OF THE NEW SURFACING DESIGN, WHICHEVER IS DEEPER, THEN AT A 1:4 TAPER TO THE BOTTOM OF THE RECOMMENDED SUBGRADE EXCAVATION.
19. USE TACK COAT BETWEEN ALL BITUMINOUS MIXTURES AND PRIOR TO PLACING ANY BITUMINOUS MIXTURES ON THE EXISTING PAVEMENT. THE BITUMINOUS TACK COAT MATERIAL SHALL BE APPLIED AT A UNIFORM RATE OF 0.03 TO 0.05 GALLONS/SQ. YD. BETWEEN BITUMINOUS LAYERS AND 0.07 TO 0.10 GALLONS/SQ. YD. ON CONCRETE OR MILLED BITUMINOUS SURFACES PRIOR TO BEING OVERLAID. THE APPLICATION RATES ARE FOR UNDILUTED EMULSIONS (AS SUPPLIED FROM THE REFINERY) OR MC AND RC LIQUID ASPHALTS. THE ASPHALT EMULSION MAY BE FURTHER DILUTED IN THE FIELD IN ACCORDANCE WITH SPECIFICATION 2357.
20. PROVIDE A SAWCUT WHERE PLACING NEW PAVEMENT ADJACENT TO INPLACE PAVEMENT TO ENSURE A UNIFORM JOINT.
21. STRIP ALL TOPSOIL WHERE PRESENT IN AREAS TO BE DISTURBED BY CONSTRUCTION AND REUSE AS TOPSOIL. FOR ESTIMATING PURPOSES, THE DEPTH OF TOPSOIL AVAILABLE IS CONSIDERED TO BE 4 INCHES.
22. EMBANKMENT QUANTITIES SHOWN ON THE EARTHWORK TABULATION REPRESENT ALL EARTHWORK QUANTITIES BELOW THE PROPOSED GRADING GRADE OF ALL PERMANENT ROADWAYS. QUANTITIES REQUIRED ABOVE THE GRADING GRADE OR FOR TEMPORARY CONSTRUCTION ARE PROVIDED IN DETAIL ON THE BITUMINOUS SUMMARY TAB.
23. THE CONSTRUCTION LIMITS AS SHOWN IN THE PLANS REPRESENT THE POINT OF INTERSECTION BETWEEN THE REQUIRED FILL OR CUT SLOPE AND THE EXISTING GROUND LINE AS DEPICTED ON THE CROSS SECTIONS. THE CONSTRUCTION LIMITS DO NOT INCLUDE AREAS REQUIRED FOR SLOPE ROUNDING.
24. DITCH BOTTOMS, TOE OF FILL, CUT RUNOUTS AND THE TOP EDGE OF THE BACKSLOPES SHALL BE ROUNDED REGARDLESS OF THE SECTION USED ON THE CROSS SECTION SHEETS.
25. ANY DEBRIS WHICH MAY BE ENCOUNTERED DURING GRADING SHALL BE DISPOSED OF BY THE CONTRACTOR OFF THE PROJECT RIGHT OF WAY IN A SUITABLE DISPOSAL AREA AS APPROVED BY THE ENGINEER.
26. UNSUITABLE SOILS NOT USED ON THE PROJECT SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND REMOVED FROM THE PROJECT AND DISPOSED OF IN ACCORDANCE WITH MN/DOT SPECIFICATIONS.
27. DISPOSITION OF EXCAVATED MATERIAL SHALL BE IN ACCORDANCE WITH SPECIFICATION 2105.3D.
28. INPLACE BITUMINOUS PAVEMENT RANGES FROM 4" TO 11" THICK. FOR INFORMATION ONLY, IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY PAVEMENT DEPTH PRIOR TO PLACING BID. PAYMENT FOR BITUMINOUS REMOVAL IS BY THE SQUARE YARD REGARDLESS OF THE DEPTH.
29. AGGREGATE BASE MATERIAL SHALL MEET THE REQUIREMENTS OF MN/DOT SPEC. 3138, CLASS 5.

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<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 5%;">NO</th> <th style="width: 10%;">DATE</th> <th style="width: 5%;">BY</th> <th style="width: 5%;">CKD</th> <th style="width: 5%;">APPR</th> <th style="width: 15%;">REVISION</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table> NAME: P:\02-611-34\Final\002-611-034_SCN.dgn 05/13/2016 1:59:42 PM	NO	DATE	BY	CKD	APPR	REVISION							Sheet <u>8</u> of <u>298</u> Sheets				
NO	DATE	BY	CKD	APPR	REVISION												

THE FOLLOWING STANDARD PLATES, APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION, SHALL APPLY ON THIS PROJECT.

STANDARD PLATES

PLATE NO.	DESCRIPTION
3000L	REINFORCED CONCRETE PIPE (5 SHEETS)
3006G	GASKET JOINT FOR R.C. PIPE (2 SHEETS)
3007E	SHEAR REINFORCEMENT FOR PRECAST DRAINAGE STRUCTURES
3022C	PRECAST CONCRETE SAFETY APRON (3 SHEETS)
3100G	CONCRETE APRON FOR REINFORCED CONCRETE PIPE
3133D	RIPRAP AT RCP OUTLETS
3145G	CONCRETE PIPE OR PRECAST BOX CULVERT TIES
4005M	MANHOLE OR CATCH BASIN TYPE A & B CONE SECTIONS PRECAST - DESIGN F
4006L	MANHOLE OR CATCH BASIN PRECAST - DESIGNS G AND H
4010H	CONCRETE SHORT CONE & ADJUSTING RING (SECTIONAL CONCRETE)
4011E	PRECAST CONCRETE BASE
4018B	MANHOLE OR CATCH BASIN (REDUCER CONE SECTION PRECAST) - DESIGN D
4020J	MANHOLE OR CATCH BASIN FOR USE WITH OR WITHOUT TRAFFIC LOADS (2 SHEETS)
4026A	CONCRETE ENCASED CONCRETE ADJUSTING RINGS
4101D	RING CASTING FOR MANHOLE OR CATCH BASIN
4108F	ADJUSTING RINGS FOR CATCH BASINS AND MANHOLES
4110F	COVER CASTING FOR MANHOLE (FOR USE IN ALL TRAFFIC AREAS) - CASTING NO. 715 AND 716
4125D	CATCH BASIN FRAME CASTING (FOR SQUARE GRATE) - CASTING NO. 806
4134A	CURB BOX CASTING FOR CATCH BASIN (FOR DESIGN B CURBS)- CASTING NO. 825
4150C	GRATE CASTING FOR ALL PIPE DRAINAGE STRUCTURES
4154B	CATCH BASIN GRATE CASTING - CASTING NO. 816
4180J	MANHOLE OR CATCH BASIN STEP
7035N	CONCRETE WALK & CURB RETURNS AT ENTRANCES
7038A	DETECTABLE WARNING SURFACE TRUNCATED DOMES
7100H	CONCRETE CURB AND GUTTER (DESIGN B and DESIGN V)
7102J	CONCRETE CURB AND GUTTER (DESIGN BR, D, S, B4, B5 AND D3) (2 SHEETS)
7111J	INSTALLATION OF CATCH BASIN CASTINGS (CONCRETE CURB AND GUTTER)
7113A	CONCRETE APPROACH NOSE DETAIL
8000J	CHANNELIZERS (3 SHEETS)
8110E	TRAFFIC SIGNAL BRACKETING (POLE MOUNTED)
8118D	SERVICE EQUIPMENT & POLE TRAFFIC CONTROL SIGNALS
8119C	GROUND MOUNTED CABINET FOUNDATION
8121H	TRANSFORMER BASE AND POLE BASE PLATE (PA85, PA90 AND PA100) (2 SHEETS)
8123G	POLE AND MAST ARM - LUMINAIRES AND TRAFFIC LIGHTS ASSEMBLY (FOR ALL POLE TYPES) (2 SHEETS)
8126L	POLE FOUNDATION (PA90 AND PA100)
8337C	TEMPORARY PORTABLE PRECAST CONCRETE BARRIER (TYPE "F") (3 SHEETS)
9102E	TURF ESTABLISHMENT AREAS (AT PIPE CULVERT ENDS)
9322K	CHAIN LINK FENCE (2 SHEETS)
9350A	MAILBOX SUPPORT (SWING-AWAY TYPE)

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BASIS OF QUANTITIES

SPEC NO	DESCRIPTION	RATE
2357	BITUMINOUS MATERIAL FOR TACK COAT	0.05 GAL / SQ YD / LIFT
2360	TYPE SP9.5 WEARING COURSE MIXTURE	115 LBS / SQ YD / IN
2360	TYPE SP12.5 WEARING COURSE MIXTURE	115 LBS / SQ YD / IN
2360	TYPE SP12.5 NON-WEARING COURSE MIXTURE	115 LBS / SQ YD / IN
2575	SEED MIXTURE 25-131	220.0 POUNDS / ACRE
2575	SEED MIXTURE 25-151	120.0 POUNDS / ACRE
2575	SEED MIXTURE 33-261	35.0 POUNDS / ACRE
2575	FERTILIZER TYPE 3, 22-5-10 FOR SEED	350 POUNDS / ACRE
2575	FERTILIZER TYPE 4, 17-10-7 FOR SEED (POND)	120 POUNDS / ACRE

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ANOKA COUNTY
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STANDARD PLATES,
 BASIS OF QUANTITIES,
 INDEX OF TABULATIONS

Sheet 9 of 298 Sheets

CLEARING & GRUBBING SPEC (2101)					A
ALI.	STATION	OFFSET	CLEARING TREE	GRUBBING TREE	TYPE
LNB	101+30	168 RT	1	1	SINGLE
LNB	102+79	31 RT	1	1	SINGLE
LNB	102+81	41 RT	1	1	SINGLE
LNB	103+15	25 RT	1	1	SINGLE
LNB	103+16	39 RT	1	1	SINGLE
LNB	103+18	31 RT	1	1	SINGLE
LNB	103+37	39 RT	1	1	SINGLE
LNB	103+39	40 RT	1	1	SINGLE
LNB	103+68	37 RT	4	1	QUAD
LNB	103+85	38 RT	1	1	SINGLE
LNB	103+88	65 RT	1	1	SINGLE
LNB	103+99	57 RT	1	1	SINGLE
LNB	104+54	35 RT	1	1	STUMP
LNB	104+61	61 RT	1	1	SINGLE
LNB	104+74	37 RT	2	2	SINGLE
LNB	106+18	33 RT	1	1	SINGLE
LNB	106+40	34 RT	1	1	SINGLE
LNB	106+49	28 RT	1	1	SINGLE
LNB	106+76	26 RT	2	1	DBL
LNB	107+04	35 RT	1	1	SINGLE
LNB	107+17	39 RT	1	1	SINGLE
LNB	107+34	30 RT	1	1	SINGLE
LNB	107+39	23 RT	1	1	SINGLE
LNB	107+98	43 RT	1	1	SINGLE
LNB	108+10	27 RT	1	1	SINGLE
LNB	108+35	29 RT	2	2	SINGLE
LNB	108+94	29 RT	1	1	SINGLE
LNB	109+04	23 RT	1	1	SINGLE
LNB	109+42	30 RT	4	1	QUAD
LNB	109+74	25 RT	1	1	SINGLE
LNB	109+98	47 RT	1	1	SINGLE
LNB	109+99	23 RT	1	1	SINGLE
LNB	110+07	25 RT	1	1	SINGLE
LNB	110+73	18 RT	1	1	SINGLE
LNB	110+84	52 RT	1	1	SINGLE
LNB	111+09	20 RT	1	1	SINGLE
LNB	111+71	23 RT	1	1	SINGLE
LNB	112+05	16 RT	1	1	SINGLE
LNB	112+13	12 RT	2	1	DBL
LNB	112+14	43 RT	1	1	SINGLE
LNB	112+26	19 RT	1	1	SINGLE
LNB	113+20	41 RT	1	1	SINGLE
LNB	113+33	14 RT	1	1	SINGLE
LNB	114+48	21 RT	2	1	DBL
LNB	114+98	35 RT	2	1	DBL
LNB	115+60	29 RT	1	1	SINGLE
LNB	115+61	71 RT	1	1	SINGLE
LNB	115+71	44 RT	1	1	SINGLE
LNB	115+73	89 RT	1	1	SINGLE
LNB	116+16	87 RT	1	1	SINGLE
LNB	116+27	54 RT	1	1	SINGLE
LNB	116+34	43 RT	1	1	SINGLE
LNB	116+35	20 RT	1	1	SINGLE
LNB	116+59	14 RT	1	1	SINGLE
LNB	116+88	14 RT	1	1	SINGLE
LNB	117+47	25 RT	1	1	SINGLE
LNB	117+65	17 RT	1	1	SINGLE
LNB	118+23	39 RT	1	1	SINGLE
LNB	118+76	28 RT	1	1	SINGLE
LNB	118+95	22 RT	2	1	DBL
LNB	120+88	33 RT	1	1	SINGLE
SUBTOTAL A			73	63	

CLEARING & GRUBBING SPEC (2101)					A
ALI.	STATION	OFFSET	CLEARING TREE	GRUBBING TREE	TYPE
LNB	121+00	29 RT	1	1	SINGLE
LNB	121+50	36 RT	1	1	SINGLE
LNB	122+11	39 RT	1	1	SINGLE
LNB	123+22	23 RT	2	1	DBL
LNB	123+26	30 RT	1	1	SINGLE
LNB	124+30	24 RT	1	1	SINGLE
LNB	124+34	23 RT	1	1	SINGLE
LNB	124+37	23 RT	1	1	SINGLE
LNB	125+01	26 RT	1	1	SINGLE
LNB	125+25 - 125+54	28 - 45 RT	13	13	SINGLE
LNB	125+46 - 125+54	66 - 74 RT	4	4	SINGLE
LNB	126+11	41 RT	1	1	SINGLE
LNB	126+17	34 RT	1	1	SINGLE
LNB	126+21	35 RT	1	1	SINGLE
LNB	127+12	23 RT	3	1	TRIPLE
LNB	127+32	31 RT	2	1	DBL
LNB	127+69	37 RT	1	1	SINGLE
LNB	127+97	38 RT	1	1	SINGLE
LNB	128+63	37 RT	1	1	SINGLE
LNB	129+75	35 RT	1	1	SINGLE
LNB	130+02	24 RT	1	1	SINGLE
LNB	131+79	22 RT	1	1	STUMP
LNB	133+62	40 RT	1	1	SINGLE
LNB	133+75	32 RT	2	2	SINGLE
LNB	134+99	54 RT	1	1	SINGLE
LNB	135+55	29 RT	1	1	SINGLE
LNB	135+61	29 RT	1	1	SINGLE
LNB	136+26	47 RT	2	1	DBL
LNB	137+23	30 RT	1	1	SINGLE
LNB	137+45	30 RT	1	1	SINGLE
LNB	139+36	26 RT	4	1	QUAD
LNB	140+74	34 RT	1	1	SINGLE
LNB	142+09	40 RT	1	1	SINGLE
LNB	145+32	23 RT	1	1	SINGLE
LNB	146+15	24 RT	1	1	SINGLE
LNB	146+60	26 RT	1	1	SINGLE
LNB	147+15	45 RT	1	1	SINGLE
LNB	147+15	28 RT	1	1	SINGLE
LNB	149+02	30 RT	1	1	SINGLE
LNB	149+28	29 RT	1	1	SINGLE
LNB	149+55	15 RT	1	1	SINGLE
LNB	149+81	35 RT	2	1	DBL
LSB	1099+96	66 LT	3	3	SINGLE
LSB	1100+01	56 LT	3	3	SINGLE
LSB	1100+02	56 LT	1	1	SINGLE
LSB	1100+05	59 LT	1	1	SINGLE
LSB	1100+03	46 LT	2	2	SINGLE/DBL
LSB	1100+04	62 LT	1	1	SINGLE
LSB	1100+10	57 LT	1	1	SINGLE
LSB	1100+11	42 LT	1	1	SINGLE
LSB	1100+12	59 LT	1	1	SINGLE
LSB	1100+16	41 LT	1	1	SINGLE
LSB	1100+35	40 LT	1	1	SINGLE
LSB	1100+56	31 LT	1	1	SINGLE
LSB	1100+72	34 LT	2	1	DBL
LSB	1101+13	89 LT	1	1	SINGLE
LSB	1101+17	86 LT	1	1	SINGLE
LSB	1101+27	85 LT	1	1	SINGLE
LSB	1101+35	58 LT	1	1	SINGLE
SUBTOTAL B			89	80	

CLEARING & GRUBBING SPEC (2101)					A
ALI.	STATION	OFFSET	CLEARING TREE	GRUBBING TREE	TYPE
LSB	1101+76	118 LT	2	1	DBL
LSB	1101+81	28 LT	2	1	DBL
LSB	1101+87	31 LT	1	1	SINGLE
LSB	1102+23	27 LT	1	1	SINGLE
LSB	1102+42	26 LT	1	1	SINGLE
LSB	1102+59	38 LT	1	1	SINGLE
LSB	1102+83	21 LT	1	1	SINGLE
LSB	1102+83	41 LT	1	1	SINGLE
LSB	1102+87	33 LT	3	2	SINGLE/DBL
LSB	1102+90	20 LT	1	1	SINGLE
LSB	1103+41	22 LT	1	1	SINGLE
LSB	1103+62	33 LT	1	1	SINGLE
LSB	1103+77	61 LT	2	2	SINGLE
LSB	1104+17	34 LT	2	1	DBL
LSB	1104+67	28 LT	1	1	SINGLE
LSB	1104+95	17 LT	1	1	SINGLE
LSB	1104+98	27 LT	1	1	SINGLE
LSB	1105+18	16 LT	2	1	DBL
LSB	1105+47	25 LT	4	2	DBL
LSB	1105+63	25 LT	2	2	SINGLE
LSB	1105+65	19 LT	4	4	SINGLE
LSB	1105+80	23 LT	3	3	SINGLE
LSB	1105+87	23 LT	1	1	SINGLE
LSB	1105+97	23 LT	2	2	SINGLE
LSB	1106+28	15 LT	1	1	SINGLE
LSB	1106+65	16 LT	2	1	DBL
LSB	1107+03	33 LT	1	1	SINGLE
LSB	1107+76	13 LT	1	1	SINGLE
LSB	1107+98	14 LT	1	1	SINGLE
LSB	1108+53	21 LT	1	1	SINGLE
LSB	1108+74	16 LT	1	1	SINGLE
LSB	1108+79	33 LT	1	1	SINGLE
LSB	1110+64	25 LT	1	1	SINGLE
LSB	1110+86	35 LT	1	1	SINGLE
LSB	1111+20	21 LT	1	1	SINGLE
LSB	1111+63	28 LT	1	1	SINGLE
LSB	1111+76	28 LT	1	1	SINGLE
LSB	1112+13	37 LT	1	1	SINGLE
LSB	1113+43	41 LT	1	1	SINGLE
LSB	1113+63	40 LT	1	1	SINGLE
LSB	1114+90	22 LT	1	1	SINGLE
LSB	1115+27	41 LT	1	1	SINGLE
LSB	1115+64	42 LT	3	3	SINGLE
LSB	1115+82	32 LT	1	1	SINGLE
LSB	1115+84	28 LT	1	1	SINGLE
LSB	1115+89	25 LT	1	1	SINGLE
LSB	1116+11	32 LT	1	1	SINGLE
LSB	1116+12	33 LT	1	1	SINGLE
LSB	1116+70	61 LT	2	1	DBL
LSB	1116+75	44 LT	2	1	DBL
LSB	1117+48	30 LT	1	1	SINGLE
LSB	1119+78	31 LT	1	1	SINGLE
LSB	1121+83	27 LT	1	1	SINGLE
LSB	1121+95	23 LT	1	1	SINGLE
LSB	1122+10	23 LT	1	1	SINGLE
LSB	1122+54	25 LT	1	1	SINGLE
LSB	1122+86	30 LT	4	2	DBL
LSB	1123+02	25 LT	2	1	DBL
LSB	1123+50	22 LT	1	1	SINGLE
LSB	1123+68	27 LT	1	1	SINGLE
LSB	1124+88	31 LT	1	1	SINGLE
SUBTOTAL C			94	80	

CLEARING & GRUBBING SPEC (2101)					A
ALI.	STATION	OFFSET	CLEARING TREE	GRUBBING TREE	TYPE
LSB	1125+55	61 LT	1	1	SINGLE
LSB	1125+97	62 LT	1	1	SINGLE
LSB	1126+01	77 LT	1	1	SINGLE
LSB	1126+20	33 LT	1	1	SINGLE
LSB	1126+71	27 LT	1	1	STUMP
LSB	1127+43	105 LT	1	1	SINGLE
LSB	1127+45	37 LT	2	2	SINGLE
LSB	1127+50	96 LT	2	2	SINGLE
LSB	1127+52	72 LT	2	2	SINGLE
LSB	1127+52	108 LT	1	1	SINGLE
LSB	1127+55	123 LT	1	1	SINGLE
LSB	1127+57	90 LT	1	1	SINGLE
LSB	1127+83	81 LT	2	1	DBL
LSB	1128+01	47 LT	2	2	SINGLE
LSB	1128+03	98 LT	2	1	DBL
LSB	1128+23	117 LT	1	1	STUMP
LSB	1128+23	43 LT	1	1	SINGLE
LSB	1128+24	46 LT	1	1	STUMP
LSB	1128+36	33 LT	1	1	SINGLE
LSB	1128+40	34 LT	1	1	SINGLE
LSB	1128+58	63 LT	2	2	SINGLE
LSB	1128+71	122 LT	4	2	DBL
LSB	1130+75	23 LT	1	1	SINGLE
LSB	1130+86	16 LT	1	1	STUMP
LSB	1131+74	26 LT	2	1	DBL
LSB	1132+41	23 LT	1	1	SINGLE
LSB	1132+58	26 LT	2	1	DBL
LSB	1132+89	12 LT	1	1	SINGLE
LSB	1132+89	39 LT	1	1	SINGLE
LSB	1132+91	52 LT	1	1	SINGLE
LSB	1132+98	16 LT	2	2	SINGLE
LSB	1133+60	20 LT	1	1	SINGLE
LSB	1133+93	16 LT	4	1	QUAD
LSB	1135+37	17 LT	1	1	SINGLE
LSB	1135+69	13 LT	1	1	SINGLE
LSB	1135+90	33 LT	1	1	SINGLE
LSB	1136+13	22 LT	2	1	DBL
LSB	1136+46	40 LT	1	1	SINGLE
LSB	1137+07	47 LT	1	1	SINGLE
LSB	1137+44	13 LT	1	1	SINGLE
LSB	1137+89	13 LT	1	1	SINGLE
LSB	1138+02	38 LT	1	1	SINGLE
LSB	1138+18	7 LT	1	1	SINGLE
LSB	1138+60	19 LT	1	1	SINGLE
LSB	1138+68	33 LT	1	1	SINGLE
LSB	1138+72	18 LT	1	1	STUMP
LSB	1140+50	50 LT	4	4	SINGLE
LSB	1140+54	29 LT	1	1	SINGLE
LSB	1140+67	22 LT	3	3	SINGLE
LSB	1140+70	34 LT	2	2	SINGLE
LSB	1141+04	30 LT	5	4	SINGLE/DBL
LSB	1142+80	29 LT	1	1	SINGLE
LSB	1143+26	31 LT	1	1	SINGLE
LSB	1147+76	27 LT	1	1	SINGLE
LSB	1147+88	23 LT	1	1	SINGLE
LSB	1				

CLEARING & GRUBBING SPEC (2101)					A
ALI.	STATION	OFFSET	CLEARING TREE	GRUBBING TREE	TYPE
LEB	207+55	31 RT	1	1	SINGLE
LEB	208+12	32 RT		1	STUMP
LWB	212+12	33 LT	2	1	DBL
LWB	212+68	28 LT	1	1	SINGLE
LWB	213+08	27 LT	1	1	SINGLE
SUBTOTAL E			6	6	

SUBTOTAL A	73	63	
SUBTOTAL B	89	80	
SUBTOTAL C	94	80	
SUBTOTAL D	93	84	
SUBTOTAL E	6	6	
PROJECT TOTAL	337	299	

CLEARING & GRUBBING GENERAL NOTES:
 TREES WITHIN THE CONSTRUCTION LIMITS WILL BE DESIGNATED FOR REMOVAL BY THE ENGINEER.
 REMOVAL OF MISCELLANEOUS SHRUBS AND LANDSCAPING SHALL BE CONSIDERED INCIDENTAL.

AGGREGATE						E	
ALIGNMENT	STATION	TO	STATION	BASE CLASS 5			NOTES
				MAINLINE (CU YD)	CUL-DE-SAC (CU YD)	TRAIL (CU YD)	
LNB / LSB	98+63.67	-	111+00.00	1805			[1]
LNB	98+63.67	-	111+00.00		125	101	
LSB	98+63.67	-	111+00.00		120		
LNB / LSB	111+00.00	-	123+35.62	1753			
LNB	111+00.00	-	123+35.62		109	101	
LNB / LSB	123+35.62	-	135+71.01	1501			
LNB	123+35.62	-	135+71.01		116	109	
LSB	123+35.62	-	135+71.01		239		
LNB / LSB	135+71.01	-	148+06.49	2367			
LNB	135+71.01	-	148+06.49			70	
LNB / LSB	148+06.49	-	151+56.41	200			
LEB / LWB	196+50.00	-	207+05.73	593			
LEB / LWB	212+00.00	-	219+00.00	658			
	TEMPORARY WIDENING			341			[2]
	PROJECT TOTAL			8877	709	381	

AGGREGATE SUMMARY NOTES:
 [1] INCLUDES EGRET BLVD
 [2] QUANTITY FOR TEMPORARY WIDENING IN STAGE 1

BITUMINOUS SUMMARY							J
ALIGNMENT	STATION TO STATION	BITUMINOUS				NOTES	
		2360 TYPE SP 12.5 WEAR (3,F)	2360 TYPE SP 12.5 NON-WEAR (3,B)	2360 TYPE SP 9.5 WEAR (2,B) [4]	2357 BIT. TACK COAT		
		TON	TON	TON	GALLON		
LNB / LSB	98+63.67 - 111+00.00	1941.5	970.7		844.1	[1] [2]	
LNB	98+63.67 - 111+00.00	77.8	77.8	130.7	34	[3] [4]	
LSB	98+63.67 - 111+00.00	71.5	71.5		31.1	[3]	
LNB / LSB	111+00.00 - 123+35.62	2170.6	1085.3		943.7	[1]	
LNB	111+00.00 - 123+35.62	67.3	67.3	131.1	29.3	[3] [4]	
LNB / LSB	123+35.62 - 135+71.01	1829.2	914.6		795.3	[1]	
LNB	123+35.62 - 135+71.01	71.9	71.9	140.7	31.2	[3] [4]	
LSB	123+35.62 - 135+71.01	148.9	148.9		64.7	[3]	
LNB / LSB	135+71.01 - 148+06.49	3031.3	1515.6		1317.9	[1]	
LNB	135+71.01 - 148+06.49			90.9		[4]	
LNB / LSB	148+06.49 - 151+56.41	225.1	112.5		97.9	[1]	
LEB / LWB	169+50.00 - 207+05.73	664.0	332.0		288.7	[1]	
LEB / LWB	212+00.00 - 219+00.00	758.7	379.4		329.9	[1]	
	TEMPORARY WIDENING		235.0			[5]	
	PROJECT TOTAL	11057.8	5747.5	493.4	4807.6		

BITUMINOUS SUMMARY NOTES:
 [1] QUANTITY FOR MAINLINE CONSTRUCTION
 [2] INCLUDES EGRET
 [3] CUL-DE-SAC
 [4] BIT TRAIL
 [5] QUANTITY FOR TEMPORARY WIDENING IN STAGE 1

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_TAB.dgn 05/13/2016 12:55:07 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



ANOKA COUNTY
 HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

TABULATIONS

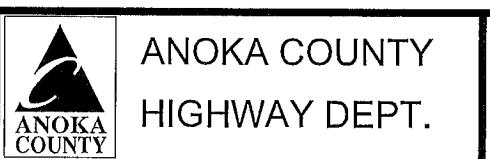
REMOVALS, SAWING AND MILLING											B	
ADDRESS / DESCRIPTION	ALIGNMENT	STATION TO STATION	REMOVE (SPEC. 2104)						SAWING (SPEC. 2104)		NOTES	
			BIT. PAVEMENT	BIT. DR-WAY PAVEMENT	CONC. CURB & GUTTER	CONCRETE DRIVEWAY PAVEMENT	CONC. WALK	RETAINING WALL	SANITARY SERVICE PIPE	BIT. PAVEMENT		CONC. PAVEMENT
			(SQ YD)	(SQ YD)	(LIN FT)	(SQ YD)	(SQ YD)	(LIN FT)	(LIN FT)	(LIN FT)		(LIN FT)
LNB FOLEY BLVD.												
	P_NB_11_3	99+76.95 - 103+67.19	1638		609			565			302	[4], [5]
337 W EGRET	P_NB_11_3	101+14.30		39			14				16	
337 E EGRET	P_NB_11_3	101+23.20		29			16				16	
10709 FOLEY	P_NB_11_3	101+36.42 - 101+94.19		49					41		32	5
10715 S FOLEY	P_NB_11_3	102+19.15		17							13	
10715 N FOLEY	P_NB_11_3	102+76.50		5								
396 107TH LN	P_NB_11_3	103+67.19 - 108+40.23	1718		332			233				
395 107TH LN	P_NB_11_3	104+12.92		19							26	
	P_NB_11_3	104+58.23 - 104+61.72							4			
	P_NB_11_3	108+40.23 - 112+62.48	1159		383			221				
10807 FOLEY	P_NB_11_3	108+63.79 - 108+98.21		7			96					22
10821 FOLEY	P_NB_11_3	109+21.41 - 109+93.36		106			10		27		37	
10831 FOLEY	P_NB_11_3	110+64.77 - 110+93.80		81			20		60		20	
10861 FOLEY	P_NB_11_3	111+50.80		7			95		28			52
	P_NB_11_3	112+62.46 - 118+93.39	1805		516			312				
10901 FOLEY	P_NB_11_3	113+86.22					73					31
10931 FOLEY	P_NB_11_3	115+29.46					82					24
10937 FOLEY	P_NB_11_3	117+27.15					118					37
10961 FOLEY	P_NB_11_3	117+90.91					75					17
	P_NB_11_3	118+93.39 - 125+47.97	1892		515			317				
11001 FOLEY	P_NB_11_3	120+35.19					33					8
11021 FOLEY	P_NB_11_3	121+21.37					97					23
11041 FOLEY	P_NB_11_3	123+91.76		25			9				12	
11051 FOLEY	P_NB_11_3	124+81.75 - 125+36.32		36			15		15		25	
	P_NB_11_3	125+47.97 - 129+40.39	1179		258			178				
11061 FOLEY	P_NB_11_3	126+88.41		70			15				24	
11103 FOLEY	P_NB_11_3	127+51.85 - 128+60.17					39		73			11
	P_NB_11_3	129+40.39 - 133+53.83	1192		351			205				46
11121 FOLEY	P_NB_11_3	130+24.77		9			70					15
112TH LN	P_NB_11_3	133+40.00						18				
112TH WALK	P_NB_11_3	133+43.71										
	P_NB_11_3	133+53.83 - 137+23.41	1002		291			191				
11275 FOLEY	P_NB_11_3	134+33.68		141			19				52	
113TH WALK	P_NB_11_3	136+51.60										5
	P_NB_11_3	137+23.41 - 142+27.01	1529		504			269				
11239 FOLEY	P_NB_11_3	138+38.41		127			19				124	
11247 FOLEY	P_NB_11_3	140+34.15		15			85					109
370 NORTHDAL	P_NB_11_3	141+56.65 - 142+23.56		83			19		17		31	DRWY CURB
	P_NB_11_3	142+27.01 - 144+99.29	1127		193			2				
380 S NORTHDAL	P_NB_11_3	142+67.07 - 143+57.33					36					
	P_NB_11_3	144+99.29 - 150+20.04	1521		518			262				
11403 FOLEY	P_NB_11_3	145+64.65 - 146+96.16		195			52				102	
11427 FOLEY	P_NB_11_3	148+15.20		105		51	26				56	DRWY CURB
11431 FOLEY	P_NB_11_3	149+28.26 - 150+21.33		60			14		70		38	5
SIDE STREET	P_NB_11_3	107TH LN E.	317		226						26	
SIDE STREET	P_NB_11_3	108TH AVE	195		139						26	
SIDE STREET	P_NB_11_3	109TH AVE E.	269		197						26	
SIDE STREET	P_NB_11_3	109TH LN E.	379		270						26	
SIDE STREET	P_NB_11_3	110TH AVE E.	219		156						26	
SIDE STREET	P_NB_11_3	110TH LN E.	213		151						26	
SIDE STREET	P_NB_11_3	111TH AVE E.	347		236						26	
SIDE STREET	P_NB_11_3	111TH LN E.	213		145						26	
SUBTOTAL A			17914	1225	6267	1147	2773	335		1134	410	

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_TAB.dgn 07/06/2016 1:25:31 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 7-7-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



SP 002-611-034
 SP 114-020-050
 CP 13-10

REMOVALS, SAWING AND MILLING

ADDRESS / DESCRIPTION	ALIGNMENT	STATION TO STATION	REMOVE (SPEC. 2104)							SAWING (SPEC. 2104)		NOTES	
			BIT. PAVEMENT	BIT. DR-WAY PAVEMENT	CONC. CURB & GUTTER	CONCRETE DRIVEWAY PAVEMENT	CONC. WALK	RETAINING WALL	SANITARY SERVICE PIPE	BIT. PAVEMENT	CONC. PAVEMENT		
			(SQ YD)	(SQ YD)	(LIN FT)	(SQ YD)	(SQ YD)	(LIN FT)	(LIN FT)	(LIN FT)	(LIN FT)		
SIDE STREET	P_NB_11_3	112TH LN E.	203		145						25		
SIDE STREET	P_NB_11_3	113TH AVE E.	260		110						40		
LSB FOLEY BLVD.													
BEG PROJECT	P_SB_11_3	1099+83.34 1103+66.08	1603		363			117			168	11	[5]
10714 FOLEY	P_SB_11_3	1102+03.90		31		9					11		
10740 FOLEY	P_SB_11_3	1103+05.68		47		10					27		
10744 FOLEY	P_SB_11_3	1103+66.08 - 1108+39.27	1152		414								
10764,10772,10780	P_SB_11_3	1104+69.26 - 1105+33.56				32		12				20	
400 S,N 108TH LN	P_SB_11_3	1107+88.48 1108+28.43		27		18					21		
	P_SB_11_3	1108+39.27 - 1112+61.35	1122	39		16					50		
401 108TH LN	P_SB_11_3	1110+44.50											
	P_SB_11_3	1112+61.35 - 1118+92.61	1593			63						17	
109TH WALK	P_SB_11_3	1112+87.98						10					
10900 FOLEY	P_SB_11_3	1113+92.90		65		11					25		
10908 S FOLEY	P_SB_11_3	1114+42.52 1115+09.76				108							
10930 FOLEY	P_SB_11_3	1116+00.59				67							28
10936 S FOLEY	P_SB_11_3	1116+60.06 1118+92.54		118		33							35
	P_SB_11_3	1118+92.61 - 1125+47.10	1651			583					26		
10960 FOLEY	P_SB_11_3	1119+49.21		13		15					23		
11010 FOLEY	P_SB_11_3	1122+03.29		6		8					10		
11032 FOLEY	P_SB_11_3	1122+72.98				45							20
11040 FOLEY	P_SB_11_3	1124+02.91				44							20
11052 FOLEY	P_SB_11_3	1124+55.71		95		18					59		
	P_SB_11_3	1125+47.10 - 1129+39.83	945			253							
11100 FOLEY	P_SB_11_3	1126+78.77 - 1126+95.87		11		9		17			18		
	P_SB_11_3	1129+39.83 - 1133+53.14	1051			352							
401 111TH LN	P_SB_11_3	1130+20.49				13							
404 112TH LN	P_SB_11_3	1130+79.75 - 1133+00.00				73		7	141				20
	P_SB_11_3	1133+53.14 - 1137+22.68	994			294							
405 112TH LN	P_SB_11_3	1133+46.47 - 1134+90.73		44		11		21	137	65	18	17	
404 113TH AVE	P_SB_11_3	1135+83.86 - 113+00.00		33		10		16		70	10	17	
113TH AV WALK	P_SB_11_3	1136+56.23											32
413 113TH AVE	P_SB_11_3	1136+96.56		3							7		
	P_SB_11_3	1137+22.68 - 1142+26.37	1190			504							
405 113TH AVE	P_SB_11_3	1137+30.00		75		18		7		60	29	3	
11356 FOLEY	P_SB_11_3	1138+94.38 - 1139+40.00		239		165		7			74	6	DRWY CURB
	P_SB_11_3	1142+26.37 - 1145+30.09	770			280		164					
11364 FOLEY	P_SB_11_3	1142+49.76 - 1143+55.68		251		173					108		DRWY CURB
WALK	P_SB_11_3	1144+72.85				44						5	
	P_SB_11_3	1145+30.09 - 1150+33.00	1178			292		180			48		
401 S NORTHDALE	P_SB_11_3	1146+40.62		106		82					130		DRWY CURB
SIDE STREET	P_SB_11_3	107TH AVE W.	333			311					28		
SIDE STREET	P_SB_11_3	107TH LN W.	149			130					20		
SIDE STREET	P_SB_11_3	108TH LN W.	154			110					26		
SIDE STREET	P_SB_11_3	109TH AVE W.	302			205		17			26		
SIDE STREET	P_SB_11_3	110TH AVE W.	181			131					26		
SIDE STREET	P_SB_11_3	111TH AVE W.	308			207					27		
SIDE STREET	P_SB_11_3	111TH LN W.	150			104					27		
SIDE STREET	P_SB_11_3	112TH LN W.	322			217					26		
SIDE STREET	P_SB_11_3	113TH AVE W.	473			321		44			28		
SIDE STREET	P_SB_11_3	115TH AVE W.	31			49					28		
SUBTOTAL B			16115	1203	6706	742	580	317	195	1189	258		

NO	DATE	BY	CKD	APPR	REVISION
NAME: P:02-611-34\Plan002-611-034_TAB.dgn					07/06/2016 1:25:36 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 7-7-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

REMOVALS, SAWING AND MILLING

ADDRESS / DESCRIPTION	ALIGNMENT	STATION TO STATION	REMOVE (SPEC. 2104)							SAWING (SPEC. 2104)		NOTES
			BIT. PAVEMENT	BIT. DR-WAY PAVEMENT	CONC. CURB & GUTTER	CONCRETE DRIVEWAY PAVEMENT	CONC. WALK	RETAINING WALL	SANITARY SERVICE PIPE	BIT. PAVEMENT	CONC. PAVEMENT	
			(SQ YD)	(SQ YD)	(LIN FT)	(SQ YD)	(SQ YD)	(LIN FT)	(LIN FT)	(LIN FT)	(LIN FT)	
LEB NORTHDALE BLVD.												
BEG PROJECT	P_EB_12_3	200+00.00 - 208+65.77	3226		872			338			21	5
550 NORTHDALE	P_EB_12_3	201+63.30		18			46				32	
460 NORTHDALE	P_EB_12_3	203+98.65		18		8					40	
420 NORTHDALE	P_EB_12_3	206+13.46		20		14					31	
	P_EB_12_3	210+26.47 - 218+78.53	4180		1945			341			23	6
380 NORTHDALE	P_EB_12_3	210+69.83 - 211+42.91								75		
370 NORTHDALE	P_EB_12_3	214+02.73					40					
	P_EB_12_3						34					
LWB NORTHDALE BLVD.												
ON CENTER LINE	P_WB_12_3	200+00.00 - 204+00.00									400	
	P_WB_12_3	204+23.94 - 208+51.82			449			174				
441 W NORTHDALE	P_WB_12_3	205+20.95		108	75						54	
421 NORTHDALE	P_WB_12_3	206+78.60		144	42						47	
	P_WB_12_3	209+39.69 - 218+86.73			849							
11401 FOLEY	P_WB_12_3	209+71.35 - 211+14.64		286				173				
	P_WB_12_3	211+98.84 - 218+86.73									243	
END WALK	P_WB_12_3	212+83.27										
END PROJECT	P_WB_12_3	218+86.73										32
	P_WB_12_3	ILEX ST			25						20	
	P_WB_12_3	FLINTWOOD ST	139		72						43	
SUBTOTAL C			7545	594	4351	266	1026	75	980	43		
PROJECT TOTAL			41574	3022	17324	2155	4379	652	270	3303	711	

REMOVALS NOTES:
 [1] MAINLINE FOLEY BLVD.
 [2] MAINLINE NORTHDALE BLVD.
 [3] SIDE ST.
 [4] INCLUDES MEDIAN
 [5] INCLUDES EGRET

WATERMAIN

ALIGN	STATION TO STATION	DIS-CONNECT WATER SERVICE	REMOVE WATERMAIN SPEC. (2104)						WATERMAIN (SPEC. 2504.602) [4]												WATERMAIN (SPEC. 2504.603) [4]											INSTALL MON. BOX [3]								
			REMOVE WATER SERVICE PIPE	REMOVE WATER MAIN	REMOVE UTILITY VAULT	REMOVE GATE VALVE & BOX	REMOVE HYD.	CONNECT TO EXIST. WATER MAIN	CONNECT TO EXIST. WATER SERVICE	HYD. [2]	1" CORP. STOP	2" CORP. STOP	16" BUTTER-FLY VALVE & BOX	18" BUTTER-FLY VALVE & BOX	30" BUTTER-FLY VALVE & BOX	6" GATE VALVE	6" GATE VALVE & BOX	8" GATE VALVE & BOX	12" GATE VALVE & BOX	HYD. RISER	LOWER WATER MAIN [2]	CURB STOP & BOX	WATER SERVICE (DIRECTIONAL DRILLED)	TAP SERVICE PIPING 1"	TAP SERVICE PIPING 2"	6" WATER MAIN DUCTILE IRON CL 52	8" WATER MAIN DUCTILE IRON CL 52	16" WATER MAIN DUCTILE IRON CL 52	30" WATER MAIN DUCTILE IRON CL 52	8" PVC WATER MAIN	12" PVC WATER MAIN		18" PVC WATER MAIN	30" PVC WATER MAIN	4" POLY. INSUL.	DUCTILE IRON FITTINGS				
			EACH	LIN FT	LIN FT	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	LF	LF	EACH	LIN FT	LIN FT	LIN FT	LIN FT	LIN FT	LIN FT	LIN FT	LIN FT		LIN FT	LIN FT	LIN FT	LIN FT	LIN FT	SQ YD	POUND	
SB	1100+00-1114+00	9	575	2400	1	10	4	9	9	8	9		2		8	13			1	9	260	320																		
NB	100+00-114+00																																							
SB	1114+00 - 1127+00	11	490	2225		9	5	7	11	7	11		2		7	11			1	11	80	410												1370	25	9200	1			
NB	114+00 - 127+00																																							
SB	1127+00 - 1140+00	13	580	1975		6	3	6	8	6	8		1		6	7	2			8	250	200													1300	61	7700	1		
NB	127+00 - 140+00																																							
SB	1140+00 - 1149+00	4	260	1475	3	7	2	4	4	3	3	1	1	3	1	3	3	3			4	230	10	40	350	35	30								465	560	60	25	10500	1
NB	140+00 - 149+00																																							
EB / WB	204+50 - 210+00	4	200	550	2	2		2	4	2	4			3		2					4	70	130		90															
EB / WB	210+00 - 218+10	7	360	1200		3	2	3	7	4	7			2	4		2	2			655	7	310	130		90														
TOTALS		48	2465	9825	6	37	16	31	43	30	42	1	1	11	3	30	34	7	2	2	655	43	1200	1120	40	2930	245	30	260	465	435	5105	425	186	64000	6				

NOTES:
 [1] 2 HYDRANTS ON NORTHDALE BLVD ARE 11' IN LENGTH AND 1 IS 9.5' LENGTH
 [2] LOWER 655 LF WATERMAIN ON NORTHDALE BLVD
 [3] CITY OF COON RAPIDS TO FURNISH MONUMENT BOX AND THE CONTRACTOR TO INSTALL MONUMENT BOX
 [4] 3500 CY AGGREGATE BASE (CV) CLASS 5 TO BE USED FOR TEMPORARY PATCHING FOR WATERMAIN CUTS AND INSTALLATION

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 7-8-16 LICENSE NO. 22713

DRAWN BY OFF DATE 02/27/16
 DESIGN BY OFF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16




**ANOKA COUNTY
HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

DRIVEWAYS											C
ADDRESS	ALIGN	STATION	APRON WIDTH	DRIVEWAY WIDTH	TURN-AROUND WIDTH	BASE CLASS 5	6" CONC DRIVEWAY APRON	6" CONC DRIVEWAY PAVEMENT	2.5" BIT DRIVEWAY PAVEMENT	RELOCATE MAIL BOX SUPPORT	NOTES
						CU YD [4]	SQ YD [5]	SQ YD	TON [2]		
10709 FOLEY	LNB	101+36	24	13		2.2	8.2		4.9		
10714 FOLEY	LSB	1102+03	16	10		1.1	3.4		2.6		
10715 FOLEY	LNB	102+19	20	13		1.0	5.7		1.4		
10740 FOLEY	LSB	1103+05	23	17	10	2.1	4.0		6.4		[1]
10744 FOLEY	LSB	1105+33	23	17		0.8	3.6	3.4		1	
10764 FOLEY	LSB	1107+42	43	23		1.1	6.3		1.7	1	
400 108TH LN	LSB	1108+28	18	12		2.0	3.2		6.4		
10807 FOLEY	LNB	108+18	22	17	10	3.6	7.4	24.7		1	[1]
10821 FOLEY	LNB	109+21	21	18	5	3.9	6.6		12.1	1	[1]
401 108TH LN	LSB	1110+44	23	17	14	2.7	4.2	20.3			[1]
10831 FOLEY	LNB	110+64	20	14	10	3.0	5.9		9.0	1	[1]
10861 FOLEY	LNB	111+50	22	18		4.6	6.9	34.3			
406 109TH AVE	LSB	1112+33	14	11		0.3	2.9			1	
10901 FOLEY	LNB	113+86	25	19		3.9	9.8	25.3		1	
10900 FOLEY	LSB	1113+92	23	17	10	2.0	4.0		6.2	2	[1]
10908 FOLEY	LSB	1114+42	17	11		0.7	3.2		1.3		
10908 FOLEY	LSB	1115+09	26	20	10	2.4	4.3	17.7		1	[1]
10931 FOLEY	LNB	115+29	25	20	10	3.6	9.9	22.3		1	[1]
10930 FOLEY	LSB	1116+00	16	10		4.0	3.0	33.3		1	
10936 FOLEY	LSB	1116+60	54	24		4.8	16.6		11.5	1	
10937 FOLEY	LNB	117+27	28	22		3.5	11.3	20.3		1	
10961 FOLEY	LNB	117+90	22	16	15	2.9	8.3	17.5		1	[1]
10936 FOLEY	LSB	1118+92	26	20		1.0	4.5		1.9		
10960 FOLEY	LSB	1119+49	29	23	10	3.1	5.7		9.5	1	[1]
11001 FOLEY	LNB	120+35	15	8		1.1	5.9	3.8		1	
11021 FOLEY	LNB	121+21	23	17	10	3.8	9.3	25.3			[1]
11010 FOLEY	LSB	1122+03	18	12		0.6	3.2		0.9	1	
11032 FOLEY	LSB	1122+72	20	14		2.4	3.8	17.6		1	
11041 FOLEY	LNB	123+90	22	11		1.3	8.0		1.6		
11040 FOLEY	LSB	1124+02	28	22		2.1	4.4	14.1		1	
11052 FOLEY	LSB	1124+55	36	30	17	6.1	6.9		20.8	1	[1]
11051 FOLEY	LNB	124+81	31	25		1.7	12.6		1.2	1	
11100 FOLEY	LSB	1126+86	20	14		0.8	3.6		1.6	1	
11061 FOLEY	LNB	126+88	31	25	12	3.8	8.7		11.1	1	[1]
11103 FOLEY	LNB	128+40	15	9	11.5	2.0	6.3	12.1		1	[1]
11121 FOLEY	LNB	130+24	22	16		2.1	8.4	10.6		1	
TOTAL										27	

NOTES:
[1] TURNAROUND LENGTHS ARE 10 FT UNLESS OTHERWISE NOTED. TURNAROUND PAVEMENT TO BE SAME AS DRIVEWAY
[2] PAID FOR AS TYPE SP 9.5 WEARING COURSE MIX (2,B)
[3] COMMERCIAL DRIVEWAY, 4 INCH BITUMINOUS DEPTH
[4] 4 INCH DEPTH
[5] PAID FOR AS 6" CONCRETE DRIVEWAY PAVEMENT

DRIVEWAYS											C
ADDRESS	ALIGN	STATION	APRON WIDTH	DRIVEWAY WIDTH	TURN-AROUND WIDTH	BASE CLASS 5	6" CONC DRIVEWAY APRON	6" CONC DRIVEWAY PAVEMENT	2.5" BIT DRIVEWAY PAVEMENT	RELOCATE MAIL BOX SUPPORT	NOTES
						CU YD [4]	SQ YD [5]	SQ YD	TON [2]		
404 112TH LN	LSB	1131+61	23	17		0.4	3.7				
11275 FOLEY	LNB	134+33	34	24		5.7	9.7		28.7		[3]
11239 FOLEY	LNB	138+38	39	29		4.1	14.6		15.7		[3]
11356 FOLEY	LSB	1138+94	33	23		10.8	7.1		62.2		[3]
11247 FOLEY	LNB	140+34	36	37		2.3	12.4	8.6			
370 NORTHDAL	LNB	141+56	41	31		2.0	11.9		3.9		[3]
11364 FOLEY	LSB	1142+49	54	25		2.2	7.9		8.0		[3]
11364 FOLEY	LSB	1143+55	61	31		6.9	9.3		36.3		[3]
11401 FOLEY	LWB	210+35	39	29		3.0	11.3		10.6		[3]
11401 FOLEY	LWB	211+49	25	17		1.3	4.1		5.0		[3]
11403 FOLEY	LNB	146+96	36	26		4.1	9.9		18.9		[3]
401 NORTHDAL	LSB	1147+62	34	24		4.1	9.1		19.4		[3]
11427 FOLEY	LNB	148+15	35	25		5.5	9.7		27.4		[3]
421 NORTHDAL	LSB	1148+25	24	14		1.2	5.5		3.9		[3]
11431 FOLEY	LNB	150+01	21	15		3.1	7.9		13.8		[3]
550 NORTHDAL	LEB	201+63	35	33		1.7	13.7		1.3		[3]
460 NORTHDAL	LEB	203+98	50	40		2.0	15.2		2.2		[3]
441 NORTHDAL	LWB	205+20	34	24		1.3	9.4		1.6		[3]
441 NORTHDAL	LWB	205+74	40	30		1.6	11.4		2.0		[3]
420 NORTHDAL	LEB	206+13	40	30		1.7	6.0		6.7		[3]
421 NORTHDAL	LWB	206+61	34	24		1.7	9.4		4.4		[3]
421 NORTHDAL	LWB	206+80	33	23		1.5	9.1		2.9		[3]
311 NORTHDAL	LWB	213+80	31	21		1.1	9.6				
370 NORTHDAL	LEB	214+02	39	29		3.7	14.6		13.0		[3]
11239 FOLEY	LEB	215+00	37	24		1.0	9.4				
295 NORTHDAL	LWB	215+82	29	21		1.1	9.9				
330 NORTHDAL	LEB	216+34	32	24		0.5	4.4				
265 NORTHDAL	LWB	216+83	33	24		1.3	11.4				
337 EGRET	LNB	101+14	22	16		1.7	6.6		3.7		
337 EGRET	LNB	101+23	22	16		1.7	6.6		3.7	1	
TOTAL						168.4	510.8	311.2	407.4	27	

NO				DATE				BY				CKD				APPR				REVISION				I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: GINA M. PIZZO SIGNATURE: <u>Gina Pizzo</u> DATE: <u>5-19-16</u> LICENSE NO. 22713	DRAWN BY: <u>DFP</u> DATE: <u>02/27/16</u> DESIGN BY: <u>DFP</u> DATE: <u>02/27/16</u> CHECKED BY: <u>JEO</u> DATE: <u>02/27/16</u>	 ANOKA COUNTY HIGHWAY DEPT.	SP 002-611-034 SP 114-020-050 CP 13-10	TABULATIONS	
Sheet <u>15</u> of <u>298</u> Sheets																													

REMOVE EXISTING STORM SEWER							D
ALIGN.	STATION TO STATION	OFFSET LEB		REMOVE (SPEC 2104)		NOTES	
				DRAINAGE STRUCTURE	STORM SEWER PIPE		
		LEFT	RIGHT	(EACH)	(LIN FT)		
LSB	1100+67	12'		1			
LSB	1100+67 - 1100+68	12'	24'		38		
LSB	1100+68		24'	1			
LSB / LNB	1100+68 - 100+68	13'	24'		6		
LNB	100+68	13'		1			
LNB	100+68 - 100+80	13'	14'		29		
LNB	100+80		14'	1			
LNB	100+80 - 101+23		14' - 2'		44		
LNB	101+23		2'	1			
LNB	101+23 - 103+86		2' - 7'		264		
LNB	103+86		7'	1			
LNB	103+86 - 105+44		7' - 9'		157		
LNB	105+44		9'	1			
LSB / LNB	1105+45 - 105+44	5'	9'		56		
LSB	1105+45	5'		1			
LSB	1112+22	10'		1			
LSB / LNB	1112+22 - 112+29	10'	-11'		43		
LNB	112+29	11'		1			
LNB	112+29 - 112+25	11' - 4'			8		
LNB	112+25	4'		1			
LNB	112+29 - 113+13	11'			84		
LNB	112+50		20'	1			
LNB	112+50 - 112+78		20' - 19'		28		
LNB	112+78		19'	1			
LNB	113+03 - 112+78	4'	19'		35		
LNB	113+03	4'		1			
LNB	113+03 - 113+13	4' - 11'			12		
LSB	1113+00	10'		1			
LSB / LNB	1113+00 - 113+03	10' - 11'			45		
LNB	113+13	11'		1			
LNB	113+13 - 115+58	11' - 12'			245		
LNB	115+58	12'		1			
LNB	115+58 - 115+54	12' - 5.4'			9		
LNB	115+58 - 116+17	12' - 13'			59		
LNB	115+80		19'	1			
LNB	115+80 - 116+07		19' - 18'		28		
LNB	116+07		18'	1			
LNB	116+17 - 116+07	13'	18'		33		
LNB	116+17	13'		1			
LSB	1116+18	13'		1			
LSB / LNB	1116+18 - 116+17	13'			43		
LNB	116+17 - 118+86	13' - 7'			269		
LNB	118+83	1'		1			
LNB	118+83 - 118+86	7' - 1'			8		
LNB	118+86	7'		1			
LNB	118+86 - 119+54	7' - 3'			68		
LNB	119+35		27'	1			
LNB	119+35 - 119+54	3'	27'		36		
LNB	119+54	3'		1			
LNB	119+54 - 119+78	3' - 2'			24		
LNB	119+71		1'	1			
LNB	119+78 - 119+71	2'	1'		9		
LSB	1119+71	14'		1			
LSB / LNB	1119+71 - 119+78	14' - 2'			47		
LNB	119+78	2'		1			
SUBTOTAL A				28	1727		

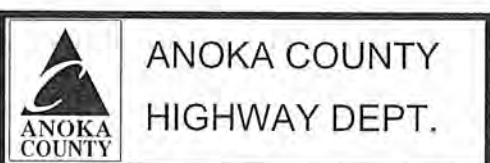
REMOVE EXISTING STORM SEWER							D
ALIGN.	STATION TO STATION	OFFSET LEB		REMOVE (SPEC 2104)		NOTES	
				DRAINAGE STRUCTURE	STORM SEWER PIPE		
		LEFT	RIGHT	(EACH)	(LIN FT)		
LNB	119+78 - 121+00	2'	1'		122		
LSB	1120+98 - 1120+98	44' - 20'			24		
LSB	1120+98		20'	1			
LSB	1120+98 - 1121+23	20' - 13'			26		
LSB	1121+23	13'		1			
LSB / LNB	1120+98 - 121+00	20'	1'		54		
LNB	121+00		1'	1			
LNB	121+00 - 122+21		1' - 1'		120		
LNB	122+21		1'	1			
LNB	122+21 - 122+81		1' - 1'		61		
LNB	122+21 - 122+38		1' - 27'		31		
LNB	122+38		27'	1			
LNB	122+38 - 122+65		27' - 29'		27		
LNB	122+65		29'	1			
LNB	122+81		1'	1			
LNB	122+81 - 125+66		1' - 1'		284		
LSB	1125+38	12'		1			
LSB	1125+38	12'			25		
LSB	1125+62	12'		1			
LSB	1125+62 - 1125+64	12 - 36'			24		
LSB	1125+64	36'		1			
LSB	1125+64 - 1125+92	36' - 38'			28		
LSB	1125+92	38'		1			
LSB / LNB	1125+62 - 125+66	12'	1'		46		
LNB	125+66 - 125+69		1' - 110'		110		
LNB	125+66		1'	1			
LNB	125+66 - 127+81		1' - 0'		216		
LNB	125+69		110'	1			
LNB	125+69 - 125+95		110'		26		
LNB	125+95		110'	1			
LSB	1127+74	13'		1			
LSB / LNB	1127+74 - 127+81	13'	0'		46		
LNB	127+75		5'	1			
LNB	127+75 - 127+81		5' - 0'		8		
LNB	127+81		0'	1			
LNB	127+81 - 128+89	0' - 2'			108		
LNB	128+89	2'		1			
LNB	128+98 - 129+52	2'	5'		63		
LNB	128+89 - 128+98	2'	30'		34		
LNB	128+98		30'	1			
LNB	128+98 - 129+25		30' - 30'		28		
LNB	129+25		30'	1			
LSB	1129+47	12'		1			
LSB / LNB	1129+47 - 129+52	12'	5'		51		
LNB	129+52		5'	1			
LNB	143+79		5'	1			
LNB	143+79 - 144+05		5' - 12'		26		
LSB	1144+56	2'		1			
LSB	1144+56 - 1145+16	2' - 26'			65		
LSB	1145+16	26'		1			
LSB / LNB	1144+56 - 144+05	2'	12'		77		
LNB	144+05		12'	1			
LNB / LEB	144+05 - 211+27		12' - 20'		121		
LNB	144+95		2'	1			
LNB	144+95 - 145+44		2' - 2'		49		
SUBTOTAL B				27	1900		

REMOVE EXISTING STORM SEWER							D
ALIGN.	STATION TO STATION	OFFSET LEB		REMOVE (SPEC 2104)		NOTES	
				DRAINAGE STRUCTURE	STORM SEWER PIPE		
		LEFT	RIGHT	(EACH)	(LIN FT)		
LNB	145+44		2'	1			
LNB	145+44 - 147+40		2' - 0'		195		
LSB	1145+84		1'	1			
LSB / LNB	1145+84 - 145+44		1' - 2'		59		
LSB	1145+85	4'		1			
LSB	1145+85 - 1145+84	4'	1'		6		
LNB	147+40		0'	1			
LNB	147+40 - 148+91		0' - 7'		150		
LNB	148+91		7'	1			
LWB / LNB	210+15 - 144+05	11'	12'		54		
LWB	210+15	11'		1			
LWB	210+15 - 211+88	11' - 12'			173		
LWB / LNB	210+15 - 144+95	11'	2'		75		
LEB	211+27		20'	1			
LWB / LEB	211+88 - 211+27	12'	20'		74		
LWB	211+88	12'		1			
LWB	211+88 - 214+91	12' - 14'			303		
SUBTOTAL C				8	1089		
PROJECT TOTAL				63	4716		

NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_TAB.dgn					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina Pizzo*
 DATE: 5-19-14 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



SP 002-611-034
 SP 114-020-050
 CP 13-10

7 OF 10
 TABULATIONS
 Sheet 16 of 298 Sheets

VEHICULAR GATES					H
STATION	LOCATION			INSTALL. GATE (1) EACH	GATE & POST EACH
	ALIGNMENT	LT/RT	OFFSET		
1101+42	CSAH SB 11	LT	36	1	
1101+55	CSAH SB 11	LT	36	1	
104+24	CSAH NB 11	RT	31	1	
104+37	CSAH NB 11	RT	31	1	
116+02	CSAH NB 11	RT	32	1	
116+15	CSAH NB 11	RT	32	1	
125+76	CSAH NB 11	RT	31	1	
1125+76	CSAH SB 11	LT	24	1	
125+89	CSAH NB 11	RT	31	1	
1125+89	CSAH SB 11	LT	24	1	
1135+19	CSAH SB 11	LT	24		1
1135+32	CSAH SB 11	LT	24		1
TOTAL				10	2

NOTES:
 (1) ITEM INCLUDES CONTRACTOR FURNISH & INSTALL POSTS AND INSTALL GATE ARMS. GATE ARMS LOCATED AT CITY OF COON RAPIDS; CONTACT TIM HIMMER, PUBLIC WORKS DIRECTOR
 SEE SHEET 25 FOR GATE DETAILS

CONCRETE														I
ALIGN	STATION		OFFSET	CONCRETE CURB & GUTTER DESIGN B424	CONCRETE CURB & GUTTER DESIGN B418 (MOD)	CONCRETE CURB & GUTTER DESIGN B612	CONCRETE CURB & GUTTER DESIGN B618 (MOD)	CONCRETE CURB & GUTTER DESIGN S518	4" CONCRETE WALK	6" CONCRETE WALK	4" CONC. MEDIAN (1), (3)	TRUNCATED DOMES	CONCRETE MEDIAN NOSE (2)	
	BEGIN	END		LIN FT	LIN FT	LIN FT	LIN FT	LIN FT	SQ FT	SQ FT	SQ FT	SQ FT	SQ FT	
	LNB	98+63.67		111+00.00	RT	1125	1061		254	201	1063	683	7337	32
LSB	98+63.67	111+00.00	LT	1250	1091			285	751	4435		48		
LNB	111+00.00	123+35.62	RT	1556	1172			202		660	5329	48	84	
LSB	111+00.00	123+35.62	LT	1537	1171				5289	223		40		
LNB	123+35.62	135+71.01	RT	1432	1236		94	199		508	4837	32		
LSB	123+35.62	135+71.01	LT	1281	1235			396	5723	111		16		
LNB	135+71.01	148+06.49	RT	1502	963	84			2798	1165	3406	56	210	
LSB	135+71.01	148+06.49	LT	1426	962	140		193	6021	944		56		
LEB	135+71.01	148+06.49	LT		311						858		126	
LWB	135+71.01	148+06.49	RT	25	312							16		
LNB	148+06.49	151+56.41	RT	212					1030					
LSB	148+06.49	151+56.41	LT	153			128		394	89		16		
LEB	196+50.00	207+05.73	RT	563					3085					
LWB	196+50.00	207+05.73	LT	306			62		1473			16		
LEB	212+00.00	219+00.00	RT	611	586				2912	32	3631	16		
LWB	212+00.00	219+00.00	LT	599	586				896	69		16		
PROJECT TOTAL				13578	10686	224	538	1476	31435	8919	25398	408	462	


CONCRETE NOTES:
 (1) PAID FOR AS 4" CONCRETE WALK
 (2) PAID FOR AS 6" CONCRETE WALK
 (3) 2521.618 SPECIAL SURFACE TREATMENT TO BE APPLIED TO CONCRETE MEDIAN

TURF ESTABLISHMENT AND EROSION CONTROL												N
ALIGNMENT	LOCATION	SILT FENCE TYPE MS	SEEDING	SEED MIXTURE 25-131	SEED MIXTURE 25-151	SEED MIXTURE 33-261	MULCH MATERIAL TYPE 3	DISK ANCHORING	FERTILIZER TYPE 2	STORM DRAIN INLET PROTECTION	EROSION CONTROL BLANKETS CAT. 00	SODDING TYPE SALT TOLERANT
												LIN FT
LNB	98+63.67 - 111+00.00	630	0.04		5.4		0.08	0.04	16	11	3881	1423
LSB	98+63.67 - 111+00.00	686								12	2853	1315
LNB	111+00.00 - 123+35.62	17	0.07		9.9		0.14	0.07	28	34	4609	1373
LSB	111+00.00 - 123+35.62	184								16	2129	1083
LNB	123+35.62 - 135+71.01	168	0.09		11.9		0.18	0.09	36	27	6925	1643
LSB	123+35.62 - 135+71.01	868	0.42			16.2	0.84	0.42	168	13	5463	2447
LNB	135+71.01 - 148+06.49		0.21	51.1			0.42	0.21	84	18	4644	810
LSB	135+71.01 - 148+06.49	478	0.08	2.1		2.7	0.16	0.08	32	12	4088	1434
LEB	207+05.73 - 212+00.00	52	0.02	5.5			0.04	0.02	8	9	604	199
LWB	207+05.73 - 212+02.05		0.03	6.5			0.06	0.03	12	7	658	138
LNB	148+06.49 - 151+56.41		0.02	5.8			0.04	0.02	8	3	421	232
LSB	148+06.49 - 151+56.41		0.01	2.1			0.02	0.01	4	2	175	81
EGRET	29+16.91 - 31+61.63		0.02		2.4		0.04	0.02	8	3	603	197
LEB	196+50.00 - 207+05.73	112	0.04	9.6			0.08	0.04	16	1	327	212
LWB	196+50.00 - 207+05.73	175	0.02	5.3			0.04	0.02	8	1	1155	71
LEB	212+00.00 - 219+00.00		0.14	33.8			0.28	0.14	56	13	884	
LWB	212+00.00 - 219+00.00		0.12	29.6			0.24	0.12	48	13	1199	195
PROJECT TOTAL		3370	1.33	151.4	29.6	18.9	2.66	1.33	532	195	40618	12853

MODULAR BLOCK RETAINING WALL								F
WALL	CSAH 11		CONSTRUCTION			NOTES		
	STATION		LOCATION	LENGTH	HEIGHT		BLOCK	
	START	END		LIN FT	LIN FT		SQ FT	
WALL B	29+74.80	30+21.87	48 LT TO 43 LT	47	3.2' - 3.3'	209 (1)		
WALL C	109+32.39	110+59.52	30 RT TO 37 RT	127	4.9' - 5.6'	674 (1)		
WALL D	110+77.05	110+95.14	37 RT TO 37 RT	18	4.0' - 4.5'	152 (1)		
WALL E	1131+82.00	1132+87.00	24 LT TO 24 LT	105	4.0' - 4.5'	453 (1)		
WALL F	149+32.36	149+90.87	28 RT TO 31 RT	59	4.3' - 4.4'	134 (1)		
WALL G	1146+72.82	1147+25.98	21 LT TO 21 LT	53	3.3' - 3.4'	266 (1)		
TOTAL						1888		

(1) SEE SHEETS 110-113 FOR RETAINING WALL DETAILS

FENCING							L
ALIGN	STATION TO STATION		OFFSET		FURNISH & INSTALL (SPEC. 2557) DESIGN 48V-9332	NOTES	
			LEFT	RIGHT			
			NB	109+32.39 - 110+59.52			
SB	1127+43.11 - 1128+75.57	29' - 122'		227			
SB	1133+69.54 - 1136+54.38	89' - 94'		287			
TOTAL					641		

NO DATE BY CKD APPR REVISION NAME: P:\02-611-34\Plan\002-611-034_TAB.dgn 05/16/2016 3:22:50 PM	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: GINA M. PIZZO SIGNATURE: <i>[Signature]</i> DATE: 5-19-14 LICENSE NO. 22713	DRAWN BY: DFF DATE: 02/27/16 DESIGN BY: DFF DATE: 02/27/16 CHECKED BY: JEO DATE: 02/27/16	 ANOKA COUNTY HIGHWAY DEPT.	SP 002-611-034 SP 114-020-050 CP 13-10	TABULATIONS Sheet 17 of 298 Sheets
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UTILITY CONTACTS		Q
CENTURYLINK 425 MONROE ST ANOKA, MN 55303 CONTACT: BILL BYERS TEL. 763-712-5002		
CITY OF COON RAPIDS 11155 ROBINSON DRIVE COON RAPIDS, MN 55433 CONTACT: TIM HIMMER TEL 763-767-6494		
CONNEXUS ENERGY 14601 RAMSEY BLVD RAMSEY, MN 55303 CONTACT: SHANNON MCDONALD TEL. 763-323-2695		
COMCAST CABLE 2611 FAIRVIEW AVE ROSEVILLE, MN 55113 CONTACT: DOUG ZAHN TEL 651-493-5316		
CENTERPOINT ENERGY 700 WEST LINDEN AVE P.O. BOX 1165 MINNAPOLIS, MN 55440-1165 CONTACT: STEVE GUHANICK TEL 763-427-3456		
ZAYO FIBER SOLUTIONS 5005 CHESHIRE LN N PLYMOUTH, MN 55446 CONTACT: STEVEN SENGER STEVEN.SENGER@ZAYO.COM		
ANOKA COUNTY 1440 BUNKER LAKE BLVD ANDOVER, MN 55304 CONTACT: HARRY GRAMS TEL 763-862-4200		
GREAT RIVER ENERGY 12300 ELM CREEK BOULEVARD MAPLE GROVE, MN 55369 CONTACT: MICHELLE MACMILLAN TEL 763-445-5984		

SANITARY SEWER											S	
ALIGN	STATION	OFFSET		EXISTING ITEM	EXISTING RIM ELEV. (FT)	PROPOSED RIM ELEVATION (FT)	ADJUST FRAME RING & CASTING (SPEC. 2506) (EACH)	RECONSTRUCT DRAINAGE STRUCTURE	NOTES			
		LEFT	RIGHT									
LNB	101+16.64		275	SAN MH	897.97							
LNB	101+16.69		314	SAN MH	897.95							
LNB	101+36.87		186	SAN MH	901.46							
LNB	101+43.51		273	SAN MH	898.62							
LNB	104+26.75		107	SAN MH	902.38	902.6	1					
LNB	107+73.74	-25		SAN MH	907.02	907.2	1					
LNB	109+37.54	-30		SAN MH	907.03	906.9	1	7.1	ROTATE			
LNB	112+65.39	-97		SAN MH	900.11	900.1	1					
LNB	112+69.93	-29		SAN MH	901.19	901.4	1					
LNB	115+01.57		2	SAN MH	901.61	901.1	1					
LNB	115+98.99	-31		SAN MH	900.84	901.0	1					
LNB	116+75.90	-4		SAN MH	901.00	900.4	1					
LNB	119+29.09	-24		SAN MH	897.26	897.4	1					
LNB	120+92.87	-22		SAN MH	897.45	897.6	1					
LNB	122+56.41	-20		SAN MH	898.28	898.4	1	6.3	ROTATE			
LNB	125+79.39	-20		SAN MH	897.21	897.1	1	6.2	ROTATE			
LNB	125+83.32	-90		SAN MH	897.48	897.3	1					
LNB	129+13.81	-20		SAN MH	896.75	896.7	1	6.2	ROTATE			
LNB	129+88.98		26	SAN MH	898.66	898.1	1					
LNB	132+14.63	-20		SAN MH	902.30	902.8	1					
LNB	133+26.08	-20		SAN MH	902.93	903.5	1	7.1	ROTATE			
LNB	133+26.54	82		SAN MH	903.35							
LNB	133+29.58	-89		SAN MH	902.89	902.4	1					
LNB	135+69.61	-20		SAN MH	903.43	903.9	1					
LNB	136+87.93		80	SAN MH	904.56							
LNB	136+89.10	-98		SAN MH	901.34	902.7	1					
LNB	138+97.52	-20		SAN MH	907.01	906.9	1	9.1	ROTATE			
LNB	141+84.29	-20		SAN MH	905.65	905.3	1					
LNB	142+14.34	-20		SAN MH	904.82	904.6	1					
LNB	142+45.43	-79		SAN MH	903.47							
LNB	144+90.58	-20		SAN MH	900.94	901.2	1					
LNB	148+98.57	-41		SAN MH	899.80	899.8	1					
LEB	197+86.34		1	SAN MH	899.33							
LEB	201+84.96	-6		SAN MH	902.96	903.1	1					
LEB	202+62.49	-8		SAN MH	903.28	903.4	1					
LEB	206+61.23	-10		SAN MH	902.21	901.8	1					
LEB	208+01.41	-13		SAN MH	901.39	901.4	1	6.2	ROTATE			
LEB	211+63.83	-11		SAN MH	902.75	902.2	1					
LEB	213+32.41	-13		SAN MH	901.69	901.5	1					
LEB	215+81.78	-11		SAN MH	899.19	899.7	1	6.7	ROTATE			
LEB	217+66.13		240	SAN MH	899.19							
LEB	218+31.49	-8		SAN MH	900.18							
PROJECT TOTAL							32	54.9				

NOTES:
 ROTATE: MANHOLE COVER IS IN PROPOSED CURB LINE AND NEEDS TO BE ROTATED/ADJUSTED TO AVOID BEING IN FINISHED CURB LINE. PAID FOR AS RECONSTRUCT DRAINAGE STRUCTURE

FIBER - ZAYO					R
STATION		ALIGN	OFFSET	INPLACE ITEM	REMARKS
BEGIN	END				
198+00	219+00	EB	20' - 45' LT	BURIED FIBER	ADJUST
199+90		EB	25' LT	BURIED SPLICE BOX	
208+32		EB	40' LT	BURIED SPLICE BOX	RELOCATE
209+17		EB	54' LT	BURIED SPLICE BOX	RELOCATE
211+26		EB	43' LT	BURIED SPLICE BOX	RELOCATE

ADJUST/RELOCATE DONE BY OTHERS

GAS - CENTERPOINT ENERGY									T
STATION		OFFSET FROM LNB			REMARKS	SIZE & ITEM	STATION		
BEGIN	END						BEGIN	END	
99+84		54	LT	TO 200	LT	NEW - EGRET		2" ST	
99+84	112+80	54	LT	TO 56	LT			8" ST	
102+67		49	LT	TO 100	LT	TO 10714 FOLEY			
103+45	103+45	48	LT	TO 70	RT	TO 388/396 107TH LN			
103+63		48	LT	TO 100	LT	TO 10740 FOLEY			
104+42		52	LT					VALVE	
106+72		46	LT	TO 100	LT	TO 10764/10772/10780			
108+00	108+00	48	LT	TO 200	RT	108TH AVE CROSSING		2" ST	
109+12	109+12	71	LT	TO 200	LT				
109+56		53	LT	TO 60	RT	TO 10821 FOLEY			
110+14		54	LT	TO 75	RT	TO 10831 FOLEY			
110+87	110+87	72	LT	TO 200	LT				
112+80	112+80	200	LT	TO 200	RT	109TH AVE CROSSING		8" ST	
112+80	115+05	60	LT	TO 61	LT			1/2" PLASTIC	
114+56	114+56	61	LT	TO 200	LT	TO 10932 FOLEY			
116+13	116+13	59	RT	TO 200	RT	109TH LANE		2" ST	
117+27	125+98	58	LT	TO 47	LT			2" ST	
119+41	119+41	52	LT	TO 200	RT	110TH AVE CROSSING		2" ST	
121+05	121+05	47	LT	TO 200	LT	110TH AVE		2" ST	
122+69	122+69	47	LT	TO 200	RT	110TH LN CROSSING		2" ST	
125+98	125+98	200	LT	TO 200	RT	111TH AVE CROSSING		2" ST	
125+35		46	LT	TO 90	RT	TO 11051 FOLEY			
125+98	128+58	47	LT	TO 48	LT			3/4" ST	
127+08		47	LT			TO 11061 FOLEY			
127+32		47	LT			TO 11063 FOLEY			
128+58	128+58	48	LT	TO 200	LT			3/4" ST	
128+88	130+72	40	LT	TO 43	LT			2" ST	
128+88	128+88	40	LT	TO 200	LT			2" ST	
129+28	129+28	41	LT	TO 200	RT	111TH LANE CROSSING		2" ST	
130+72	130+72	43	LT	TO 200	LT			2" ST	
129+28	142+29	19	RT	TO 13	RT			2" ST	
133+48		13	RT	TO 75	RT			2" ST	
135+05	135+05	73	LT	TO 200	LT			2" ST	
137+02	137+02	103	LT	TO 200	LT			2" ST	
139+34		13	RT	TO 100	RT	TO 11247 FOLEY			
141+24		13	RT	TO 120	RT	TO 370 NORTHDALE			
141+89		13	RT	TO 60	LT	TO 11364 FOLEY			
145+27	149+06	2	RT	TO 22	LT			2" ST	
149+06	149+36	22	LT	TO 200	LT	115TH AVE CROSSING		2" ST	
149+10	EOP	47	LT	TO 41	LT			2" PLASTIC	
STATION		OFFSET FROM LEB			REMARKS	SIZE & ITEM	STATION		
BEGIN	END						BEGIN	END	
204+13	216+61	39	LT	TO 26	LT			2" ST	
204+13	204+13	39	LT	TO 155	LT			2" ST	
211+84		32	LT	TO 200	LT	FLINTWOOD ST		2" ST	
214+06		35	LT	TO 200	LT			2" ST	

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_TAB.dgn 05/19/2016 12:55:45 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

TABULATIONS
 Sheet 18 of 298 Sheets

OVERHEAD POWER - CONNEXUS/GRE U

STATION	ALIGN	OFFSET	INPLACE ITEM	REMARKS
100+26.23	NB	-62.6	SPLICE BOX	RELOCATE
100+89.78	NB	39.5	SPLICE BOX	REMAIN AS-IS
100+90.30	NB	64.1	SPLICE BOX	REMAIN AS-IS
100+90.42	NB	52.6	SPLICE BOX	REMAIN AS-IS
101+04.22	NB	-134.2	POWER POLE	ADJUST
101+06.79	NB	-94.8	POWER POLE	ADJUST
101+50.88	NB	299	POWER POLE	ON EGRET/REMAIN AS-IS
102+81.04	NB	-59.9	POWER POLE	RELOCATE
103+54.71	NB	32.1	SPLICE BOX	RELOCATE
103+59.62	NB	32.6	SPLICE BOX	RELOCATE
105+52.66	NB	-57.6	POWER POLE	RELOCATE
105+56.17	NB	-101.7	POWER POLE	REMAIN AS-IS
105+87.17	NB	14.8	POWER POLE	RELOCATE
106+56.66	NB	13.8	POWER POLE	RELOCATE
107+66.23	NB	-56	POWER POLE	RELOCATE
108+04.32	NB	9	POWER POLE	RELOCATE
109+04.95	NB	5.8	POWER POLE	RELOCATE
110+97.89	NB	3.9	POWER POLE	RELOCATE
110+97.98	NB	-61.1	POWER POLE	RELOCATE
113+19.93	NB	2.9	POWER POLE	RELOCATE
114+32.11	NB	3.9	POWER POLE	RELOCATE (GRE)
116+51.90	NB	1.7	POWER POLE	RELOCATE
117+61.30	NB	48.6	POWER POLE	
117+63.03	NB	4.8	POWER POLE	RELOCATE
118+86.37	NB	7.2	POWER POLE	RELOCATE
119+20.84	NB	-57.1	POWER POLE	RELOCATE
120+84.74	NB	57.9	POWER POLE	
120+89.05	NB	12.4	POWER POLE	RELOCATE
122+10.38	NB	13.4	POWER POLE	RELOCATE
122+48.14	NB	-56.4	POWER POLE	RELOCATE
124+13.31	NB	13.1	POWER POLE	RELOCATE
124+14.43	NB	-96.5	POWER POLE	
125+08.40	NB	12.9	POWER POLE	RELOCATE
126+08.14	NB	13.5	POWER POLE	RELOCATE
127+43.08	NB	-141.7	POWER POLE	
127+49.73	NB	13.4	POWER POLE	RELOCATE
128+78.01	NB	-130.3	POWER POLE	
128+78.33	NB	-58.5	POWER POLE	RELOCATE
128+83.02	NB	13.5	POWER POLE	RELOCATE
130+73.09	NB	13	POWER POLE	RELOCATE
132+95.04	NB	13.5	POWER POLE	RELOCATE
134+01.47	NB	13.3	POWER POLE	RELOCATE
135+05.01	NB	-135.3	POWER POLE	
135+64.08	NB	12.2	POWER POLE	RELOCATE
136+54.01	NB	12.9	POWER POLE	RELOCATE
138+16.56	NB	9.6	POWER POLE	RELOCATE
139+03.30	NB	11.6	POWER POLE	RELOCATE
140+57.29	NB	12.9	POWER POLE	RELOCATE
145+04.87	NB	13.3	POWER POLE	RELOCATE
147+77.22	NB	-75.9	POWER POLE	ADJUST
148+87.25	NB	23.3	POWER POLE	RELOCATE
149+30.94	NB	68.8	SPLICE BOX	REMAIN AS-IS
150+26.26	NB	-85.1	POWER POLE	RELOCATE
199+18.33	EB	31.4	POWER POLE	REMAIN AS-IS
203+73.26	EB	22.1	POWER POLE	RELOCATE
206+64.14	EB	21.3	POWER POLE	RELOCATE
211+29.73	EB	-42.9	POWER POLE	RELOCATE
212+16.50	EB	19.8	POWER POLE	RELOCATE
212+30.99	EB	-43.3	POWER POLE	RELOCATE
214+35.72	EB	53.1	POWER POLE	REMAIN AS-IS
214+45.76	EB	-42.4	POWER POLE	RELOCATE
215+30.76	EB	44.9	POWER POLE	REMAIN AS-IS
217+77.40	EB	-42.1	POWER POLE	RELOCATE

ADJUST/RELOCATE DONE BY OTHERS

TELEPHONE - CENTURYLINK V				
STATION		OFFSET FROM	INPLACE ITEM	REMARKS
BEGIN	END	LNB		
99+20		60 - 67 LT	BURIED LINE	
100+97		68 RT	SB TELE	
101+05		78 LT	SB TELE	
101+49		293 RT	SB TELE	EGRET
102+81		62 LT	SB TELE	
103+45		31 RT	SB TELE	
105+54		57 LT	SB TELE	
107+69		56 LT	SB TELE	
108+00	144+00	10 RT	BURIED LINE	
110+91		58 LT	MH TEL	
111+06		60 LT	SB TELE	CROSSING
112+82		142 LT	SB TELE	
114+26		2 RT	SB TELE	CROSSING
134+03		34 RT	SB TELE	
141+34		116 LT	SB TELE	
143+88		52 LT	SB TELE	
143+88		50 LT	SB TELE	
143+92		53 LT	SB TELE	CROSSING
143+92		50 LT	SB TELE	
148+83		119 LT	SB TELE	
150+14		27 RT	SB TELE	
150+26		87 LT	SB TELE	

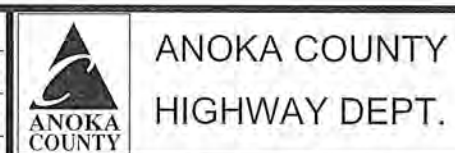
STATION		OFFSET FROM	INPLACE ITEM	REMARKS
BEGIN	END	LEB		
197+06	217+00	20 - 28 RT	MH_TEL	9 & 12 WAY CONCRETE
203+25		20 RT	MH_TEL	
203+66		13 RT	MH_TEL	
203+75		22 RT	SB TELE	
210+36		23 RT	SB TELE	
211+79		11 RT	MH_TEL	CROSSING
211+79		17 RT	MH_TEL	
212+14		20 RT	SB TELE	
213+47		14 RT	MH_TEL	CROSSING
215+30		42 RT	SB TELE	

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_TAB.dgn 05/13/2016 12:55:51 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina Pizzo*
 DATE: 5-19-14 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16




SP 002-611-034
 SP 114-020-050
 CP 13-10

TABULATIONS
 Sheet 19 of 298 Sheets

STATION NB CSAH 11	EXCAVATION TOTALS		EMBANKMENT VOLUMES		
	COMMON (C.Y.)	SUBGRADE (C.Y.)	TOPSOIL (C.Y.)	SUITABLE (C.Y.)	SEL. GRAN. (C.Y.)
100+00.00	0	0	0	0	0
100+50.00	177	126	4	36	162
101+00.00	246	166	7	23	189
101+27.00	270	121	6	7	128
101+37.38	134	59	6	7	128
101+46.00	79	50	1	1	24
101+50.00	31	23	2	3	69
101+61.48	82	66	8	13	187
102+00.00	202	174	2	3	26
102+07.00	28	23	2	3	26
102+13.00	25	20	1	1	19
102+18.58	24	18	1	2	23
102+25.00	26	21	4	9	88
102+50.00	91	79	8	20	172
103+00.00	144	152	8	20	172
103+06.65	17	20	1	1	9
103+09.50	8	8	5	13	129
103+50.00	90	116	11	51	262
104+00.00	106	201	6	34	172
104+24.98	82	133	6	34	172
104+50.00	126	137	9	31	226
105+00.00	203	195	4	11	68
105+25.00	53	57	1	3	17
105+31.31	12	14	1	3	20
105+39.00	14	17	1	3	20
105+50.00	22	25	7	18	132
106+00.00	123	114	8	17	131
106+50.00	148	114	9	19	133
107+00.00	162	114	6	12	85
107+32.00	108	73	6	12	85
107+50.00	54	44	1	6	64
107+69.88	52	58	0	7	58
107+85.00	44	51	0	1	14
107+88.91	13	13	1	1	18
107+95.00	22	17	1	1	18
108+00.00	16	13	5	4	53
108+20.00	79	49	2	1	18
108+27.24	37	17	1	1	10
108+31.00	20	9	6	4	52
108+50.00	105	48	6	4	52
108+79.01	176	78	3	2	26
108+88.00	55	24	5	3	36
109+00.00	77	33	4	3	47
109+14.00	75	44	1	7	33
109+20.79	28	26	1	7	33
109+28.00	32	30	0	6	25
109+32.45	23	19	2	10	79
109+50.00	101	69	9	8	180
110+00.00	309	172	9	8	125
110+36.00	246	117	9	8	125

STATION NB CSAH 11	EXCAVATION TOTALS		EMBANKMENT VOLUMES		
	COMMON (C.Y.)	SUBGRADE (C.Y.)	TOPSOIL (C.Y.)	SUITABLE (C.Y.)	SEL. GRAN. (C.Y.)
110+42.96	48	23	3	2	25
110+50.00	46	23	4	3	36
110+60.00	66	33	2	2	24
110+66.56	43	22	2	2	20
110+72.00	35	18	2	2	20
111+00.00	148	93	3	14	97
111+25.00	94	83	5	11	95
111+50.00	103	84	2	2	26
111+57.00	33	24	8	12	157
112+00.00	200	145	8	12	157
112+50.00	190	216	0	5	74
112+62.28	42	69	3	5	182
113+00.00	158	177	10	13	183
113+50.00	277	170	6	6	87
113+73.93	149	81	6	6	87
113+81.93	47	27	3	2	31
113+90.63	52	29	3	2	33
114+00.00	52	31	9	11	153
114+42.57	202	142	1	2	27
114+50.00	31	25	1	2	27
115+00.00	229	165	2	2	27
115+07.78	39	25	3	2	29
115+16.00	41	27	1	1	17
115+21.00	25	16	2	2	31
115+29.89	42	29	2	2	31
115+39.00	44	30	2	3	39
115+50.00	53	36	9	29	228
115+93.66	243	199	2	6	44
116+00.00	42	38	3	5	41
116+06.00	42	36	3	5	41
116+15.00	72	56	4	18	108
116+30.00	156	90	8	16	103
116+50.00	165	87	4	3	26
116+57.75	37	23	7	4	40
116+70.00	59	36	7	4	40
117+00.00	136	82	5	5	50
117+18.00	76	45	3	3	28
117+28.21	43	25	3	3	26
117+38.00	40	23	3	3	31
117+50.00	47	28	3	3	31
117+82.00	127	72	2	2	18
117+89.28	30	16	2	2	17
117+96.00	28	15	1	1	10
118+00.00	17	9	7	14	126
118+50.00	173	112	7	14	126
118+91.00	120	99	1	3	27
119+00.00	23	24	1	13	94
119+22.09	67	81	1	12	88
119+43.00	67	76	1	2	21
119+50.00	20	19	1	2	21

STATION NB CSAH 11	EXCAVATION TOTALS		EMBANKMENT VOLUMES		
	COMMON (C.Y.)	SUBGRADE (C.Y.)	TOPSOIL (C.Y.)	SUITABLE (C.Y.)	SEL. GRAN. (C.Y.)
119+62.00	35	30	6	11	103
120+00.00	117	92	5	9	82
120+30.50	94	73	1	1	11
120+34.80	12	10	0	1	7
120+37.50	8	6	0	1	7
120+50.00	33	30	3	28	134
120+86.32	104	106	1	11	56
121+00.00	40	45	1	2	25
121+08.00	22	23	1	1	14
121+13.00	15	13	1	1	14
121+21.02	28	19	3	2	18
121+28.00	24	16	1	1	13
121+33.00	16	12	2	4	44
121+50.00	49	40	7	13	129
122+00.00	133	116	7	13	129
122+07.00	17	16	4	17	160
122+50.00	122	143	3	6	69
122+67.00	50	63	1	1	17
122+72.47	15	16	1	1	16
122+78.00	15	15	1	1	16
122+87.00	26	22	2	3	34
123+00.00	38	31	9	13	129
123+50.00	159	116	7	9	95
123+87.00	126	86	1	1	11
123+91.11	14	10	1	1	11
123+97.00	20	14	1	1	8
124+00.00	11	7	2	2	16
124+06.00	23	14	1	2	18
124+13.00	25	16	7	10	96
124+50.00	121	86	7	10	96
124+70.00	69	46	4	3	32
124+82.69	43	29	1	3	27
124+93.00	32	24	1	2	18
125+00.00	22	16	22	37	241
125+50.00	327	189	22	37	241
125+79.45	266	211	5	9	191
126+00.00	183	182	16	18	291
126+50.00	343	273	8	7	70
126+77.00	112	63	1	1	10
126+81.00	17	9	1	1	10
126+88.79	33	18	2	1	13
126+94.00	23	12	2	1	15
127+00.00	27	14	13	12	128
127+50.00	224	116	14	43	160
128+00.00	255	117	14	43	160
128+37.00	208	86	1	3	11
128+40.44	17	8	1	1	9
128+44.00	15	8	2	2	16
128+50.00	27	14	7	9	195
129+00.00	214	186	7	9	195

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: GINA M. PIZZO SIGNATURE: <i>[Signature]</i> DATE: 5-19-16 LICENSE NO. 22713		DRAWN BY: DFF DATE 02/27/16 DESIGN BY: DFF DATE 02/27/16 CHECKED BY: JEO DATE 02/27/16		 ANOKA COUNTY HIGHWAY DEPT.	SP 002-611-034 SP 114-020-050 CP 13-10	EARTHWORK TABULATION Sheet 20 of 298 Sheets
NO: _____ DATE: _____ BY: _____ CKD: _____ APPR: _____ NAME: P:\02-611-34\Plan\002-611-034_EW.dgn	REVISION: _____ 05/13/2016 12:55:56 PM					

STATION NB CSAH 11	EXCAVATION TOTALS		EMBANKMENT VOLUMES		
	COMMON (C.Y.)	SUBGRADE (C.Y.)	TOPSOIL (C.Y.)	SUITABLE (C.Y.)	SEL. GRAN. (C.Y.)
129+10.55	118	56	3	6	160
129+50.00	414	154	7	14	133
130+00.00	127	119	3	5	48
130+18.00	39	43	1	2	21
130+25.78	15	19	1	2	21
130+32.00	12	15	2	5	49
130+50.00	33	43	3	9	83
130+80.31	52	71	2	6	55
131+00.00	32	45	4	17	143
131+50.00	81	117	4	17	143
132+00.00	104	125	0	2	12
132+04.00	10	10	1	3	25
132+12.45	21	22	1	3	23
132+20.00	19	20	1	2	15
132+25.00	14	13	1	2	15
132+50.00	79	66	8	16	154
133+00.00	164	136	2	19	100
133+22.95	68	81	3	21	121
133+50.00	85	100	14	11	166
134+00.00	253	155	14	11	166
134+23.00	133	70	4	3	34
134+33.13	53	31	5	3	40
134+45.00	65	37	2	1	17
134+50.00	28	16	10	8	114
134+83.00	185	106	10	8	114
134+90.72	45	25	3	2	29
134+99.00	48	27	0	0	3
135+00.00	6	3	14	13	177
135+50.00	280	164	8	6	89
135+75.00	138	83	8	6	89
135+83.19	43	27	1	0	6
135+85.00	10	6	5	4	54
136+00.00	84	50	11	14	187
136+50.00	232	172	2	6	441
136+86.60	97	169	2	6	441
137+00.00	27	172	6	6	605
137+50.00	193	583	7	7	94
137+75.91	156	87	7	6	87
138+00.00	155	81	6	5	76
138+21.00	130	71	6	5	76
138+33.76	72	43	3	4	52
138+48.00	78	48	0	1	8
138+50.00	11	7	6	8	116
138+82.00	187	108	3	2	29
138+90.00	63	27	3	2	29
139+00.00	97	34	9	3	43
139+12.00	122	40	4	2	29
139+20.00	69	27	7	8	109
139+50.00	184	101	10	15	187
140+00.00	270	172	10	15	187

STATION NB CSAH 11	EXCAVATION TOTALS		EMBANKMENT VOLUMES		
	COMMON (C.Y.)	SUBGRADE (C.Y.)	TOPSOIL (C.Y.)	SUITABLE (C.Y.)	SEL. GRAN. (C.Y.)
140+22.00	126	77	2	3	46
140+34.14	80	43	2	3	41
140+45.00	81	38	1	1	18
140+50.00	40	17	7	16	187
141+00.00	314	171	7	16	187
141+50.00	229	165	2	5	76
141+72.00	112	71	3	7	96
142+00.00	141	89	4	13	171
142+50.00	201	158	0	1	8
142+52.20	8	7	0	1	8
142+64.20	38	38	0	1	11
142+67.20	9	10	1	4	45
142+80.00	40	41	2	6	70
143+00.00	66	64	9	32	153
143+38.00	182	121	9	32	153
143+50.00	67	43	2	7	43
143+59.00	45	36	2	8	36
143+67.00	36	28	5	36	138
144+00.00	128	102	3	53	205
144+50.00	181	152	3	53	205
145+00.00	186	155	7	32	183
145+50.00	160	151	8	36	169
146+00.00	194	133	6	12	78
146+25.00	143	66	6	5	58
146+44.63	117	53	6	5	58
146+50.00	32	14	13	7	101
146+85.00	190	94	6	3	43
147+00.00	75	40	4	2	26
147+09.00	45	24	1	1	12
147+13.21	20	11	1	1	12
147+24.50	52	30	11	5	74
147+50.00	130	69	5	2	27
147+59.19	49	25	5	2	33
147+70.82	56	31	10	5	81
147+99.99	124	76	10	5	81
148+05.59	23	14	6	2	25
148+14.82	39	23	4	1	15
148+20.27	23	14	3	1	12
148+24.69	17	11	5	2	32
148+36.91	44	30	5	2	32
148+49.96	42	32	12	46	160
148+99.91	135	114	10	87	165
149+49.86	134	78	15	88	137
149+99.83	156	49	974	1919	18226
CSAH 11	21406	15864	1035	1986	19024

STATION EB CSAH 12	EXCAVATION TOTALS		EMBANKMENT VOLUMES		
	COMMON (C.Y.)	SUBGRADE (C.Y.)	TOPSOIL (C.Y.)	SUITABLE (C.Y.)	SEL. GRAN. (C.Y.)
200+07.00	0	0	0	0	0
200+50.00	0	0	0	0	0
200+84.00	0	0	0	0	0
201+00.00	4	7	0	26	7
201+50.00	49	45	3	9	71
201+58.00	13	7	1	47	16
202+00.00	67	40	4	34	87
202+32.00	54	32	2	19	66
202+50.00	33	18	1	51	37
203+00.00	92	53	3	8	104
203+08.00	14	9	1	25	17
203+50.00	78	66	4	33	91
204+00.00	96	116	4	32	149
204+50.00	105	118	4	7	150
205+00.00	122	110	6	3	117
205+20.00	54	45	2	5	48
205+50.00	89	70	3	4	75
205+75.00	79	61	2	4	65
206+00.00	82	62	3	2	66
206+12.00	42	30	2	6	32
206+50.00	156	94	7	8	100
207+00.00	223	123	9	8	131
207+50.00	211	124	8	19	132
208+00.00	212	125	6	13	144
208+24.71	99	64	2	0	77
210+00.00	0	0	0	11	0
210+38.00	129	96	7	3	107
210+50.00	39	30	3	12	33
210+70.00	70	49	3	19	61
211+00.00	103	73	4	30	92
211+50.00	203	161	6	9	191
211+60.00	51	40	1	24	49
212+00.00	180	129	5	13	153
212+50.00	179	122	9	14	135
213+00.00	132	118	10	17	132
213+50.00	91	114	10	6	131
213+70.00	34	43	4	8	49
214+00.00	54	60	8	15	68
214+50.01	88	95	13	16	110
215+00.01	73	96	10	18	112
215+50.01	66	109	9	9	127
215+75.01	32	60	4	8	69
216+00.01	31	61	4	12	69
216+32.01	47	78	5	8	90
216+50.01	31	44	3	9	52
216+75.01	44	61	5	8	70
217+00.01	44	62	4	17	70
217+50.01	99	129	6	14	146
218+00.01	114	142	6	0	156
CSAH 12	3908	3391	216	663	4054

NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_EW.dgn 05/13/2016 12:56:02 PM					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE 02/27/16
 DESIGN BY: DFF DATE 02/27/16
 CHECKED BY: JEO DATE 02/27/16



ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

EARTHWORK
TABULATION

SP 002-611-034		
ELEVATION	AREA (SY)	COMMON EX (CY)
POND A (@ 11102 FOLEY)		
ABOVE 895		192
894 - 895	1114	138
893 - 894	950	230
893 - 893	835	260
891 - 892	725	224
890 - 891	622	191
889 - 890	526	160
888 - 889	350	131
887 - 888	272	104
886 - 887	202	79
SUBTOTAL		1709
ELEVATION	AREA (SY)	COMMON EX (CY)
POND B (BETWEEN 112TH & 113TH)		
ABOVE 903		307
902 - 903	1365	132
901 - 902	1169	105
900 - 901	979	79
899 - 900	796	56
898 - 899	618	36
897 - 898	444	23
896.5 - 897	282	8
SUBTOTAL		746
POND EXCAVATION TOTAL		2455

EARTHWORK BALANCE

EXCAVATION (CY)			
COMMON (EV) (P) (1)	29,605	COMMON	23,968 (EV) / 1.2 = 19,974 (CV)
		EXISTING PAVEMENT	1,791 (8)
		EXISTING TOPSOIL	3,846 (EV) / 1.2 = 3,205 (CV)
SUBGRADE EXCAVATION (EV) (P) (2)	19,460	SUBGRADE	19,460 (EV) / 1.2 = 16,217 (CV) (3)
EMBANKMENT (CY)			
SUITABLE	2,724 (CV)		2,724 (CV)
TOPSOIL	1,278 (CV)		1,278 (CV)
SELECT GRANULAR	23,358 (CV)		23,358 (CV) (4)
EXCESS (CY)			
SUITABLE	19,974 (CV) - 2,724 (CV)	=	17,250 (CV) (5)
TOPSOIL	3,205 (CV) - 1,278 (CV)	=	1,927 (CV) (6)
BORROW (CY)			
SELECT GRANULAR	23,358 (CV) - 16,217 (CV)	=	7,141 (CV) *1.4 = 9,997 (LV) (7)

- (1) TOTAL COMMON EXCAVATION FOR PROJECT (INCLUDING TOPSOIL AND PONDS)
- (2) TOTAL SUBGRADE EXCAVATION FOR PROJECT
- (3) ALL SUBGRADE EXCAVATION ASSUMED TO MEET REQUIREMENTS FOR SELECT GRANULAR
- (4) SELECT GRANULAR QUANTITY
- (5) TOTAL COMMON EXCESS FOR THIS PROJECT
- (6) TOTAL TOPSOIL EXCESS FOR PROJECT
- (7) TOTAL SELECT GRANULAR BORROW FOR PROJECT
- (8) QUANTITY BASED ON 6.0" THICK (VARIES 4" - 11") EXISTING BITUMINOUS PAVEMENT FOR THE PROJECT

STATION WB EGRET	EXCAVATION TOTALS		EMBANKMENT VOLUMES		
	COMMON (C.Y.)	SUBGRADE (C.Y.)	TOPSOIL (C.Y.)	SUITABLE (C.Y.)	SEL. GRAN. (C.Y.)
29+00.00	0	0	0	0	0
29+25.01	19	12	2	5	17
29+50.01	41	24	3	9	33
29+75.01	44	24	4	7	31
29+90.56	25	15	2	3	18
29+99.93	12	9	1	1	10
30+08.22	12	8	1	1	9
30+24.85	29	16	2	3	19
30+49.74	49	24	4	7	31
30+74.62	41	23	3	7	30
30+99.49	36	20	1	10	30
31+24.34	32	16	2	11	27
31+49.17	28	14	2	11	25
EGRET	368	205	27	75	280
TOTAL	25682	19460	1278	2724	23358

POND LINING EXCAVATION, CLAY, GRAVEL & ENGINEERED SOIL				
STATION	COMMON EX CY (1)	CLAY CY	GRAVEL CY	ENG SOIL CY
POND A				
1127+49	231	231		
1128+00	230	230		
1128+50	136	136		
SUBTOTAL	597	597		
POND B				
1133+74	81	72	3	6
1134+00	171	141	9	21
1134+50	165	141	7	17
1135+00	165	143	6	16
1135+50	171	145	8	19
1136+00	118	107	3	7
SUBTOTAL	871	749	36	86
TOTAL	1468	1346	36	86

(1) PAID FOR AS COMMON EXCAVATION

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_EW.dgn 05/13/2016 12:58:07 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: OFF DATE: 02/27/16
 DESIGN BY: OFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



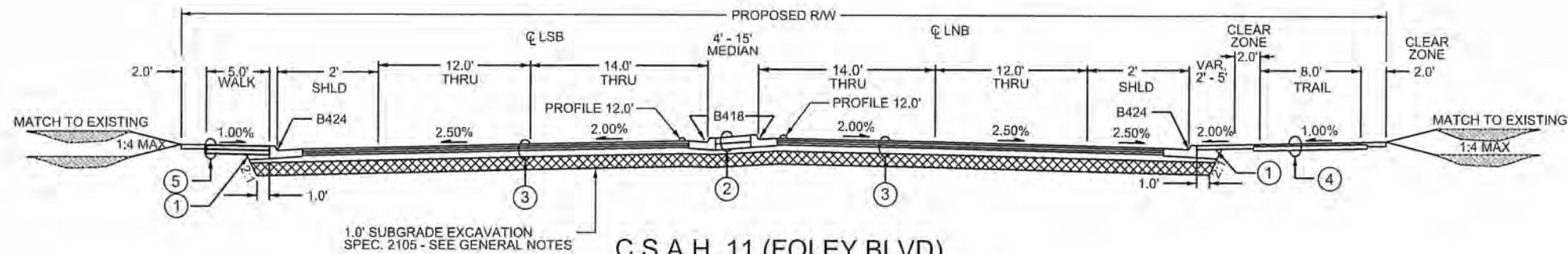
**ANOKA COUNTY
HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

**EARTHWORK
TABULATION/BALANCE**

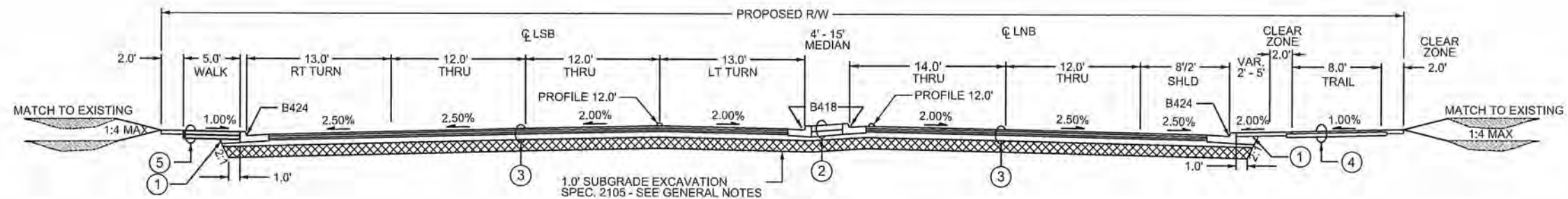
C.S.A.H. 11 (FOLEY BLVD)

NB STA. 104+80 - STA. 107+70
NB STA. 117+57 - STA. 131+89



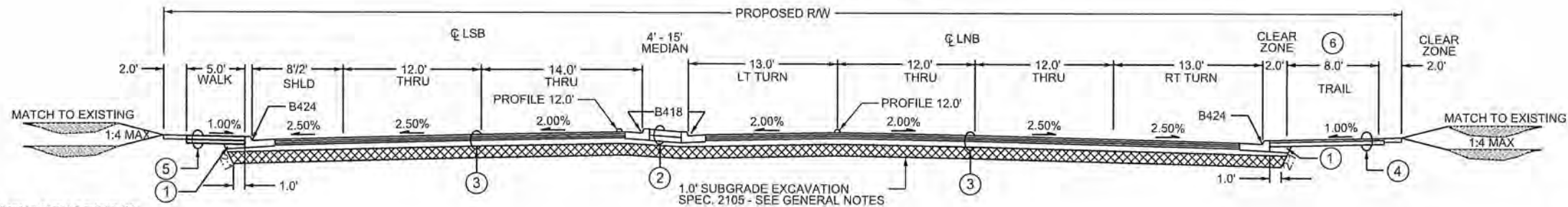
C.S.A.H. 11 (FOLEY BLVD)

NB STA. 99+77 - STA. 104+80
NB STA. 112+63 - STA. 117+57
NB STA. 136+92 - STA. 140+37

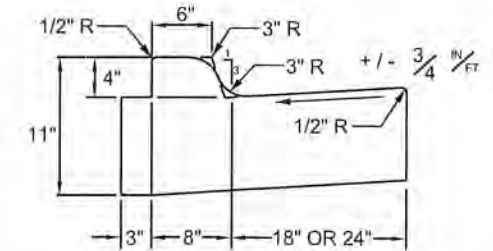


C.S.A.H. 11 (FOLEY BLVD)

NB STA. 107+70 - STA. 112+63
NB STA. 131+89 - STA. 136+92
NB STA. 140+37 - NORTHDALE BLVD



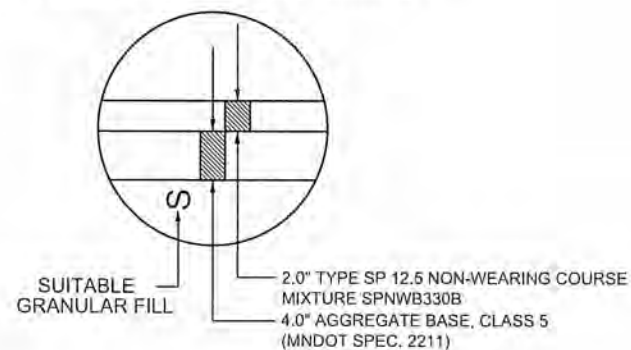
B4 MODIFIED CURB & GUTTER



GENERAL NOTES:

- SEE CONSTRUCTION PLANS FOR TURN LANE LOCATIONS
- ALL CROSS SLOPES ARE EXPRESSED AS A PERCENT
- UNLESS OTHERWISE SPECIFIED, THE GRADING GRADE CROSS SLOPES SHALL BE THE SAME AS THE FINISHED SURFACE OF THE MAINLINE.
- UNLESS OTHERWISE SPECIFIED, CLASS 5 AGGREGATE WILL EXTEND 1' BEYOND BACK OF CURB.
- 4.0' TOPSOIL & SEED ON ALL DISTURBED AREAS
- ALL STATIONING BASED ON NB ALIGNMENT 11_NB_3 (NB CSAH 11) OR EB ALIGNMENT 12_EB_3 (EB CSAH 12) UNLESS OTHERWISE NOTED
- UNLESS OTHERWISE SPECIFIED CLASS 5 AGGREGATE WILL EXTEND 6" BEYOND THE EDGE OF BITUMINOUS TRAIL
- WALK AND TRAIL CROSS SLOPE SHALL BE 2% MAX AT DRIVEWAYS
- 1.0' SUBGRADE EXCAVATION SHALL BE THE BLENDING OF THE EXISTING SUBGRADE AS TO UNIFY THE SOILS AT LEAST 1.0' BENEATH THE GRADING GRADE. PAID FOR AS 2105.507 - (SUBGRADE EXCAVATION)

TEMPORARY WIDENING



NOTES:

- ① SUITABLE MATERIAL
- ② SEE DETAIL "A" PAGE 24
- ③ SEE INSET "B" PAGE 24
- ④ SEE INSET "A" PAGE 24
- ⑤ SEE INSET "C" PAGE 24
- ⑥ A 2.0' CLEAR ZONE SHALL BE STRIPED, WHERE BITUMINOUS TRAIL IS ADJACENT TO BACK OF CURB.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_TS_P1.dgn 05/13/2016 12:56:16 PM

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PRINT NAME: GINA M. PIZZO
SIGNATURE: *GINA M. PIZZO*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
DESIGN BY: DFF DATE: 02/27/16
CHECKED BY: JEO DATE: 02/27/16

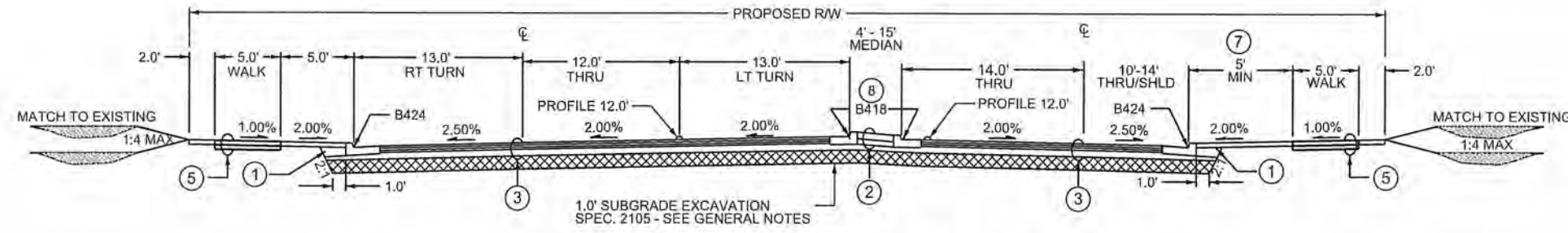


ANOKA COUNTY
HIGHWAY DEPT.

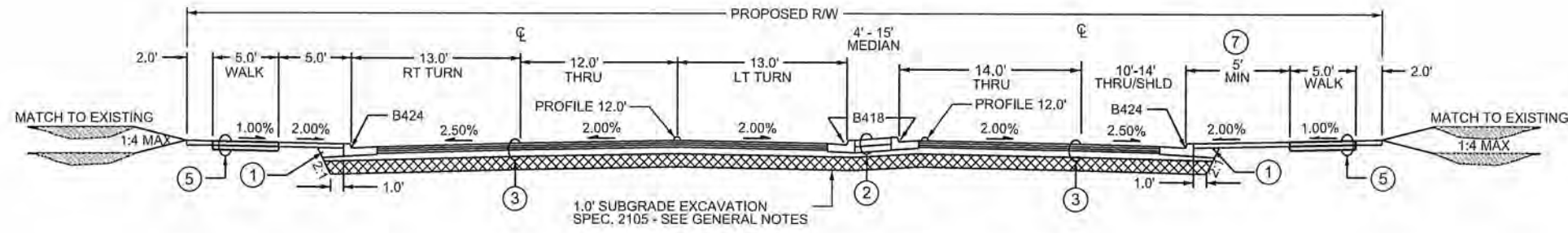
SP 002-611-034
SP 114-020-050
CP 13-10

PROPOSED
TYPICAL SECTIONS

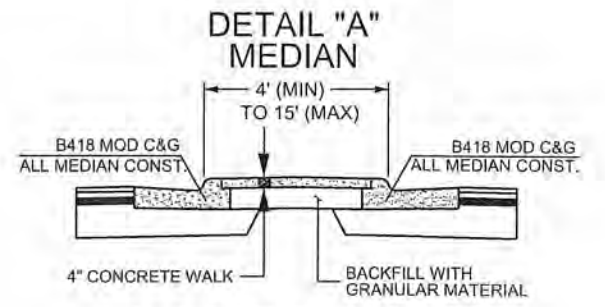
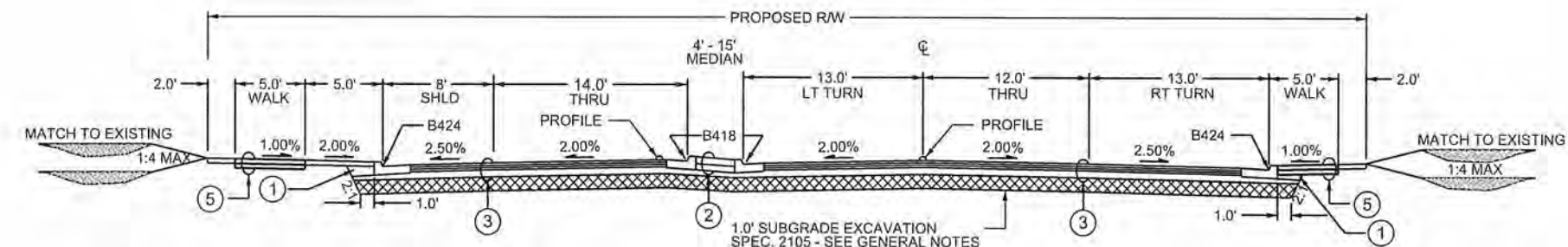
C.S.A.H. 11 (FOLEY BLVD)
NB NORTHDAL BLVD - STA. 146+75



C.S.A.H. 12 (NORTHDAL BLVD)
EB STA. 209+20 - STA. 214+38

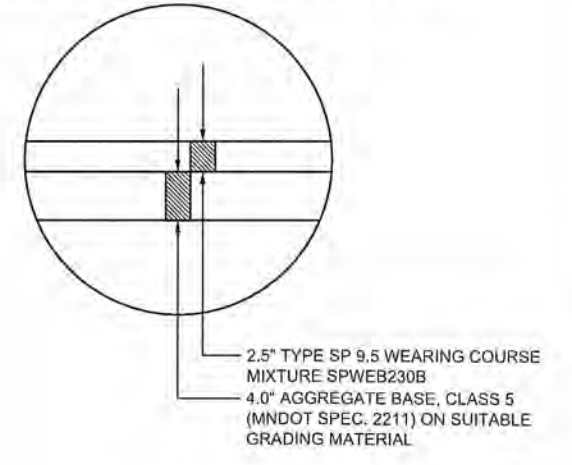


C.S.A.H. 12 (NORTHDAL BLVD)
EB STA. 207+21 - STA. 209+20
EB STA. 214+38 - STA. 218+05

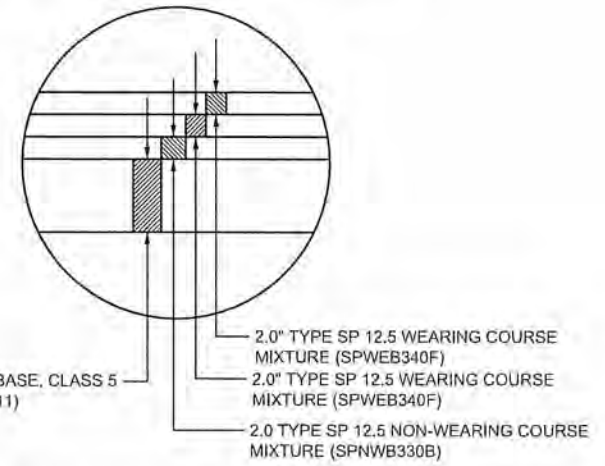


NB STA. 100+30 - STA. 112+37
NB STA. 112+89 - STA. 136+59
NB STA. 137+26 - STA. 144+26
NB STA. 145+71 - STA. 146+75 (8)
EB STA. 207+21 - STA. 208+38
EB STA. 209+91 - STA. 217+92

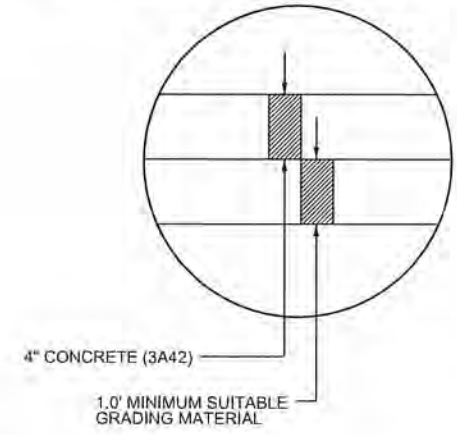
INSET "A" BITUMINOUS PATH



INSET "B" PAVEMENT DESIGN



INSET "C" CONCRETE SIDEWALK



GENERAL NOTES:

- SEE CONSTRUCTION PLANS FOR TURN LANE LOCATIONS
- ALL CROSS SLOPES ARE EXPRESSED AS A PERCENT
- UNLESS OTHERWISE SPECIFIED, THE GRADING GRADE CROSS SLOPES SHALL BE THE SAME AS THE FINISHED SURFACE OF THE MAINLINE.
- UNLESS OTHERWISE SPECIFIED, CLASS 5 AGGREGATE WILL EXTEND 1' BEYOND BACK OF CURB.
- 4.0' TOPSOIL & SEED ON ALL DISTURBED AREAS
- ALL STATIONING BASED ON NB ALIGNMENT 11_NB_3 (NB CSAH 11) OR EB ALIGNMENT 12_EB_3 (EB CSAH 12) UNLESS OTHERWISE NOTED
- UNLESS OTHERWISE SPECIFIED CLASS 5 AGGREGATE WILL EXTEND 6" BEYOND THE EDGE OF BITUMINOUS TRAIL
- WALK AND TRAIL CROSS SLOPE SHALL BE 2% MAX AT DRIVEWAYS
- 1.0' SUBGRADE EXCAVATION SHALL BE THE BLENDING OF THE EXISTING SUBGRADE AS TO UNIFY THE SOILS AT LEAST 1.0' BENEATH THE GRADING GRADE. PAID FOR AS 2105.507 - (SUBGRADE EXCAVATION)

AGGREGATE BASE CLASS 5 TO EXTEND 6" BEYOND EDGE OF TRAIL

NOTES:

- (1) SUITABLE MATERIAL
- (2) SEE DETAIL "A" PAGE 24
- (3) SEE INSET "B" PAGE 24
- (4) SEE INSET "A" PAGE 24
- (5) SEE INSET "C" PAGE 24
- (6) A 2.0' CLEAR ZONE SHALL BE STRIPED, WHERE BITUMINOUS TRAIL IS ADJACENT TO BACK OF CURB.
- (7) 8.0' MIN ON NORTHDAL BLVD (CSAH 12)
- (8) REVERSE SLOPE GUTTER SECTION

NO	DATE	BY	CKD	APPR.	REVISION

NAME: P:\02-611-34\Plan\002-611-034_TS_P1.dgn 05/13/2016 12:58:16 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *[Signature]*
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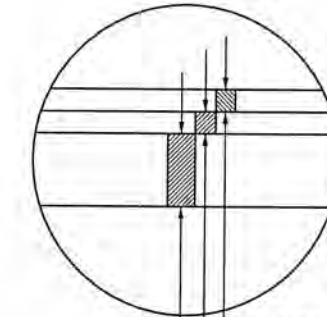
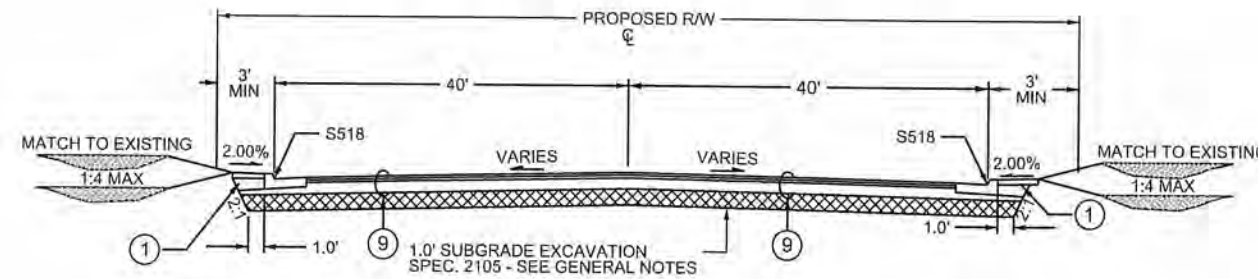
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

PROPOSED
TYPICAL SECTIONS
Sheet 24 of 298 Sheets

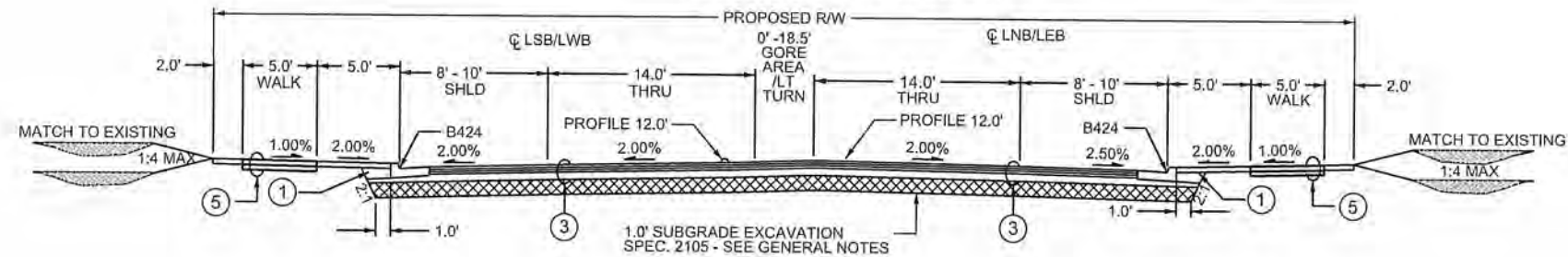
107TH AVE CUL-DE-SAC
 107TH LANE CUL-DE-SAC
 109TH LANE CUL-DE-SAC
 111TH AVE (WEST SIDE) CUL-DE-SAC
 111TH AVE (EAST SIDE) CUL-DE-SAC
 112TH LANE CUL-DE-SAC

INSET "D" CITY PAVEMENT
 DESIGN

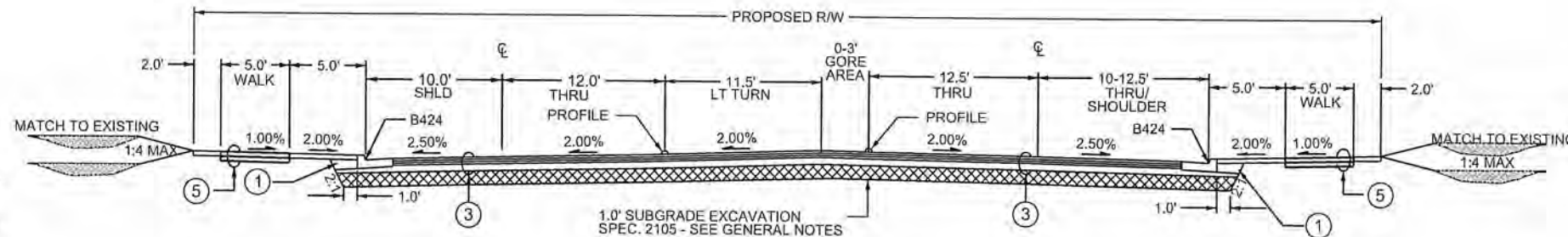


2.0" TYPE SP 12.5 WEARING COURSE MIXTURE (SPWEB340F)
 2.0" TYPE SP 12.5 NON-WEARING COURSE MIXTURE (SPNWB330B)
 6.0" AGGREGATE BASE, CLASS 5 (MNDOT SPEC. 2211)

C.S.A.H. 11 (FOLEY BLVD)
 NB STA. 147+70 - STA. 152+34
 C.S.A.H. 12 (NORTHDALE BLVD)
 EB STA. 203+62 - STA. 207+21



C.S.A.H. 11 (FOLEY BLVD)
 NB STA. 146+75 - STA. 147+70



GENERAL NOTES:

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NOTES:

- ① SUITABLE MATERIAL
- ② SEE DETAIL "A" PAGE 24
- ③ SEE INSET "B" PAGE 24
- ⑤ SEE INSET "C" PAGE 24
- ⑨ SEE INSET "D" PAGE 25

NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_TS_P1.dgn					
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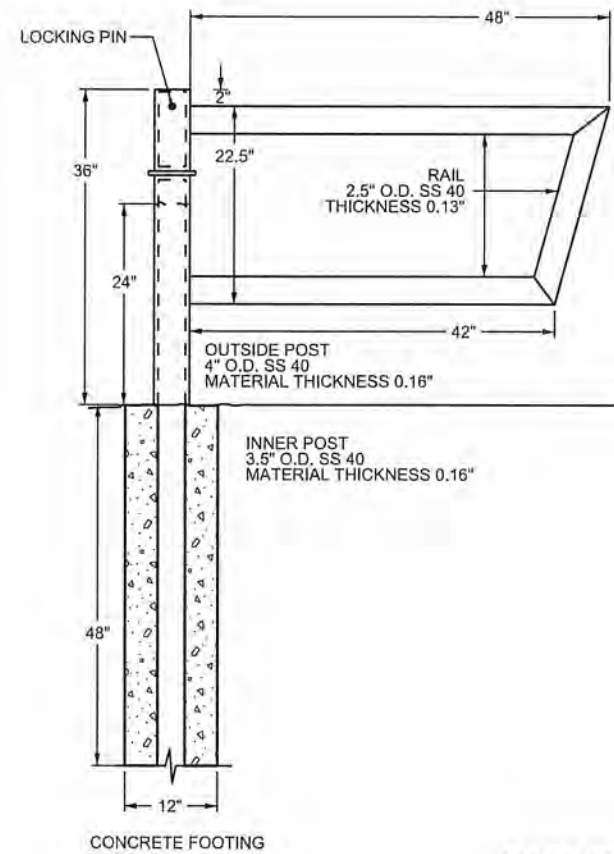
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
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ANOKA COUNTY
 HIGHWAY DEPT.

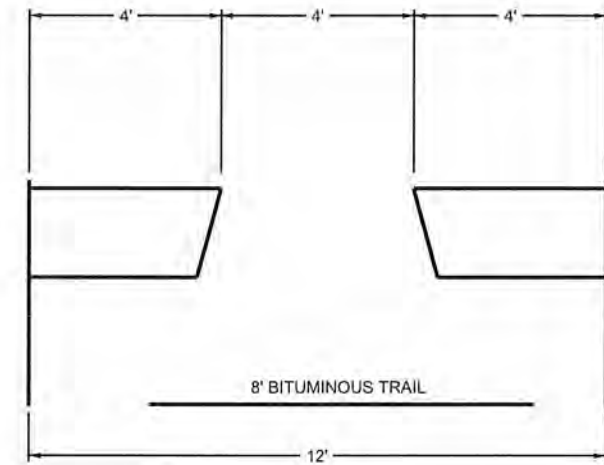
SP 002-611-034
 SP 114-020-050
 CP 13-10



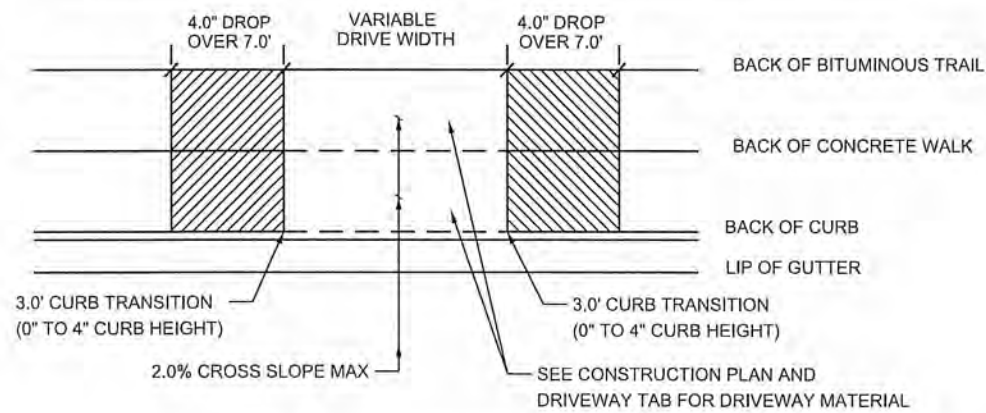
GATE DETAIL



LOCKING PIN DETAIL
6\"/>



WALK / TRAIL DEPRESSION FOR DRIVEWAYS WHERE WALK / TRAIL MEETS BACK OF CURB



NOTE: TRAIL/WALK GRADE MUST NOT EXCEED 5.0%

NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_TS_P1.dgn 05/13/2016 12:56:16 PM

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ANOKA COUNTY
HIGHWAY DEPT.

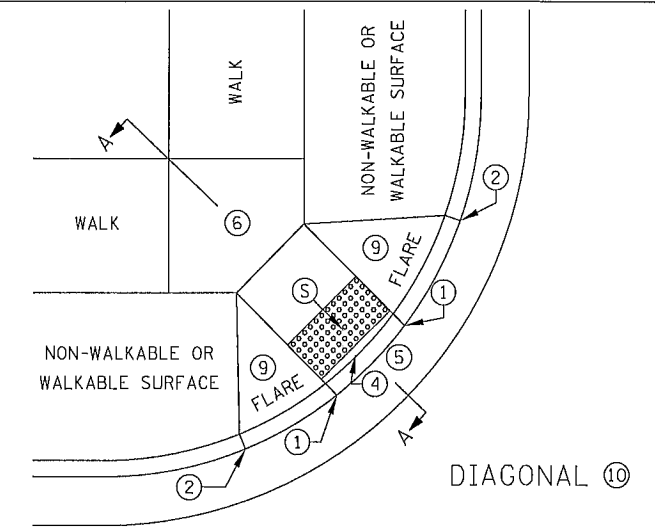
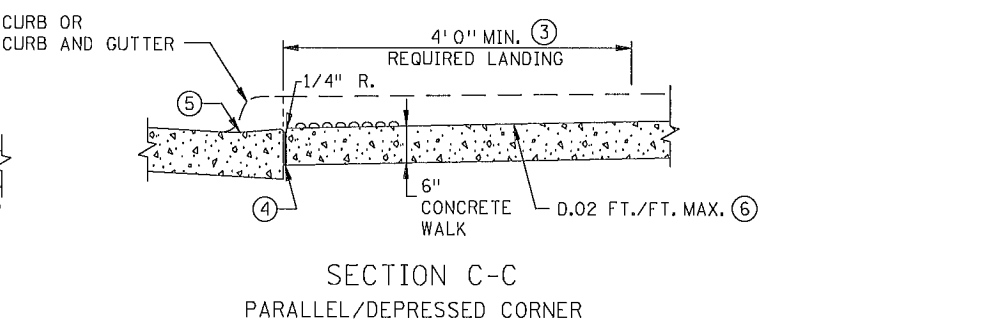
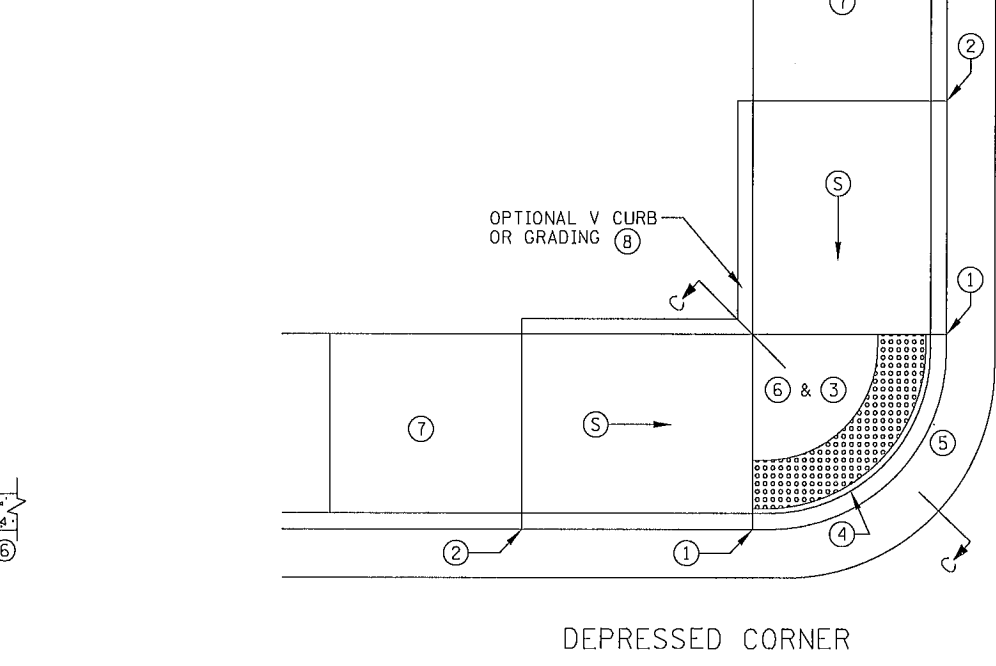
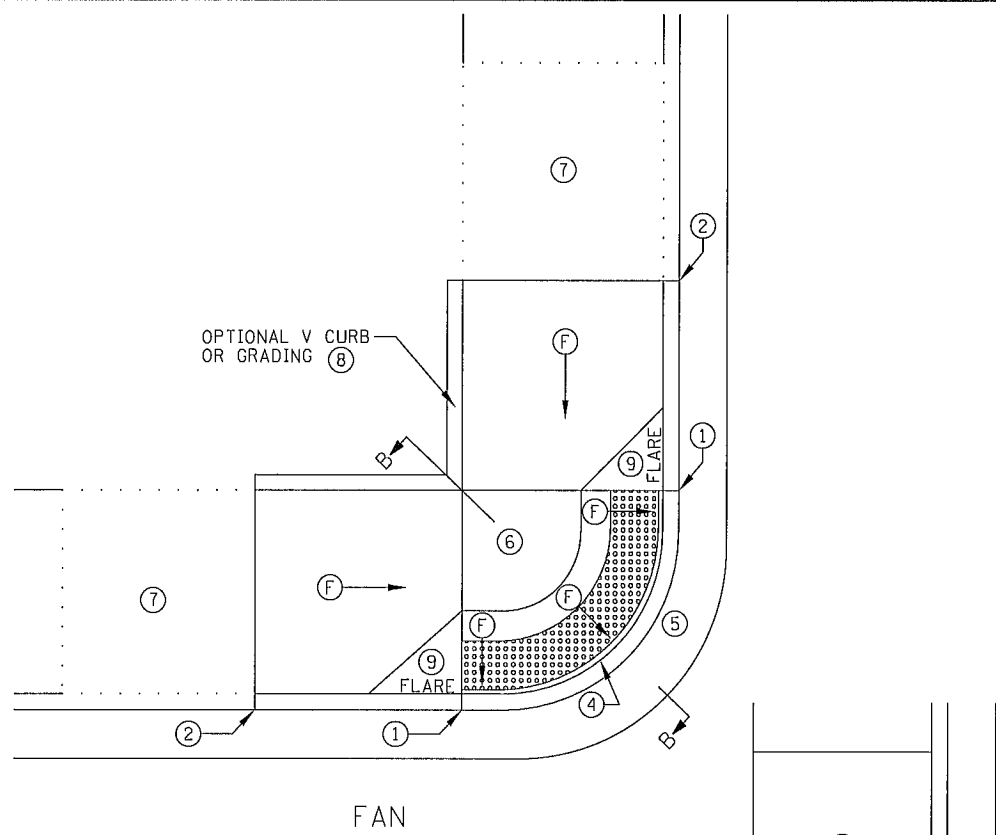
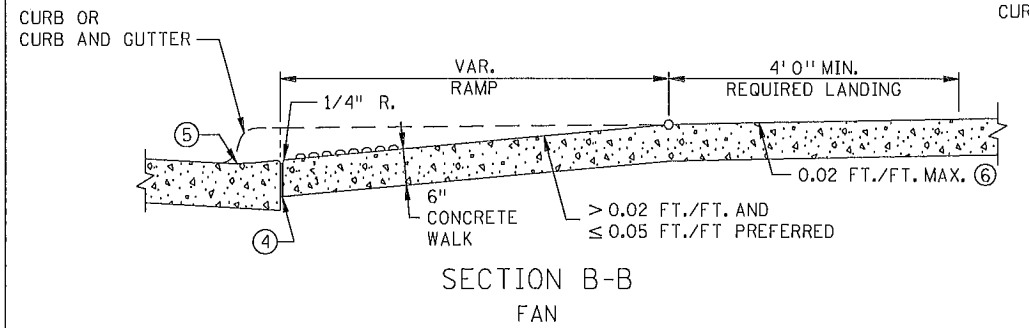
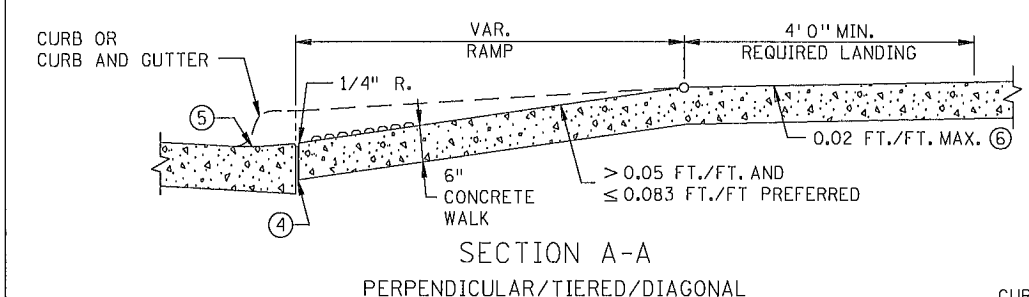
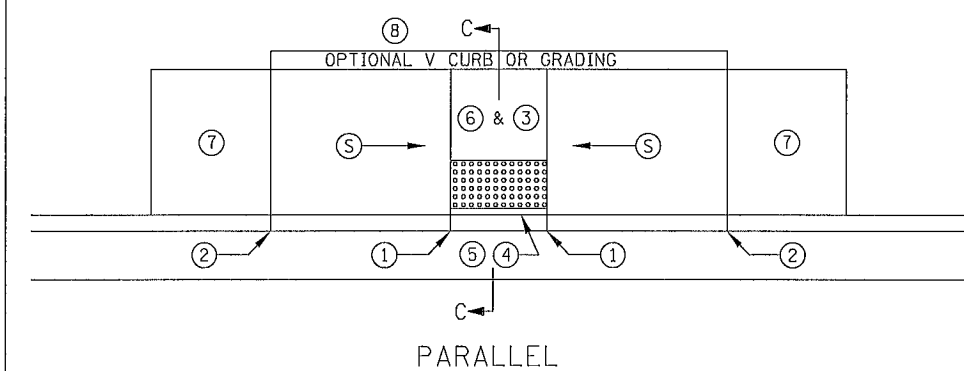
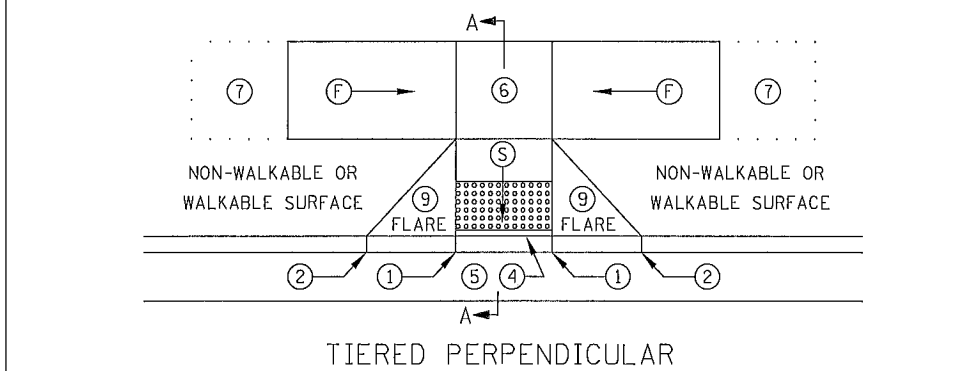
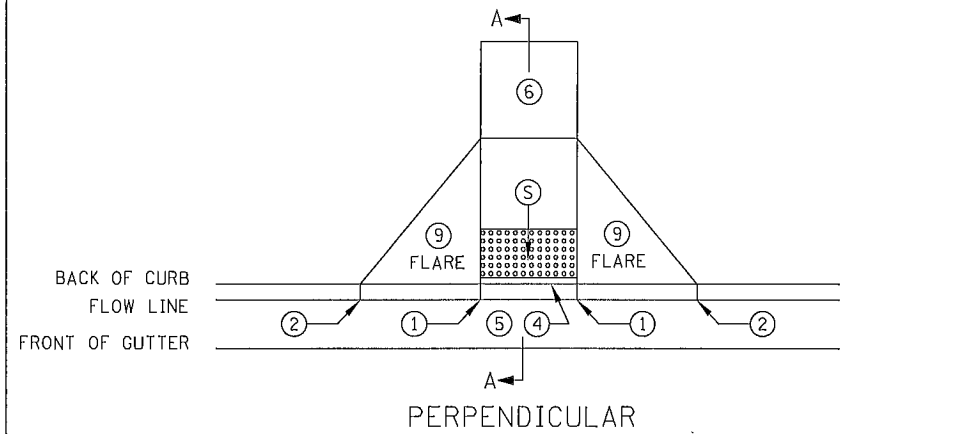
SP 002-611-034
SP 114-020-050
CP 13-10

MISCELLANEOUS DETAILS

Sheet 26 of 298 Sheets

PLOTTED/REVISED:
05/13/2016

DISTRICT #: USER NAME: dffrey PATH & FILENAME: P:\02-611-34\Plan\Standard_Plans\250_J_spr.dgn



- NOTES:
- LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.
 - INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE.
 - SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30" OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.
 - CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS.
 - ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL.
 - TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS MAY BE CAST SEPARATELY, FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 5 WHEN LANDINGS ARE CAST SEPARATELY.
 - ALL SLOPES ARE ABSOLUTE, RATHER THAN RELATIVE TO SIDEWALK/ROADWAY GRADES.
 - TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.
 - 4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MINIMUM OF 24" IN THE PATH OF TRAVEL. SHARED USE PATHS SHALL HAVE DETECTABLE WARNING ACROSS THE ENTIRE WIDTH OF PATH WHEN THE PATH CROSSES A ROAD.
 - SEE STANDARD PLATE 7038 AND SHEET 4 OF 5 FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.
 - ① 0" CURB HEIGHT.
 - ② FULL CURB HEIGHT.
 - ③ DETECTABLE WARNINGS MAY BE PART OF 4' X 4' LANDING AREA IF IT IS NOT FEASIBLE TO CONSTRUCT THE LANDING OUTSIDE OF THE DETECTABLE WARNING AREA.
 - ④ 1/2" PREFORMED JOINT FILLER MATERIAL AASHTO M 213. JOINT FILLER SHALL BE PLACED FLUSH WITH THE BACK OF CURB AND ADJACENT SIDEWALK. JOINT SHALL BE FREE OF DEBRIS. RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.
 - ⑤ SEE PEDESTRIAN ACCESS ROUTE CURB AND GUTTER DETAIL FOR INFORMATION ON CONSTRUCTING CURB AND GUTTER AT CURB OPENINGS. SEE SHEET NO. 3 OF 5.
 - ⑥ 4' BY 4' MIN. LANDING WITH MAX. 2.0% SLOPE IN ALL DIRECTIONS.
 - ⑦ IF LONGITUDINAL SLOPE IS GREATER THAN 5.0%, 4' X 4' MIN. LANDING WITH MAX 2.0% SLOPE IN ALL DIRECTIONS REQUIRED.
 - ⑧ V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. SEE SHEET 5 OF 5.
 - ⑨ SEE SHEET 4 OF 5, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS.
 - ⑩ DIAGONAL RAMPS SHOULD ONLY BE USED AFTER ALL OTHER CURB RAMP TYPES HAVE BEEN EVALUATED AND DEEMED IMPRACTICAL.

LEGEND	
THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.	
⑤	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%
⑦	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

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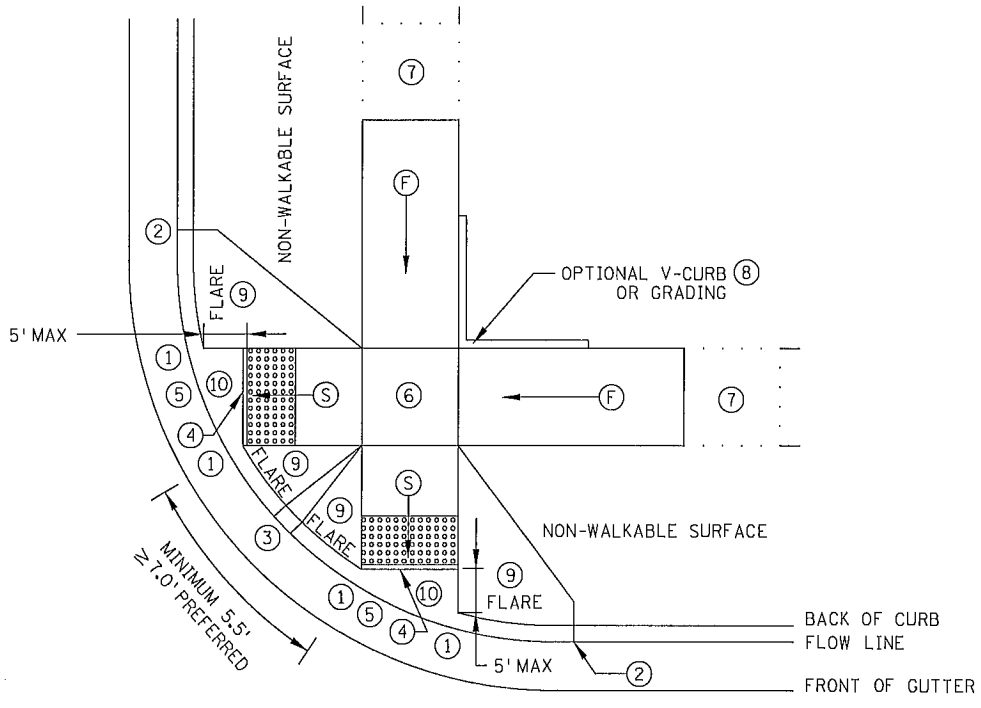
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DEPARTMENT OF TRANSPORTATION
REVISOR:
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8-6-2014
STATE DESIGN ENGINEER

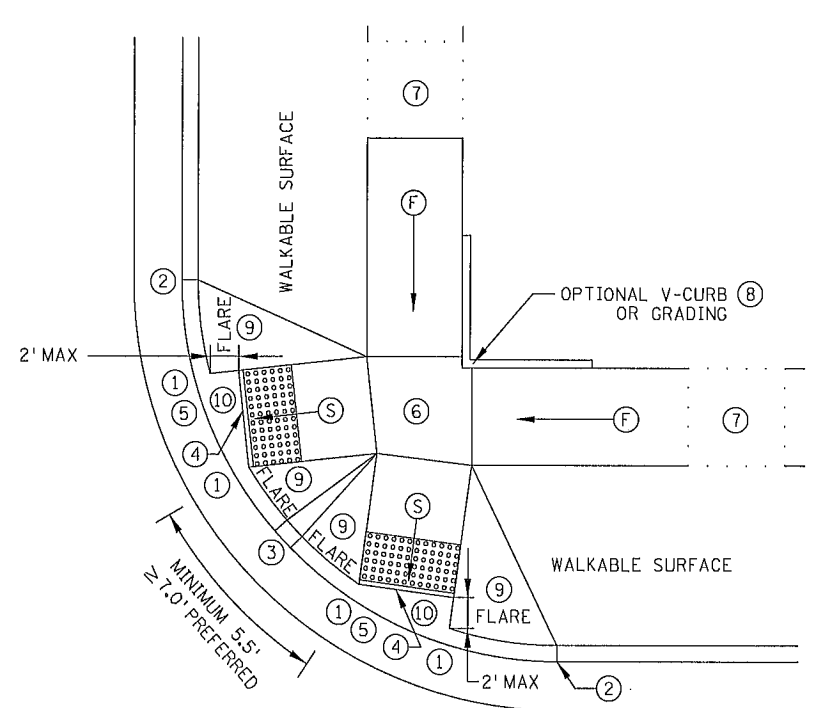
PEDESTRIAN CURB RAMP DETAILS
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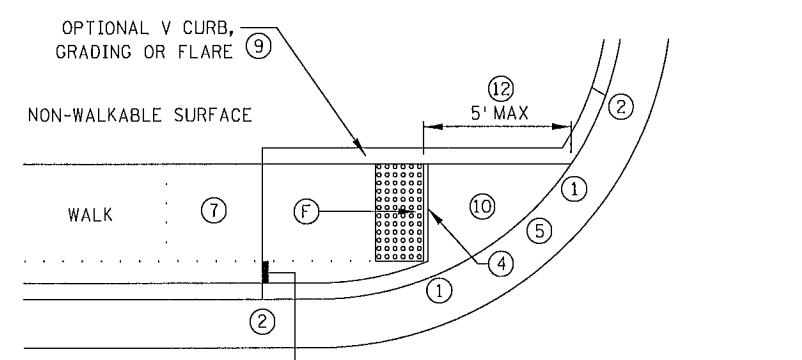


ADJACENT TO NON-WALKABLE SURFACE



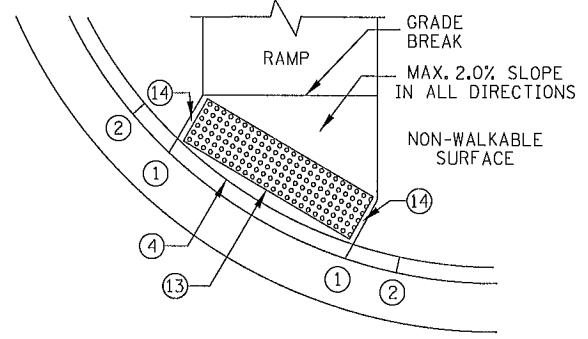
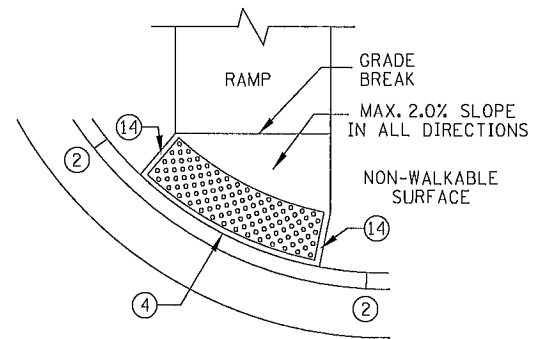
ADJACENT TO WALKABLE SURFACE

COMBINED DIRECTIONAL 15

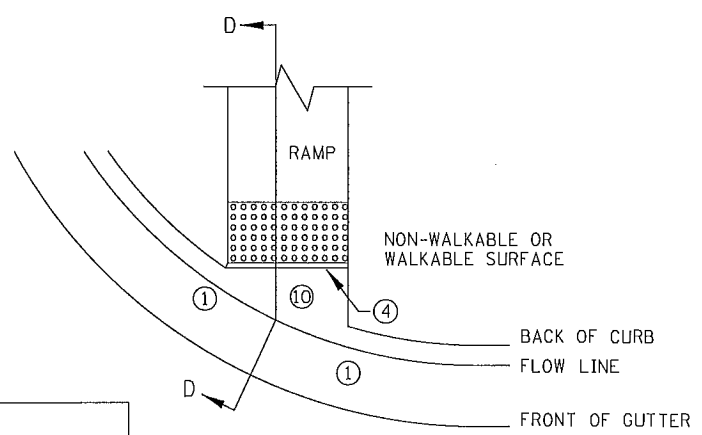


ONE-WAY DIRECTIONAL

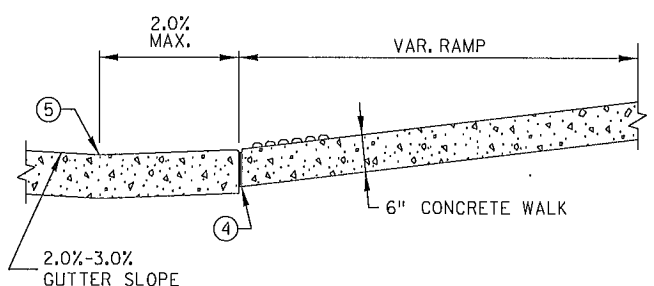
IF NON-CONCRETE BLVD. IS CONSTRUCTED AND IS LESS THAN 2' IN WIDTH AT TOP OF CURB TRANSITION, PAVE CONCRETE RAMP WIDTH TO ADJACENT BACK OF CURB.



DETECTABLE WARNING PLACEMENT WHEN SETBACK CRITERIA IS EXCEEDED



SECTION D-D



NOTES:

- LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.
- INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE.
- SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30" OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.
- CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS.
- ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL.
- TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS MAY BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 5 WHEN LANDINGS ARE CAST SEPARATELY.
- ALL SLOPES ARE ABSOLUTE, RATHER THAN RELATIVE TO SIDEWALK/ROADWAY GRADES.
- TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.
- 4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MINIMUM OF 24" IN THE PATH OF TRAVEL. SHARED USE PATHS SHALL HAVE DETECTABLE WARNING ACROSS THE ENTIRE WIDTH OF PATH WHEN THE PATH CROSSES A ROAD.
- SEE STANDARD PLATE 7D38 AND SHEET 4 OF 5 FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.
- 1 0" CURB HEIGHT.
- 2 FULL CURB HEIGHT.
- 3 3" MINIMUM CURB HEIGHT, 4" PREFERRED.
- 4 1/2" PREFORMED JOINT FILLER MATERIAL AASHTO M 213. JOINT FILLER SHALL BE PLACED FLUSH WITH THE BACK OF CURB AND ADJACENT SIDEWALK. JOINT SHALL BE FREE OF DEBRIS. RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MIN. TO 6" MAX. FROM THE BACK OF CURB.
- 5 SEE PEDESTRIAN ACCESS ROUTE CURB AND GUTTER DETAIL FOR INFORMATION ON CONSTRUCTING CURB AND GUTTER AT CURB OPENINGS. SEE SHEET NO. 3 OF 5.
- 6 4' BY 4' MIN. LANDING WITH MAX. 2.0% SLOPE IN ALL DIRECTIONS.
- 7 IF LONGITUDINAL SLOPE IS GREATER THAN 5.0%, 4' X 4' MIN. LANDING WITH MAX 2.0% SLOPE IN ALL DIRECTIONS REQUIRED.
- 8 V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.
- 9 SEE SHEET 4 OF 5, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS.
- 10 MAX. 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK AND DRAIN TO FLOW LINE. SHALL BE CONSTRUCTED INTEGRAL WITH CURB AND GUTTER.
- 11 TO BE USED FOR ALL DIRECTIONAL RAMPS.
- 12 PLACE DOMES AT THE BACK OF CURB WHEN ALLOWABLE SETBACK CRITERIA IS EXCEEDED.
- 13 RECTANGULAR DETECTABLE WARNINGS MAY BE SETBACK 9" FROM THE BACK OF CURB WITH CORNERS SET 3" FROM BACK OF CURB. IF 9" SETBACK IS EXCEEDED USE RADIAL DETECTABLE WARNINGS.
- 14 WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE BACK OF CURB. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- 15 FRONT EDGE OF DETECTABLE WARNING SHALL BE SET BACK 2' MAXIMUM WHEN ADJACENT TO WALKABLE SURFACE, AND 5' MAXIMUM WHEN ADJACENT TO NON-WALKABLE SURFACE WITH ONE CORNER SET 3" FROM BACK OF CURB. WHETHER A SURFACE IS WALKABLE OR NOT SHALL BE DETERMINED BY THE ENGINEER.

LEGEND

THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.

S INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%

F INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%

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APPROVED: 8-6-2014
Michael R. ...
OPERATIONS ENGINEER

CURB FOR DIRECTIONAL RAMPS 11

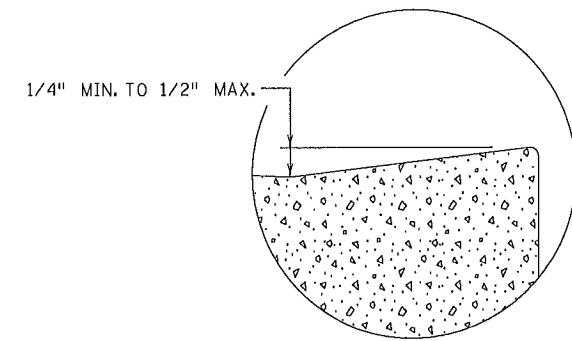
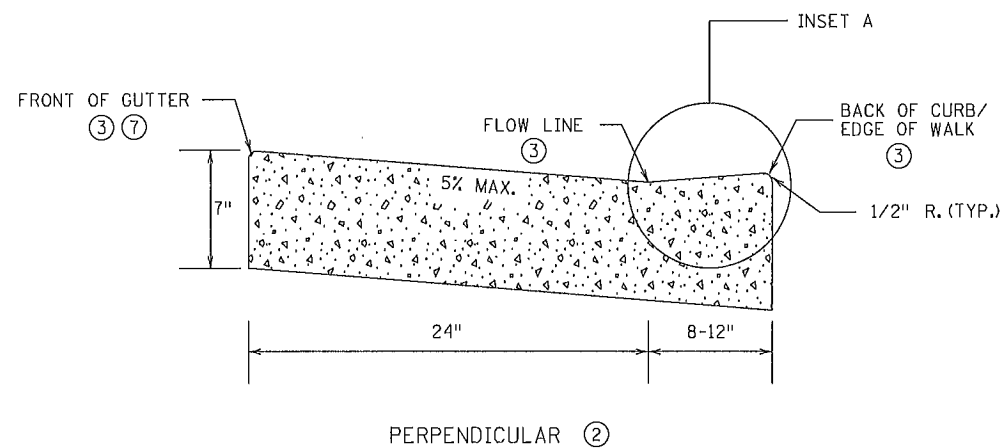
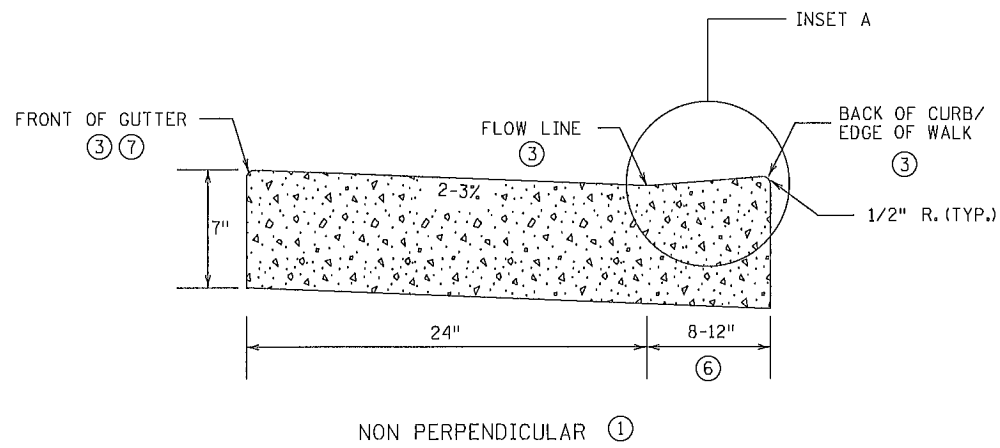
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SP 114-020-050
REVISED:
APPROVED: 8-6-2014
Christy ...
STATE DESIGN ENGINEER

2 OF 5
PEDESTRIAN CURB RAMP DETAILS
STANDARD PLAN 5-297.250 28 OF 298

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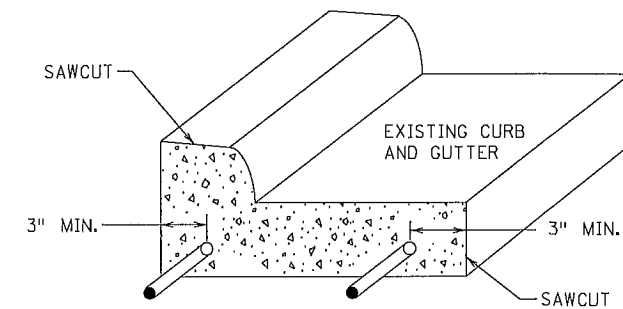
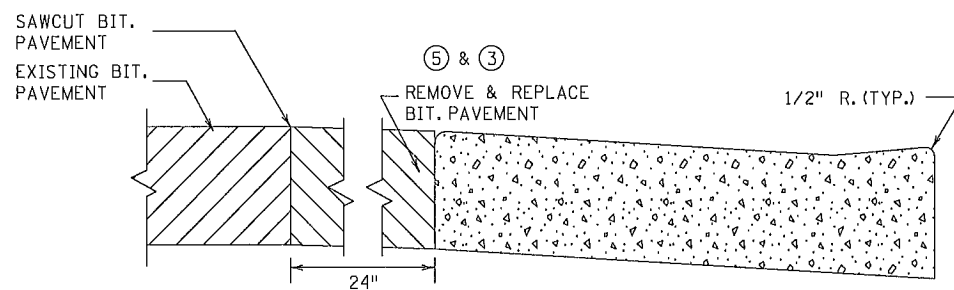
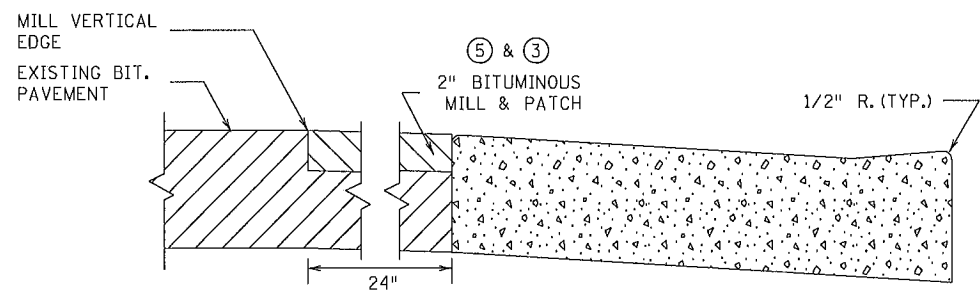
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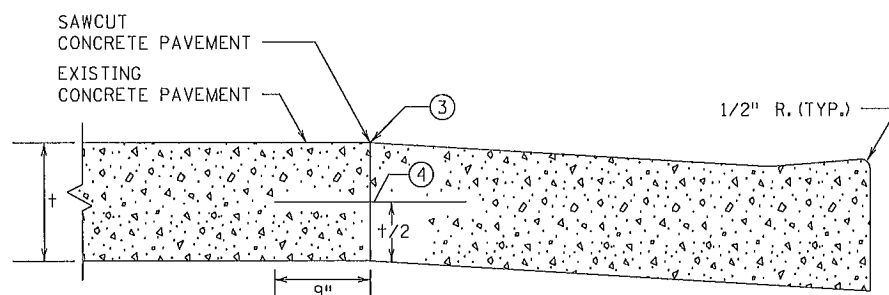
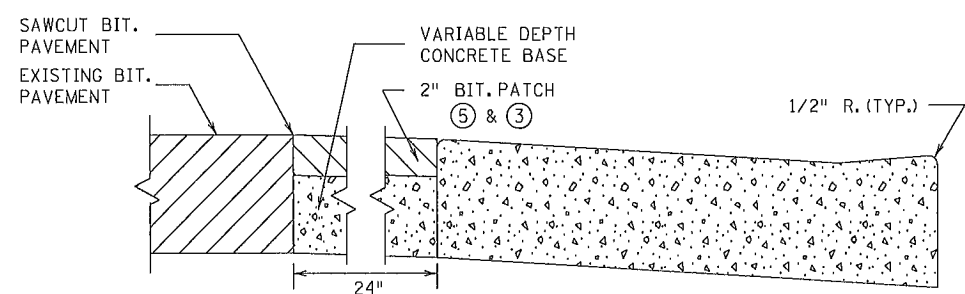


INSET A

PEDESTRIAN ACCESS ROUTE
CURB & GUTTER DETAIL



CURB AND GUTTER REINFORCEMENT (8)
FOR USE ON CURB RAMP RETROFITS



PAVEMENT TREATMENT OPTIONS
IN FRONT OF CURB & GUTTER
FOR USE ON CURB RAMP RETROFITS

NOTES:

POSITIVE FLOW LINE DRAINAGE SHALL BE MAINTAINED THROUGH THE PEDESTRIAN ACCESS ROUTE (PAR) AT A 2% MAXIMUM.

NO PONDING SHALL BE PRESENT IN THE PAR.

ANY VERTICAL LIP THAT OCCURS AT THE FLOW LINE SHALL NOT BE GREATER THAN 1/4 INCH.

(1) FOR USE AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED NON PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: FANS, DEPRESSED CORNERS, & ONE WAY AND COMBINED DIRECTIONALS.

(2) FOR USE AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: PERPENDICULAR, TIERED PERPENDICULAR, PARALLEL, AND DIAGONAL RAMPS.

(3) THERE SHALL BE NO VERTICAL DISCONTINUITIES GREATER THAN 1/4\".

(4) DRILL AND GROUT NO. 4 EPOXY-COATED 18\"/>

(5) ELEVATION CHANGE TAKES PLACE FROM THE EXISTING TO NEW FRONT OF GUTTER. PATCH IS USED TO MATCH THE NEW GUTTER FACE INTO THE EXISTING ROADWAY.

(6) VARIABLE WIDTH FOR DIRECTIONAL CURB APPLICATIONS.

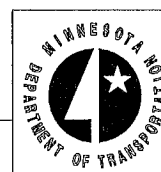
(7) TOP FRONT OF GUTTER SHALL BE CONSTRUCTED FLUSH WITH PROPOSED ADJACENT PAVEMENT ELEVATION. PAR GUTTER SHALL NOT BE OVERLAID.

(8) WHERE PLAN SPECIFIES, DRILL AND GROUT 2 - NO. 4 X 12\"/>

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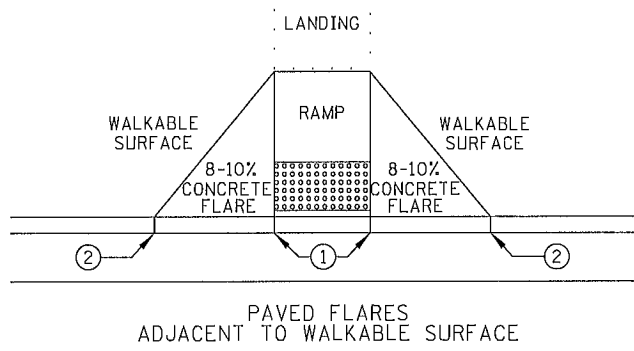
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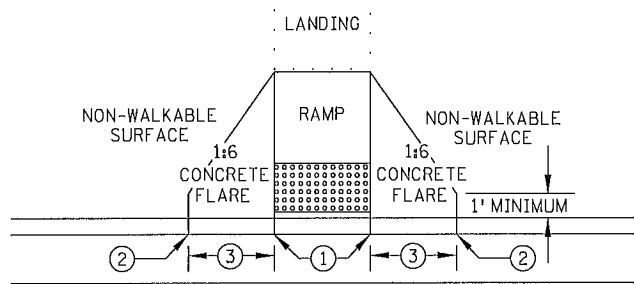
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8-6-2014

PEDESTRIAN CURB RAMP DETAILS

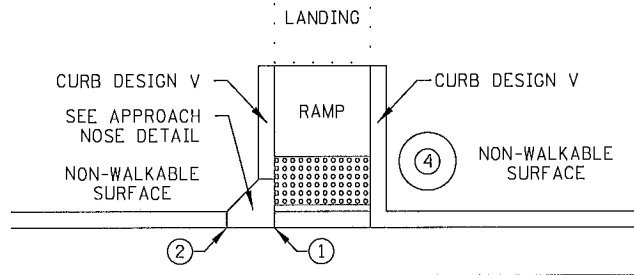
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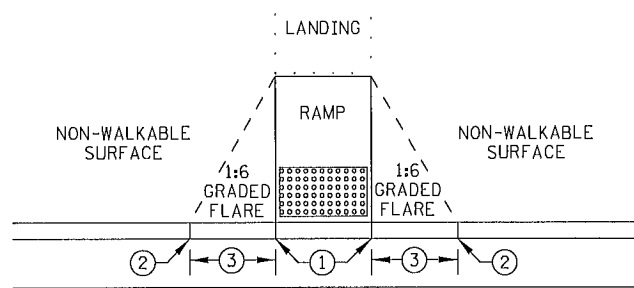
PAVED FLARES
ADJACENT TO WALKABLE SURFACE



PAVED FLARES
ADJACENT TO NON-WALKABLE SURFACE

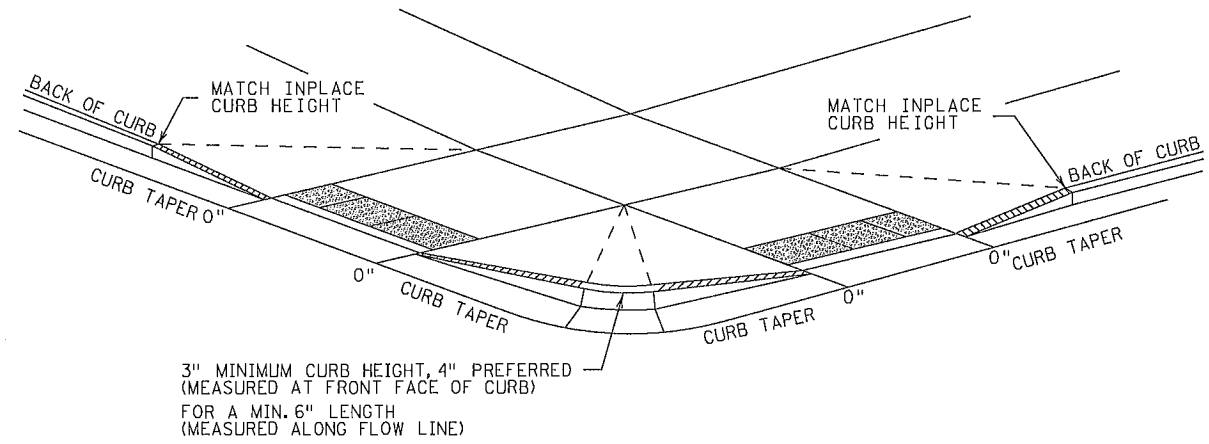


RETURNED CURB



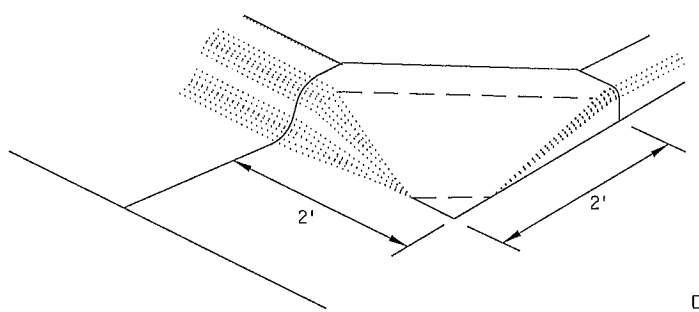
GRADED FLARES

TYPICAL SIDE TREATMENT OPTIONS ⑤

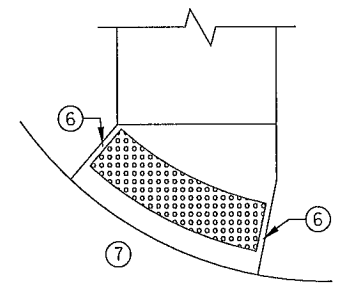
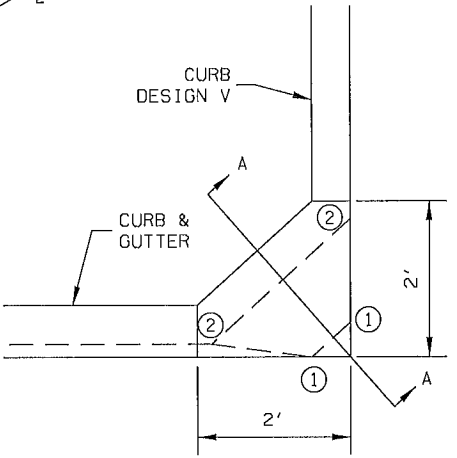
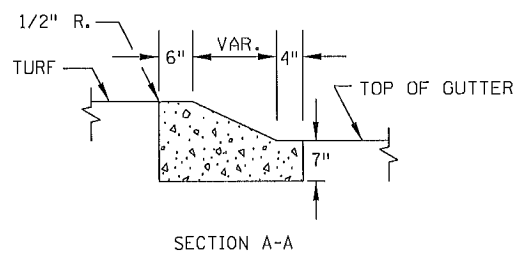


3" MINIMUM CURB HEIGHT, 4" PREFERRED
(MEASURED AT FRONT FACE OF CURB)
FOR A MIN. 6" LENGTH
(MEASURED ALONG FLOW LINE)

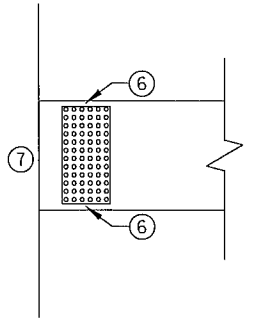
DETECTABLE EDGE WITH
CURB AND GUTTER ⑧



APPROACH NOSE DETAIL
FOR DOWNSTREAM SIDE OF TRAFFIC



RADIAL DETECTABLE WARNING



RECTANGULAR DETECTABLE WARNING

DETECTABLE EDGE WITHOUT CURB AND GUTTER

NOTES:
SEE STANDARD PLATE 703B AND THIS SHEET FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.
WHETHER A SURFACE IS WALKABLE OR NOT SHALL BE DETERMINED BY THE ENGINEER.
CONCRETE FLARE LENGTHS ADJACENT TO NON-WALKABLE SURFACES SHOULD BE LESS THAN 8' LONG
MEASURED ALONG THE RAMPS FROM THE BACK OF CURB.

- ① 0" CURB HEIGHT.
- ② FULL CURB HEIGHT.
- ③ 2' - 3' FLARE.
- ④ IMMOVABLE OBJECT OR OBSTRUCTION.
- ⑤ SIDE TREATMENTS ARE APPLICABLE TO ALL RAMP TYPES AND SHOULD BE IMPLEMENTED AS NEEDED ON ALL RAMPS AS FIELD CONDITIONS DICTATE. THE ENGINEER SHALL DETERMINE THE RAMP SIDE TREATMENTS BASED ON MAINTENANCE OF BOTH ROADWAY AND SIDEWALK, ADJACENT PROPERTY CONSIDERATIONS, AND MITIGATING CONSTRUCTION IMPACTS.
- ⑥ WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE EDGE OF ROADWAY. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- ⑦ IF NO CURB AND GUTTER IS PLACED IN RURAL SECTIONS, DETECTABLE WARNINGS SHALL BE PLACED 1' FROM THE EDGE OF ROADWAY TO PROVIDE VISUAL CONTRAST.
- ⑧ ALL CONSTRUCTED CURBS MUST HAVE A CONTINUOUS DETECTABLE EDGE FOR THE VISUALLY IMPAIRED. THIS DETECTABLE EDGE REQUIRES DETECTABLE WARNINGS WHEREVER THERE IS ZERO-INCH HIGH CURB. CURB TAPERS ARE CONSIDERED A DETECTABLE EDGE WHEN THE TAPER STARTS WITHIN 3" OF THE EDGE OF THE DETECTABLE WARNINGS AND UNIFORMLY RISES TO A 3-INCH MINIMUM CURB HEIGHT. ANY CURB NOT PART OF A CURB TAPER AND LESS THAN 3 INCHES IN HEIGHT IS NOT CONSIDERED A DETECTABLE EDGE AND THEREFORE IS NOT COMPLIANT WITH ACCESSIBILITY STANDARDS.

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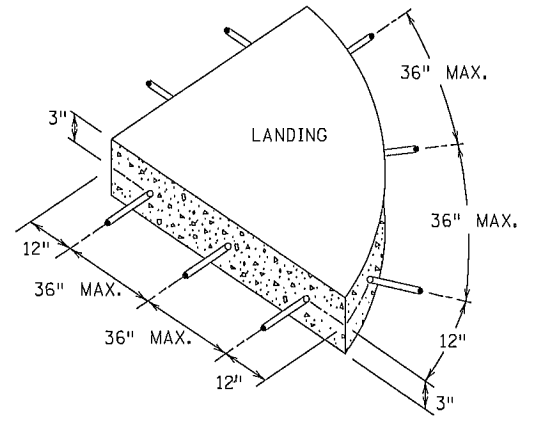
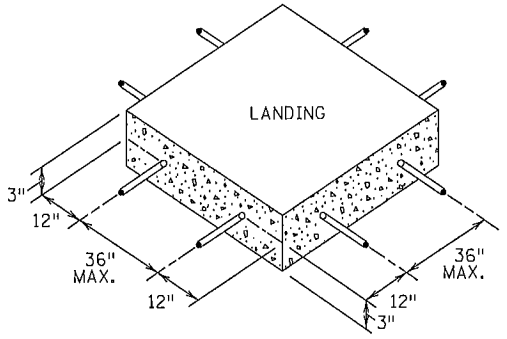
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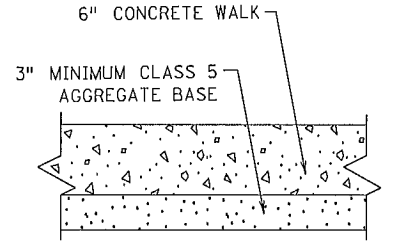
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PEDESTRIAN CURB RAMP DETAILS	
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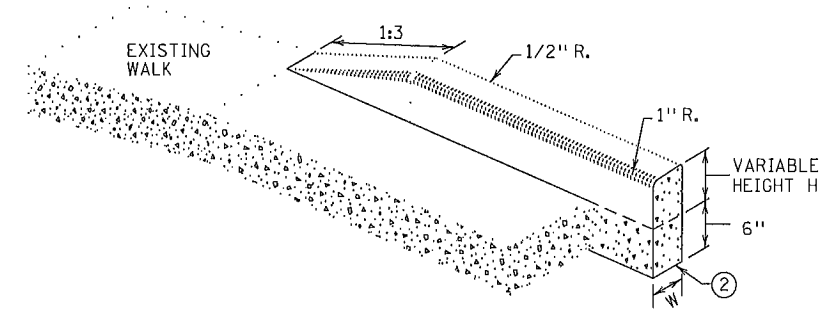
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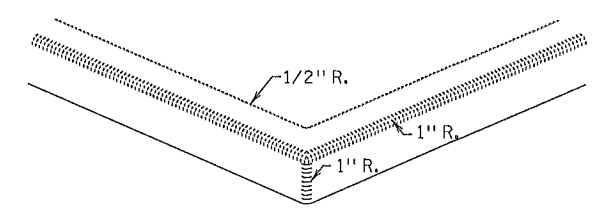
SIDEWALK REINFORCEMENT ⑥ ⑦



TYPICAL SIDEWALK SECTION WITHIN INTERSECTION CORNER

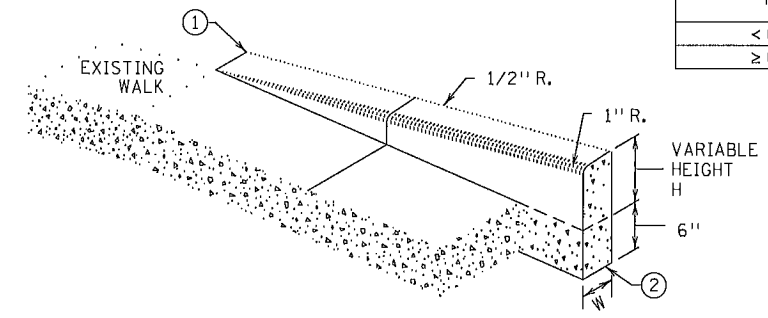


V CURB ADJACENT TO LANDSCAPE
CURB WITHIN SIDEWALK LIMITS

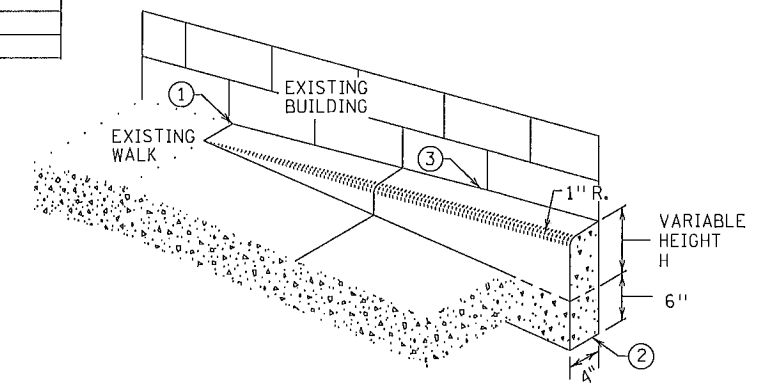


V CURB INTERSECTION

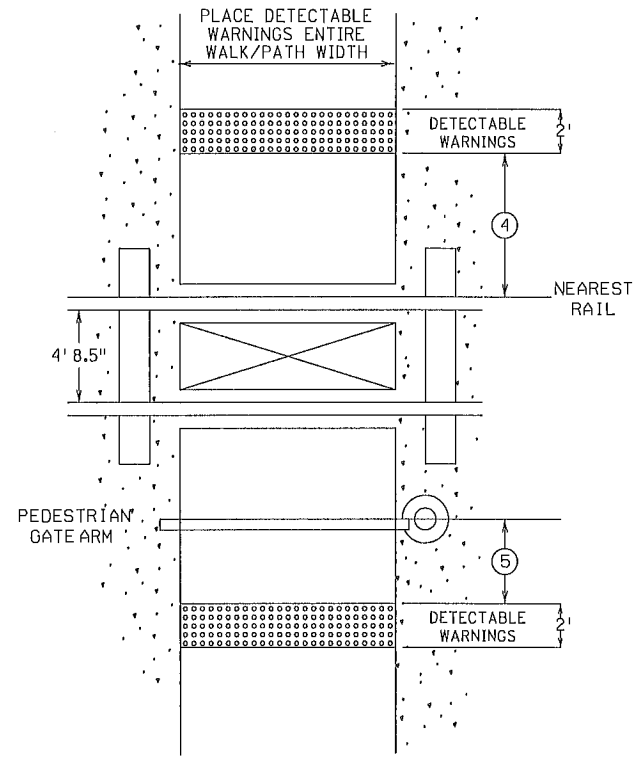
CONCRETE CURB DESIGN V	
CURB HEIGHT H	CURB WIDTH W
< 6"	4"
≥ 6"	6"



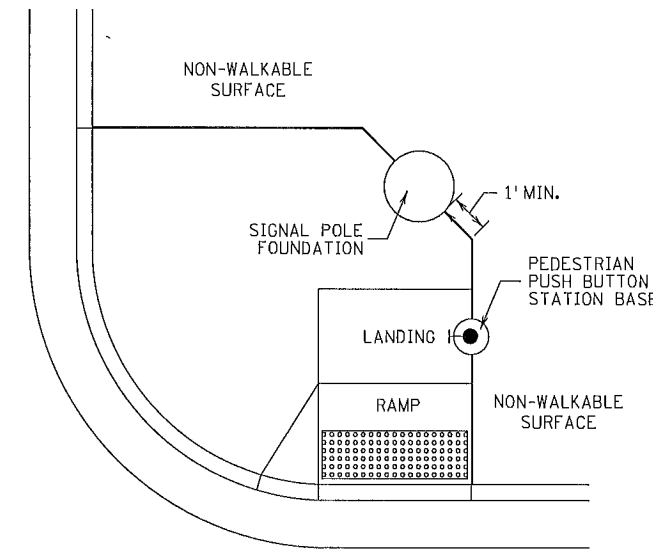
V CURB ADJACENT TO LANDSCAPE
CURB OUTSIDE SIDEWALK LIMITS



V CURB ADJACENT TO BUILDING
OR BARRIER



RAILROAD CROSSING
PLAN VIEW



CONCRETE WALK EDGES ADJACENT
TO CONCRETE STRUCTURES

NOTES:

- ALL V CURB CONTRACTION JOINTS SHALL MATCH CONCRETE WALK JOINTS.
- WHERE RIGHT-OF-WAY ALLOWS, USE OF V CURB SHOULD BE MINIMIZED. GRADING ADJACENT TURF OR SLOPING ADJACENT PAVEMENT IS PREFERRED.
- V CURB SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.
- V CURB NEXT TO BUILDING SHALL BE A 4" WIDTH AND SHALL MATCH PREVIOUS TOP OF SIDEWALK ELEVATIONS.
- ① END TAPERS AT TRANSITION SECTION SHALL MATCH INPLACE SIDEWALK GRADES.
- ② ALL V CURB SHALL MATCH BOTTOM OF ADJACENT WALK.
- ③ EDGE BETWEEN NEW V CURB AND INPLACE STRUCTURE SHALL BE SEALED AND BOND BREAKER SHALL BE USED BETWEEN EXISTING STRUCTURE AND PLACED V-CURB.
- ④ NEAREST EDGE OF DETECTABLE WARNING SURFACES SHALL BE PLACED 12' MINIMUM TO 15' MAXIMUM FROM THE NEAREST RAIL. FOR SKEWED RAILWAYS IN NO INSTANCE SHALL THE DETECTABLE WARNING BE CLOSER THAN 12' MEASURED PERPENDICULAR TO THE NEAREST RAIL.
- ⑤ WHEN PEDESTRIAN GATES ARE PROVIDED, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE SIDE OF THE GATES OPPOSITE THE RAIL, 2' FROM THE APPROACHING SIDE OF THE GATE ARM.
- ⑥ WHEN PLAN SPECIFIES, DRILL AND GROUT NO. 4 12" LONG REINFORCEMENT BARS AT 36" MAX. CENTER TO CENTER (EPOXY COATED).
- ⑦ TO ENSURE RAMP AND LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS MAY BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON THIS SHEET WHEN LANDINGS ARE CAST SEPARATELY.

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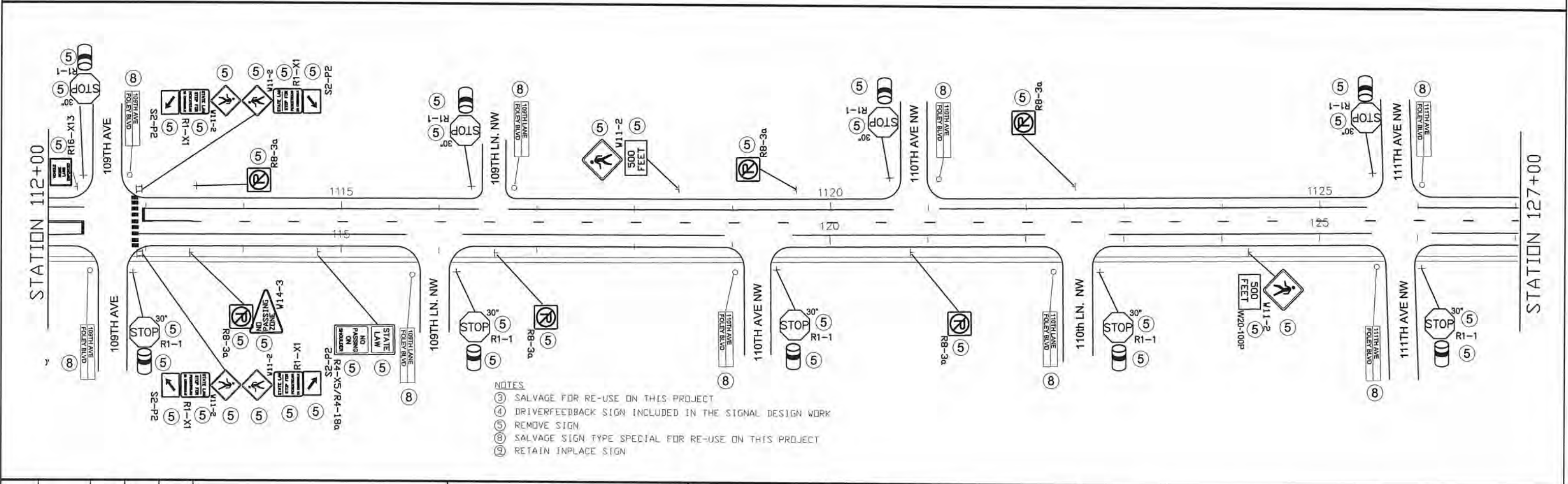
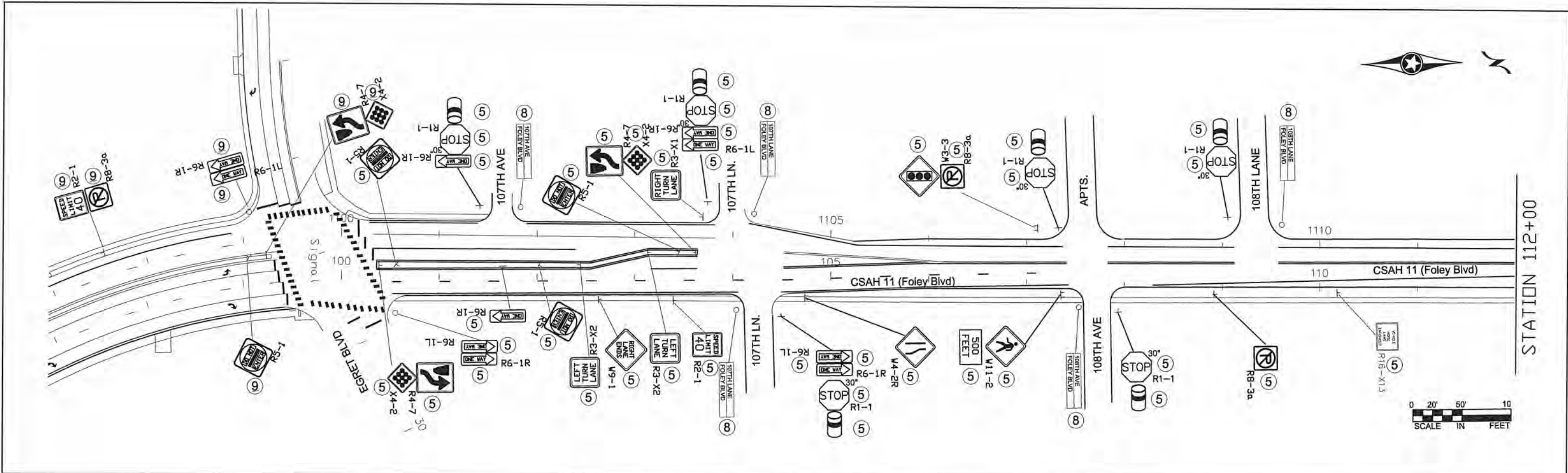
REVISION:
APPROVED: 2-9-2015
<i>[Signature]</i> OPERATIONS ENGINEER

SP 002-611-034 CP 13-10
SP 114-020-050

MINNESOTA DEPARTMENT OF TRANSPORTATION

REVISOR: *[Signature]* APPROVED: 2-9-2015
STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS	
STANDARD PLAN 5-297.250	31 OF 298



- NOTES
- ③ SALVAGE FOR RE-USE ON THIS PROJECT
 - ④ DRIVERFEEDBACK SIGN INCLUDED IN THE SIGNAL DESIGN WORK
 - ⑤ REMOVE SIGN
 - ⑥ SALVAGE SIGN TYPE SPECIAL FOR RE-USE ON THIS PROJECT
 - ⑦ RETAIN INPLACE SIGN

NO	DATE	BY	CKD	APPR	REVISION
NAME: T:\Traffic\dwg\CSAH 11 (Foley_Northdale)\From CSAH 78 to CSAH 12.dwg					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: DOUGLAS W. FISCHER

SIGNATURE: *[Signature]*

DATE: 5/27/16 LICENSE NO. 20235

DRAWN BY: TV DATE: 03/16/16

DESIGN BY: TV DATE: 03/16/16

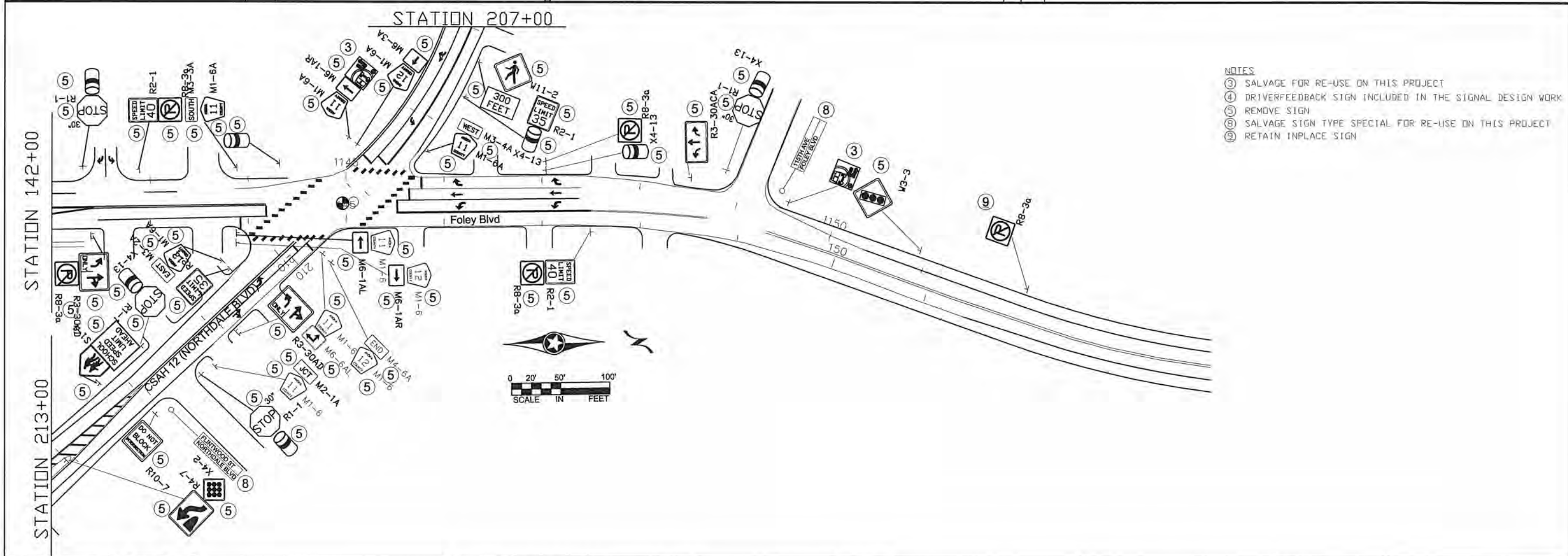
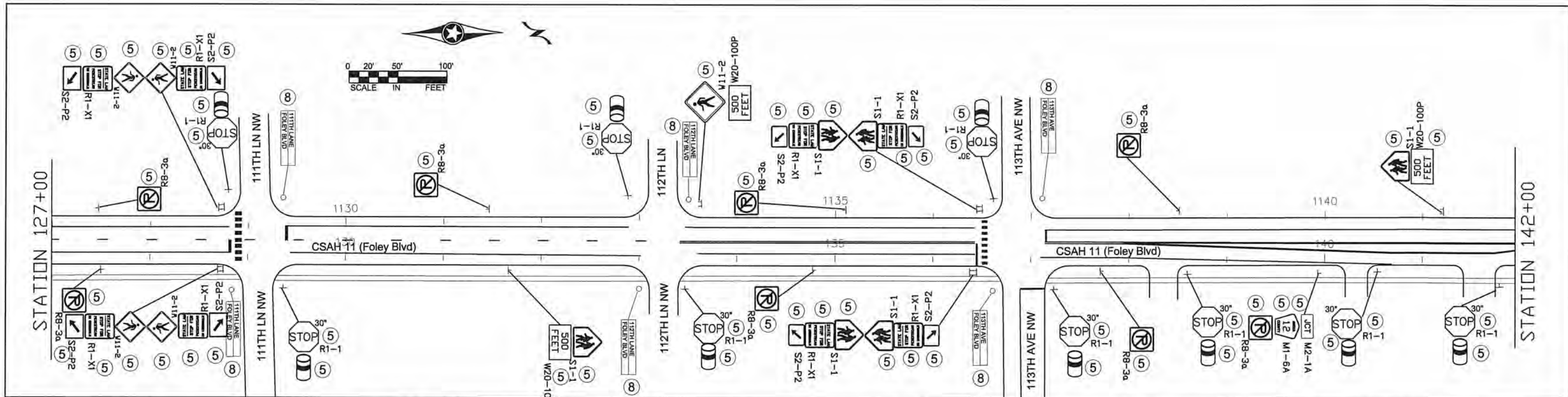
CHECKED BY: RB DATE: 03/16/16

ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

EXISTING SIGNING AND STRIPING

Sheet 32 of 298 Sheets



- NOTES
- ③ SALVAGE FOR RE-USE ON THIS PROJECT
 - ④ DRIVERFEEDBACK SIGN INCLUDED IN THE SIGNAL DESIGN WORK
 - ⑤ REMOVE SIGN
 - ⑥ SALVAGE SIGN TYPE SPECIAL FOR RE-USE ON THIS PROJECT
 - ⑦ RETAIN INPLACE SIGN

NO	DATE	BY	CKD	APPR	REVISION

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SIGNATURE: *[Signature]*


DATE: 5/27/16 LICENSE NO. 20235

DRAWN BY: TV DATE: 03/16/16

DESIGN BY: TV DATE: 03/16/16

CHECKED BY: RB DATE: 03/16/16

ANOKA COUNTY
HIGHWAY DEPT.



STATE PROJECT NO. 002-611-034

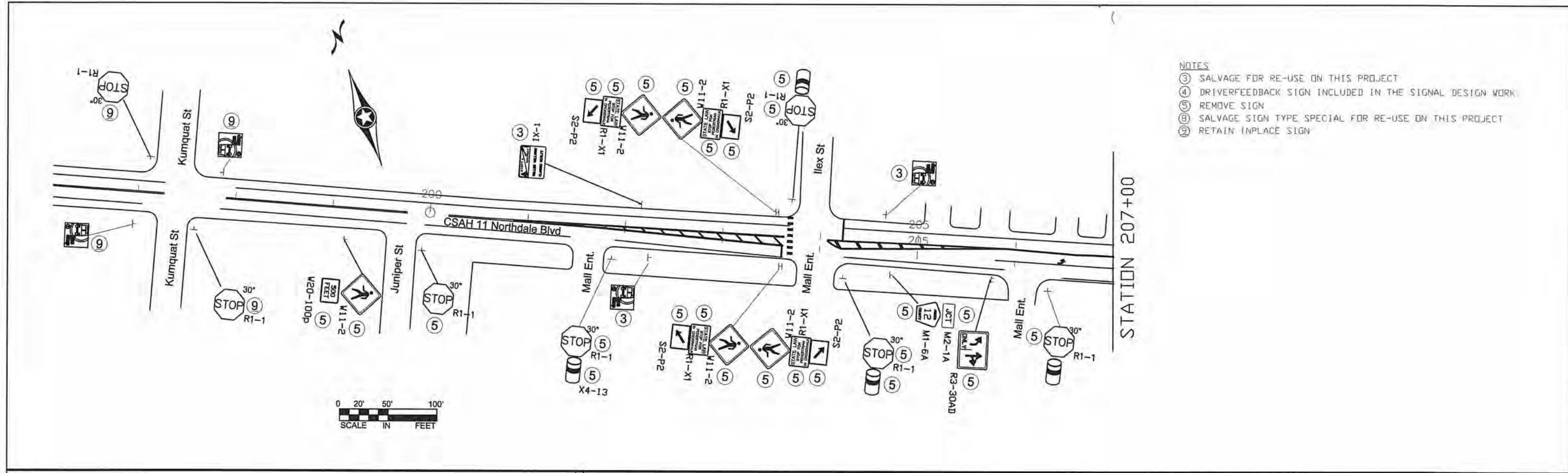
STATE PROJECT NO. 114-020-050

CITY PROJECT NO. 13-10

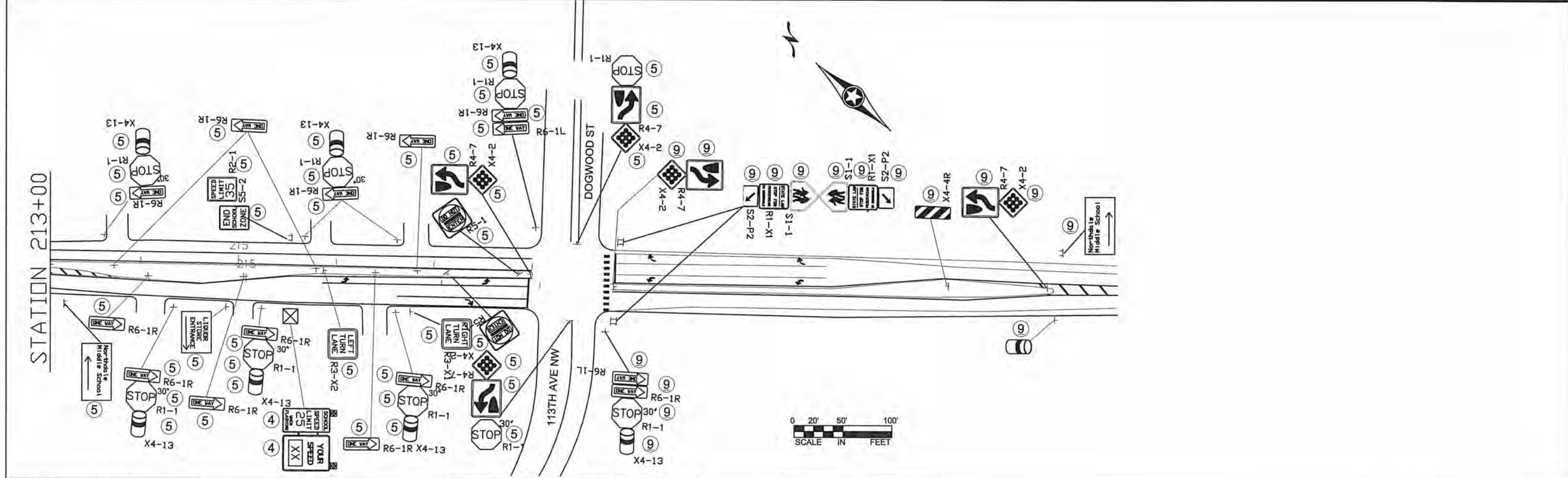
COUNTY PROJECT NO. _____

EXISTING SIGNING
AND STRIPING

Sheet 33 of 298 Sheets



- NOTES
- ③ SALVAGE FOR RE-USE ON THIS PROJECT
 - ④ DRIVERFEEDBACK SIGN INCLUDED IN THE SIGNAL DESIGN WORK
 - ⑤ REMOVE SIGN
 - ⑥ SALVAGE SIGN TYPE SPECIAL FOR RE-USE ON THIS PROJECT
 - ⑦ RETAIN INPLACE SIGN



NO	DATE	BY	CKD	APPR	REVISION
NAME: T:\Traffic\dwg\CSAH 11 (Foley_Northdale)\From CSAH 78 to CSAH 12.dwg					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: DOUGLAS W. EISCHER
 SIGNATURE: *[Signature]*
 DATE: 5/27/16 LICENSE NO. 20235

DRAWN BY: TV DATE: 03/16/16
 DESIGN BY: TV DATE: 03/16/16
 CHECKED BY: RB DATE: 03/16/16

ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

SIGN REMOVAL TAB									K
ALIGNMENT	STATION	ADDRESS/ DESCRIPTION (NOTES)	REMOVE	SALVAGE	INSTALL	SIGN NUMBER	SIGN LEGEND	NOTES	
			SIGN TYPE C	SIGN TYPE SPECIAL	SIGN TYPE SPECIAL [1]				
			EACH	EACH	EACH				
CSAH 11	100+40	Median				R4-7	Keep Right		
CSAH 11	100+40	Median	1			X4-2	Nine Button		
CSAH 11	100+50	Rt	0			R6-1R	One Way	2	
CSAH 11	100+50	Rt				R6-1L	One Way	2	
CSAH 11	100+60	Median	1			R5-1	Do Not Enter		
CSAH 11	1101+45	Lt				R6-1R	One Way		
CSAH 11	1101+45	Lt	1			R1-1	30" Stop		
CSAH 11	1101+45	Lt				X4-13	Delineator		
CSAH 11	101+70	Median	1			R6-1R	One Way		
CSAH 11	1101+75	Lt		1	1		Street Sign	1	
CSAH 11	102+00	Median	1			R5-1	Do Not Enter		
CSAH 11	102+40	Median	1			R3-X2	Left Turn Lane		
CSAH 11	102+60	Rt	1			W9-1	Right Lane Ends		
CSAH 11	103+15	Median	1			R3-X2	Left Turn Lane		
CSAH 11	103+40	Rt	1			R2-1	Speed Limit 40		
CSAH 11	103+50	Median	1			R5-1	Do Not Enter		
CSAH 11	103+65	Median	1			R4-7	Keep Right		
CSAH 11	103+65	Median	1			X4-2	Nine Button		
CSAH 11	1103+70	Lt	1			R3-X1	Right Turn Lane		
CSAH 11	1103+80	Lt				R6-1R	One Way		
CSAH 11	1103+80	Lt				R6-1L	One Way		
CSAH 11	1103+80	Lt	1			R1-1	30" Stop		
CSAH 11	1103+80	Lt				X4-13	Delineator		
CSAH 11	104+00	Rt		1	1		Street Sign	1	
CSAH 11	1104+20	Lt		1	1		Street Sign	1	
CSAH 11	104+50	Rt				R6-1R	One Way		
CSAH 11	104+50	Rt	1			R6-1L	One Way		
CSAH 11	104+50	Rt				R1-1	30" Stop		
CSAH 11	104+50	Rt				X4-13	Delineator		
CSAH 11	104+70	Rt	1			W4-2R	Ln Ends		
CSAH 11	1107+10	Lt	1			W3-3	Signal Ahead		
CSAH 11	1107+10	Lt				R8-3a	No Parking		
CSAH 11	107+35	Rt	1			W11-2	Ped Crossing		
CSAH 11	107+35	Rt				W20-100P	500 Feet		
CSAH 11	1107+35	Lt	1			R1-1	30" Stop		
CSAH 11	1107+35	Lt				X4-13	Delineator		
CSAH 11	107+50	Rt		1	1		Street Sign	1	
CSAH 11	107+95	Rt	1			R1-1	30" Stop		
CSAH 11	107+95	Rt				X4-13	Delineator		
CSAH 11	108+90	Rt	1			R8-3a	No Parking		
CSAH 11	1109+10	Lt	1			R1-1	30" Stop		
CSAH 11	1109+10	Lt				X4-13	Delineator		
CSAH 11	1109+60	Lt		1	1		Street Sign	1	
CSAH 11	110+20	Rt	1			R16-X13	Vehicle Noise		
CSAH 11	112+30	Lt	1			R16-X13	Vehicle Noise		
CSAH 11	1112+40	Lt	1			R1-1	30" Stop		
CSAH 11	1112+40	Lt				X4-13	Delineator		
CSAH 11	112+40	Rt		1	1		Street Sign	1	
CSAH 11	1112+80	Lt		1	1		Street Sign	1	
CSAH 11	112+85	Rt	1			R1-1	30" Stop		
CSAH 11	112+85	Rt				X4-13	Delineator		
CSAH 11	1112+95	Lt				W11-2	Ped Crossing		
CSAH 11	1112+95	Lt				R1-X1	State Law		
CSAH 11	1112+95	Lt	1			S2-P2	Down Arrow		
CSAH 11	1112+95	Lt				W11-2	Ped Crossing		
CSAH 11	1112+95	Lt				R1-X1	State Law		
CSAH 11	1112+95	Lt				S2-P2	Down Arrow		
CSAH 11	112+95	Rt				W11-2	Ped Crossing		
CSAH 11	112+95	Rt				R1-X1	State Law		
CSAH 11	112+95	Rt	1			S2-P2	Down Arrow		
CSAH 11	112+95	Rt				W11-2	Ped Crossing		
CSAH 11	112+95	Rt				R1-X1	State Law		
CSAH 11	112+95	Rt				S2-P2	Down Arrow		
CSAH 11	113+45	Rt	1			W14-3	No Passing Zone		
CSAH 11	113+45	Rt				R8-3a	No Parking		
CSAH 11	1113+50	Lt	1			R8-3a	No Parking		
CSAH 11	114+75	Rt				R4-X5	State Law		
CSAH 11	114+75	Rt	1			R4-18a	No Pass Shldr		
CSAH 11	115+70	Rt		1	1		Street Sign	1	
CSAH 11	116+20	Rt				R1-1	30" Stop		
CSAH 11	116+20	Rt	1			X4-13	Delineator		


SIGN REMOVAL TAB									K
ALIGNMENT	STATION	ADDRESS/ DESCRIPTION (NOTES)	REMOVE	SALVAGE	INSTALL	SIGN NUMBER	SIGN LEGEND	NOTES	
			SIGN TYPE C	SIGN TYPE SPECIAL	SIGN TYPE SPECIAL [1]				
			EACH	EACH	EACH				
CSAH 11	1116+35	Lt				R1-1	30" Stop		
CSAH 11	1116+35	Lt	1			X4-13	Delineator		
CSAH 11	116+60	Rt	1			R8-3a	No Parking		
CSAH 11	1116+75	Lt		1	1		Street Sign	1	
CSAH 11	1118+50	Lt				W11-2	Ped Crossing		
CSAH 11	1118+50	Lt	1			W20-100P	500 Feet		
CSAH 11	119+00	Rt		1	1		Street Sign	1	
CSAH 11	119+45	Rt				R1-1	30" Stop		
CSAH 11	119+45	Rt	1			X4-13	Delineator		
CSAH 11	1119+70	Lt	1			R8-3a	No Parking		
CSAH 11	1120+60	Lt				R1-1	30" Stop		
CSAH 11	1120+60	Lt	1			X4-13	Delineator		
CSAH 11	120+80	Rt	1			R8-3a	No Parking		
CSAH 11	1121+10	Lt		1	1		Street Sign	1	
CSAH 11	122+30	Rt		1	1		Street Sign	1	
CSAH 11	1122+50	Lt	1			R8-3a	No Parking		
CSAH 11	122+75	Rt				R1-1	30" Stop		
CSAH 11	122+75	Rt	1			X4-13	Delineator		
CSAH 11	124+25	Rt				W11-2	Ped Crossing		
CSAH 11	124+25	Rt	1			W20-100P	500 Feet		
CSAH 11	125+60	Rt		1	1		Street Sign	1	
CSAH 11	1125+60	Lt				R1-1	30" Stop		
CSAH 11	1125+60	Lt	1			X4-13	Delineator		
CSAH 11	126+00	Rt	1			R1-1	30" Stop		
CSAH 11	126+00	Rt				X4-13	Delineator		
CSAH 11	1126+05	Lt		1	1		Street Sign	1	
CSAH 11	127+50	Rt	1			R8-3a	No Parking		
CSAH 11	1127+50	Lt	1			R8-3a	No Parking		
CSAH 11	128+70	Rt				W11-2	Ped Crossing		
CSAH 11	128+70	Rt				R1-X1	State Law		
CSAH 11	128+70	Rt				S2-P2	Down Arrow		
CSAH 11	128+70	Rt	1			W11-2	Ped Crossing		
CSAH 11	128+70	Rt				R1-X1	State Law		
CSAH 11	128+70	Rt				S2-P2	Down Arrow		
CSAH 11	1128+70	Lt				W11-2	Ped Crossing		
CSAH 11	1128+70	Lt	1			R1-X1	State Law		
CSAH 11	1128+70	Lt				S2-P2	Down Arrow		
CSAH 11	1128+70	Lt				W11-2	Ped Crossing		
CSAH 11	1128+70	Lt				R1-X1	State Law		
CSAH 11	1128+70	Lt				S2-P2	Down Arrow		
CSAH 11	128+80	Rt		1	1		Street Sign	1	
CSAH 11	1128+80	Lt				R1-1	30" Stop		
CSAH 11	1128+80	Lt	1			X4-13	Delineator		
CSAH 11	129+35	Rt				R1-1	30" Stop		
CSAH 11	129+35	Rt	1			X4-13	Delineator		
CSAH 11	1129+35	Lt		1	1		Street Sign	1	
CSAH 11	1131+50	Lt	1			R8-3a	No Parking		
CSAH 11	131+65	Rt				W11-2	Ped Crossing		
CSAH 11	131+65	Rt	1			W20-100P	500 Feet		
CSAH 11	132+90	Lt				R1-1	30" Stop		
CSAH 11	132+90	Lt	1			X4-13	Delineator		
CSAH 11	133+00	Rt		1	1		Street Sign	1	
CSAH 11	133+50	Lt		1	1		Street Sign	1	
CSAH 11	1133+50	Rt				R1-1	30" Stop		
CSAH 11	1133+50	Rt	1				Delineator		
CSAH 11	1133+65	Lt				W11-2	Ped Crossing		
CSAH 11	1133+65	Lt	1			W20-100P	500 Feet		
CSAH 11	134+80	Rt	1			R8-3a	No Parking		
CSAH 11	1135+10	Lt	1			R8-3a	No Parking		
CSAH 11	136+45	Rt				S1-1	School Crossing		
CSAH 11	136+45	Rt				R1-X1	State Law		
CSAH 11	136+45	Rt	1			S2-P2	Down Arrow		
CSAH 11	136+45	Rt				S1-1	School Crossing		
CSAH 11	136+45	Rt				R1-X1	State Law		
CSAH 11	136+45	Rt				S2-P2	Down Arrow		
CSAH 11	1136+45	Lt				S1-1	School Crossing		
CSAH 11	1136+45	Lt	1			R1-X1	State Law		
CSAH 11	1136+45	Rt				S2-P2	Down Arrow		
CSAH 11	136+60	Rt		1	1		Street Sign	1	
CSAH 11	1136+60	Lt				R1-1	30" Stop		
CSAH 11	1136+60	Lt	1			X4-13	Delineator		
CSAH 11	1137+10	Lt	1		1		Street Sign	1	

SIGN REMOVAL TAB									K
ALIGNMENT	STATION	ADDRESS/ DESCRIPTION (NOTES)	REMOVE	SALVAGE	INSTALL	SIGN NUMBER	SIGN LEGEND	NOTES	
			SIGN TYPE C	SIGN TYPE SPECIAL	SIGN TYPE SPECIAL [1]				
			EACH	EACH	EACH				
CSAH 11	137+20	Rt				R1-1	30" Stop		
CSAH 11	137+20	Rt	1			X4-13	Delineator		
CSAH 11	137+70	Rt	1			R8-3a	No Parking		
CSAH 11	138+50	Lt	1			R8-3a	No Parking		
CSAH 11	138+60	Rt				R1-1	30" Stop		
CSAH 11	138+60	Rt	1			X4-13	Delineator		
CSAH 11	139+95	Rt				M2-1A	Jct		
CSAH 11	139+95	Rt	1			M1-6A	12 Rt Mkr		
CSAH 11	139+95	Rt				R8-3a	No Parking		
CSAH 11	140+50	Rt				R1-1	30" Stop		
CSAH 11	140+50	Rt	1			X4-13	Delineator		
CSAH 11	1141+20	Lt				S1-1	School Crossing		
CSAH 11	1141+20	Lt	1			W20-100P	500 Feet		
CSAH 11	141+80	Rt				R1-1	30" Stop		
CSAH 11	141+80	Rt	1			X4-13	Delineator		
CSAH 11	1142+30	Lt				R1-1	30" Stop		
CSAH 11	1142+30	Lt	1			X4-13	Delineator		
CSAH 11	142+40	Rt				R3-30AD	Ln Designation		
CSAH 11	142+40	Rt	1			R8-3a	No Parking		
CSAH 11	1142+90	Lt				R2-1	Speed Limit 40		
CSAH 11	1142+90	Lt	1			R8-3a	No Parking		
CSAH 11	143+90	Rt				M1-6A	11 Rte Mkr		
CSAH 11	143+90	Rt	1			M6-1AL	Left Arrow		
CSAH 11	143+								




SIGN REMOVAL TAB								K
ALIGNMENT	STATION	ADDRESS/ DESCRIPTION (NOTES)	REMOVE SIGN TYPE	SALVAGE SIGN TYPE	INSTALL SIGN TYPE	SIGN NUMBER	SIGN LEGEND	NOTES
			C	SPECIAL	SPECIAL (1)			
NORTHDALE BLVD								
NORTHDALE	199+15	Rt	1			W11-2	Ped Crossing	
NORTHDALE	199+15	Rt				W20-100P	500 Feet	
NORTHDALE	199+95	Rt	1			R1-1	30" Stop	
NORTHDALE	201+90	Rt				R1-1	30" Stop	
NORTHDALE	201+90	Rt	1			X4-13	Delineator	
NORTHDALE	202+20	Lt	1			1X-1	Adopt A Hwy	
NORTHDALE	202+30	Rt	1				Bus Stop	
NORTHDALE	203+60	Rt				W11-2	Ped Crossing	
NORTHDALE	203+60	Rt				R1-X1	State Law	
NORTHDALE	203+60	Rt	1			S2-P2	Down Arrow	
NORTHDALE	203+60	Rt				W11-2	Ped Crossing	
NORTHDALE	203+60	Rt				R1-X1	State Law	
NORTHDALE	203+60	Rt				S2-P2	Down Arrow	
NORTHDALE	203+60	Lt				W11-2	Ped Crossing	
NORTHDALE	203+60	Lt	1			R1-X1	State Law	
NORTHDALE	203+60	Lt				S2-P2	Down Arrow	
NORTHDALE	203+60	Lt				W11-2	Ped Crossing	
NORTHDALE	203+60	Lt	1			R1-X1	State Law	
NORTHDALE	203+60	Lt				S2-P2	Down Arrow	
NORTHDALE	203+70	Lt	1			R1-1	30" Stop	
NORTHDALE	203+70	Lt				X4-13	Delineator	
NORTHDALE	204+20	Rt	1			R1-1	30" Stop	
NORTHDALE	204+20	Rt				X4-13	Delineator	
NORTHDALE	204+65	Lt	1				Bus Stop	
NORTHDALE	204+75	Rt	1			M2-1A	Jct	
NORTHDALE	204+75	Rt				M1-6A	Rte Mkr 12	
NORTHDALE	205+80	Rt	1			R3-30AD	Ln Designation	
NORTHDALE	206+35	Rt	1			R1-1	30" Stop	
NORTHDALE	206+35	Rt				X4-13	Delineator	
NORTHDALE	207+30	Rt	1			W11-2	Ped Crossing	
NORTHDALE	207+30	Rt				W20-100P	300 Feet	
NORTHDALE	207+65	Rt	1			R2-1	Speed Limit 35	
NORTHDALE	207+65	Rt				X4-13	Delineator	
NORTHDALE	208+35	Rt	1			M3-4A	West	
NORTHDALE	208+35	Rt				M1-6A	Rte Mkr 11	
NORTHDALE	208+65	Lt	1			M1-6A	Rte Mkr 12	
NORTHDALE	208+65	Lt				M6-3A	Up Arrow	
NORTHDALE	208+65	Lt				M1-6A	Rte Mkr 11	
NORTHDALE	208+65	Lt	1			M6-1AR	Right Arrow	
NORTHDALE	208+65	Lt					Bus Stop	
NORTHDALE	209+75	Rt	1			M1-6A	Rte Mkr 11	
NORTHDALE	209+75	Rt				M6-6AL	Up & Left Arrow	
NORTHDALE	209+75	Rt	1			M4-6A	End	
NORTHDALE	209+75	Rt				M1-6A	Rte Mkr 12	
NORTHDALE	210+45	Lt	1			R2-1	Speed Limit 35	
NORTHDALE	210+45	Lt				M3-2A	East	
NORTHDALE	210+45	Lt	1			M1-6A	Rte Mkr 12	
NORTHDALE	210+95	Rt	1			R3-30AD	Ln Designation	
NORTHDALE	211+35	Rt	1			M2-1A	Jct	
NORTHDALE	211+35	Rt				M1-6A	Rte Mkr 11	
NORTHDALE	211+50	Rt	1			R1-1	30" Stop	
NORTHDALE	211+50	Rt				X4-13	Delineator	
NORTHDALE	211+65	Lt	1			R1-1	30" Stop	
NORTHDALE	211+65	Rt				X4-13	Delineator	
NORTHDALE	212+00	Rt		1	1		Street Sign	1
NORTHDALE	212+10	Rt	1			R10-7	Do Not Block	
NORTHDALE	212+20	Lt	1			S1-1	School Crossing	
NORTHDALE	212+20	Lt					Scl Spd Lml	
NORTHDALE	213+10	Median	1			R4-7	Keep Right	
NORTHDALE	213+10	Median				X4-2	Nine Bultron	
NORTHDALE	213+25	Rt	1				Ndale Mdle Sch	
NORTHDALE	213+60	Rt	1			R6-1R	One Way	
NORTHDALE	213+60	Rt				R1-1	30" Stop	
NORTHDALE	213+60	Rt	1			X4-13	Delineator	
NORTHDALE	213+70	Median	1			R6-1R	One Way	
NORTHDALE	214+10	Median	1			R6-1R	One Way	
NORTHDALE	214+25	Lt				R6-1R	One Way	
NORTHDALE	214+25	Lt	1			R1-1	30" Stop	
NORTHDALE	214+25	Lt				X4-13	Delineator	
NORTHDALE	214+80	Rt	1				Liquor Store	
NORTHDALE	215+00	Median	1			R6-1R	One Way	
NORTHDALE	215+15	Lt				R6-1R	One Way	
NORTHDALE	215+15	Lt	1			R1-1	30" Stop	
NORTHDALE	215+15	Lt				X4-13	Delineator	

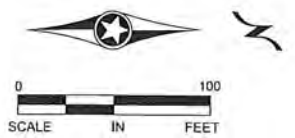
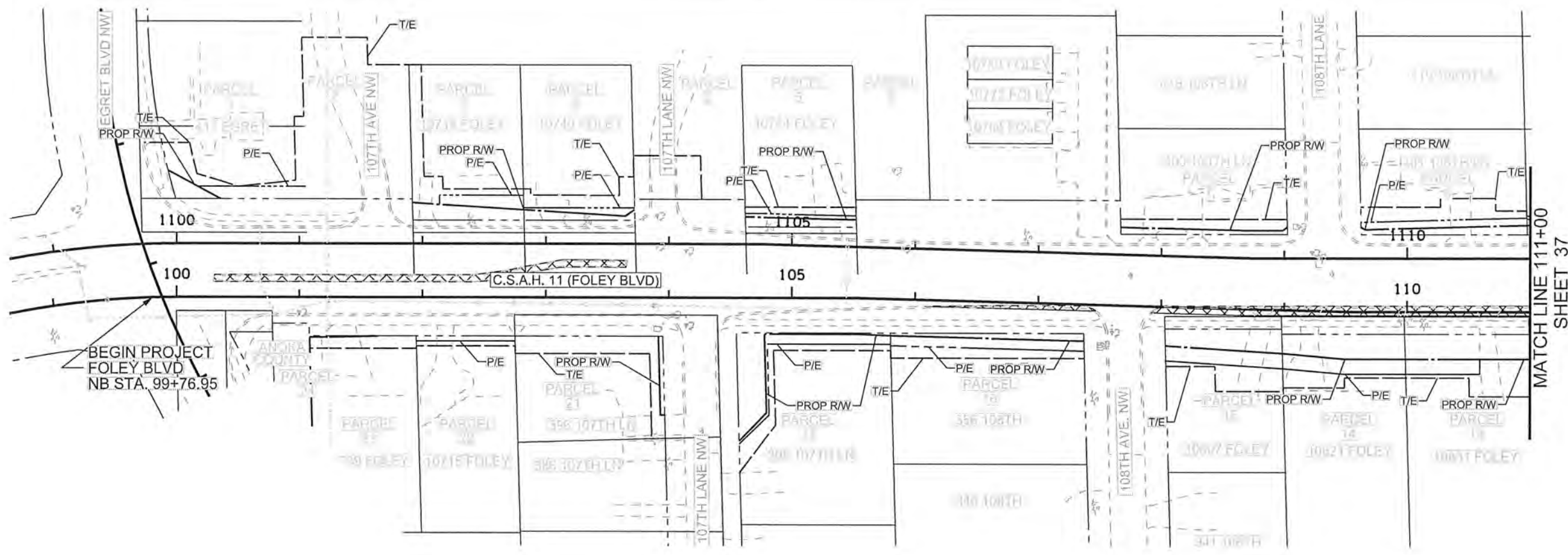
SIGN REMOVAL TAB								K
ALIGNMENT	STATION	ADDRESS/ DESCRIPTION (NOTES)	REMOVE SIGN TYPE	SALVAGE SIGN TYPE	INSTALL SIGN TYPE	SIGN NUMBER	SIGN LEGEND	NOTES
			C	SPECIAL	SPECIAL (1)			
NORTHDALE BLVD								
NORTHDALE	215+40	Lt	0				Your Speed	2
NORTHDALE	215+55	Rt	1			S5-2	End Schi Zone	
NORTHDALE	215+55	Rt	1			R2-1	Speed Limit 35	
NORTHDALE	215+70	Rt				R6-1R	One Way	
NORTHDALE	215+70	Rt	1			R1-1	30" Stop	
NORTHDALE	215+70	Rt				X4-13	Delineator	
NORTHDALE	215+80	Median	1			R6-1R	One Way	
NORTHDALE	215+85	Median	1			R3-X2	Left Turn Ln	
NORTHDALE	216+30	Median	1			R6-1R	One Way	
NORTHDALE	216+50	Lt				R6-1R	One Way	
NORTHDALE	216+50	Lt	1			R1-1	30" Stop	
NORTHDALE	216+50	Lt				X4-13	Delineator	
NORTHDALE	216+60	Rt				R6-1R	One Way	
NORTHDALE	216+60	Rt	1			R1-1	30" Stop	
NORTHDALE	216+60	Rt				X4-13	Delineator	
NORTHDALE	216+70	Lt	1			R3-X1	Right Turn Ln	
NORTHDALE	216+85	Median	1			R6-1R	One Way	
NORTHDALE	217+10	Median	1			R5-1	Do Not Enter	
NORTHDALE	217+80	Median	1			R5-1	Do Not Enter	
NORTHDALE	217+90	Median	1			R4-7	Keep Right	
NORTHDALE	217+90	Median				X4-2	Nine Bultron	
NORTHDALE	218+10	Rt				R6-1L	One Way	
NORTHDALE	218+10	Rt	1			R6-1R	One Way	
NORTHDALE	218+10	Rt				R1-1	30" Stop	
NORTHDALE	218+10	Rt				X4-13	Delineator	
NORTHDALE	218+35	Lt				R4-7	Keep Right	
NORTHDALE	218+35	Lt	1			X4-2	Nine Bultron	
NORTHDALE	218+35	Lt				R1-1	30" Stop	
NORTHDALE	218+45	Rt				R4-7	Keep Right	
NORTHDALE	218+45	Rt	1			X4-2	Nine Bultron	
NORTHDALE	218+45	Rt				R1-1	30" Stop	
SUBTOTAL NORTHDALE BLVD.			54	1	1			
TOTALS			136	22	22			

CONSTRUCTION NOTES:
1. FOR RELOCATING SIGNS DURING CONSTRUCTION, AS DIRECTED BY THE ENGINEER, RELOCATION INCIDENTAL TO TRAFFIC CONTROL.
2. "ONE WAY" ON SIGNAL POLES AND DRIVER FEEDBACK SIGN REMOVAL INCIDENTAL TO TRAFFIC SIGNAL REMOVAL.

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: DOUGLAS W. FISCHER SIGNATURE: <i>[Signature]</i> DATE: 5/27/16 LICENSE NO. 20235					DRAWN BY: TV DATE: 03/16/16 DESIGN BY: TV DATE: 03/16/16 CHECKED BY: RR DATE: 03/16/16	 ANOKA COUNTY HIGHWAY DEPT.	STATE PROJECT NO. 002-611-034 STATE PROJECT NO. 114-020-050 CITY PROJECT NO. 13-10 COUNTY PROJECT NO.	EXISTING SIGNING AND STRIPING Sheet 36 of 298 Sheets
NO DATE BY CKD APPR REVISION NAME: T:\Traffic\dwg\CSAH 11 (Foley_Northdale)\from CSAH 78 to CSAH 12.dwg								

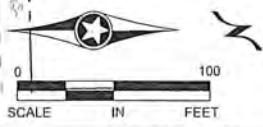
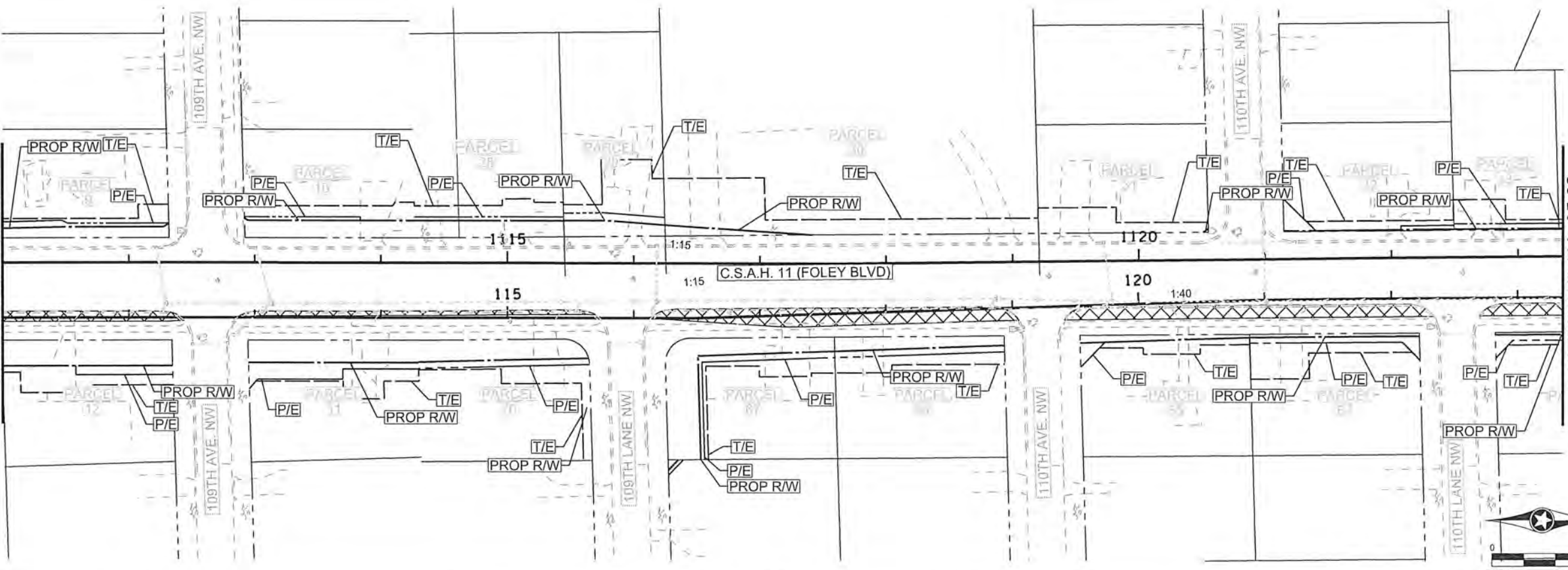
LEGEND

-  TEMPORARY PAVEMENT
-  EXISTING TOPOGRAPHY
-  INPLACE DRAINAGE PIPES



MATCH LINE 111+00
SHEET 37

SHEET 37
MATCH LINE 111+00



STAGE 1A CONSTRUCTION NOTES:
REMOVE CURB AND CONSTRUCT TEMPORARY PAVEMENT ON EAST SIDE OF FOLEY. REMOVE MEDIAN NORTH OF EGRET AND PLACE TEMPORARY PAVEMENT FOR SOUTHBOUND TRAFFIC TO CROSSOVER. INSTALL TEMPORARY SIGNAL AT EGRET.


STAGE 1A TRAFFIC NOTES:
TEMPORARY TRAFFIC CONTROL FOR SHOULDER WORK AND MEDIAN REMOVAL

MATCH LINE 123+35.62
SHEET 38

NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_STG1A_P1.dgn					
05/13/2016 2:03:52 PM					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



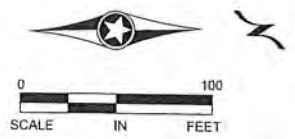
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 1A
 STA 99+76.95 TO 111+00.00
 Sheet 37 of 298 Sheets

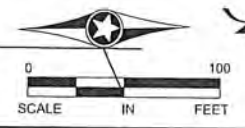
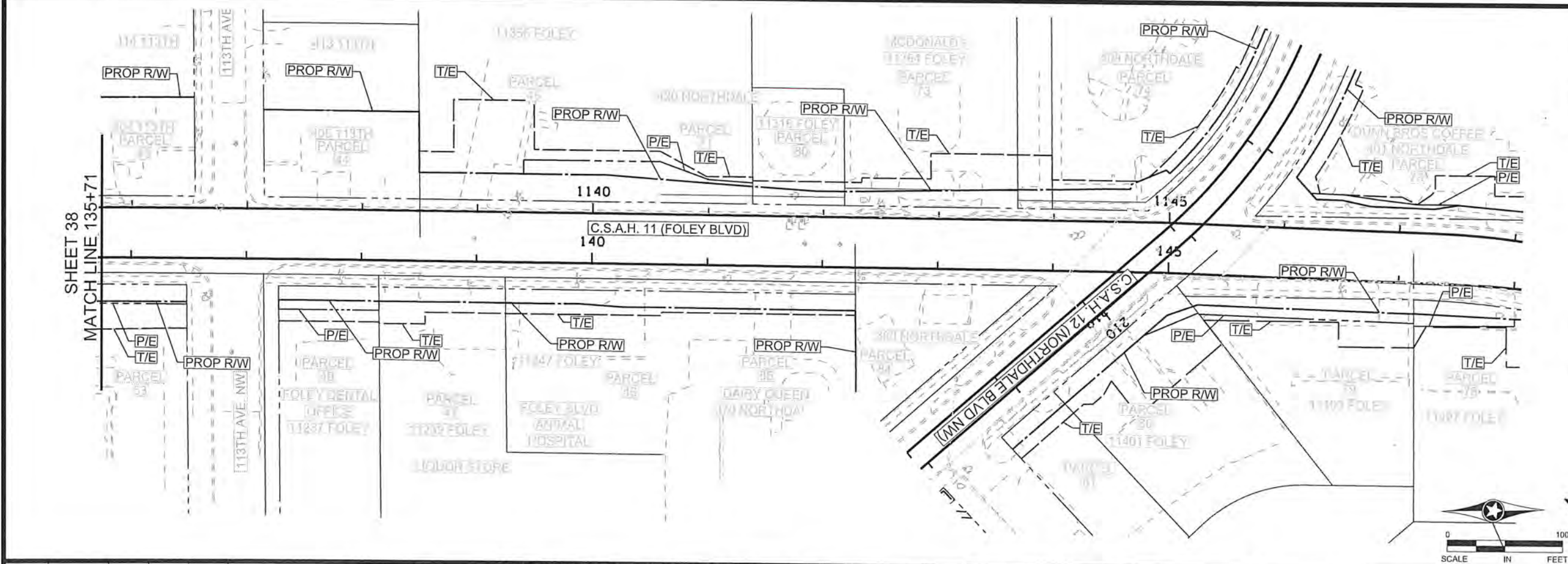
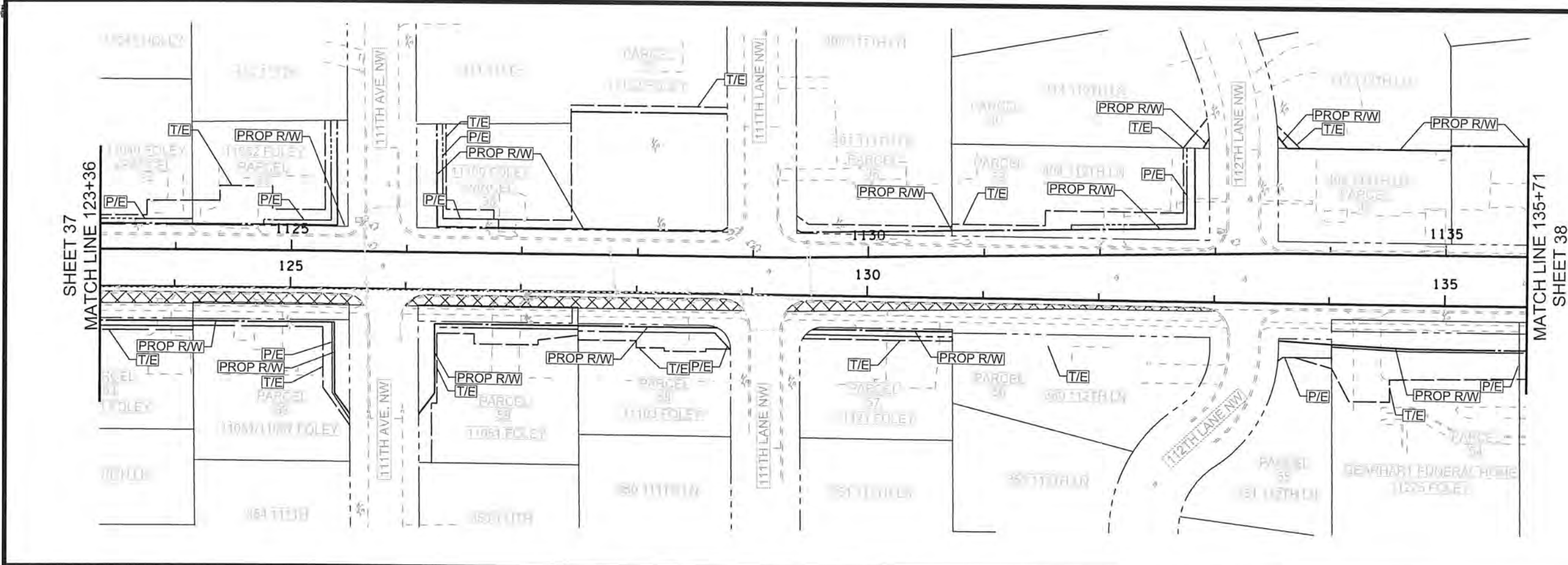
LEGEND

- TEMPORARY PAVEMENT
- EXISTING TOPOGRAPHY
- INPLACE DRAINAGE PIPES



STAGE 1A CONSTRUCTION NOTES:
 REMOVE CURB AND CONSTRUCT TEMPORARY PAVEMENT ON EAST SIDE OF FOLEY. REMOVE MEDIAN NORTH OF EGRET AND PLACE TEMPORARY PAVEMENT FOR SOUTHBOUND TRAFFIC TO CROSSOVER. INSTALL TEMPORARY SIGNAL AT EGRET.

STAGE 1A TRAFFIC NOTES:
 TEMPORARY TRAFFIC CONTROL FOR SHOULDER WORK AND MEDIAN REMOVAL



NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_STG1A_P2.dgn					
05/13/2016 2:03:53 PM					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: OFF DATE: 02/27/16
 DESIGN BY: OFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

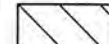
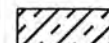

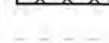









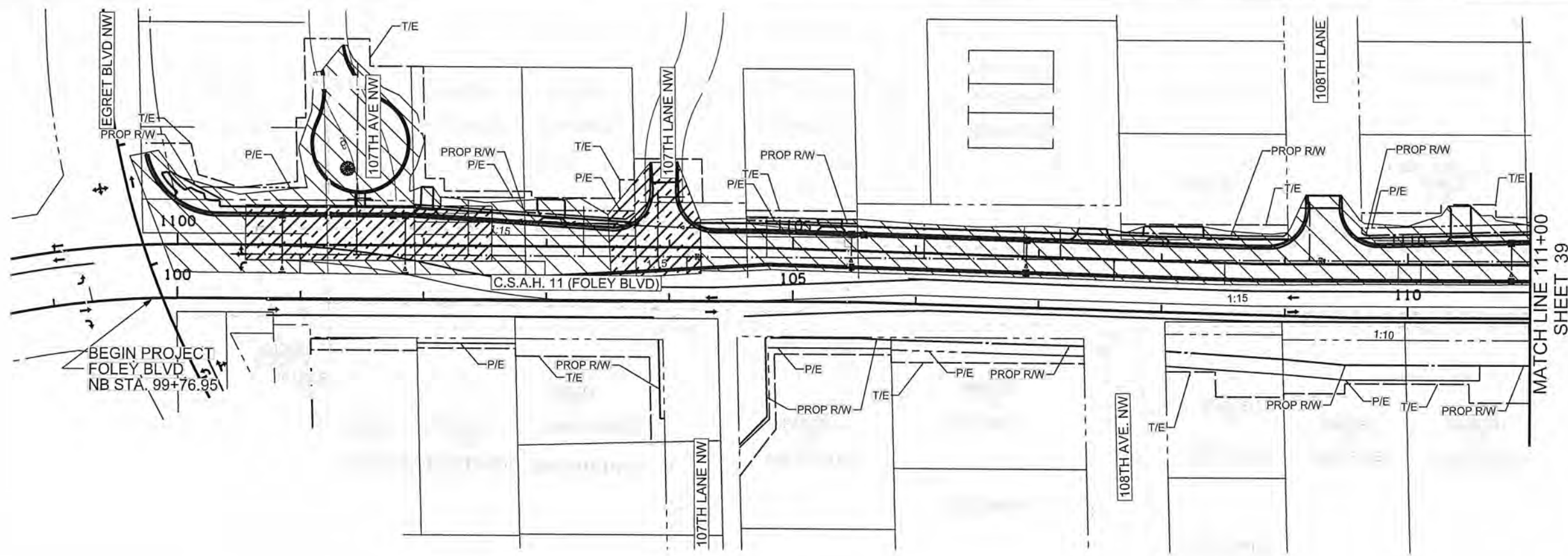
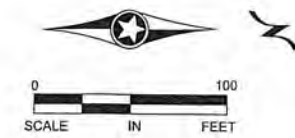
**ANOKA COUNTY
 HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

**CONSTRUCTION STAGING PLAN
 STAGE 1A**
 STA 123+36 TO 148+06
 Sheet 38 of 298 Sheets

LEGEND

-  WORK AREA
-  WORK AREA (UNDER TRAFFIC)
-  TEMPORARY PAVEMENT
-  EXISTING TOPOGRAPHY
-  CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
-  COMPLETED CONSTRUCTION
-  TRAFFIC SHIFT LANES
-  TRAFFIC FLOW DIRECTION
-  DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
-  PROPOSED WATERMAIN
-  INPLACE DRAINAGE PIPES



STAGE 1B CONSTRUCTION NOTES:

CONSTRUCT WEST SIDE OF FOLEY BLVD BETWEEN EGRET AND WATER TOWER INCLUDING STORM SEWER TRUNK LINE, WATERMAIN, AND PONDS. ACCESS TO 107TH LN, 109TH AVE AND 113TH AVE WILL BE CONSTRUCTED UNDER TRAFFIC.

WATERMAIN AT SOUTH END OF PROJECT TO BE DONE UNDER TRAFFIC.

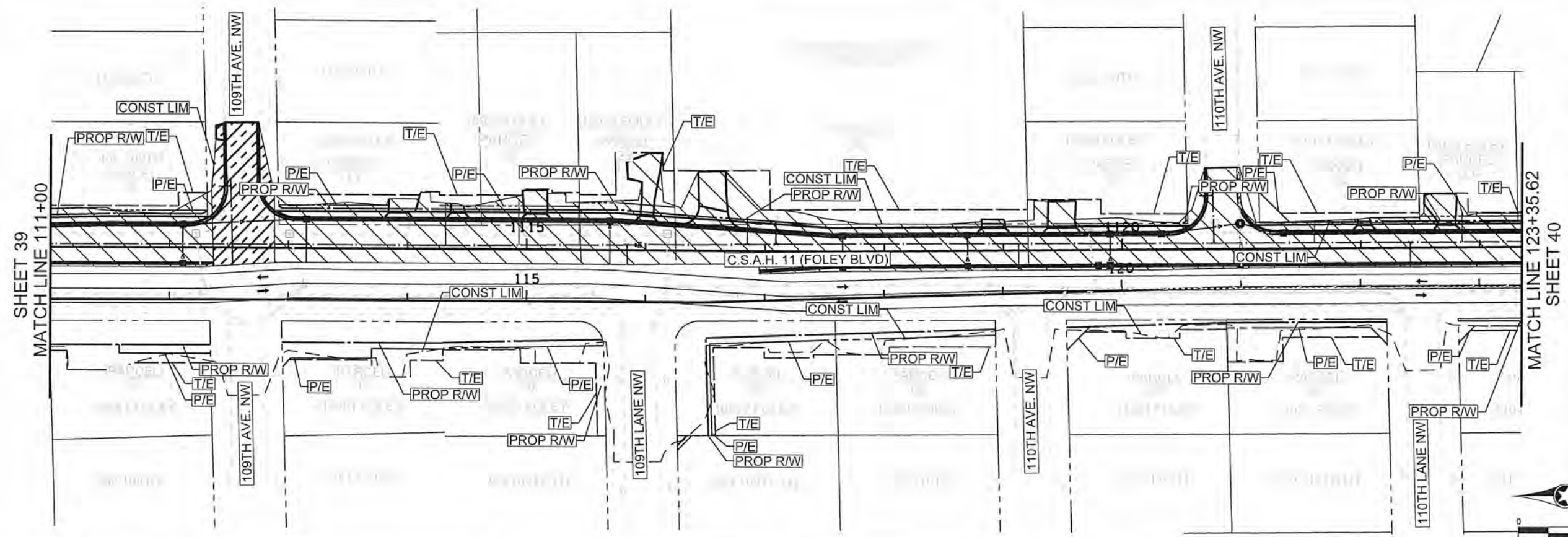
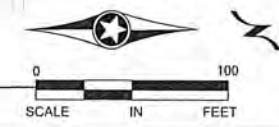
SOUTHBOUND LEFT TURN LANE TO EGRET WILL BE CONSTRUCTED AT THE END OF THE STAGE.

MEDIAN AT EGRET WILL BE CONSTRUCTED AT THE END OF THE NEXT STAGE.

STAGE 1B TRAFFIC NOTES:

SHIFT TRAFFIC TO EAST TO USE TEMPORARY PAVEMENT. PLACE TEMPORARY CONCRETE BARRIER. MAINTAIN ACCESS TO DRIVEWAYS. CLOSE ONE NORTHBOUND THRU LANE SOUTH OF EGRET. SOUTHBOUND TRAFFIC TO BE SHIFTED BACK TO THE THE SOUTHBOUND THRU LANE AT EGRET INTERSECTION.

TO CONSTRUCT THE SOUTHBOUND LEFT TURN LANE SHIFT SOUTHBOUND TRAFFIC TO THE STAGE 2 CONFIGURATION.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG1B_P1.dgn 05/17/2016 11:34:29 AM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *[Signature]*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

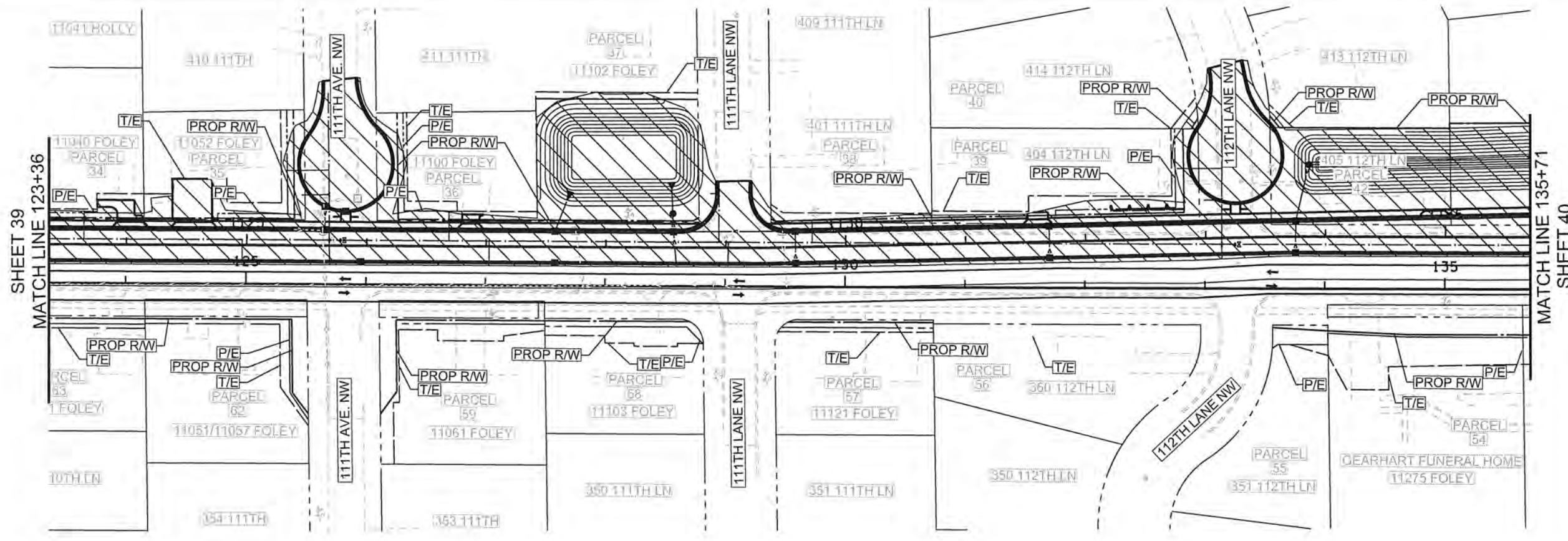
DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

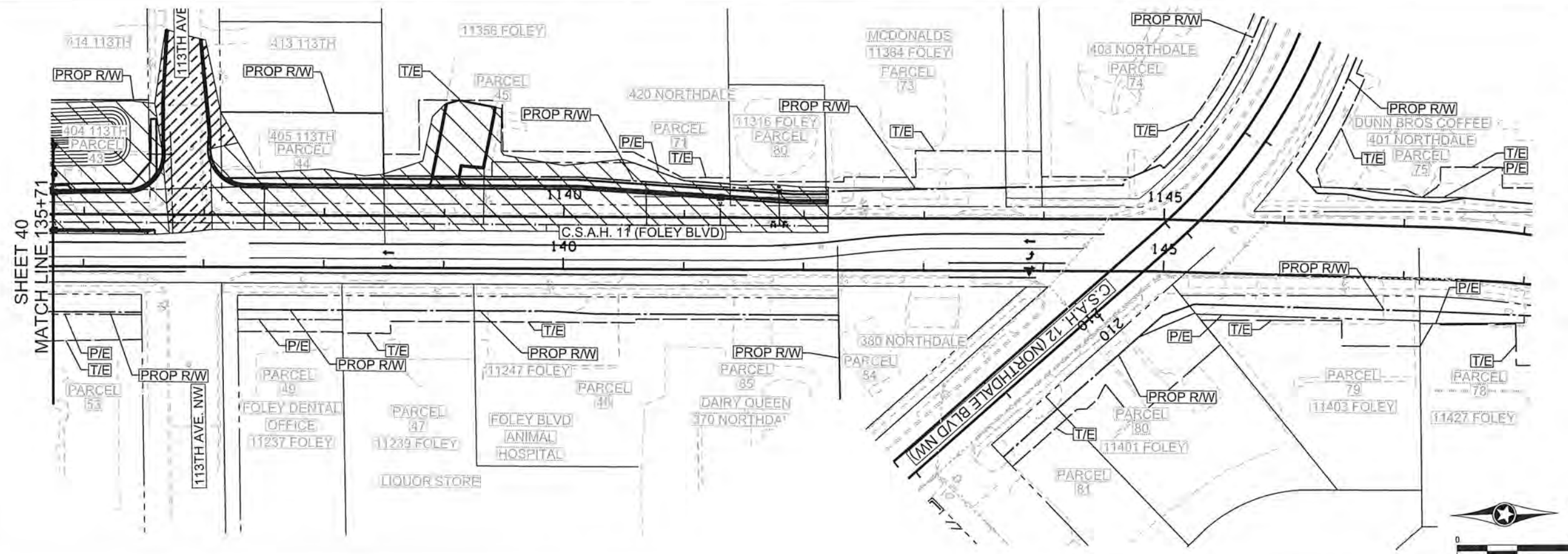
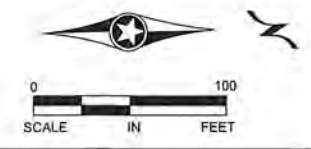
SP 002-611-034
SP 114-020-050
CP 13-10

CONSTRUCTION STAGING PLAN
STAGE 1B
STA 99+76.95 TO 111+00.00
Sheet 39 of 298 Sheets



LEGEND

- WORK AREA
- WORK AREA (UNDER TRAFFIC)
- TEMPORARY PAVEMENT
- EXISTING TOPOGRAPHY
- CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
- COMPLETED CONSTRUCTION
- TRAFFIC SHIFT LANES
- TRAFFIC FLOW DIRECTION
- DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
- PROPOSED WATERMAIN
- INPLACE DRAINAGE PIPES



STAGE 1B CONSTRUCTION NOTES:

CONSTRUCT WEST SIDE OF FOLEY BLVD BETWEEN EGRET AND WATER TOWER INCLUDING STORM SEWER TRUNK LINE, WATERMAIN, AND PONDS. ACCESS TO 107TH LN, 109TH AVE AND 113TH AVE WILL BE CONSTRUCTED UNDER TRAFFIC.

WATERMAIN AT SOUTH END OF PROJECT TO BE DONE UNDER TRAFFIC.

SOUTHBOUND LEFT TURN LANE TO EGRET WILL BE CONSTRUCTED AT THE END OF THE STAGE.

MEDIAN AT EGRET WILL BE CONSTRUCTED AT THE END OF THE NEXT STAGE.

STAGE 1B TRAFFIC NOTES:

SHIFT TRAFFIC TO EAST TO USE TEMPORARY PAVEMENT. PLACE TEMPORARY CONCRETE BARRIER. MAINTAIN ACCESS TO DRIVEWAYS. CLOSE ONE NORTHBOUND THRU LANE SOUTH OF EGRET. SOUTHBOUND TRAFFIC TO BE SHIFTED BACK TO THE SOUTHBOUND THRU LANE AT EGRET INTERSECTION.

TO CONSTRUCT THE SOUTHBOUND LEFT TURN LANE SHIFT SOUTHBOUND TRAFFIC TO THE STAGE 2 CONFIGURATION.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-341\Plan\002-611-034_STG1B_P2.dgn 05/19/2016 1:54:20 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

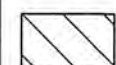









DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

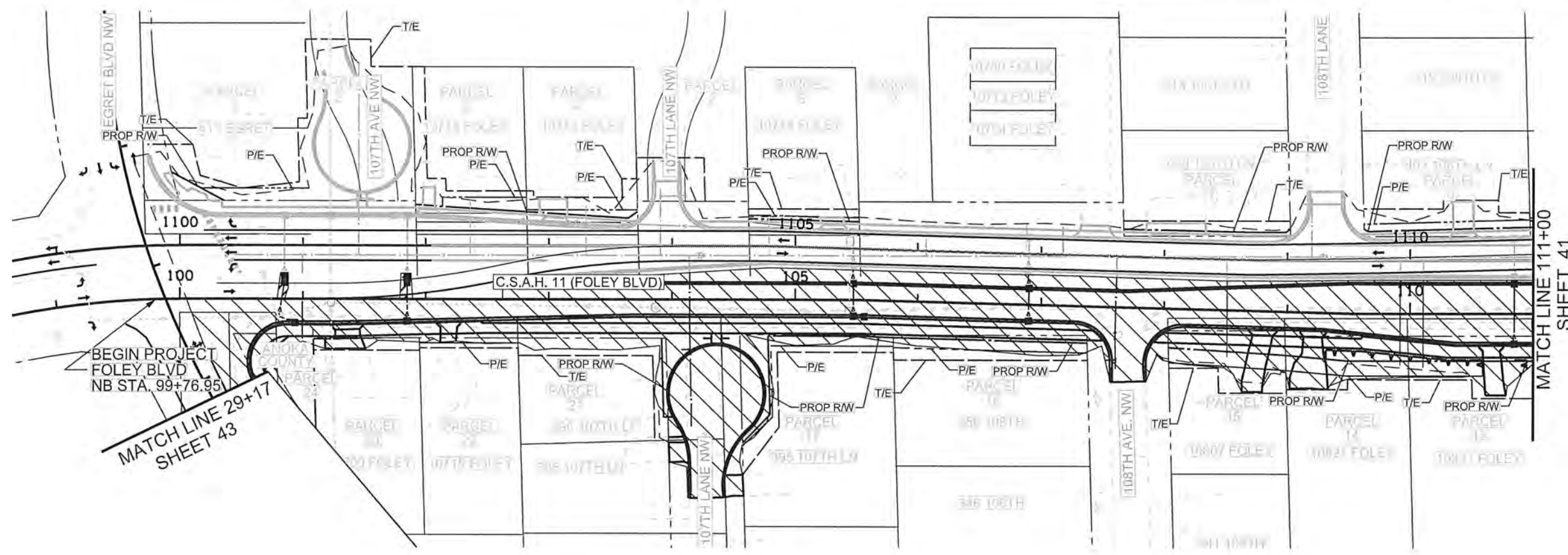
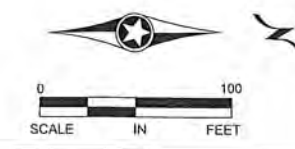
ANOKA COUNTY HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 1B
 STA 123+36 TO 148+06
 Sheet 40 of 298 Sheets

LEGEND

-  WORK AREA
-  WORK AREA (UNDER TRAFFIC)
-  EXISTING TOPOGRAPHY
-  CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
-  COMPLETED CONSTRUCTION
-  TRAFFIC SHIFT LANES
-  TRAFFIC FLOW DIRECTION
-  DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
-  PROPOSED WATERMAIN
-  INPLACE DRAINAGE PIPES

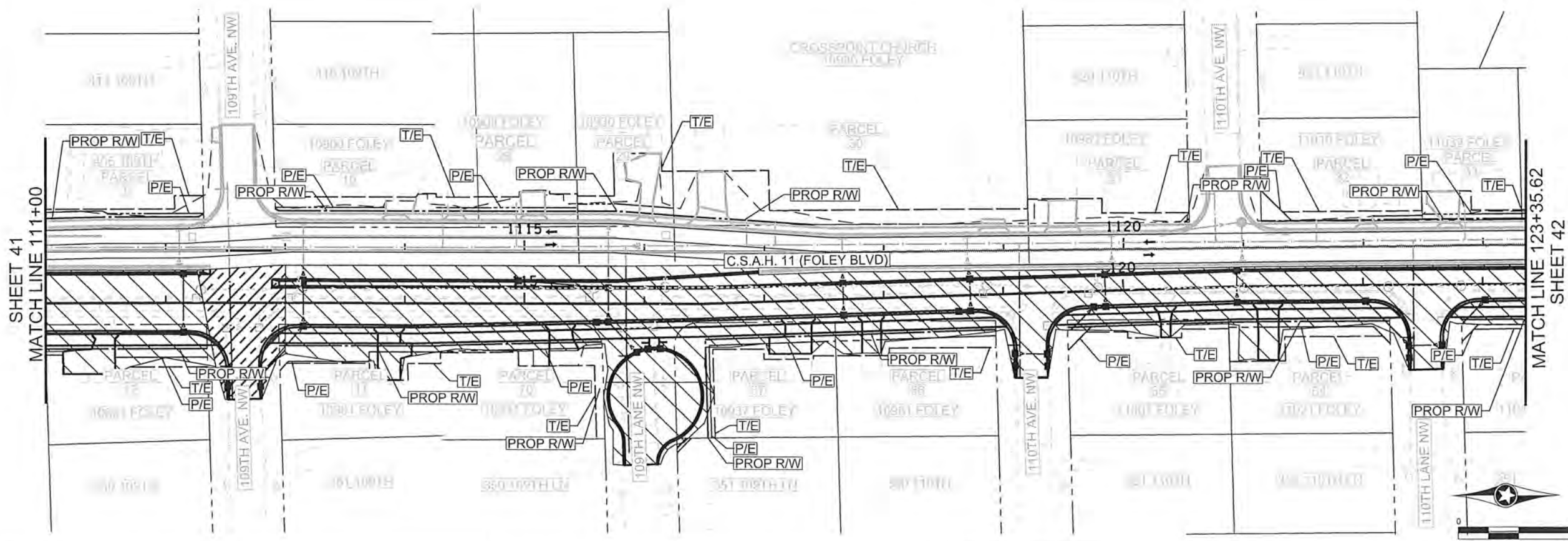
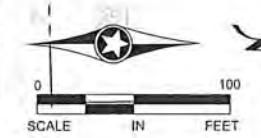


MATCH LINE 111+00
SHEET 41

BEGIN PROJECT
FOLEY BLVD
NB STA. 99+76.95
MATCH LINE 29+17
SHEET 43

STAGE 2A CONSTRUCTION NOTES:
CONSTRUCT EAST SIDE OF FOLEY BLVD PROJECT BETWEEN EGRET AND 113TH AVE. CONSTRUCT NEW WESTBOUND RIGHT TURN LANE ON EGRET.

STAGE 2A TRAFFIC NOTES:
SHIFT TRAFFIC TO WEST TO USE NEWLY CONSTRUCTED PAVEMENT. RELOCATE TEMPORARY CONCRETE BARRIER FROM PREVIOUS STAGE AS REQUIRED. 109TH AVE, 112TH LANE AND 113TH AVE CONSTRUCTED UNDER TRAFFIC. MAINTAIN ACCESS TO DRIVEWAYS.



MATCH LINE 123+35.62
SHEET 42

SHEET 41
MATCH LINE 111+00

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG2A_P1.dgn 05/13/2016 2:03:59 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/15
 DESIGN BY: DFF DATE: 02/27/15
 CHECKED BY: JEO DATE: 02/27/15













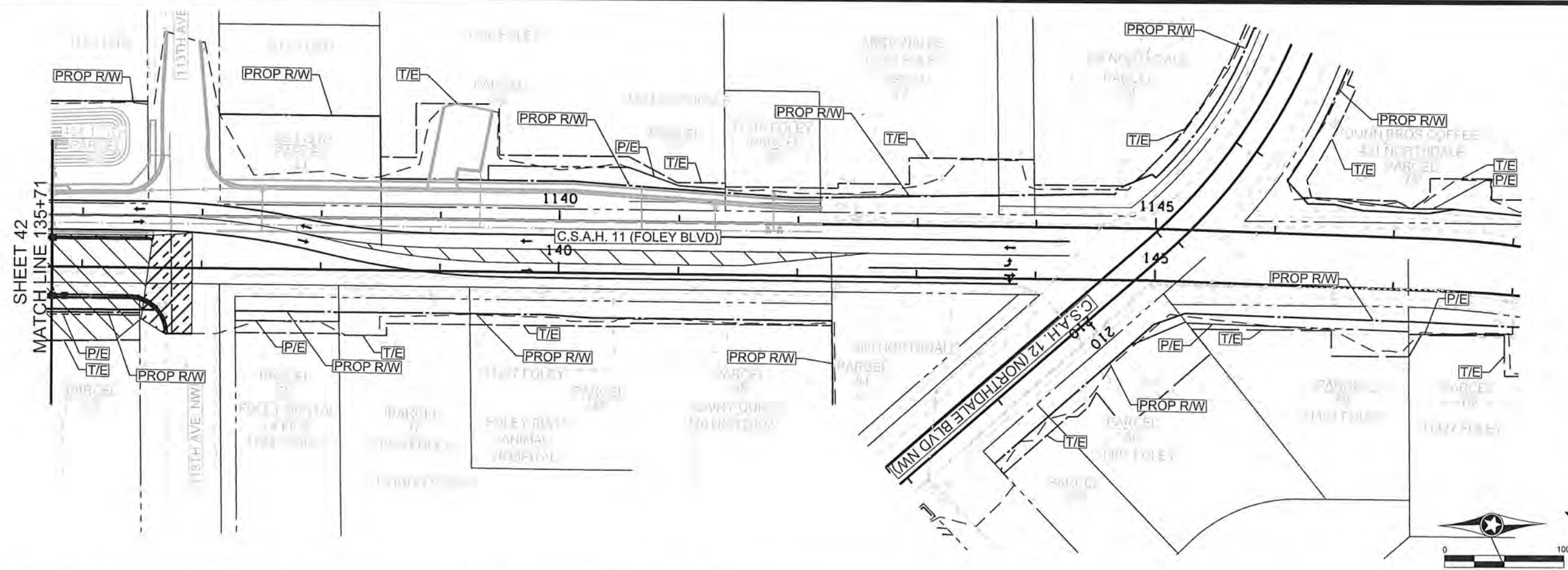
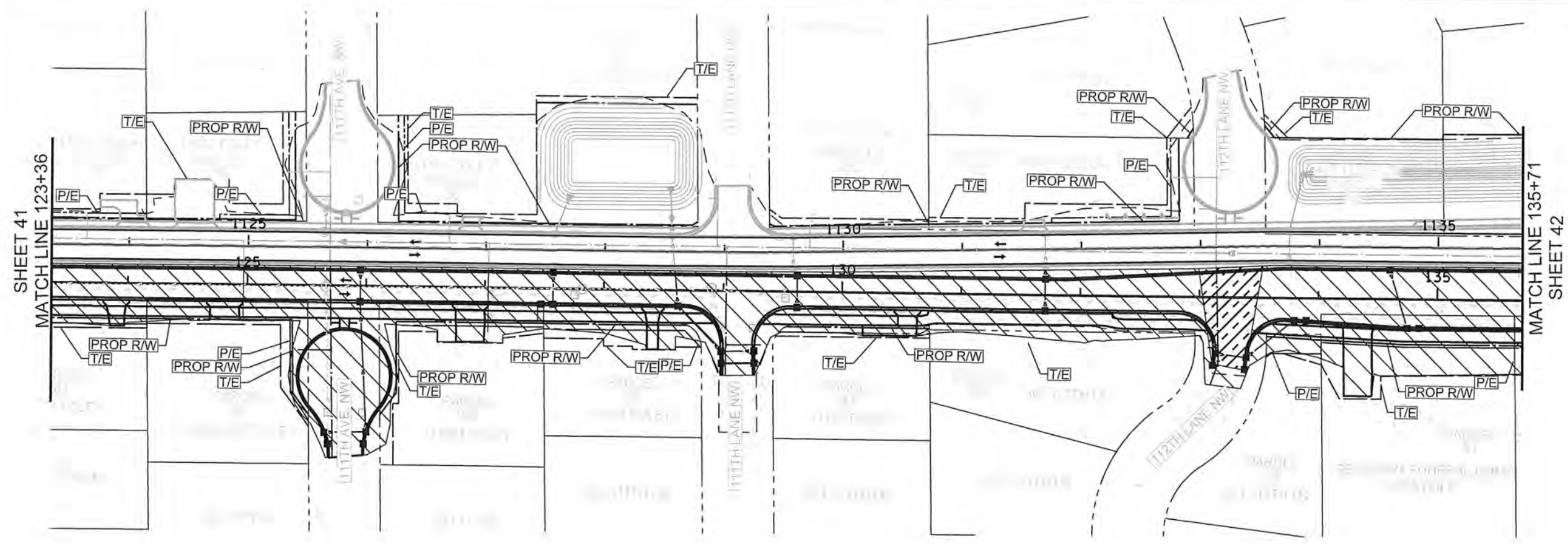
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 2A
 STA 99+76.95 TO 111+00.00
 Sheet 41 of 298 Sheets

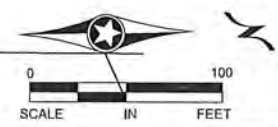
LEGEND

-  WORK AREA
-  WORK AREA (UNDER TRAFFIC)
-  EXISTING TOPOGRAPHY
-  CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
-  COMPLETED CONSTRUCTION
-  TRAFFIC SHIFT LANES
-  TRAFFIC FLOW DIRECTION
-  DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
-  PROPOSED WATERMAIN
-  INPLACE DRAINAGE PIPES



STAGE 2A CONSTRUCTION NOTES:
 CONSTRUCT EAST SIDE OF FOLEY BLVD PROJECT BETWEEN EGRET AND 113TH AVE. CONSTRUCT NEW WESTBOUND RIGHT TURN LANE ON EGRET.

STAGE 2A TRAFFIC NOTES:
 SHIFT TRAFFIC TO WEST TO USE NEWLY CONSTRUCTED PAVEMENT. RELOCATE TEMPORARY CONCRETE BARRIER FROM PREVIOUS STAGE AS REQUIRED. 109TH AVE, 112TH LANE AND 113TH AVE CONSTRUCTED UNDER TRAFFIC. MAINTAIN ACCESS TO DRIVEWAYS.




NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG2A_P2.dgn 05/16/2016 11:48:52 AM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713











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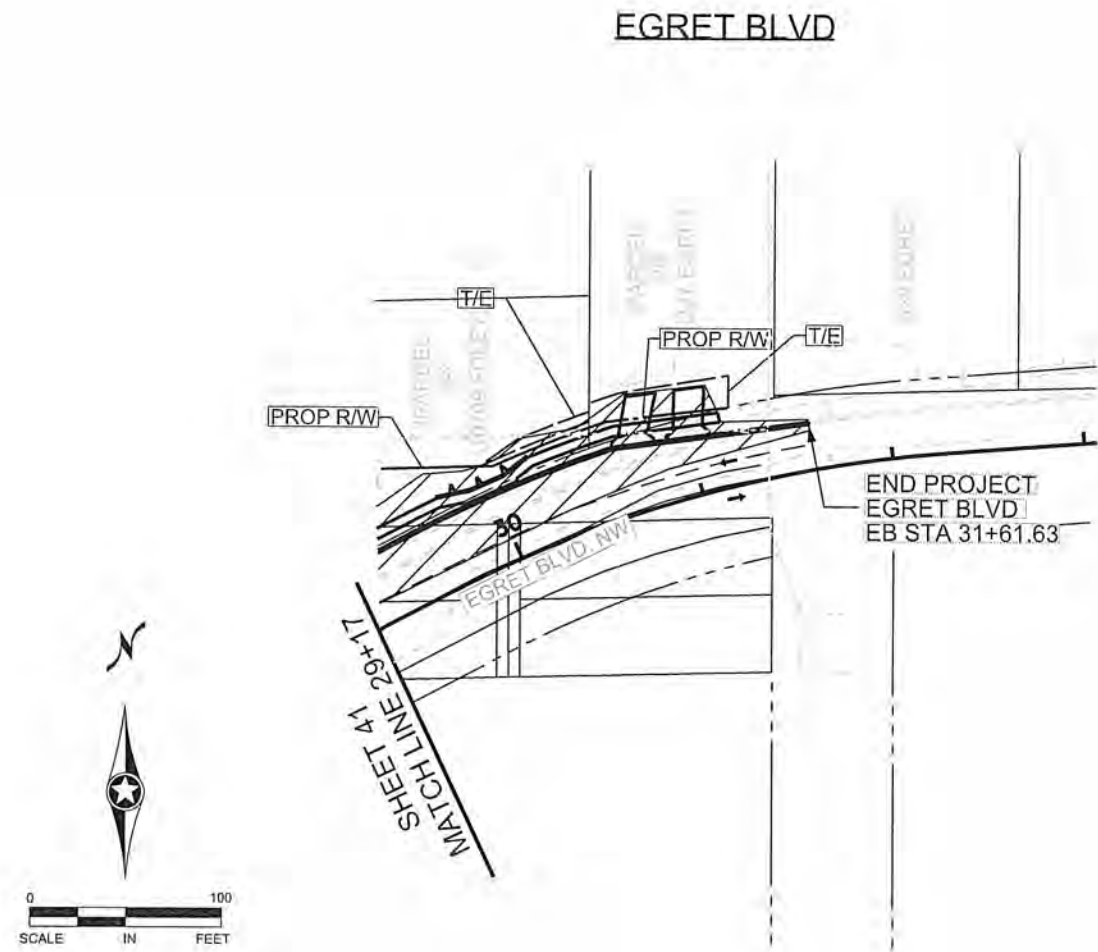


**ANOKA COUNTY
 HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 2A
 STA 123+36 TO 148+06
 Sheet 42 of 298 Sheets

LEGEND	
	WORK AREA
	WORK AREA (UNDER TRAFFIC)
	EXISTING TOPOGRAPHY
	CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
	COMPLETED CONSTRUCTION
	TRAFFIC SHIFT LANES
	TRAFFIC FLOW DIRECTION
	DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
	PROPOSED WATERMAIN
	INPLACE DRAINAGE PIPES



STAGE 2A CONSTRUCTION NOTES:
 CONSTRUCT EAST SIDE OF FOLEY BLVD PROJECT BETWEEN EGRET AND 113TH AVE. CONSTRUCT NEW WESTBOUND RIGHT TURN LANE ON EGRET.

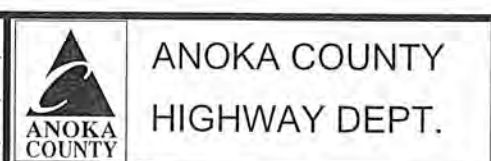
STAGE 2A TRAFFIC NOTES:
 SHIFT TRAFFIC TO WEST TO USE NEWLY CONSTRUCTED PAVEMENT. RELOCATE TEMPORARY CONCRETE BARRIER FROM PREVIOUS STAGE AS REQUIRED. 109TH AVE, 112TH LANE AND 113TH AVE CONSTRUCTED UNDER TRAFFIC. MAINTAIN ACCESS TO DRIVEWAYS.

NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG2A_P3.dgn 05/13/2016 2:04:02 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713






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 CHECKED BY: JEO DATE: 02/27/16

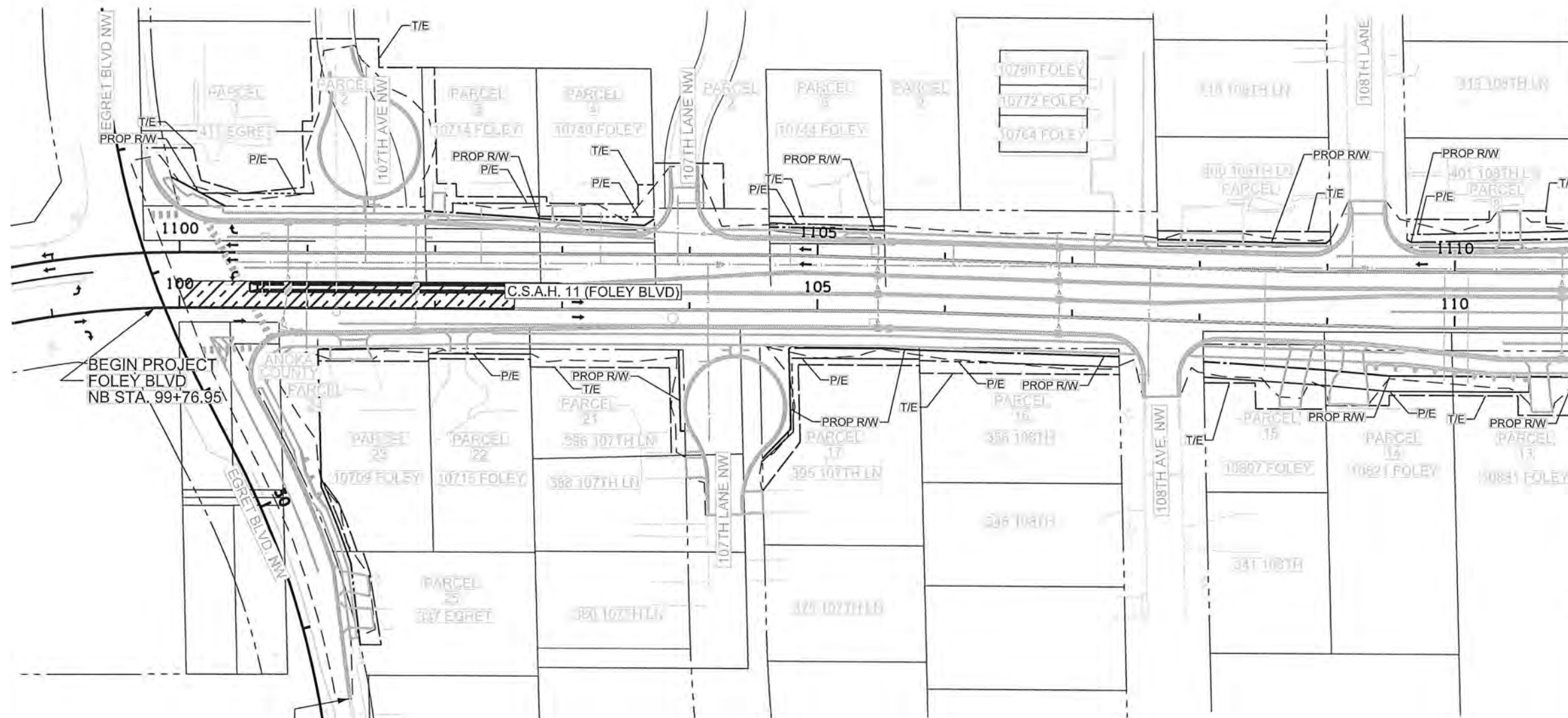


SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 2A
 FOLEY BLVD, EGRET BLVD
 & 113TH AVE/DOGWOOD ST
 Sheet 43 of 298 Sheets

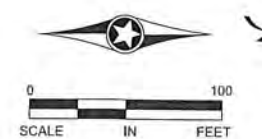
LEGEND

-  WORK AREA (UNDER TRAFFIC)
-  EXISTING TOPOGRAPHY
-  CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
-  COMPLETED CONSTRUCTION
-  TRAFFIC FLOW DIRECTION



STAGE 2B CONSTRUCTION NOTES:
 CONSTRUCT REMAINING PORTION OF NORTHBOUND FOLEY AND MEDIAN ON FOLEY AT EGRET.

STAGE 2B TRAFFIC NOTES:
 SHIFT TRAFFIC TO OUTSIDE TO USE NEWLY CONSTRUCTED PAVEMENT.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG2B_P1.dgn 05/13/2016 2:04:03 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-14 LICENSE NO. 22713

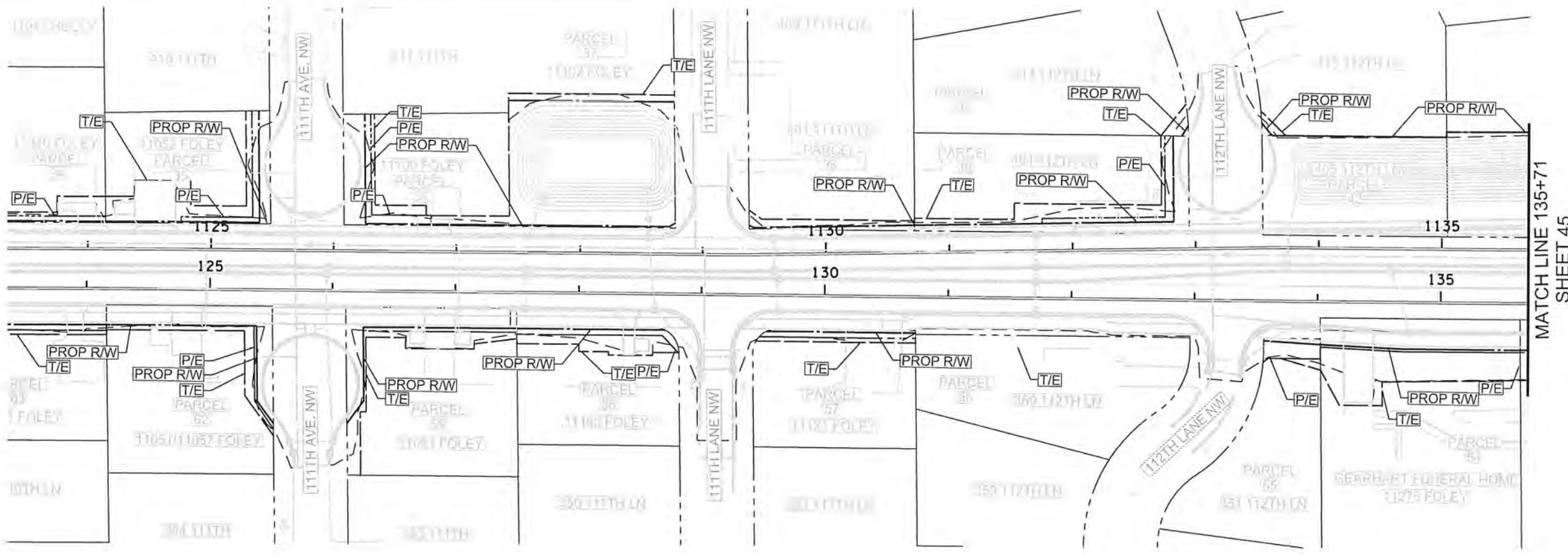
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 CHECKED BY: JED DATE: 02/27/16



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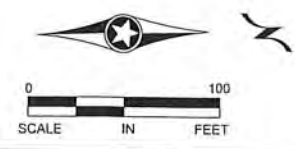
SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 2B
 STA 99+76.95 TO 111+00.00
 Sheet 44 of 298 Sheets

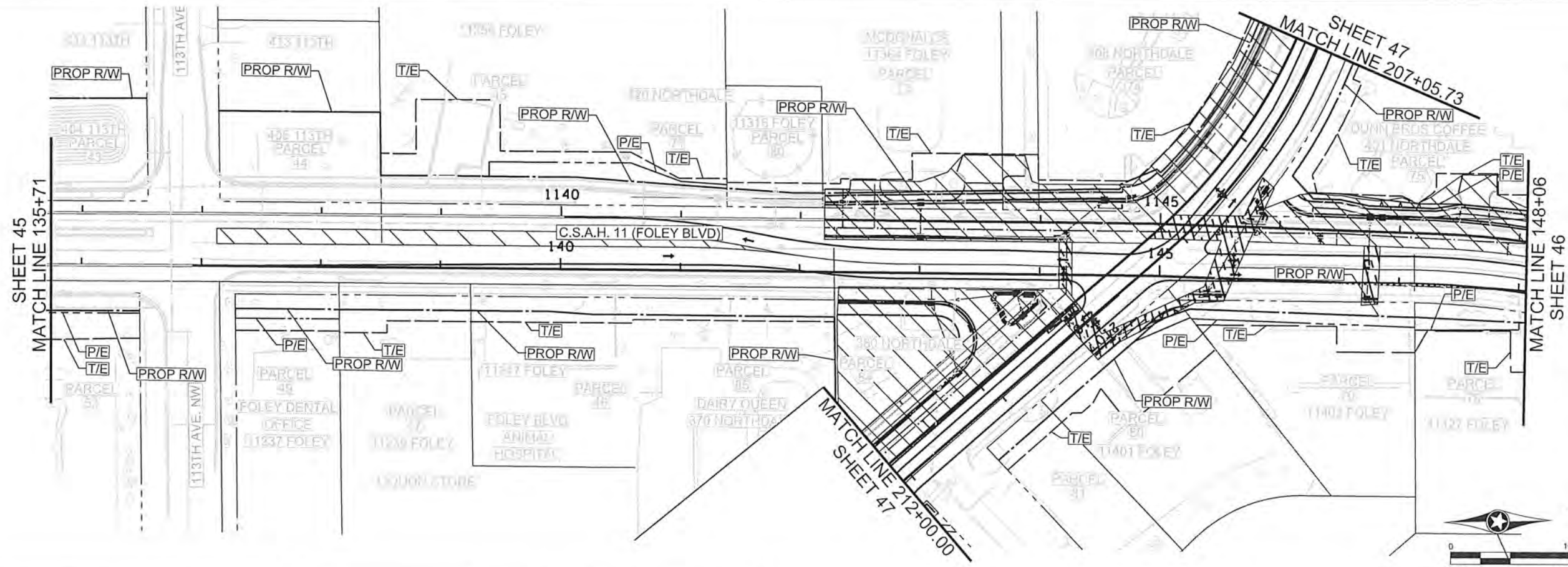


LEGEND

- WORK AREA
- WORK AREA (UNDER TRAFFIC)
- TEMPORARY PAVEMENT
- EXISTING TOPOGRAPHY
- CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
- COMPLETED CONSTRUCTION
- TRAFFIC SHIFT LANES
- TRAFFIC FLOW DIRECTION
- DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
- PROPOSED WATERMAIN
- INPLACE DRAINAGE PIPES

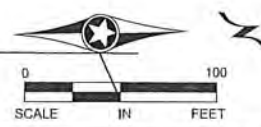


MATCH LINE 135+71
SHEET 45



STAGE 3 CONSTRUCTION NOTES:
 REMOVE MEDIAN AND PLACE TEMPORARY PAVEMENT NEAR DOGWOOD FIRST. CONSTRUCT WEST SIDE OF FOLEY NORTH OF WATER TOWER. CONSTRUCT SOUTH SIDE OF NORTHDAL.

STAGE 3 TRAFFIC NOTES:
 SHIFT FOLEY TRAFFIC TO EAST. SHIFT NORTHDAL TRAFFIC TO THE NORTH.



MATCH LINE 148+06
SHEET 46

SHEET 45
MATCH LINE 135+71

MATCH LINE 212+00.00
SHEET 47

MATCH LINE 207+05.73
SHEET 47

NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG3_P2.dgn 05/13/2016 2:04:05 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

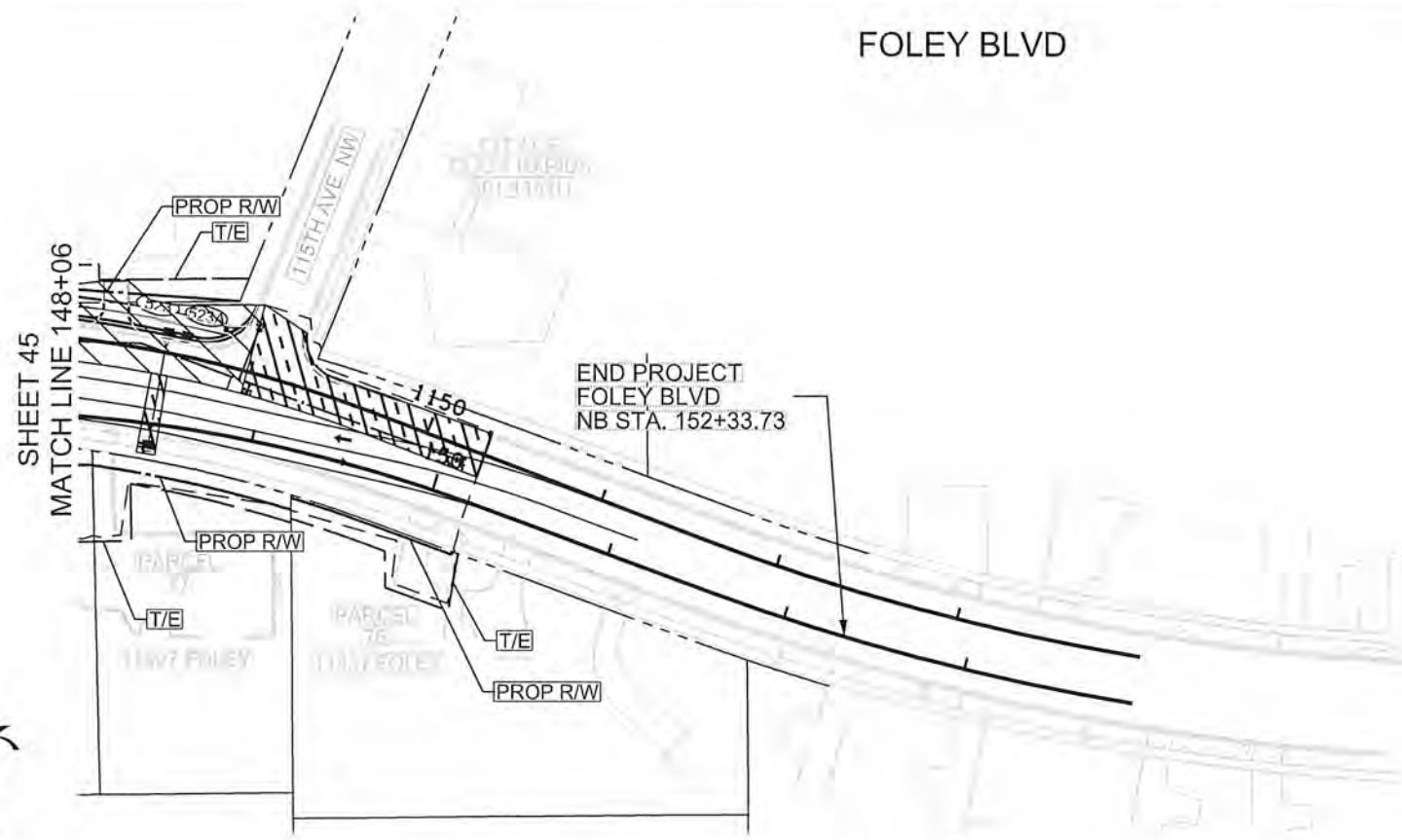


**ANOKA COUNTY
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 SP 114-020-050
 CP 13-10

**CONSTRUCTION STAGING PLAN
 STAGE 3**
 STA 123+36 TO 148+06
 Sheet 45 of 298 Sheets

FOLEY BLVD



LEGEND	
	WORK AREA
	WORK AREA (UNDER TRAFFIC)
	TEMPORARY PAVEMENT
	EXISTING TOPOGRAPHY
	CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
	COMPLETED CONSTRUCTION
	TRAFFIC SHIFT LANES
	TRAFFIC FLOW DIRECTION
	DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
	PROPOSED WATERMAIN
	INPLACE DRAINAGE PIPES

STAGE 3 CONSTRUCTION NOTES:
 REMOVE MEDIAN AND PLACE TEMPORARY PAVEMENT NEAR DOGWOOD FIRST. CONSTRUCT WEST SIDE OF FOLEY NORTH OF WATER TOWER. CONSTRUCT SOUTH SIDE OF NORTHDALE.

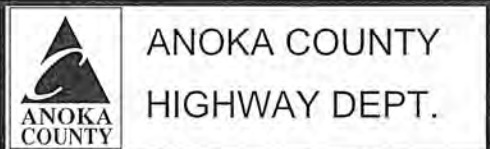
STAGE 3 TRAFFIC NOTES:
 SHIFT FOLEY TRAFFIC TO EAST. SHIFT NORTHDALE TRAFFIC TO THE NORTH.

NO	DATE	BY	CKD	APPR	REVISION

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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

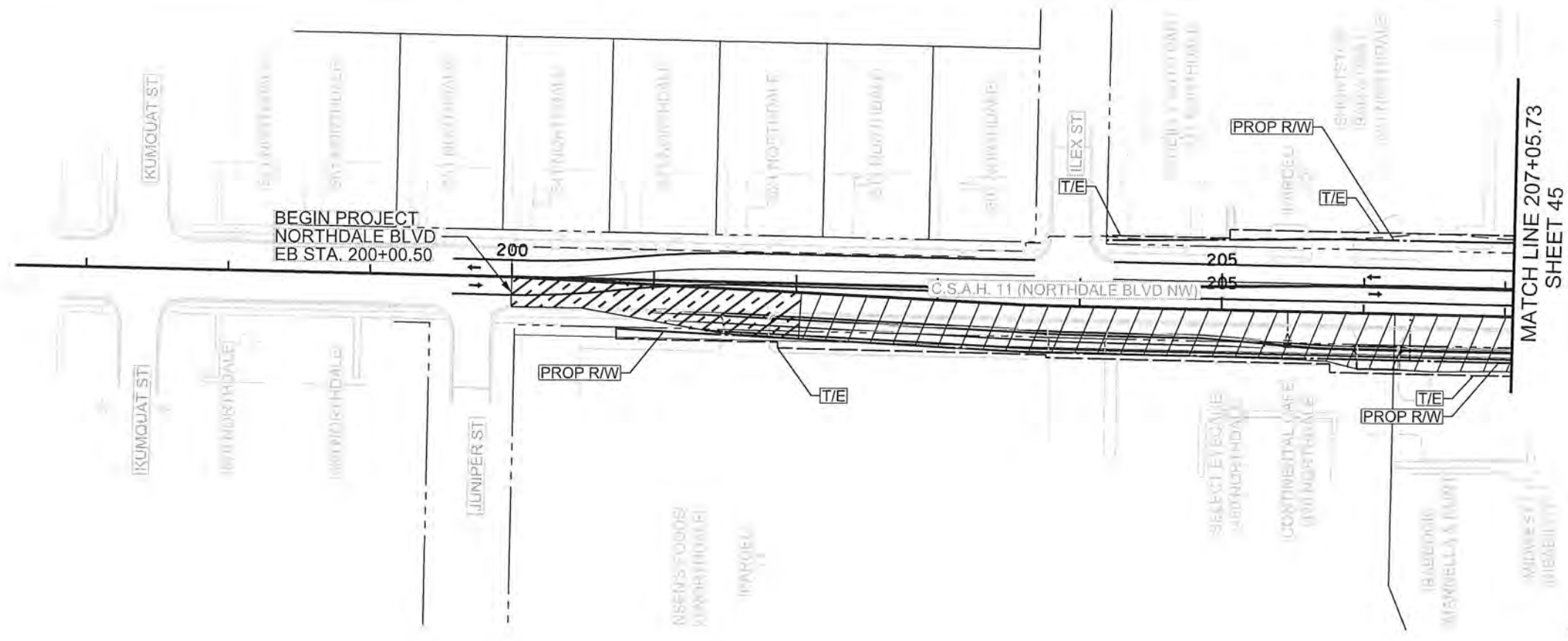
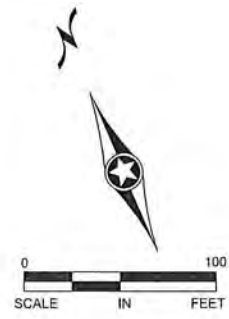
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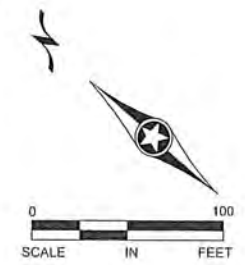
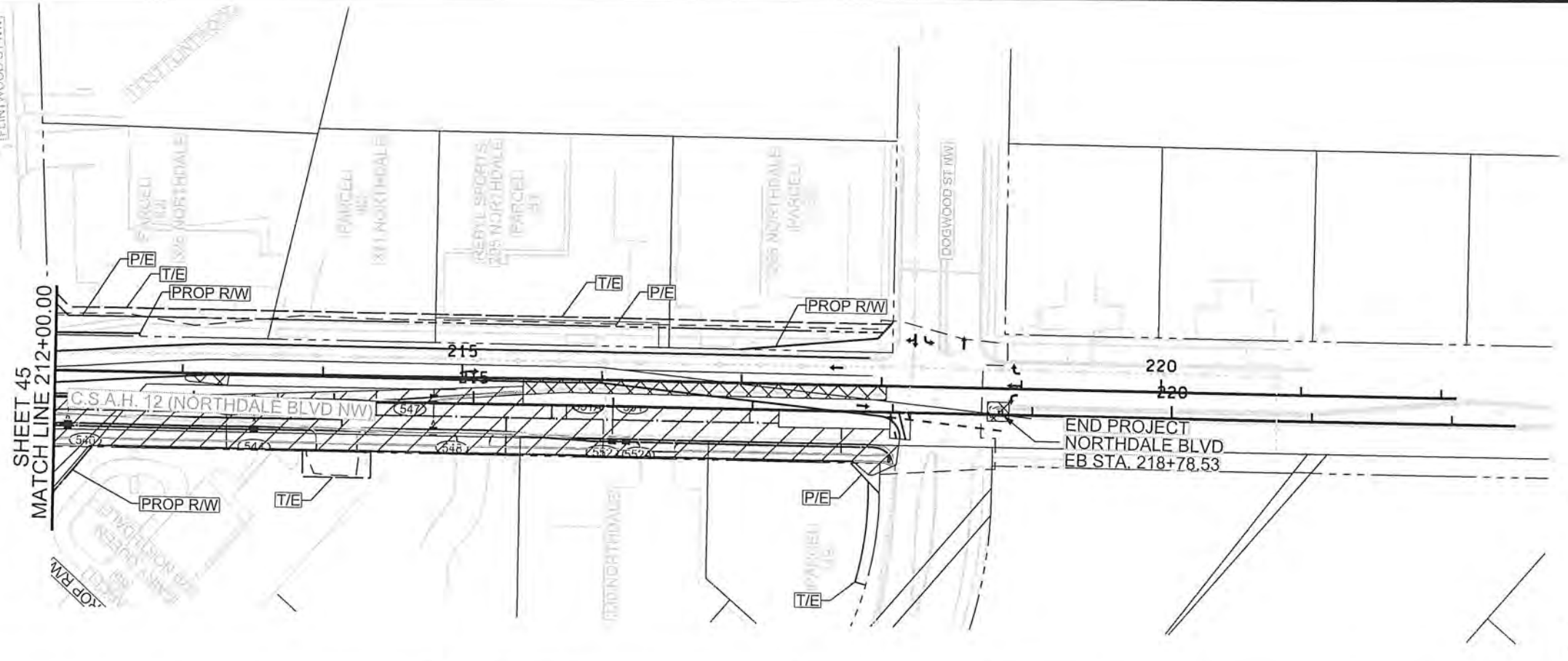
CONSTRUCTION STAGING PLAN
 STAGE 3
 FOLEY BLVD
 Sheet 46 of 298 Sheets

LEGEND	
	WORK AREA
	WORK AREA (UNDER TRAFFIC)
	TEMPORARY PAVEMENT
	EXISTING TOPOGRAPHY
	CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
	COMPLETED CONSTRUCTION
	TRAFFIC SHIFT LANES
	TRAFFIC FLOW DIRECTION
	DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
	PROPOSED WATERMAIN
	INPLACE DRAINAGE PIPES



STAGE 3 CONSTRUCTION NOTES:
 REMOVE MEDIAN AND PLACE TEMPORARY PAVEMENT NEAR DOGWOOD FIRST. CONSTRUCT WEST SIDE OF FOLEY NORTH OF WATER TOWER. CONSTRUCT SOUTH SIDE OF NORTDALE.

STAGE 3 TRAFFIC NOTES:
 SHIFT FOLEY TRAFFIC TO EAST. SHIFT NORTDALE TRAFFIC TO THE NORTH.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG3_P4.dgn 05/13/2016 2:04:09 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-19-14 LICENSE NO. 22713

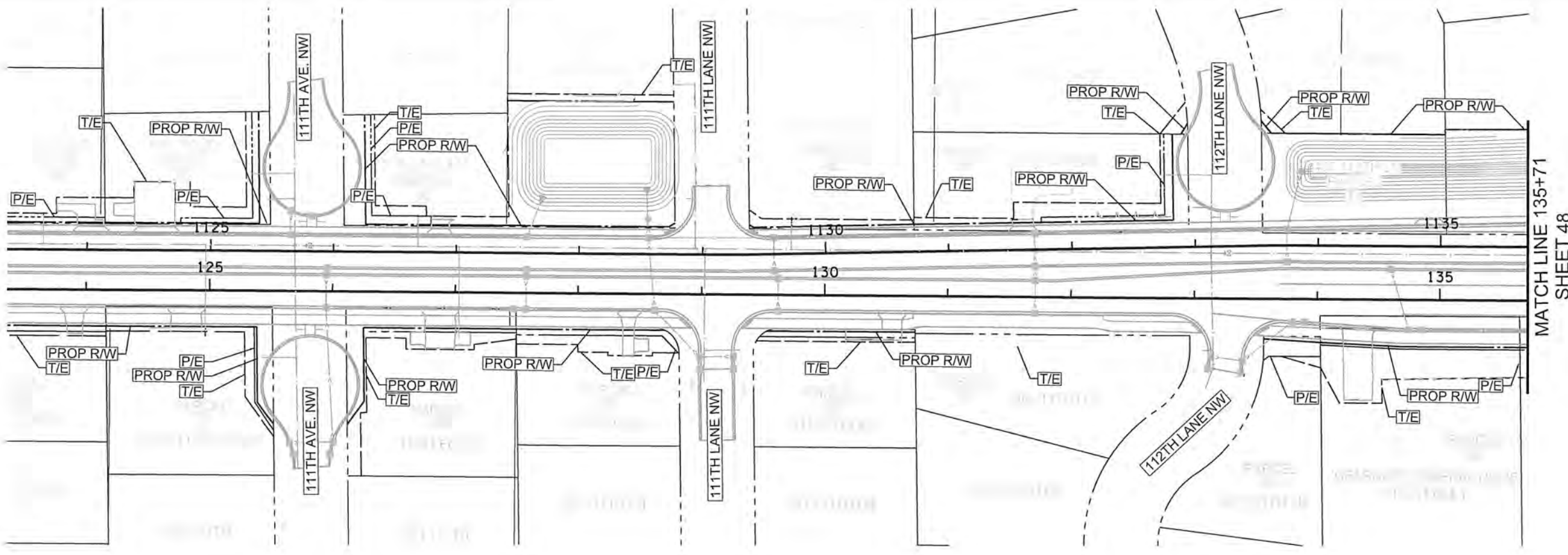
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 CHECKED BY: JEO DATE: 02/27/16



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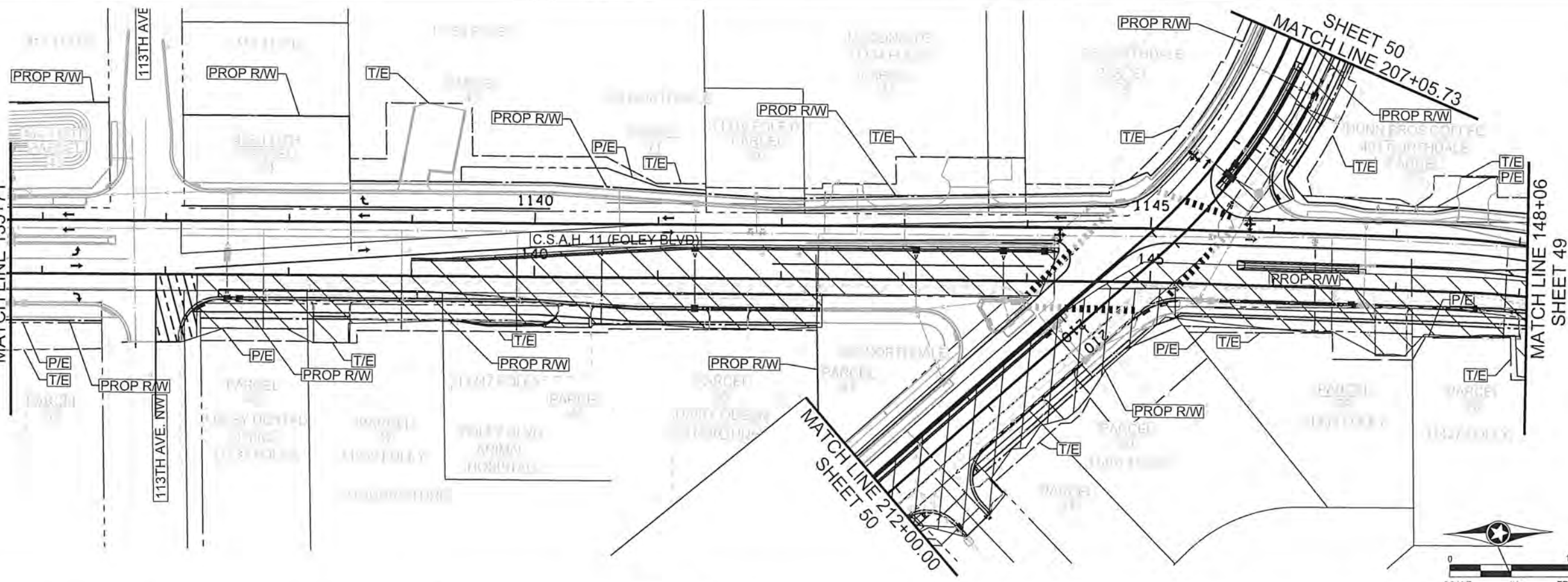
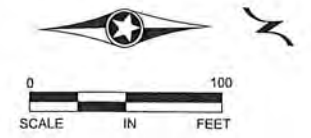
SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 3
 STA 196+50 TO 218+04.77
 Sheet 47 of 298 Sheets



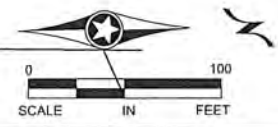
LEGEND

- WORK AREA
- WORK AREA (UNDER TRAFFIC)
- EXISTING TOPOGRAPHY
- CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
- COMPLETED CONSTRUCTION
- TRAFFIC SHIFT LANES
- TRAFFIC FLOW DIRECTION
- DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
- PROPOSED WATERMAIN
- INPLACE DRAINAGE PIPES



STAGE 4 CONSTRUCTION NOTES:
 CONSTRUCT EAST SIDE OF FOLEY BLVD NORTH OF 113TH AVE. CONSTRUCT NORTH SIDE OF NORTHDALE.

STAGE 4 TRAFFIC NOTES:
 SHIFT TRAFFIC TO WEST SIDE OF FOLEY BETWEEN 113TH AVE AND END OF PAVEMENT ON NORTHDALE BLVD. MAINTAIN ACCESS TO DRIVEWAYS.



NO	DATE	BY	CKD	APPR	REVISION
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PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

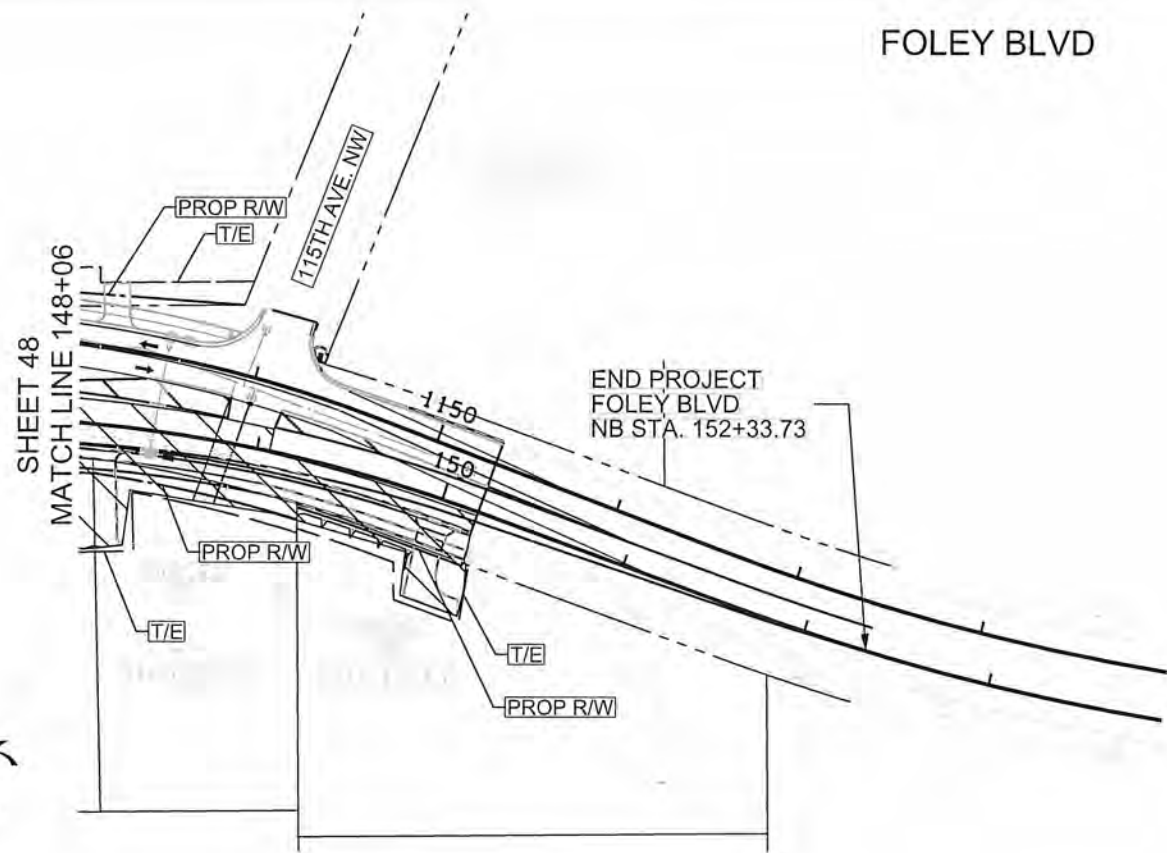
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SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 4
 STA 123+36 TO 148+06
 Sheet 48 of 298 Sheets

FOLEY BLVD



LEGEND	
	WORK AREA
	WORK AREA (UNDER TRAFFIC)
	EXISTING TOPOGRAPHY
	CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
	COMPLETED CONSTRUCTION
	TRAFFIC SHIFT LANES
	TRAFFIC FLOW DIRECTION
	DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
	PROPOSED WATERMAIN
	INPLACE DRAINAGE PIPES

STAGE 4 CONSTRUCTION NOTES:
 CONSTRUCT EAST SIDE OF FOLEY BLVD NORTH OF 113TH AVE. CONSTRUCT NORTH SIDE OF NORTHDAL.

STAGE 4 TRAFFIC NOTES:
 SHIFT TRAFFIC TO WEST SIDE OF FOLEY BETWEEN 113TH AVE AND END OF PROJECT. SHIFT TRAFFIC TO NEW PAVEMENT ON NORTHDAL BLVD. MAINTAIN ACCESS TO DRIVEWAYS.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG4_P3.dgn 05/19/2016 2:24:49 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16













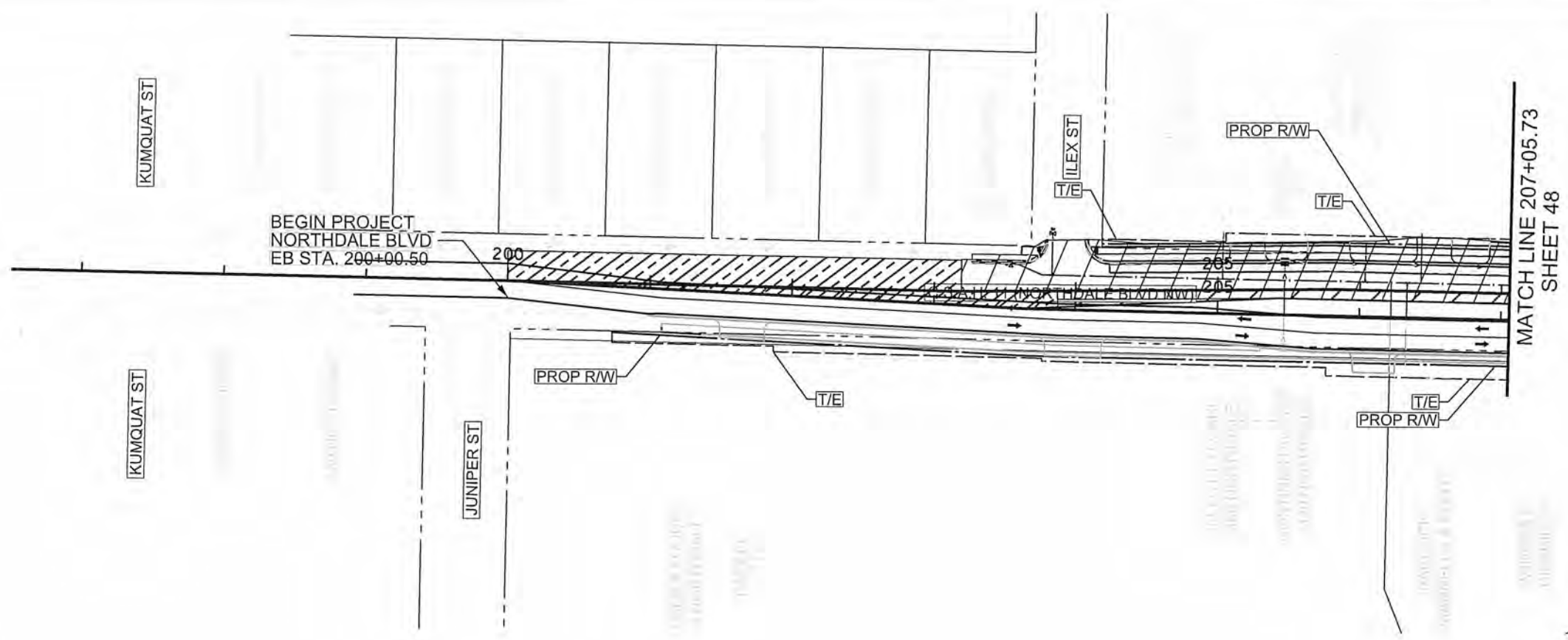
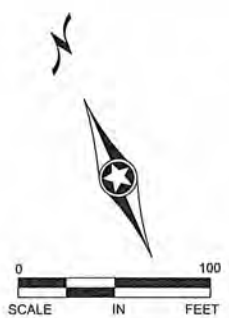
ANOKA COUNTY
 HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 4
 FOLEY BLVD
 Sheet 49 of 298 Sheets

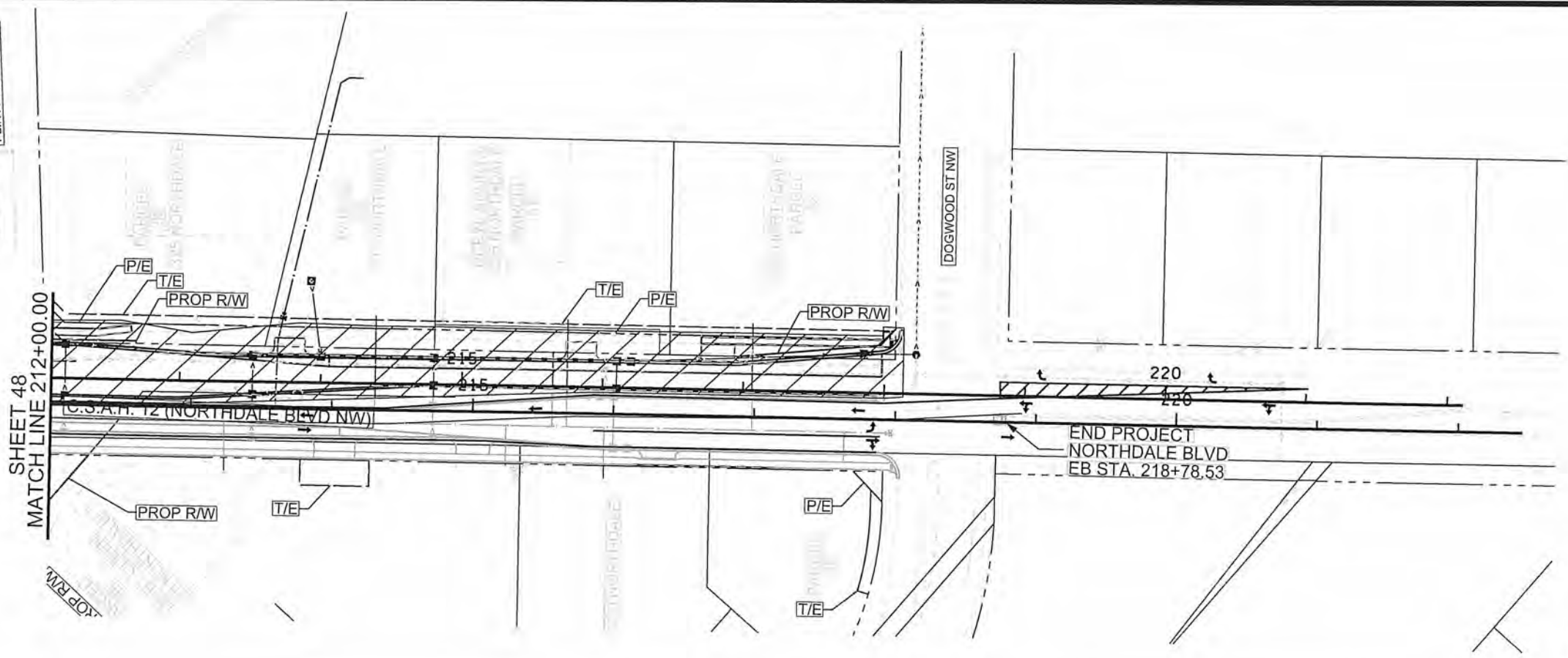
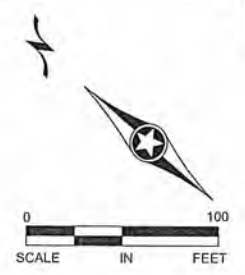
LEGEND

-  WORK AREA
-  WORK AREA (UNDER TRAFFIC)
-  EXISTING TOPOGRAPHY
-  CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
-  COMPLETED CONSTRUCTION
-  TRAFFIC SHIFT LANES
-  TRAFFIC FLOW DIRECTION
-  DRAINAGE TO BE COMPLETED DURING CURRENT STAGE
-  PROPOSED WATERMAIN
-  INPLACE DRAINAGE PIPES



STAGE 4 CONSTRUCTION NOTES:
 CONSTRUCT EAST SIDE OF FOLEY BLVD NORTH OF 113TH AVE. CONSTRUCT NORTH SIDE OF NORTHDAL.

STAGE 4 TRAFFIC NOTES:
 SHIFT TRAFFIC TO WEST SIDE OF FOLEY BETWEEN 113TH AVE AND END OF PROJECT. SHIFT TRAFFIC TO NEW PAVEMENT ON NORTHDAL BLVD. MAINTAIN ACCESS TO DRIVEWAYS.




NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG4_P4.dgn 05/19/2016 2:24:54 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
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DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16








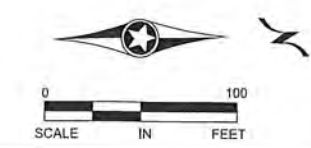
**ANOKA COUNTY
 HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

**CONSTRUCTION STAGING PLAN
 STAGE 4**
 STA 196+50 TO 218+04.77
 Sheet 50 of 298 Sheets

LEGEND

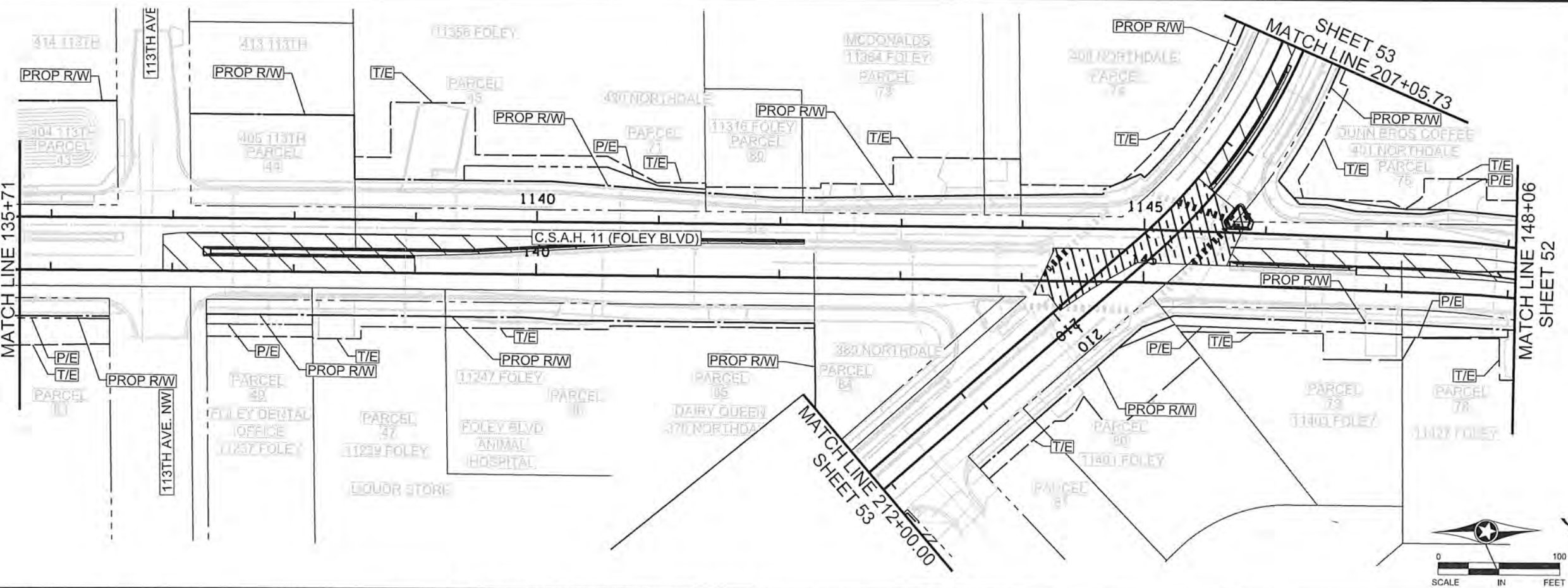
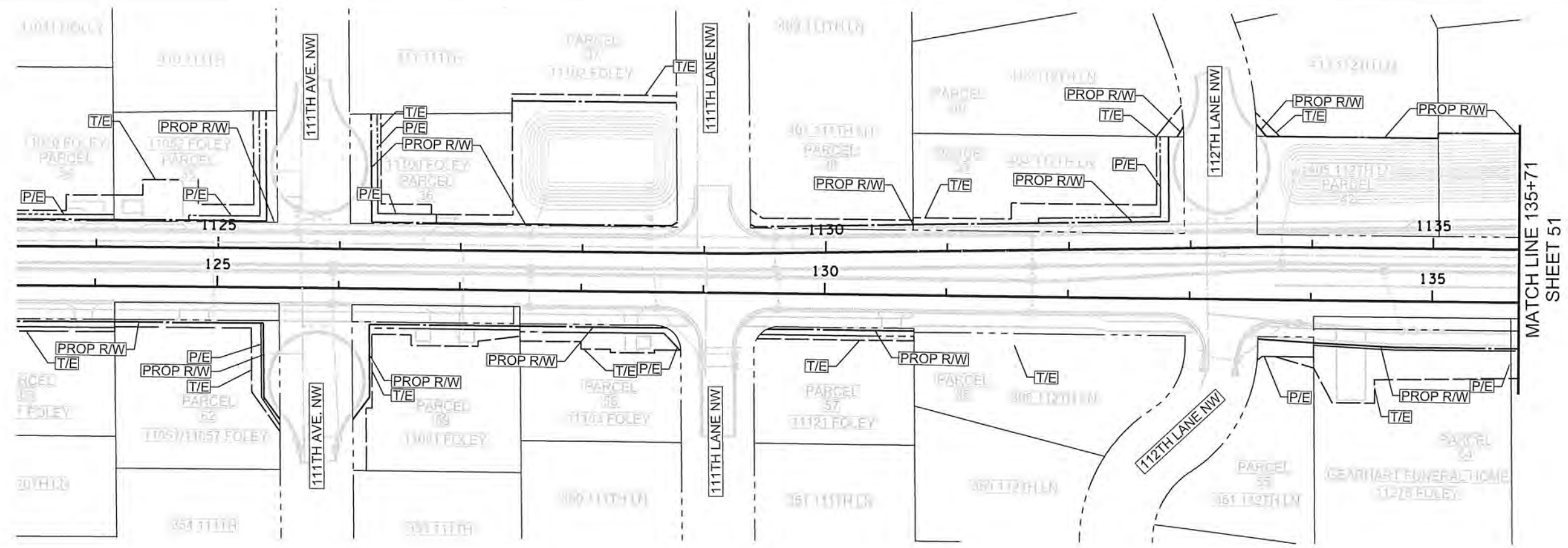
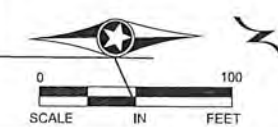
-  WORK AREA
-  WORK AREA (UNDER TRAFFIC)
-  EXISTING TOPOGRAPHY
-  CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
-  COMPLETED CONSTRUCTION



SCALE IN FEET

STAGE 5 CONSTRUCTION NOTES:
 CONSTRUCT REMAINING LEFT TURN LANES AND MEDIANS. CONSTRUCT PORKCHOP ISLAND AT NORTHWEST CORNER OF NORTHDALE BLVD AND FOLEY BLVD INTERSECTION.

STAGE 5 TRAFFIC NOTES:
 SHIFT TRAFFIC TO OUTSIDE SO CENTER CAN BE CONSTRUCTED.

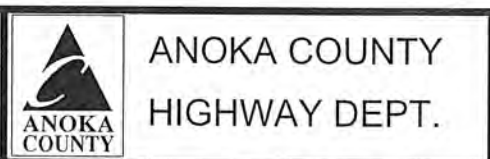
SCALE IN FEET

NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:02-611-34\Plan\002-611-034_STG5_P2.dgn 05/13/2016 2:04:17 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-9-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16








**ANOKA COUNTY
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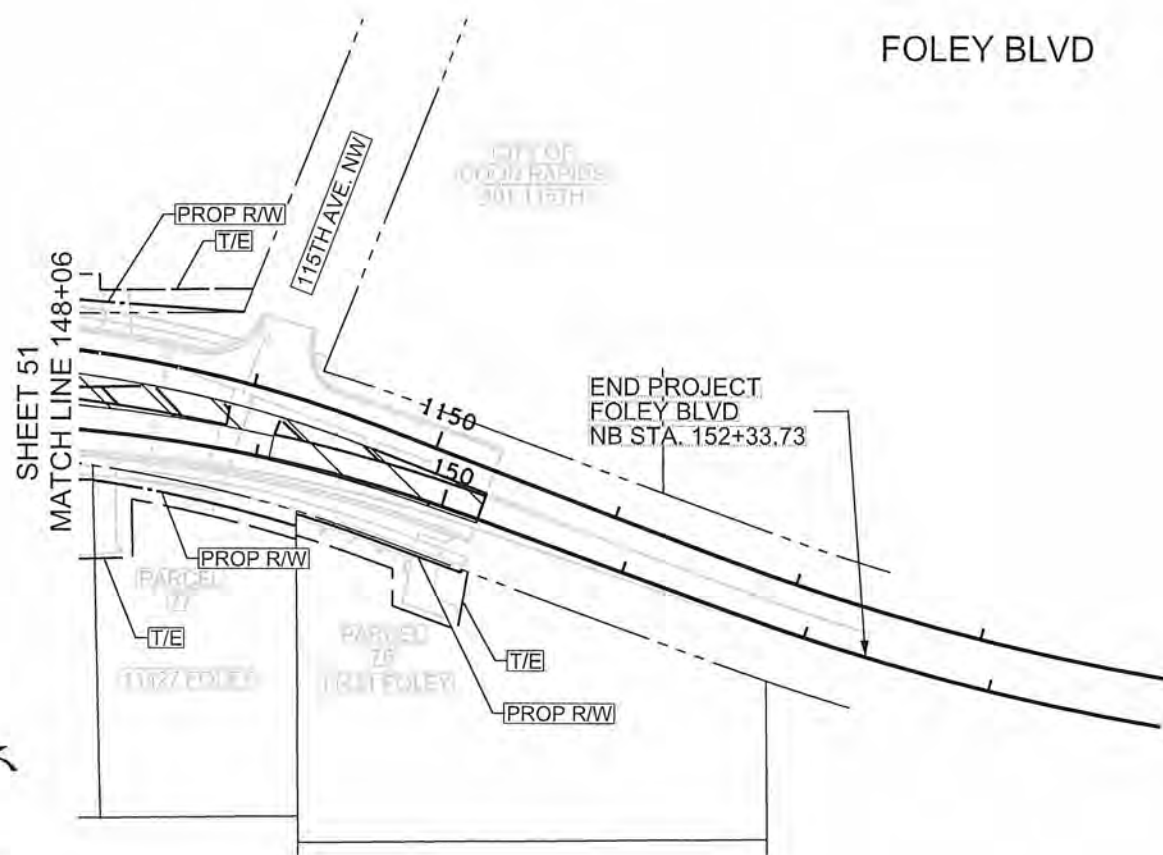
SP 002-611-034
 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 5
 STA 123+36 TO 148+06
 Sheet 51 of 298 Sheets

LEGEND

-  WORK AREA
-  WORK AREA (UNDER TRAFFIC)
-  EXISTING TOPOGRAPHY
-  CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
-  COMPLETED CONSTRUCTION

FOLEY BLVD



STAGE 5 CONSTRUCTION NOTES:
 CONSTRUCT REMAINING LEFT TURN LANES AND MEDIANS. CONSTRUCT PORKCHOP ISLAND AT NORTHWEST CORNER OF NORTHDAL BLVD AND FOLEY BLVD INTERSECTION.

STAGE 5 TRAFFIC NOTES:
 SHIFT TRAFFIC TO OUTSIDE SO CENTER CAN BE CONSTRUCTED.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_STG5_P3.dgn 05/13/2016 2:04:18 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16








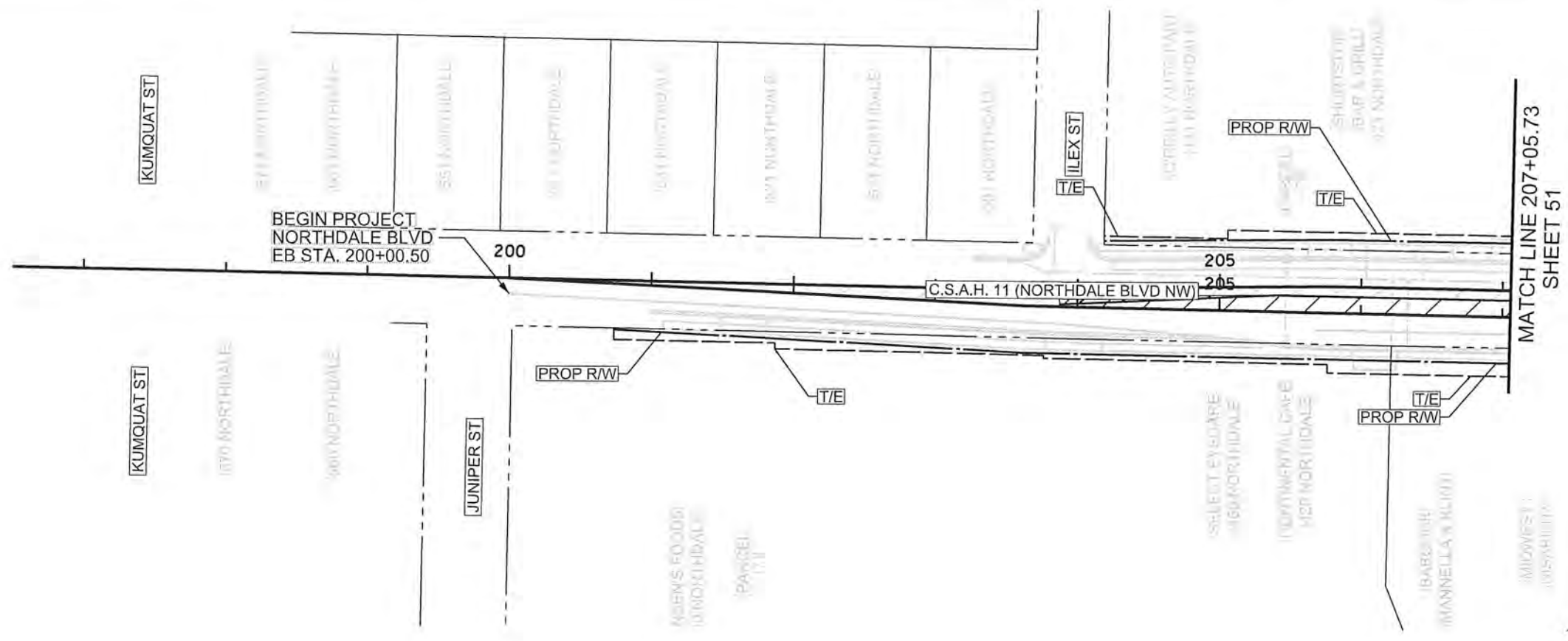
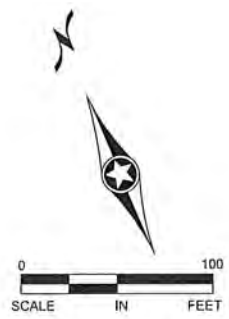
ANOKA COUNTY
 HIGHWAY DEPT.

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 SP 114-020-050
 CP 13-10

CONSTRUCTION STAGING PLAN
 STAGE 5
 FOLEY BLVD
 & 113TH AVE/DOGWOOD ST
 Sheet 52 of 298 Sheets

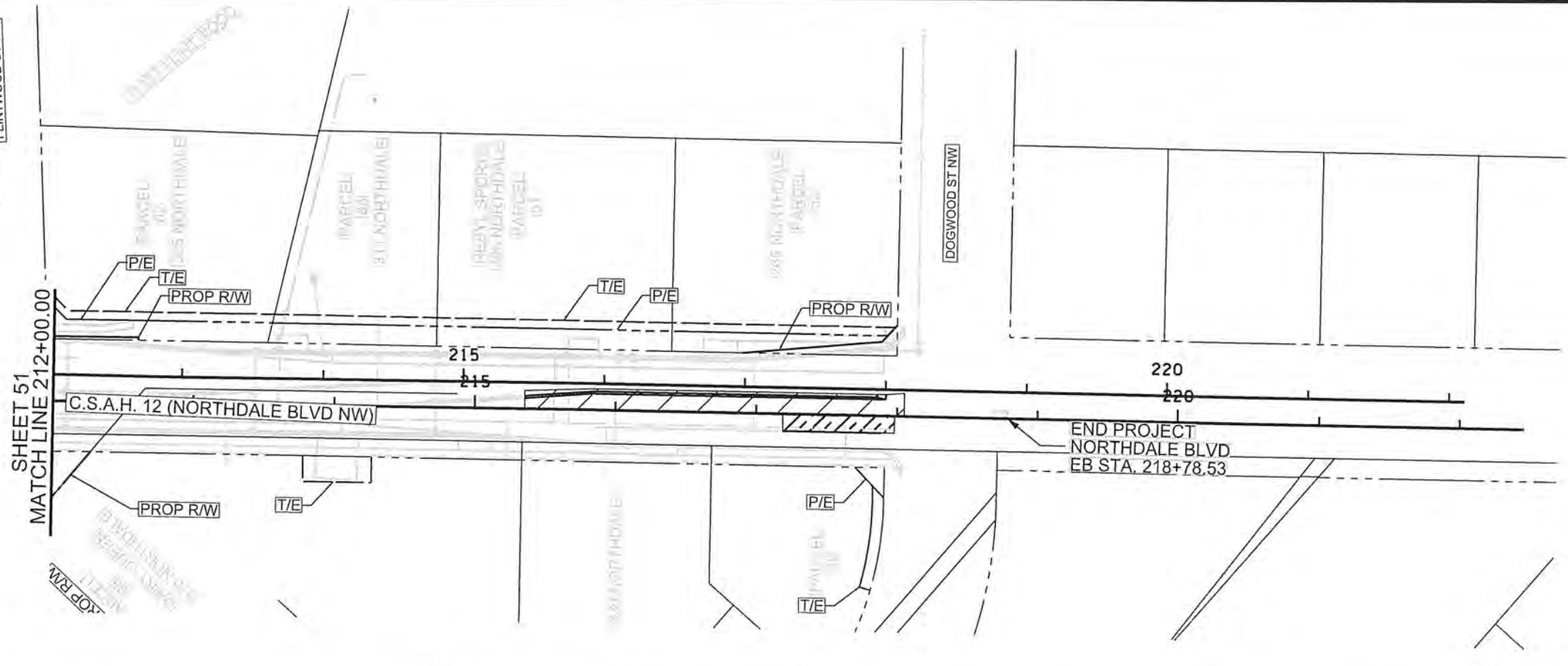
LEGEND

-  WORK AREA
-  WORK AREA (UNDER TRAFFIC)
-  EXISTING TOPOGRAPHY
-  CONSTRUCTION TO BE COMPLETED DURING CURRENT STAGE
-  COMPLETED CONSTRUCTION



STAGE 5 CONSTRUCTION NOTES:
 CONSTRUCT REMAINING LEFT TURN LANES AND MEDIANS. CONSTRUCT PORKCHOP ISLAND AT NORTHWEST CORNER OF NORTHDALE BLVD AND FOLEY BLVD INTERSECTION.

STAGE 5 TRAFFIC NOTES:
 SHIFT TRAFFIC TO OUTSIDE SO CENTER CAN BE CONSTRUCTED.



NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_STG5_P4.dgn					
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 SIGNATURE: *[Signature]*
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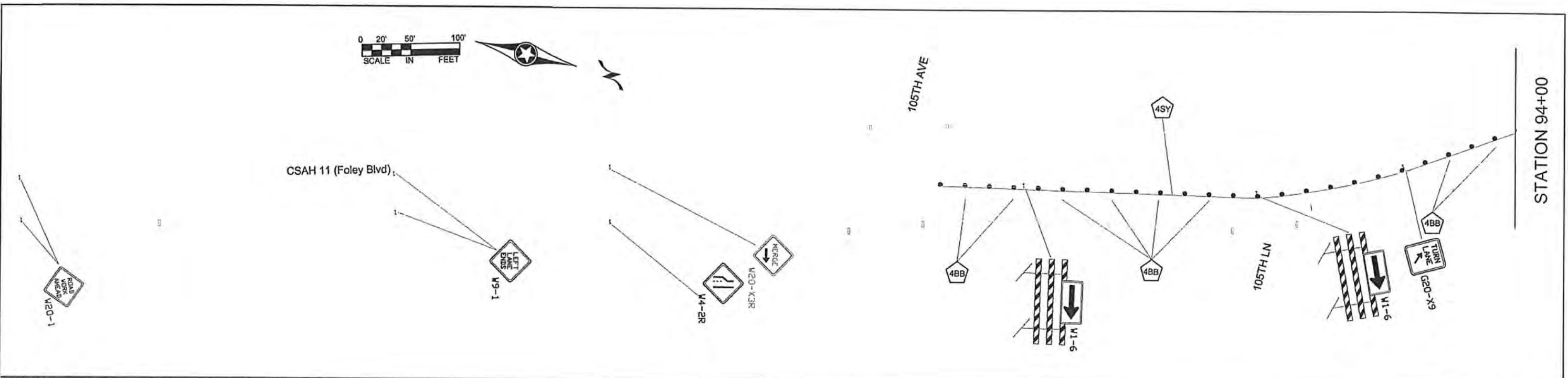
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 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



**ANOKA COUNTY
 HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

**CONSTRUCTION STAGING PLAN
 STAGE 5**
 STA 196+50 TO 218+04.77
 Sheet 53 of 298 Sheets

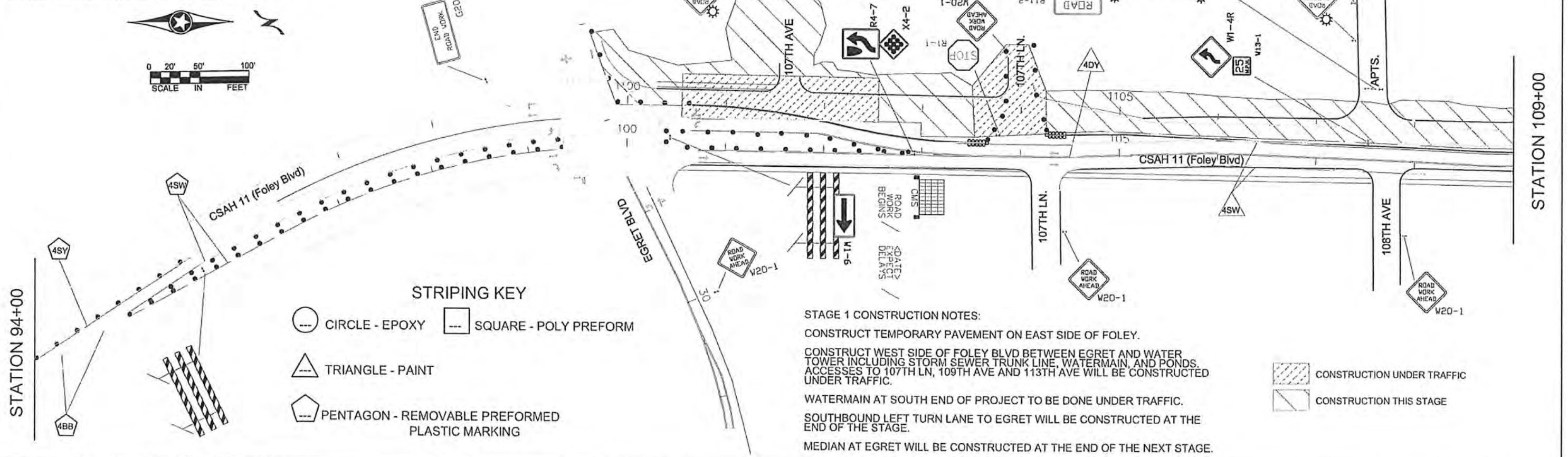


STAGE 1 TRAFFIC NOTES:

SHIFT TRAFFIC TO EAST TO USE TEMPORARY PAVEMENT. PLACE TEMPORARY CONCRETE BARRIER. MAINTAIN ACCESS TO DRIVEWAYS. CLOSE ONE NORTHBOUND THRU LANE SOUTH OF EGRET. SOUTHBOUND TRAFFIC TO BE SHIFTED BACK TO THE SOUTHBOUND THRU LANE AT EGRET INTERSECTION.

TO CONSTRUCT THE SOUTHBOUND LEFT TURN LANE SHIFT SOUTHBOUND TRAFFIC TO THE STAGE 2 CONFIGURATION.

INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).



STAGE 1 CONSTRUCTION NOTES:

CONSTRUCT TEMPORARY PAVEMENT ON EAST SIDE OF FOLEY.
 CONSTRUCT WEST SIDE OF FOLEY BLVD BETWEEN EGRET AND WATER TOWER INCLUDING STORM SEWER TRUNK LINE, WATERMAIN, AND PONDS. ACCESSES TO 107TH LN, 109TH AVE AND 113TH AVE WILL BE CONSTRUCTED UNDER TRAFFIC.
 WATERMAIN AT SOUTH END OF PROJECT TO BE DONE UNDER TRAFFIC.
 SOUTHBOUND LEFT TURN LANE TO EGRET WILL BE CONSTRUCTED AT THE END OF THE STAGE.
 MEDIAN AT EGRET WILL BE CONSTRUCTED AT THE END OF THE NEXT STAGE.

NO	DATE	BY	CKD	APPR	REVISION

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 5/21/16 REG. NO. 20235

DRAWN BY: RLB DATE: 1/15/14
 DESIGN BY: RLB DATE: 1/15/14
 CHECKED BY: JR DATE: 1/15/14

ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

STAGE 1 B
 TRAFFIC CONTROL
 LAYOUT
 Sheet 54 of 298 Sheets

STAGE 1 TRAFFIC NOTES:

SHIFT TRAFFIC TO EAST TO USE TEMPORARY PAVEMENT. PLACE TEMPORARY CONCRETE BARRIER. MAINTAIN ACCESS TO DRIVEWAYS. CLOSE ONE NORTHBOUND THRU LANE SOUTH OF EGRET. SOUTHBOUND TRAFFIC TO BE SHIFTED BACK TO THE SOUTHBOUND THRU LANE AT EGRET INTERSECTION.

TO CONSTRUCT THE SOUTHBOUND LEFT TURN LANE SHIFT SOUTHBOUND TRAFFIC TO THE STAGE 2 CONFIGURATION.

INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).

STAGE 1 CONSTRUCTION NOTES:

CONSTRUCT TEMPORARY PAVEMENT ON EAST SIDE OF FOLEY.





CONSTRUCT WEST SIDE OF FOLEY BLVD BETWEEN EGRET AND WATER TOWER INCLUDING STORM SEWER TRUNK LINE, WATERMAIN, AND PONDS. ACCESS TO 107TH LN, 109TH AVE AND 113TH AVE WILL BE CONSTRUCTED UNDER TRAFFIC.

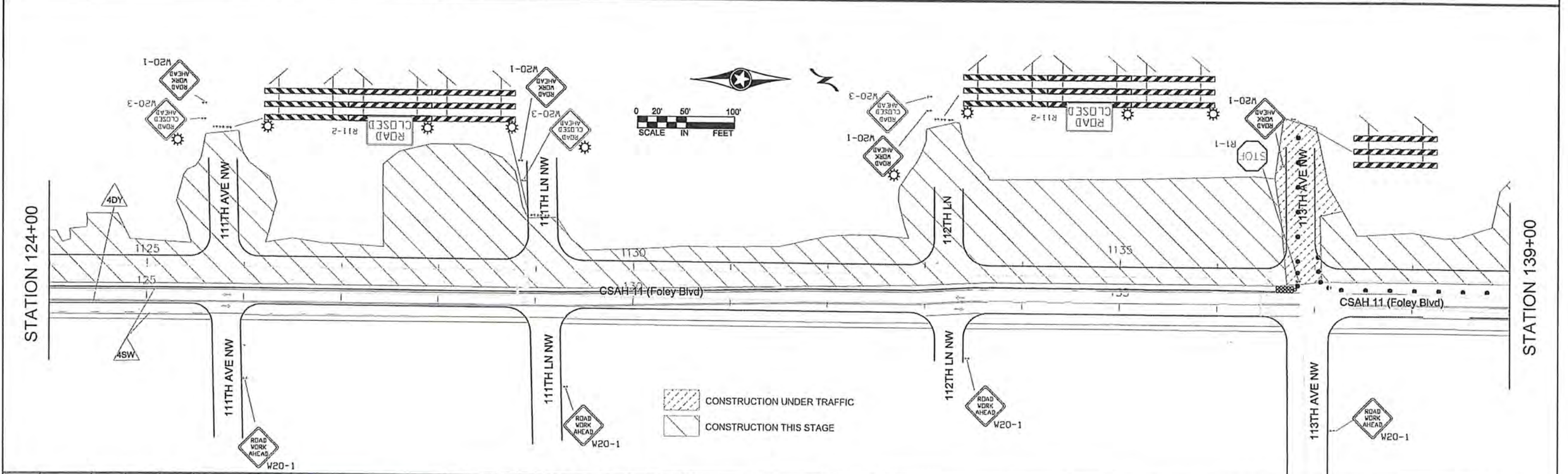
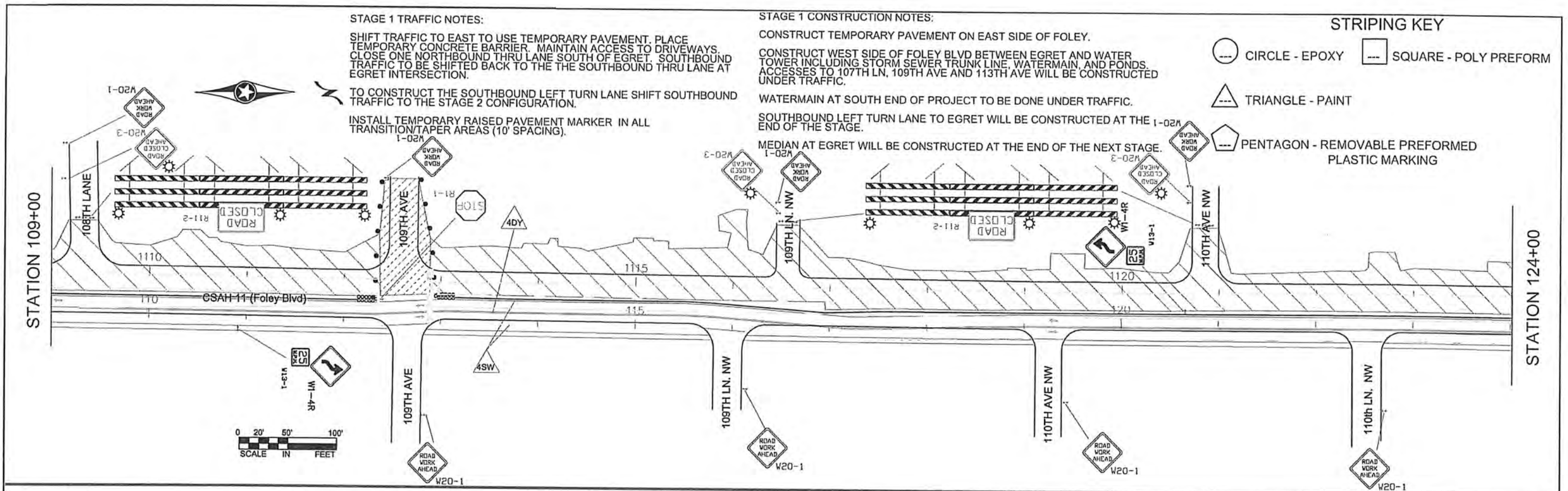
WATERMAIN AT SOUTH END OF PROJECT TO BE DONE UNDER TRAFFIC.

SOUTHBOUND LEFT TURN LANE TO EGRET WILL BE CONSTRUCTED AT THE END OF THE STAGE.

MEDIAN AT EGRET WILL BE CONSTRUCTED AT THE END OF THE NEXT STAGE.

STRIPING KEY

-  CIRCLE - EPOXY
-  SQUARE - POLY PREFORM
-  TRIANGLE - PAINT
-  PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING



NO	DATE	BY	CKD	APPR	REVISION

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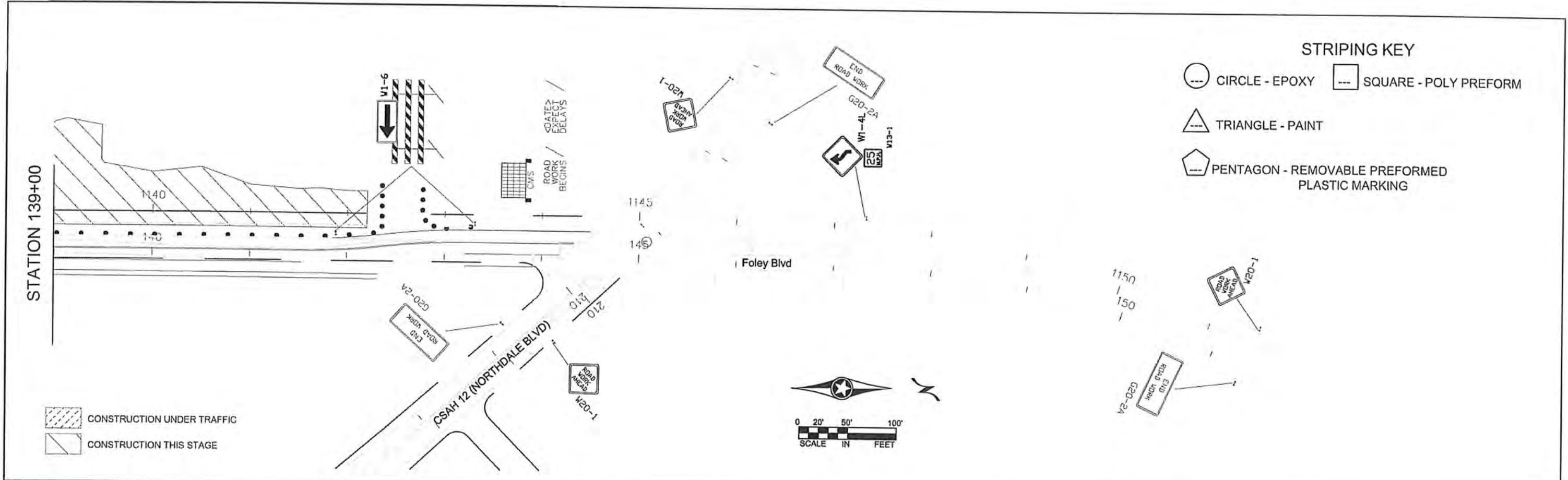
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 DESIGN BY: RLB DATE: 1/15/14
 CHECKED BY: JR DATE: 1/15/14



**ANOKA COUNTY
HIGHWAY DEPT.**

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

**STAGE 1 B
TRAFFIC CONTROL
LAYOUT**
 Sheet 55 of 298 Sheets



STAGE 1 TRAFFIC NOTES:

SHIFT TRAFFIC TO EAST TO USE TEMPORARY PAVEMENT. PLACE TEMPORARY CONCRETE BARRIER. MAINTAIN ACCESS TO DRIVEWAYS. CLOSE ONE NORTHBOUND THRU LANE SOUTH OF EGRET. SOUTHBOUND TRAFFIC TO BE SHIFTED BACK TO THE SOUTHBOUND THRU LANE AT EGRET INTERSECTION.

TO CONSTRUCT THE SOUTHBOUND LEFT TURN LANE SHIFT SOUTHBOUND TRAFFIC TO THE STAGE 2 CONFIGURATION.

INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).

STAGE 1 CONSTRUCTION NOTES:

CONSTRUCT TEMPORARY PAVEMENT ON EAST SIDE OF FOLEY.

CONSTRUCT WEST SIDE OF FOLEY BLVD BETWEEN EGRET AND WATER TOWER INCLUDING STORM SEWER TRUNK LINE, WATERMAIN, AND PONDS. ACCESS TO 107TH LN, 109TH AVE AND 113TH AVE WILL BE CONSTRUCTED UNDER TRAFFIC.

WATERMAIN AT SOUTH END OF PROJECT TO BE DONE UNDER TRAFFIC.

SOUTHBOUND LEFT TURN LANE TO EGRET WILL BE CONSTRUCTED AT THE END OF THE STAGE.

MEDIAN AT EGRET WILL BE CONSTRUCTED AT THE END OF THE NEXT STAGE.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\002-611-3-032\Bases\TRAFFIC\002-611-032_STG2a.dwg

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 SIGNATURE: *[Signature]*
 DATE: 1/15/14 REG. NO. 20235

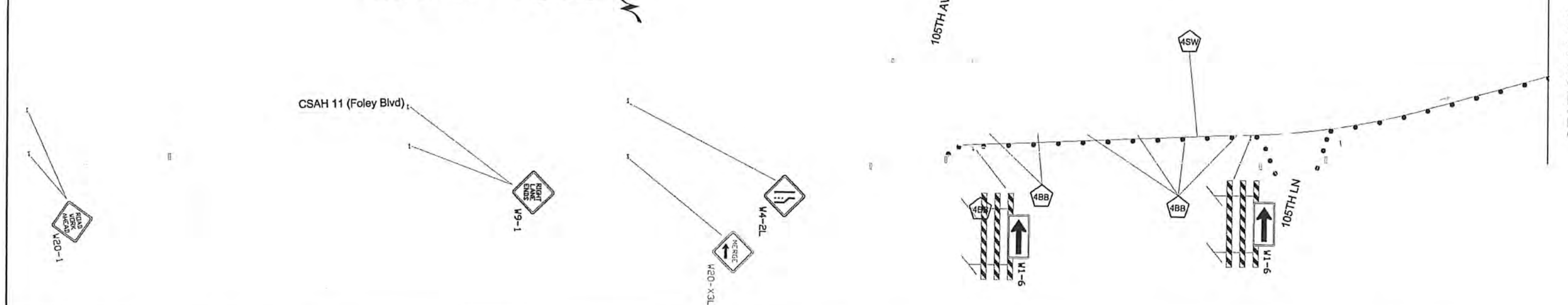
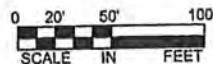
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**ANOKA COUNTY
HIGHWAY DEPT.**

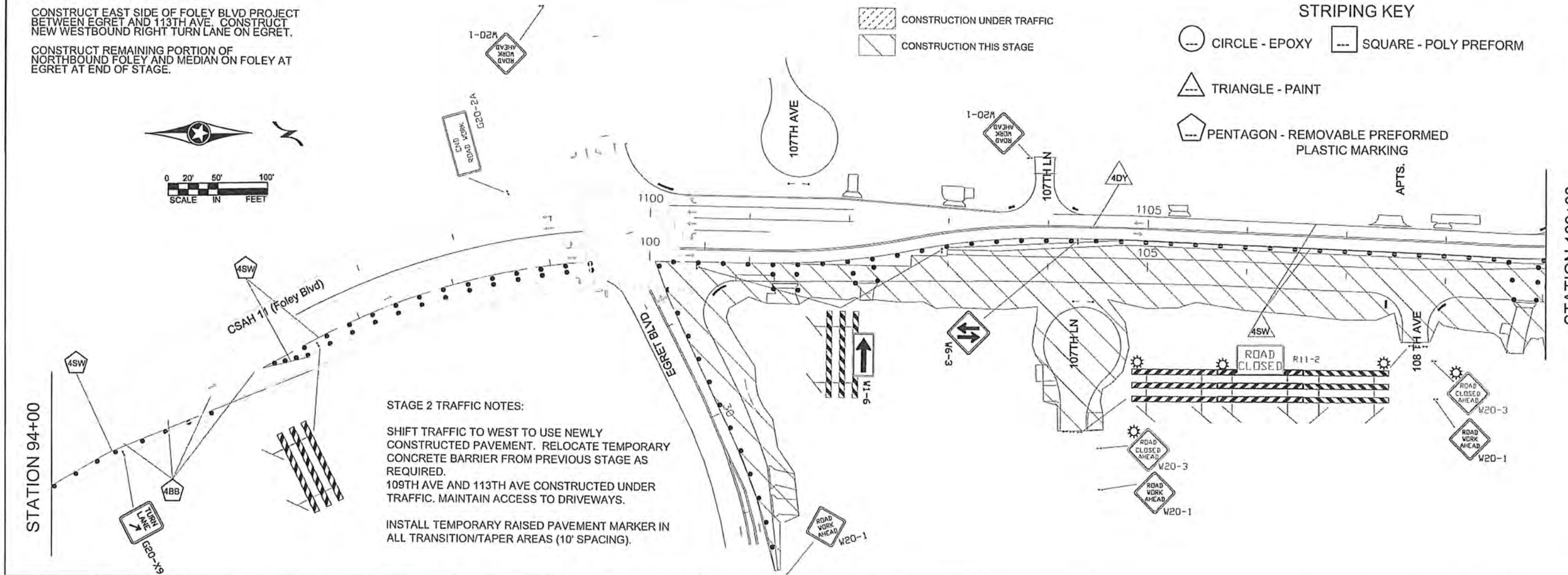
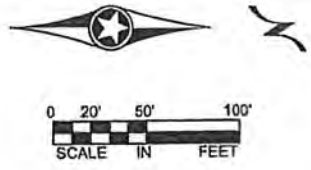
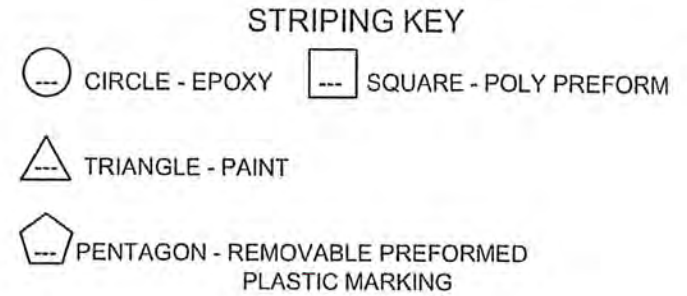
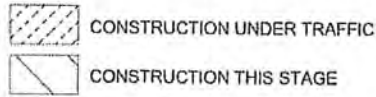
STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

STAGE 1B
 TRAFFIC CONTROL
 LAYOUT
 Sheet 56 of 298 Sheets



STAGE 2 CONSTRUCTION NOTES:

CONSTRUCT EAST SIDE OF FOLEY BLVD PROJECT BETWEEN EGRET AND 113TH AVE. CONSTRUCT NEW WESTBOUND RIGHT TURN LANE ON EGRET.
 CONSTRUCT REMAINING PORTION OF NORTHBOUND FOLEY AND MEDIAN ON FOLEY AT EGRET AT END OF STAGE.



STAGE 2 TRAFFIC NOTES:

SHIFT TRAFFIC TO WEST TO USE NEWLY CONSTRUCTED PAVEMENT. RELOCATE TEMPORARY CONCRETE BARRIER FROM PREVIOUS STAGE AS REQUIRED.
 109TH AVE AND 113TH AVE CONSTRUCTED UNDER TRAFFIC. MAINTAIN ACCESS TO DRIVEWAYS.
 INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\002-611-3-032\Bose\TRAFFIC\002-611-032_STG2a.dwg

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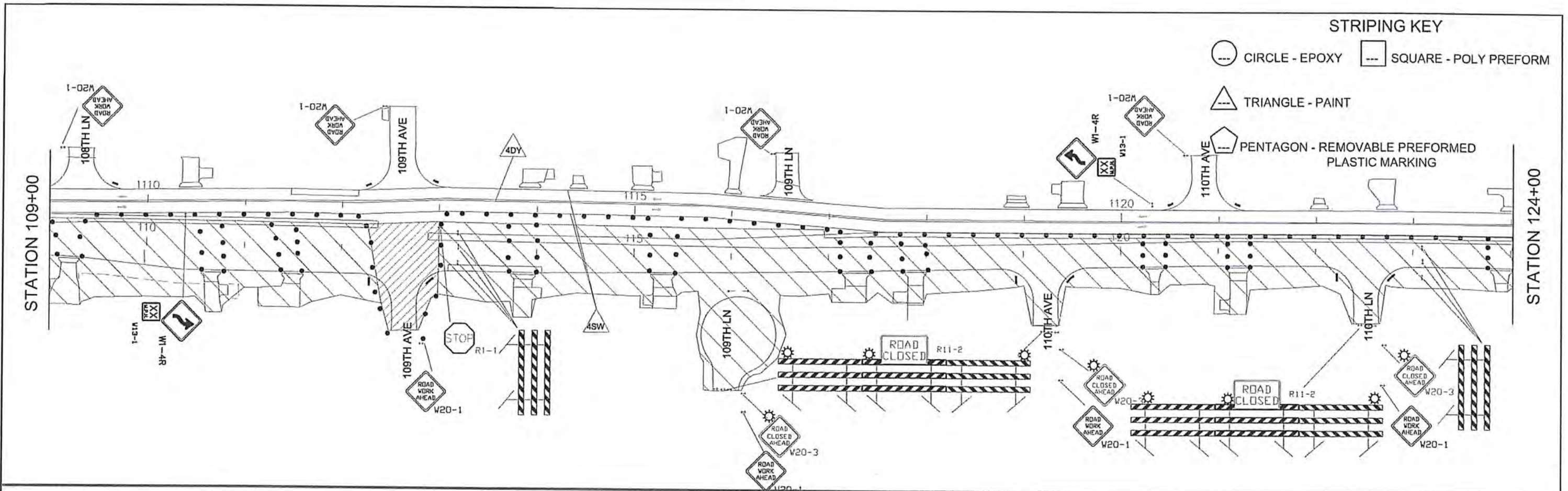
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 DESIGN BY: RLB DATE: 1/15/14
 CHECKED BY: JR DATE: 1/15/14



**ANOKA COUNTY
 HIGHWAY DEPT.**

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

STAGE 2
 TRAFFIC CONTROL
 LAYOUT
 Sheet 57 of 298 Sheets



STRIPING KEY

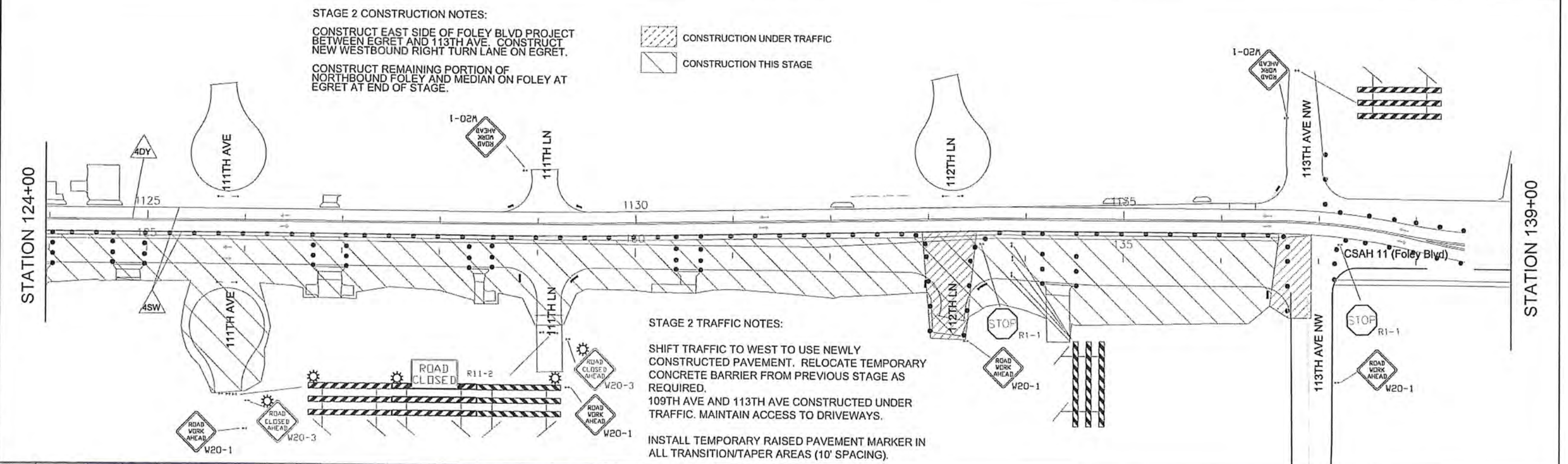
- CIRCLE - EPOXY
- SQUARE - POLY PREFORM
- TRIANGLE - PAINT
- PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING

STAGE 2 CONSTRUCTION NOTES:

CONSTRUCT EAST SIDE OF FOLEY BLVD PROJECT BETWEEN EGRET AND 113TH AVE. CONSTRUCT NEW WESTBOUND RIGHT TURN LANE ON EGRET.

CONSTRUCT REMAINING PORTION OF NORTHBOUND FOLEY AND MEDIAN ON FOLEY AT EGRET AT END OF STAGE.

- CONSTRUCTION UNDER TRAFFIC
- CONSTRUCTION THIS STAGE



STAGE 2 TRAFFIC NOTES:

SHIFT TRAFFIC TO WEST TO USE NEWLY CONSTRUCTED PAVEMENT. RELOCATE TEMPORARY CONCRETE BARRIER FROM PREVIOUS STAGE AS REQUIRED.
 109TH AVE AND 113TH AVE CONSTRUCTED UNDER TRAFFIC. MAINTAIN ACCESS TO DRIVEWAYS.

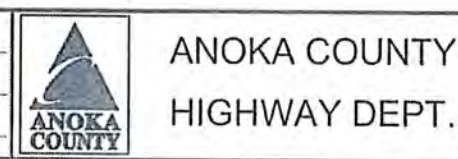
INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\002-611-3-032\TRAFFIC\002-611-032_STG2a.dwg

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STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

STAGE 2 TRAFFIC CONTROL LAYOUT
 Sheet 58 of 298 Sheets

STAGE 2 CONSTRUCTION NOTES:

CONSTRUCT EAST SIDE OF FOLEY BLVD PROJECT BETWEEN EGRET AND 113TH AVE. CONSTRUCT NEW WESTBOUND RIGHT TURN LANE ON EGRET.

CONSTRUCT REMAINING PORTION OF NORTHBOUND FOLEY AND MEDIAN ON FOLEY AT EGRET AT END OF STAGE.





STATION 139+00

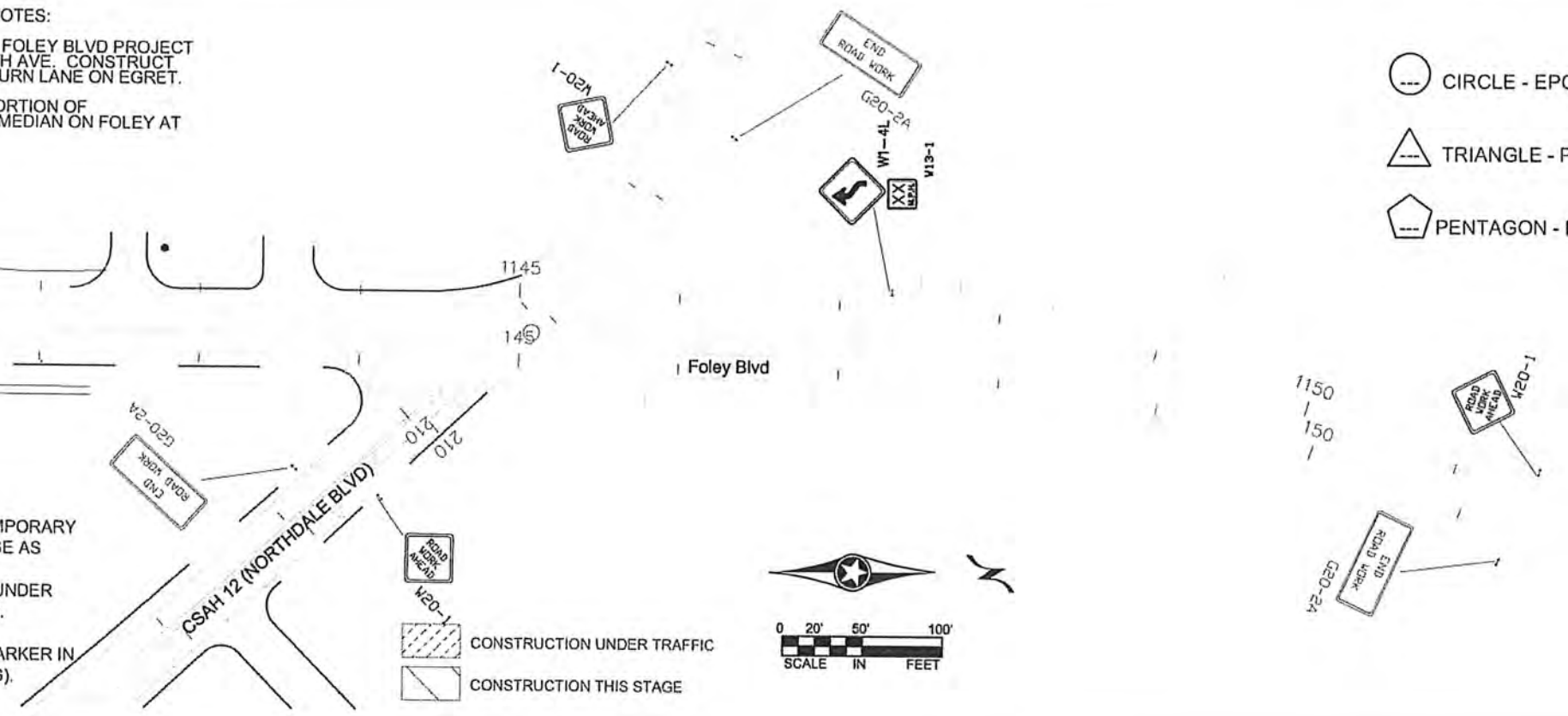
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

SHIFT TRAFFIC TO WEST TO USE NEWLY CONSTRUCTED PAVEMENT. RELOCATE TEMPORARY CONCRETE BARRIER FROM PREVIOUS STAGE AS REQUIRED. 109TH AVE AND 113TH AVE CONSTRUCTED UNDER TRAFFIC. MAINTAIN ACCESS TO DRIVEWAYS.

INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).

STRIPING KEY

-  CIRCLE - EPOXY
-  SQUARE - POLY PREFORM
-  TRIANGLE - PAINT
-  PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING



-  CONSTRUCTION UNDER TRAFFIC
-  CONSTRUCTION THIS STAGE

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\002-611-3-032\Base\TRAFFIC\002-611-032_STG2a.dwg

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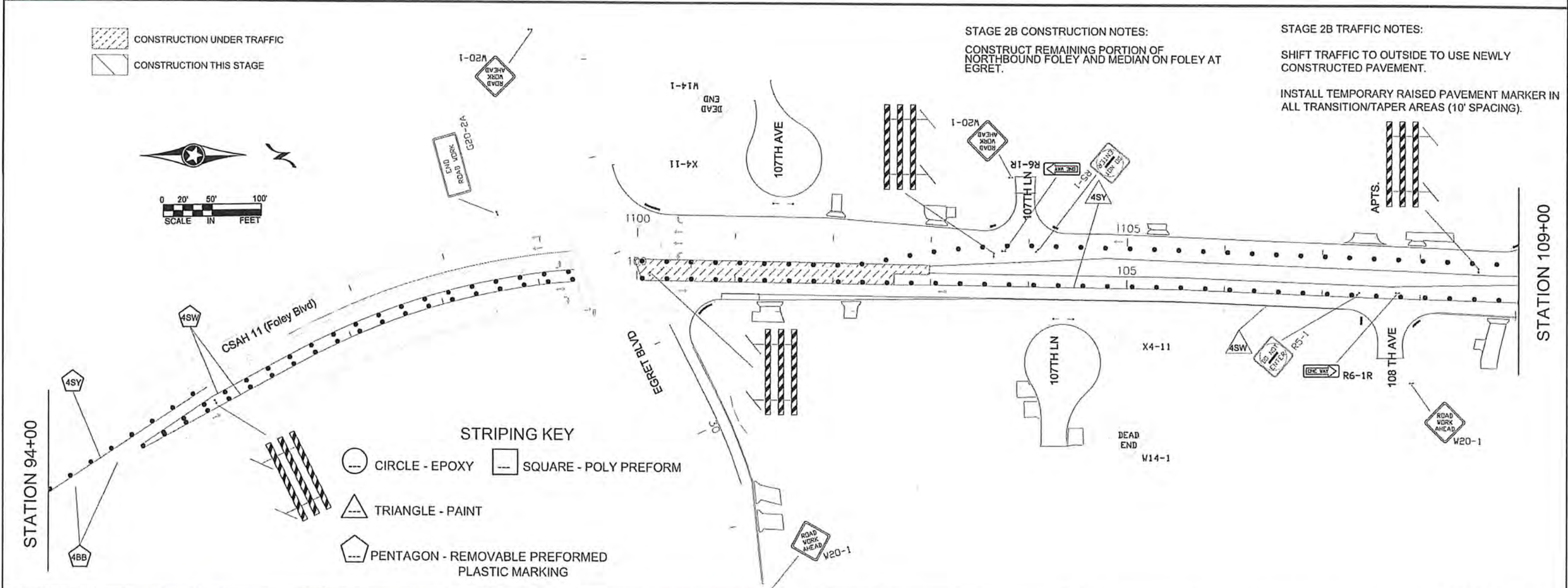
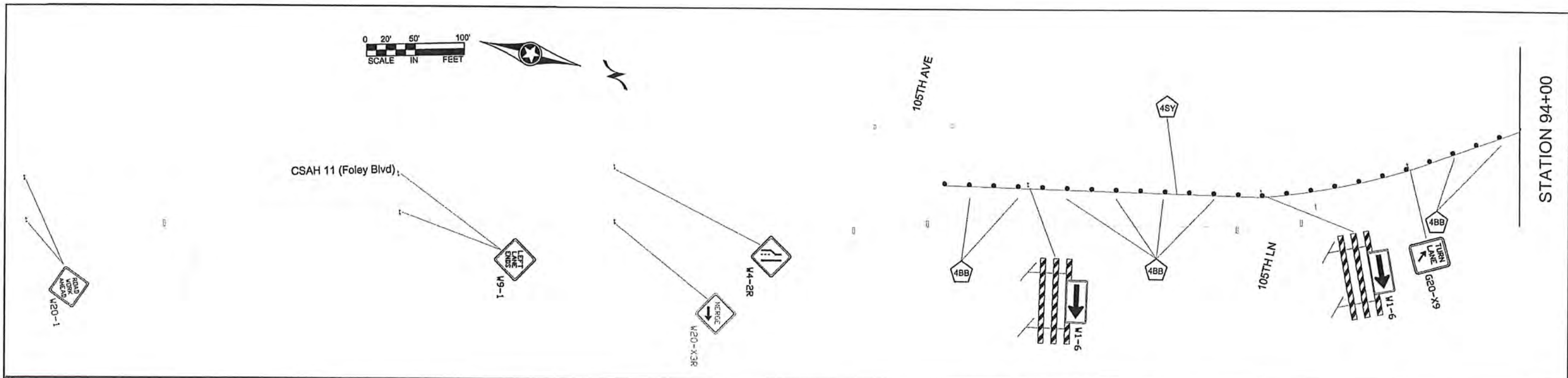


**ANOKA COUNTY
HIGHWAY DEPT.**

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO.

**STAGE 2
TRAFFIC CONTROL
LAYOUT**

Sheet 59 of 298 Sheets



STAGE 2B CONSTRUCTION NOTES:
 CONSTRUCT REMAINING PORTION OF NORTHBOUND FOLEY AND MEDIAN ON FOLEY AT EGRET.

STAGE 2B TRAFFIC NOTES:
 SHIFT TRAFFIC TO OUTSIDE TO USE NEWLY CONSTRUCTED PAVEMENT.
 INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).

CONSTRUCTION UNDER TRAFFIC
 CONSTRUCTION THIS STAGE

STRIPING KEY
 ○ CIRCLE - EPOXY □ SQUARE - POLY PREFORM
 ▲ TRIANGLE - PAINT
 ⬠ PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING

NO	DATE	BY	CKD	APPR	REVISION

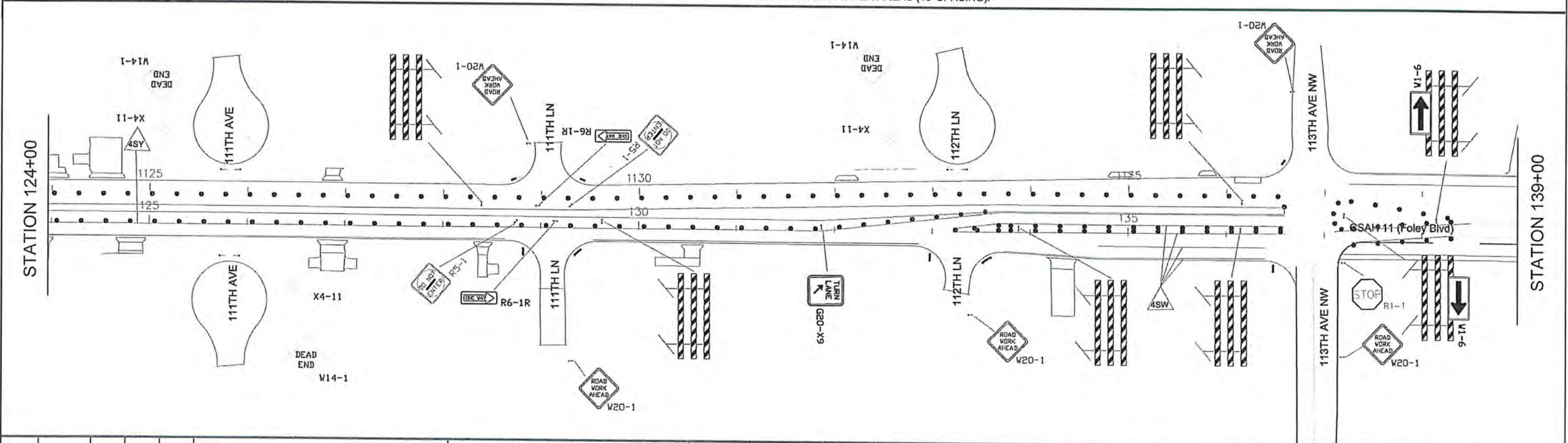
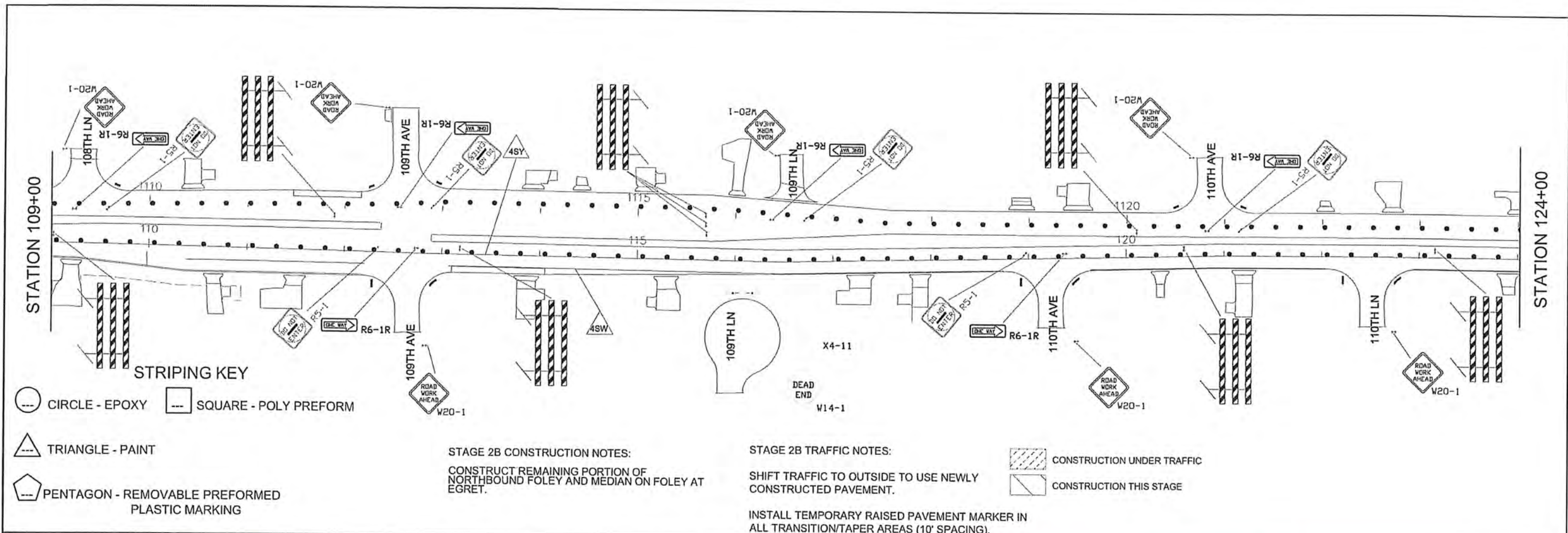
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
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ANOKA COUNTY HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

STAGE 2B TRAFFIC CONTROL LAYOUT
 Sheet 60 of 298 Sheets



NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\002-611-3-032\BASE\TRAFFIC\002-611-032_STG2a.dwg

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

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ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
STATE PROJECT NO. 114-020-050
CITY PROJECT NO. 13-10
COUNTY PROJECT NO.

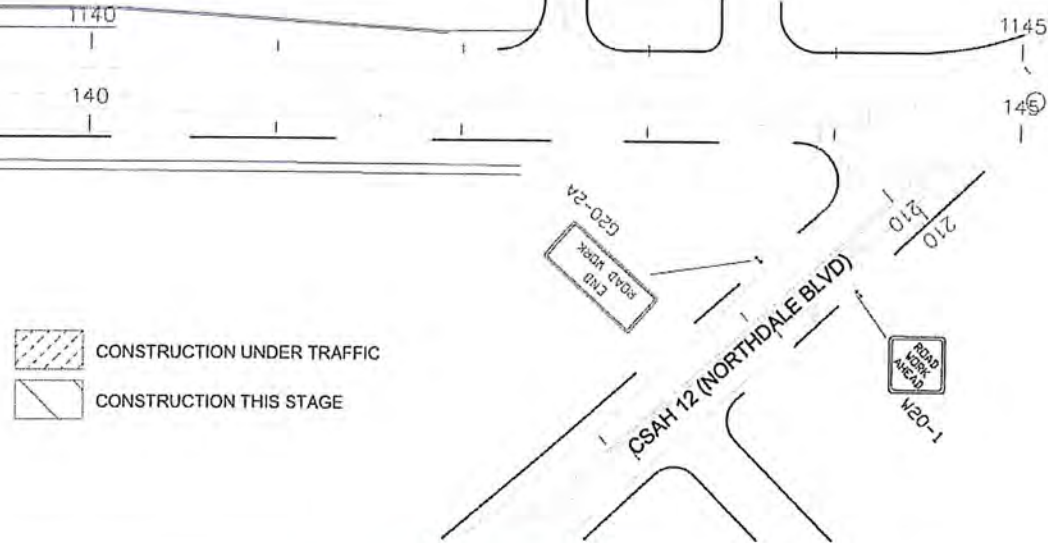
STAGE 2B
TRAFFIC CONTROL
LAYOUT
Sheet 61 of 298 Sheets

STAGE 2B CONSTRUCTION NOTES:
 CONSTRUCT REMAINING PORTION OF
 NORTHBOUND FOLEY AND MEDIAN ON FOLEY AT
 EGRET.

STAGE 2B TRAFFIC NOTES:
 SHIFT TRAFFIC TO OUTSIDE TO USE NEWLY
 CONSTRUCTED PAVEMENT.
 INSTALL TEMPORARY RAISED PAVEMENT MARKER IN
 ALL TRANSITION/TAPER AREAS (10' SPACING).

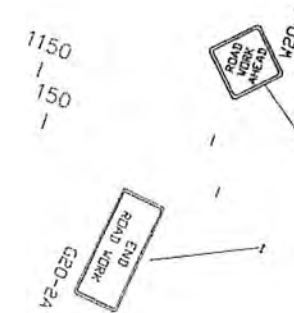
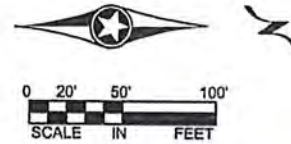
STRIPING KEY
 ○ CIRCLE - EPOXY □ SQUARE - POLY PREFORM
 ▲ TRIANGLE - PAINT
 ⬠ PENTAGON - REMOVABLE PREFORMED
 PLASTIC MARKING

STATION 139+00



Foley Blvd

CSAH 12 (NORTHDALE BLVD)



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\002-611-3-032\Base\TRAFFIC\002-611-032_STG2a.dwg

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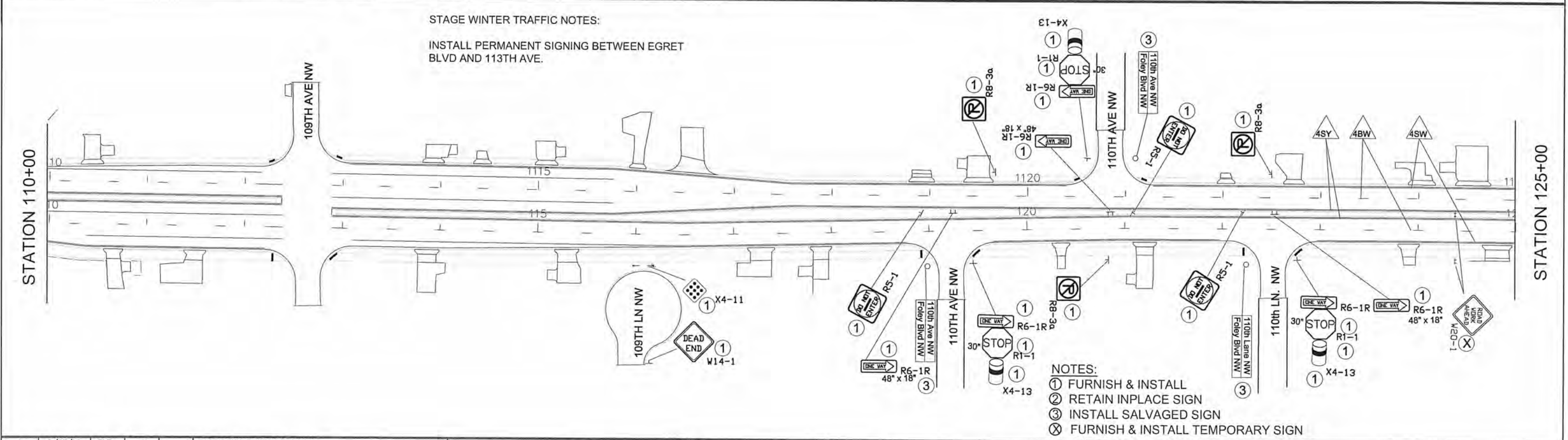
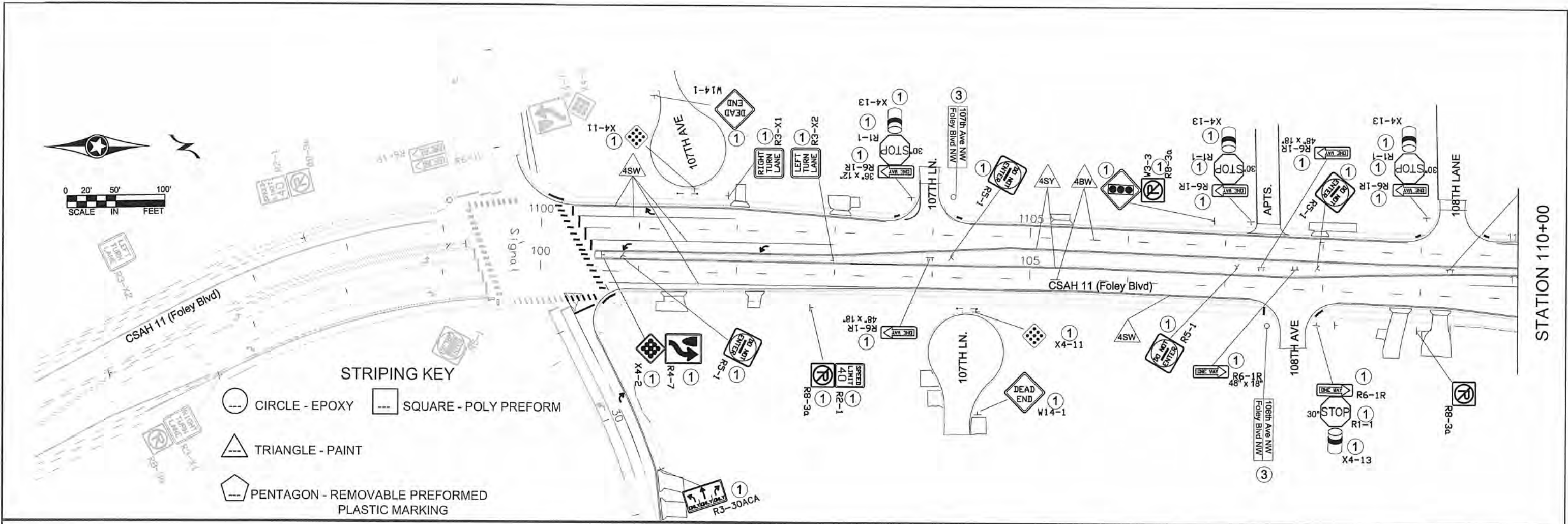
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ANOKA COUNTY
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 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO.

STAGE 2B
 TRAFFIC CONTROL
 LAYOUT
 Sheet 62 of 298 Sheets



1	6/28/16	RLB	JR		STAGE WINTER TRAFFIC NOTES
NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\002-611-3-032\Base\TRAFFIC\002-611-032_STG2a.dwg					

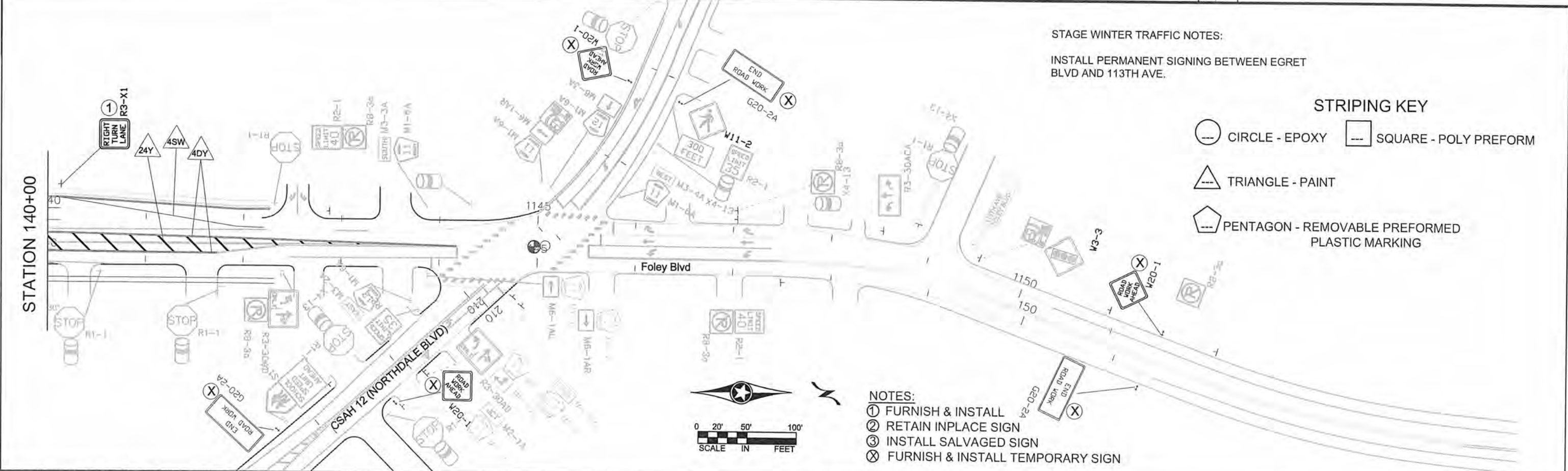
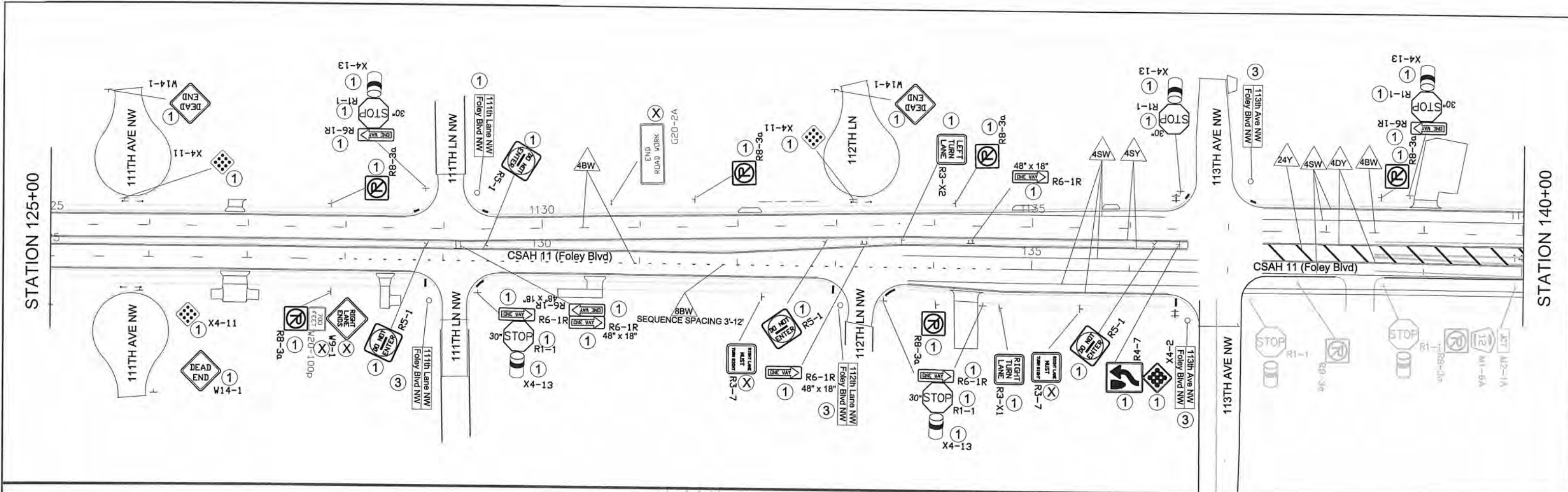
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 4/30/16 REG. NO. 20235

DRAWN BY: RLB DATE: 1/15/14
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 CHECKED BY: JR DATE: 1/15/14

ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO.

WINTER STAGE
 TRAFFIC CONTROL
 LAYOUT
 Sheet 63 of 298 Sheets



STAGE WINTER TRAFFIC NOTES:
 INSTALL PERMANENT SIGNING BETWEEN EGRET
 BLVD AND 113TH AVE.



- NOTES:**
- ① FURNISH & INSTALL
 - ② RETAIN INPLACE SIGN
 - ③ INSTALL SALVAGED SIGN
 - ⊗ FURNISH & INSTALL TEMPORARY SIGN

1	6/28/16	RLB	JR	STAGE WINTER TRAFFIC NOTES
NO	DATE	BY	CKD	APPR
NAME: P:\002-611-3-032\Base\TRAFFIC\002-611-032_STG2a.dwg				

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 DATE: 4/20/16 REG. NO. 20235

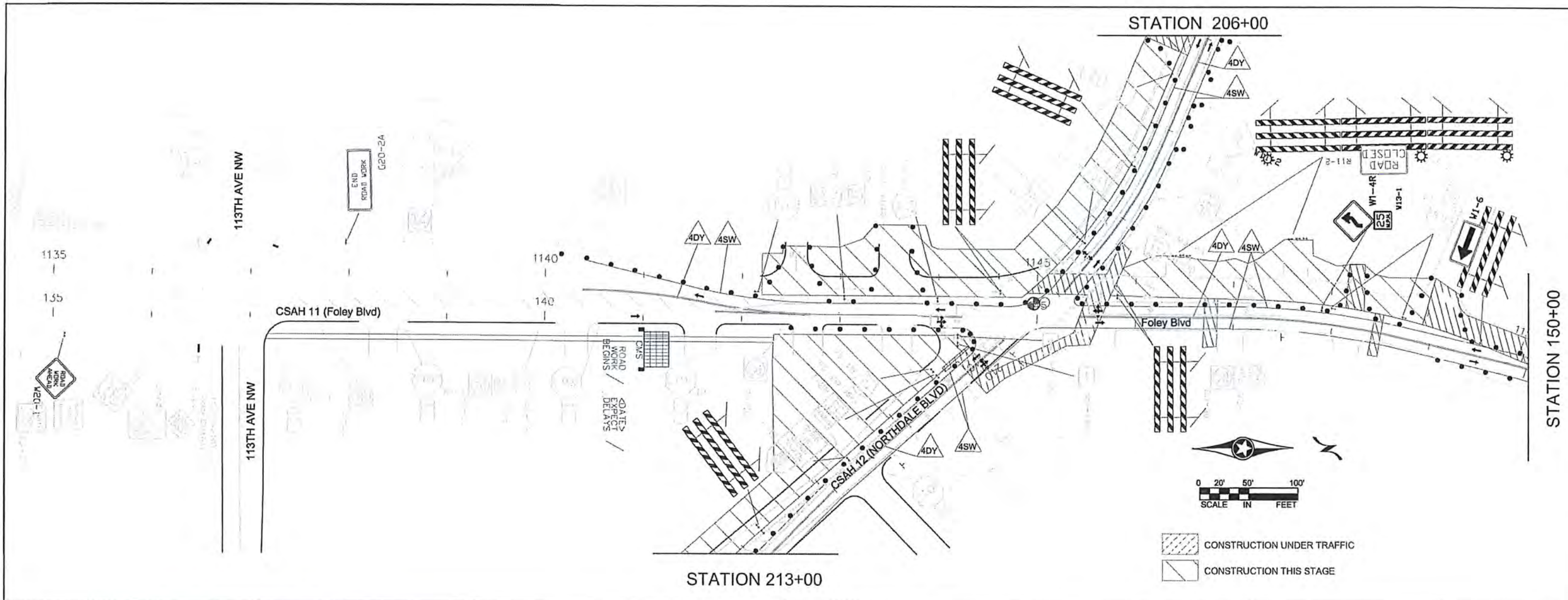
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 DESIGN BY: RLB DATE: 1/15/14
 CHECKED BY: JR DATE: 1/15/14



**ANOKA COUNTY
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STATE PROJECT NO. 002-611-034
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 COUNTY PROJECT NO.

WINTER STAGE
 TRAFFIC CONTROL
 LAYOUT
 Sheet 64 of 298 Sheets

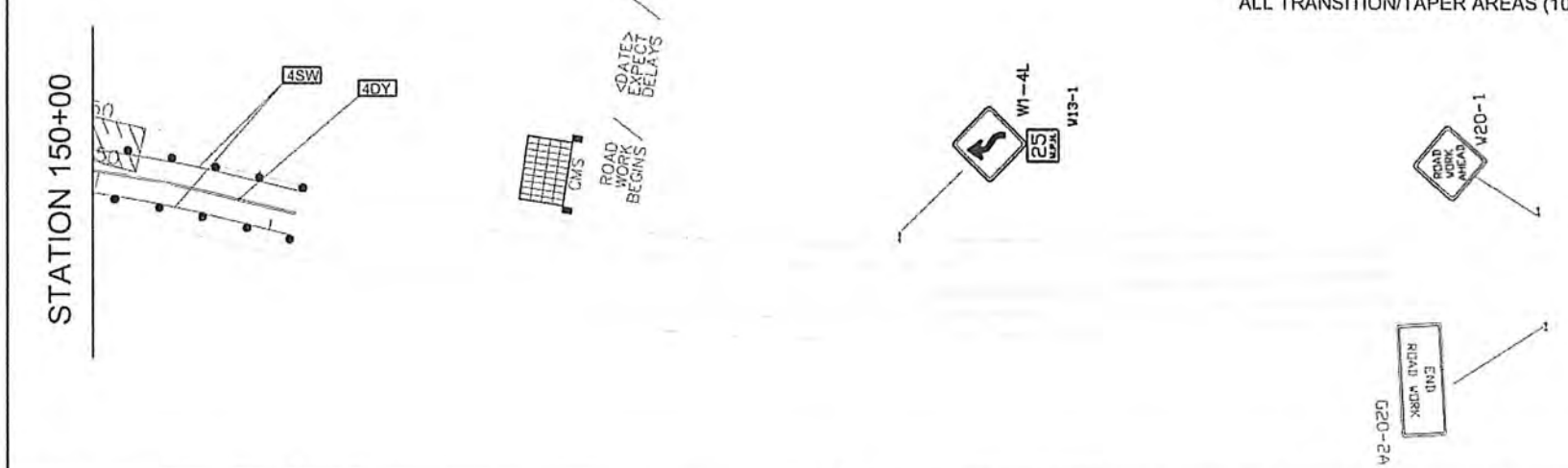


STAGE 3 CONSTRUCTION NOTES:
 CONSTRUCT WEST SIDE OF FOLEY NORTH OF WATER TOWER. CONSTRUCT SOUTH SIDE OF NORTHDAL.

STAGE 3 TRAFFIC NOTES:
 SHIFT FOLEY TRAFFIC TO EAST. SHIFT NORTHDAL TRAFFIC TO THE NORTH.
 INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).

STRIPING KEY

- CIRCLE - EPOXY
- SQUARE - POLY PREFORM
- TRIANGLE - PAINT
- PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING



NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\002-611-3-032\002-611-032_STG2a.dwg



I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
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ANOKA COUNTY
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 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO.

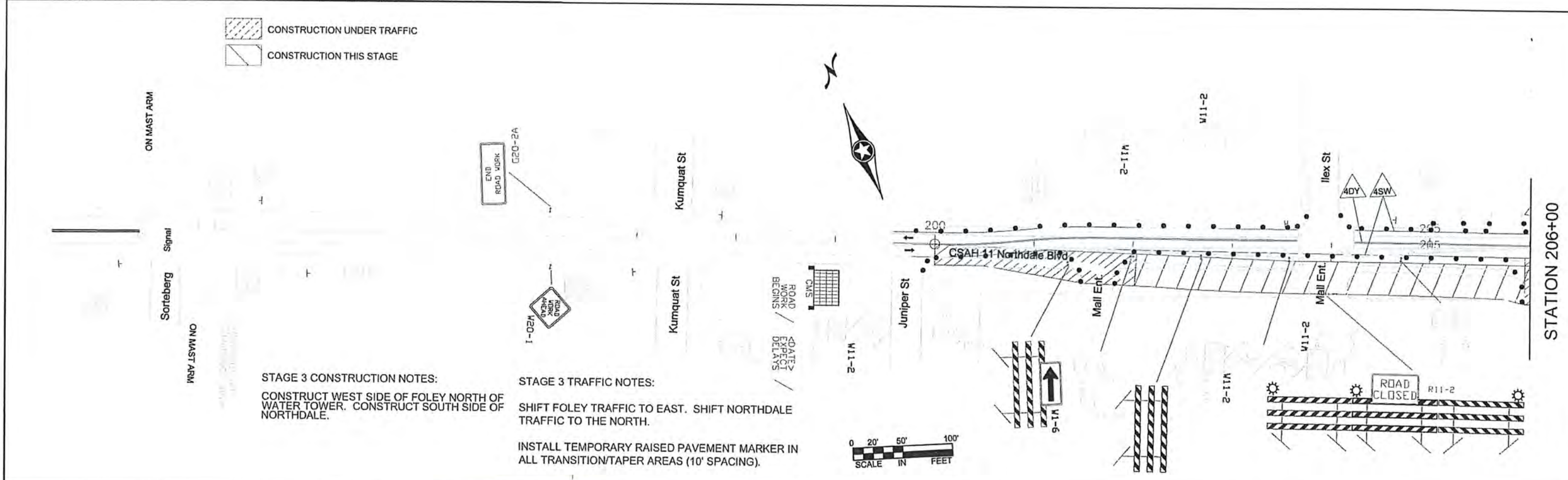
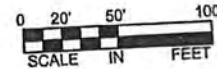
STAGE 3
 TRAFFIC CONTROL
 LAYOUT
 Sheet 65 of 298 Sheets

 CONSTRUCTION UNDER TRAFFIC
 CONSTRUCTION THIS STAGE





STAGE 3 CONSTRUCTION NOTES:
 CONSTRUCT WEST SIDE OF FOLEY NORTH OF WATER TOWER. CONSTRUCT SOUTH SIDE OF NORTHDALE.

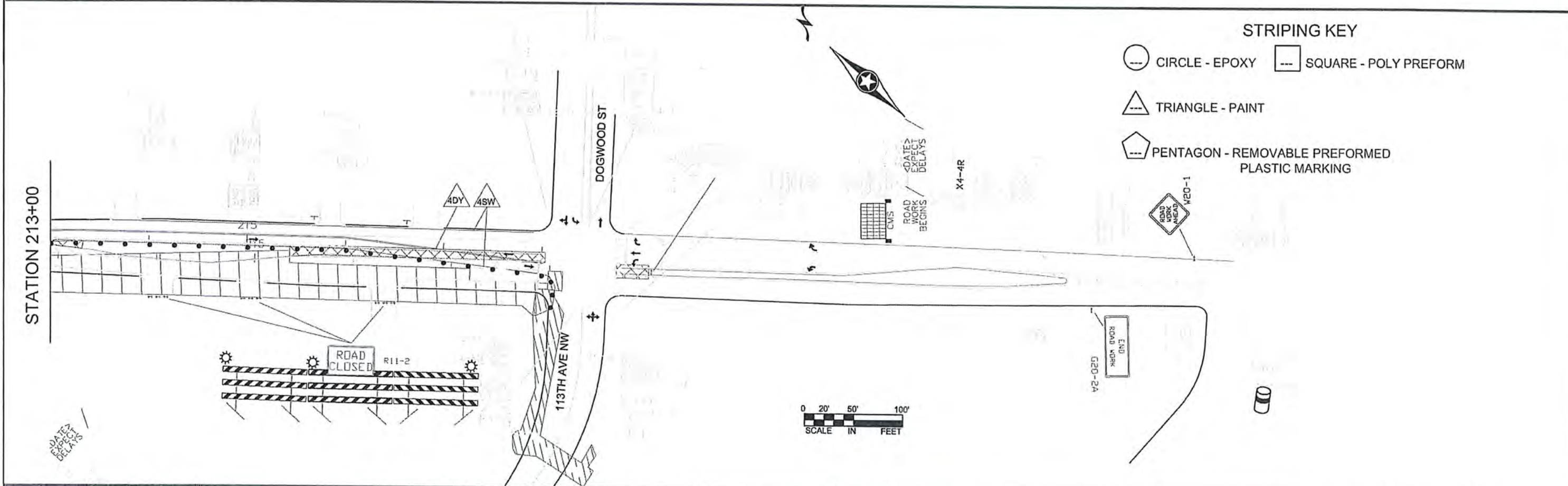
STAGE 3 TRAFFIC NOTES:
 SHIFT FOLEY TRAFFIC TO EAST. SHIFT NORTHDALE TRAFFIC TO THE NORTH.

INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).



STRIPING KEY

-  CIRCLE - EPOXY
-  SQUARE - POLY PREFORM
-  TRIANGLE - PAINT
-  PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\002-611-3-032\Bases\TRAFFIC\002-611-032_STG2a.dwg

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 5/3/16 REG. NO. 20235

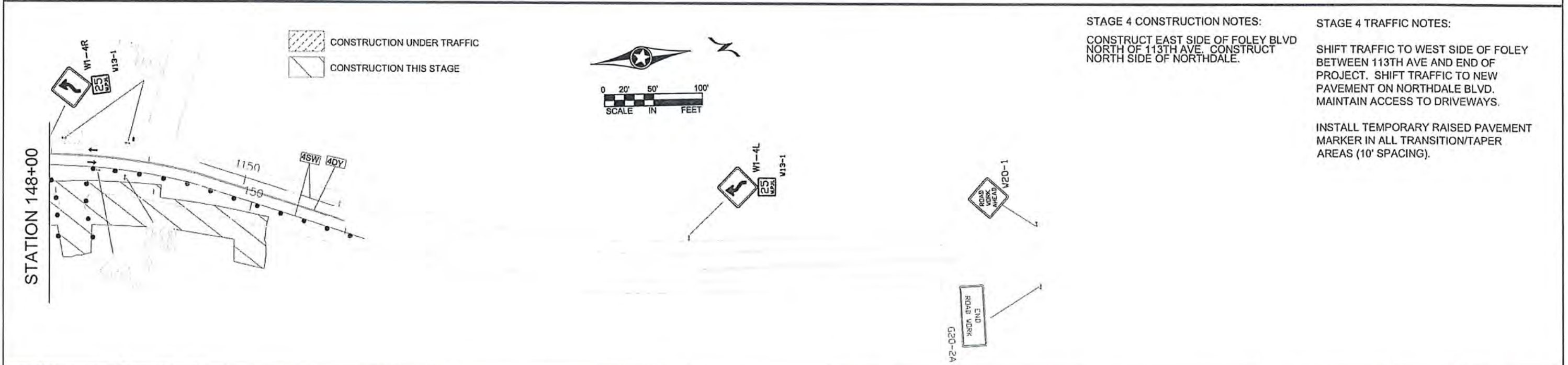
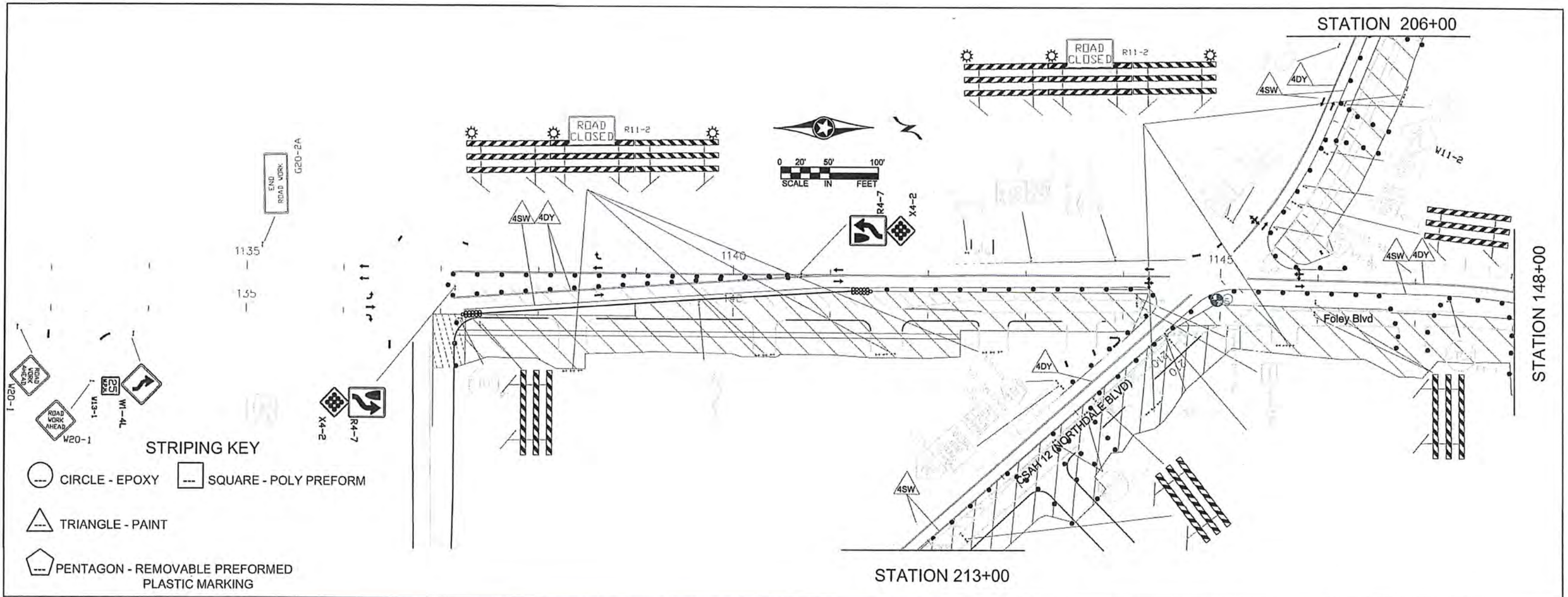
DRAWN BY: RLB DATE: 1/15/14
 DESIGN BY: RLB DATE: 1/15/14
 CHECKED BY: JR DATE: 1/15/14



ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO.

STAGE 3
 TRAFFIC CONTROL
 LAYOUT
 Sheet 66 of 298 Sheets

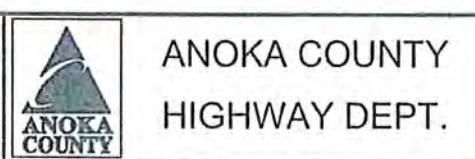


NO	DATE	BY	CKD	APPR	REVISION

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

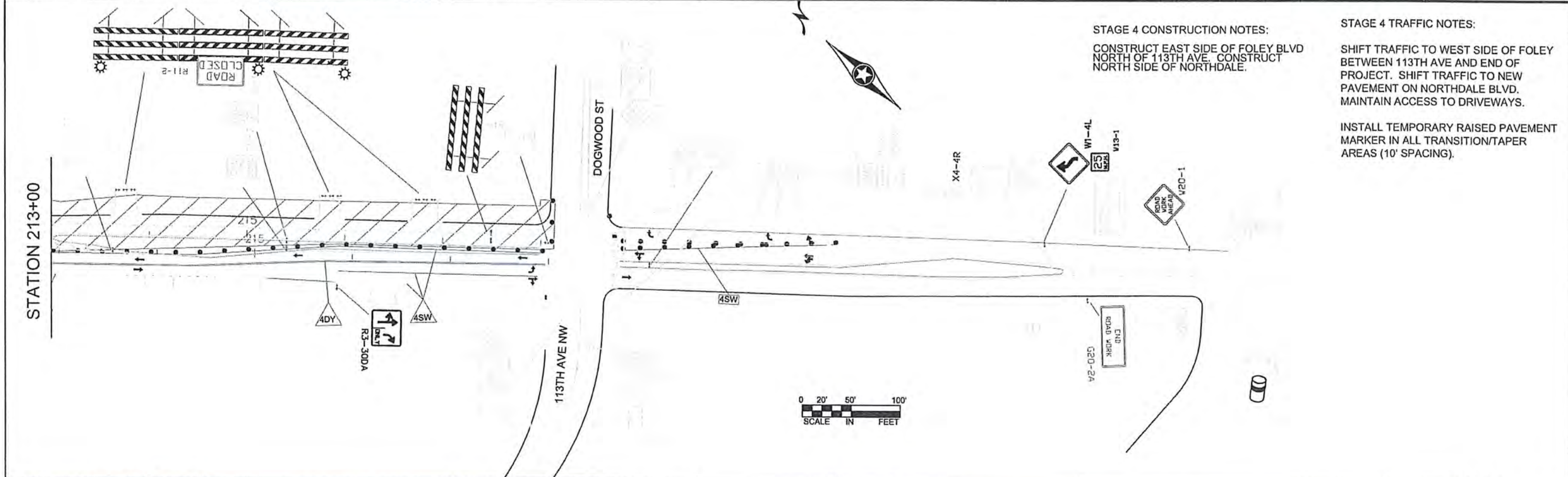
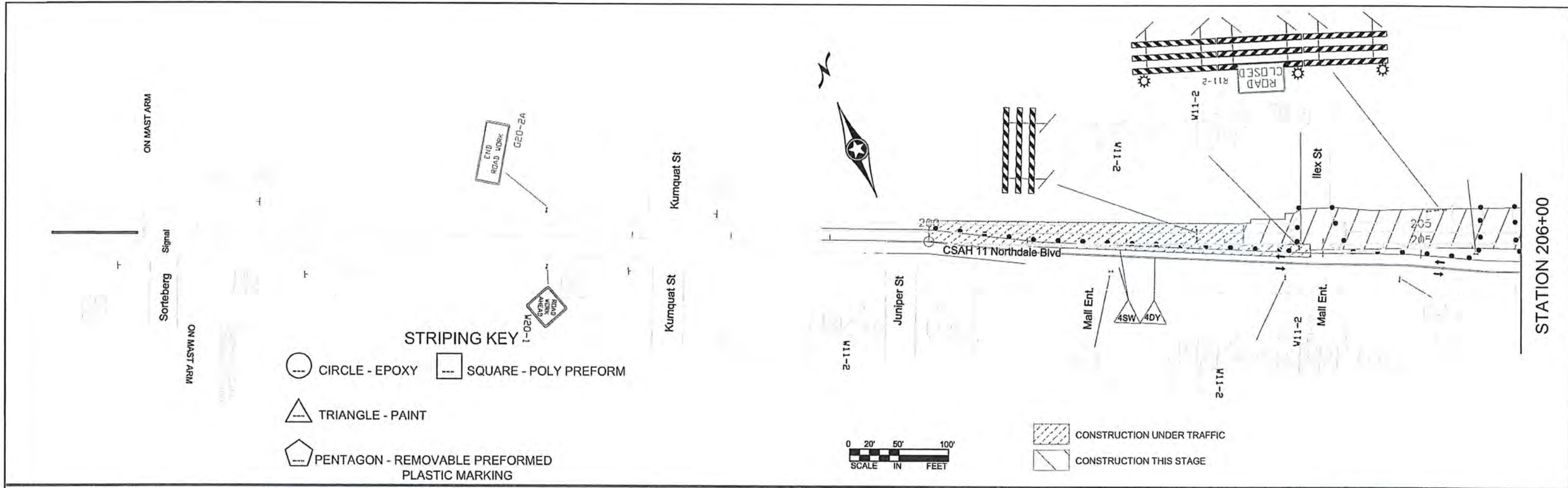
PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 5/21/16 REG. NO. 20235

DRAWN BY: RLB DATE: 1/15/14
 DESIGN BY: RLB DATE: 1/15/14
 CHECKED BY: JR DATE: 1/15/14



STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

STAGE 4
 TRAFFIC CONTROL
 LAYOUT
 Sheet 67 of 298 Sheets



STAGE 4 CONSTRUCTION NOTES:
 CONSTRUCT EAST SIDE OF FOLEY BLVD
 NORTH OF 113TH AVE. CONSTRUCT
 NORTH SIDE OF NORTHDAL.

STAGE 4 TRAFFIC NOTES:
 SHIFT TRAFFIC TO WEST SIDE OF FOLEY
 BETWEEN 113TH AVE AND END OF
 PROJECT. SHIFT TRAFFIC TO NEW
 PAVEMENT ON NORTHDAL BLVD.
 MAINTAIN ACCESS TO DRIVEWAYS.
 INSTALL TEMPORARY RAISED PAVEMENT
 MARKER IN ALL TRANSITION/TAPER
 AREAS (10' SPACING).

NO	DATE	BY	CKD	APPR	REVISION

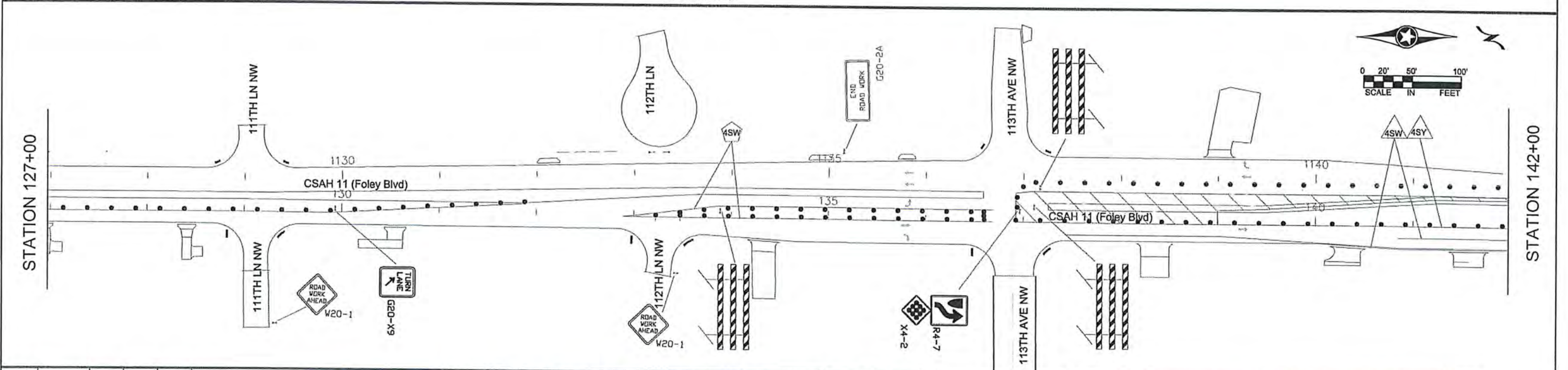
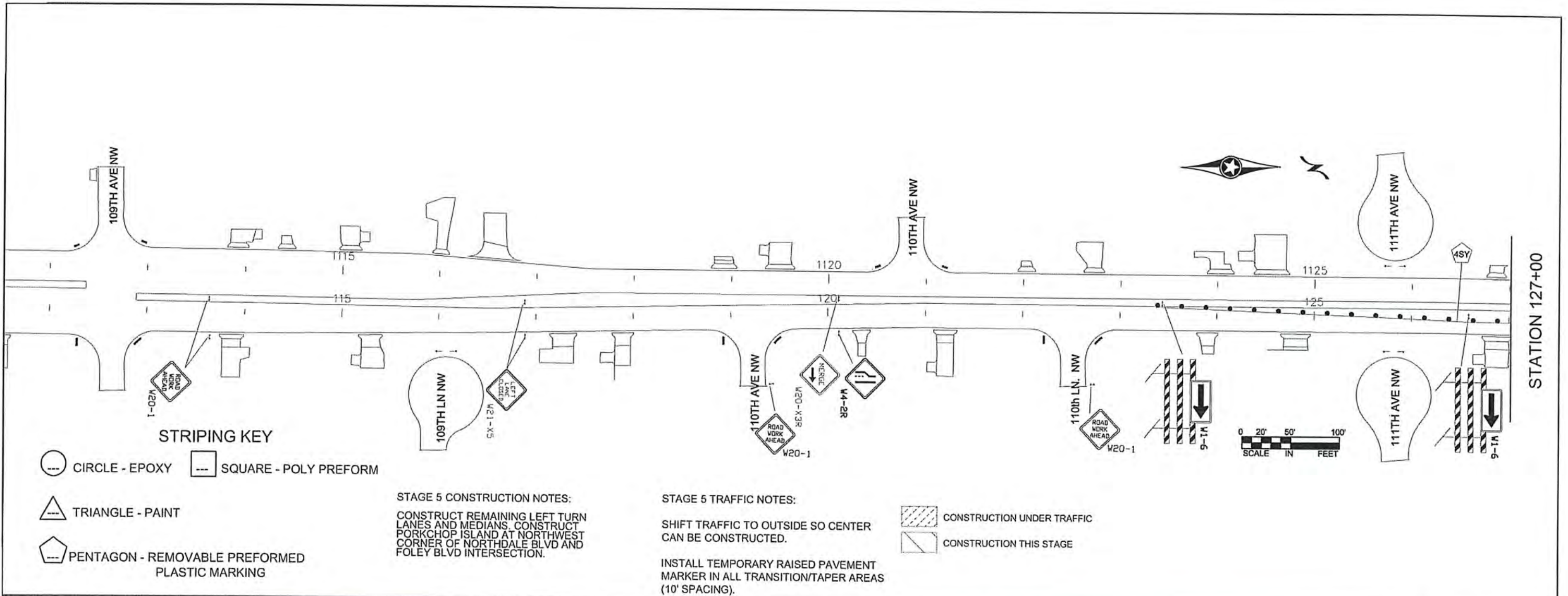
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 5/21/14 REG. NO. 20235

DRAWN BY: RLB DATE: 1/15/14
 DESIGN BY: RLB DATE: 1/15/14
 CHECKED BY: JR DATE: 1/15/14

ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

STAGE 4
 TRAFFIC CONTROL
 LAYOUT
 Sheet 68 of 298 Sheets



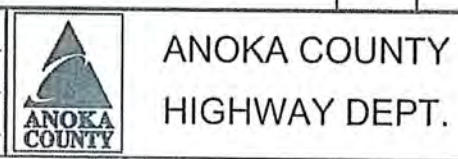
NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\002-611-3-032\Base\TRAFFIC\002-611-032_STG2a.dwg

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

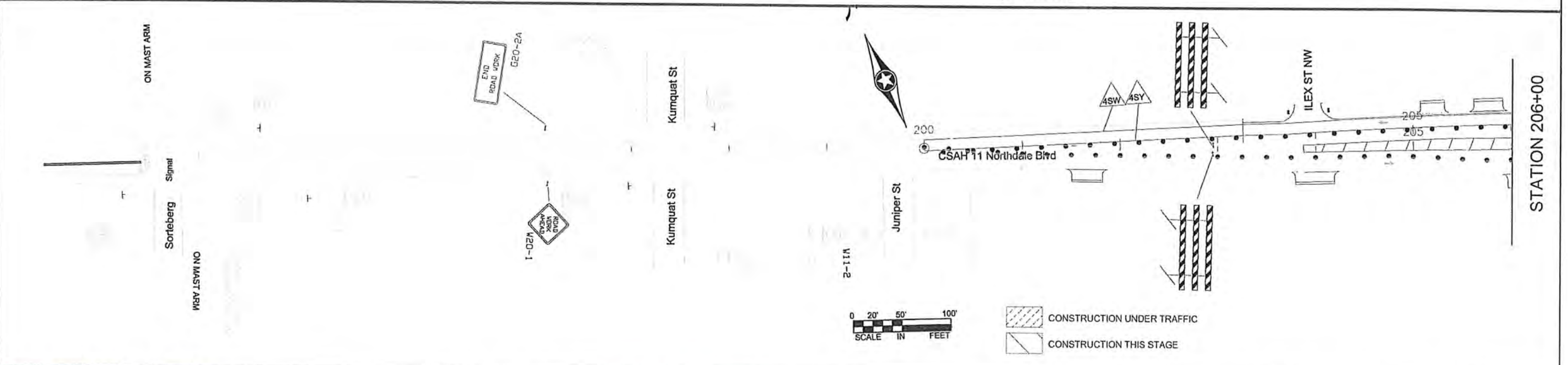
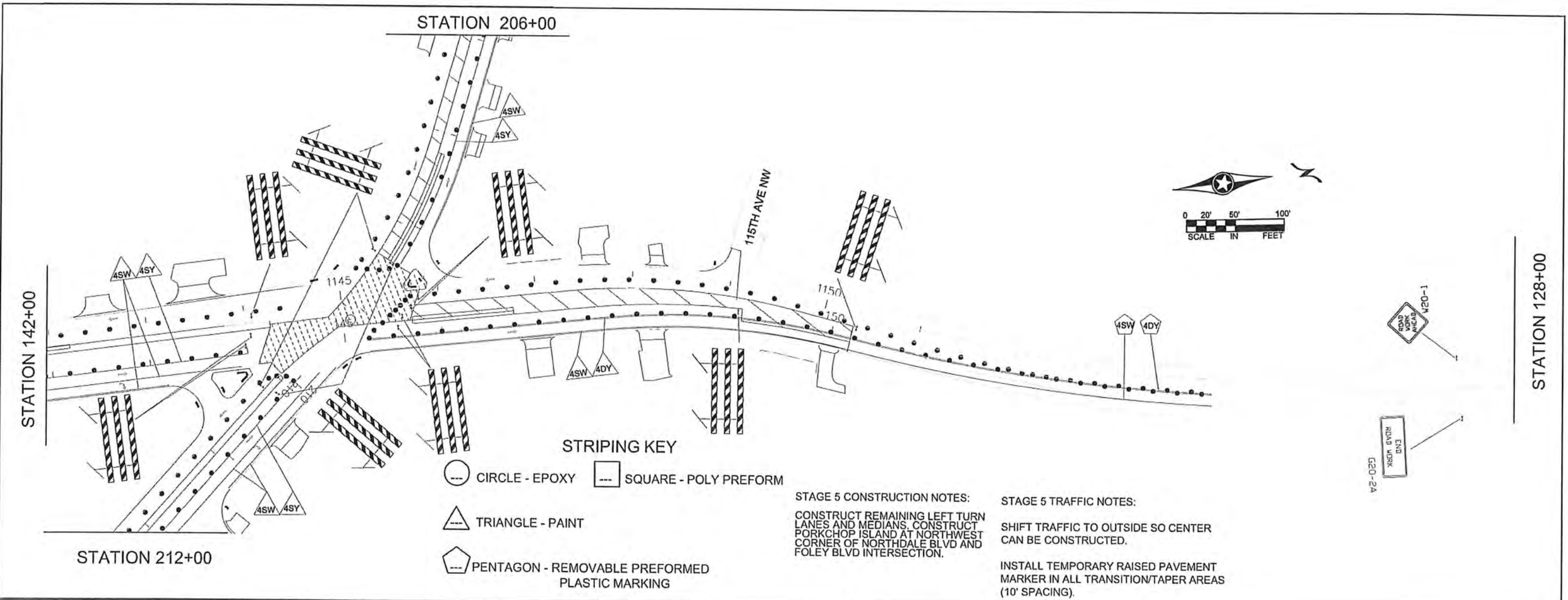
PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 5/24/14 REG. NO. 20235

DRAWN BY: RLB DATE: 1/15/14
 DESIGN BY: RLB DATE: 1/15/14
 CHECKED BY: JR DATE: 1/15/14



STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

STAGE 5
 TRAFFIC CONTROL
 LAYOUT
 Sheet 69 of 298 Sheets



I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: <u>DOUGLAS W FISCHER</u> SIGNATURE: <u>[Signature]</u> DATE: <u>5/31/14</u> REG. NO. <u>20235</u>					DRAWN BY: <u>RLB</u> DATE: <u>1/15/14</u> DESIGN BY: <u>RLB</u> DATE: <u>1/15/14</u> CHECKED BY: <u>JR</u> DATE: <u>1/15/14</u>		ANOKA COUNTY HIGHWAY DEPT.		STATE PROJECT NO. <u>002-611-034</u> STATE PROJECT NO. <u>114-020-050</u> CITY PROJECT NO. <u>13-10</u> COUNTY PROJECT NO. _____		STAGE 5 TRAFFIC CONTROL LAYOUT Sheet <u>70</u> of <u>298</u> Sheets	
NO	DATE	BY	CHKD	APPR	REVISION							
NAME: P:\002-611-3-032\Baso\TRAFFIC\002-611-032_STG2a.dwg												

STRIPING KEY

CIRCLE - EPOXY SQUARE - POLY PREFORM

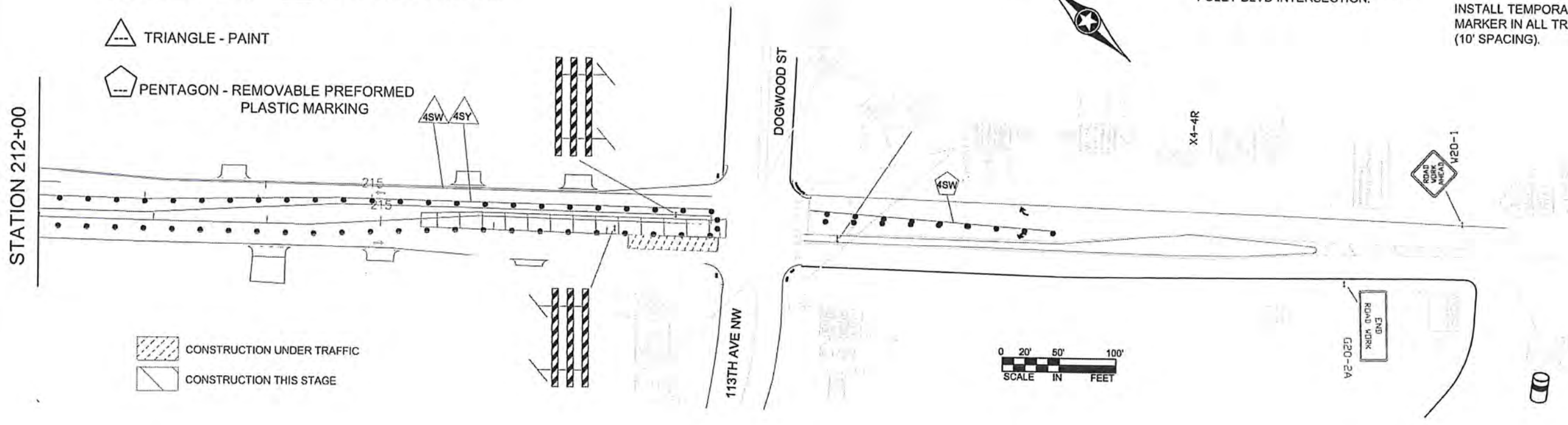
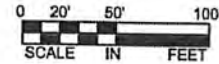
TRIANGLE - PAINT

PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING

CONSTRUCTION UNDER TRAFFIC
CONSTRUCTION THIS STAGE

STAGE 5 CONSTRUCTION NOTES:
CONSTRUCT REMAINING LEFT TURN LANES AND MEDIANS. CONSTRUCT PORKCHOP ISLAND AT NORTHWEST CORNER OF NORTHDALE BLVD AND FOLEY BLVD INTERSECTION.

STAGE 5 TRAFFIC NOTES:
SHIFT TRAFFIC TO OUTSIDE SO CENTER CAN BE CONSTRUCTED.
INSTALL TEMPORARY RAISED PAVEMENT MARKER IN ALL TRANSITION/TAPER AREAS (10' SPACING).



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\002-611-3-032\Bose\TRAFFIC\002-611-032_STG2a.dwg

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
PRINT NAME: DOUGLAS W. FISCHER
SIGNATURE: *[Signature]*
DATE: 5/31/14 REG. NO. 20235

DRAWN BY: RLB DATE: 1/15/14
DESCH BY: RLB DATE: 1/15/14
CHECKED BY: JR DATE: 1/15/14



**ANOKA COUNTY
HIGHWAY DEPT.**

STATE PROJECT NO. 002-611-034
STATE PROJECT NO. 114-020-050
CITY PROJECT NO. 13-10
COUNTY PROJECT NO. _____

STAGE 5
TRAFFIC CONTROL
LAYOUT
Sheet 71 of 298 Sheets

M.U.T.C.D. CODE	SIZE	INSERT	QTY. STG. 1B	QTY. STG. 2	QTY. STG. 2B	QTY. STG. WINTER	QTY. STG. 3	QTY. STG. 4	QTY. STG. 5
R6-1R	48" x 18"		0	0	10	0	0	0	0
R1-1	48" x 48"		3	3	0	0	0	0	0
R1-4	48" x 12"		0	0	0	0	0	0	0
R3-7	30" x 30"		0	0	0	2	0	0	0
R5-1	30" x 30"		0	0	10	0	0	0	0
W4-2R	48" x 48"		1	0	1	0	0	0	1
W4-2L	48" x 48"		0	1	0	0	0	0	0
W20-X3R	48" x 48"		1	0	1	0	0	0	1
W20-X3L	48" x 48"		0	1	0	0	0	0	0
W21-X5	48" x 48"		2	0	2	0	0	0	2
W21-X5	48" x 48"		0	2	0	0	0	0	0
R4-8	24" x 30"		1	0	0	0	0	2	1
X4-2	18" x 18"		1	0	0	0	0	2	1
W1-4L	48" x 48"		1	1	0	0	0	3	0
W13-1	30" x 30"		1	1	0	0	0	3	0
W1-4R	48" x 48"		3	2	0	0	0	0	0
W13-1	30" x 30"		3	2	0	0	0	0	0
G20-X9	30" x 36"		1	1	2	0	0	0	1
W6-3	48" x 48"		0	1	0	0	0	0	0
W3-X5	48" x 48"		2	0	0	0	0	0	0

M.U.T.C.D. CODE	SIZE	INSERT	QTY. STG. 1B	QTY. STG. 2	QTY. STG. 2B	QTY. STG. WINTER	QTY. STG. 3	QTY. STG. 4	QTY. STG. 5
W20-1	48" x 48"		27	23	23	5	4	6	10
W20-3	48" x 48"		8	7	5	0	0	0	0
W1-1R	48" x 18"		3	0	4	0	0	0	2
TYPE III	8 FOOT		3	0	4	0	0	0	2
W1-6L	48" x 18"		0	4	0	0	2	0	0
TYPE III	8 FOOT		0	4	0	0	2	0	0
R11-2	48" x 30"		8	7	0	0	6	11	0
TYPE III	8 FOOT		8	7	0	0	6	11	0
TYPE III	8 FOOT		9	17	0	0	14	22	0
TYPE III	8 FOOT		8	7	18	0	6	11	19
REFLECTORIZED REBOUNDABLE DRUM			168	376	402	0	213	218	346
G20-2A	48" x 24"		4	4	4	4	4	4	4
W9-1	48" x 48"		0	0	0	1	0	0	0
w20-100p	48" x 24"		0	0	0	1	0	0	0
R3-30DA	36" x 30"		0	0	0	0	0	1	0
CMS sign to be installed a minimum of seven days prior to actual commencement of road work. Signs to be removed when road work begins.			2	0	0	0	4	0	0

TEMPORARY PAVEMENT MARKING TABULATION		
ITEM	UNIT	TOTAL QUANTITY
PAVEMENT MARKING REMOVAL 4"	LIN FT	5200
PAVEMENT MARKING REMOVAL DOUBLE 4"	LIN FT	6005
PAVEMENT MARKING REMOVAL DOUBLE 4"	LIN FT	155
PAVEMENT MESSAGE (LT ARROW) LATEX PAINT	EACH	2
PAVEMENT MESSAGE (RT ARROW) LATEX PAINT	EACH	1
3'X6' LATEX PAINT ZEBRA CROSSWALK	SQ FT	342
TEMPORARY RAISED PAVEMENT MARKER	EACH	882
REMOVABLE PREFORM PLASTIC MARKING (BLACK)	LIN FT	60
REMOVABLE PREFORM PLASTIC MARKING (WHITE)	LIN FT	4175
REMOVABLE PREFORM PLASTIC MARKING (YELLOW)	LIN FT	3648
8" SOLID LINE WHITE - LATEX PAINT	LIN FT	135
4" SOLID LINE WHITE - LATEX PAINT	LIN FT	37325
4" SOLID LINE YELLOW - LATEX PAINT	LIN FT	14090
4" DOUBLE YELLOW - LATEX PAINT	LIN FT	14770
24" WHITE - LATEX PAINT	LIN FT	73
24" YELLOW - LATEX PAINT	LIN FT	340
PORTABLE PRECAST CONCRETE BARRIER	LIN FT	3912
IMPACT ATTENUATOR	EACH	7

- NOTES:
- ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING THE LATEST FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS.
 - ALL TYPE III BARRICADES SHALL BE REFLECTORIZED ON BOTH SIDES. BARRICADE MARKINGS SHALL BE SLANTED IN ACCORDANCE WITH THE M.U.T.C.D.



CMS sign to be installed a minimum of seven days prior to actual commencement of road work. Signs to be removed when road work begins.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-032\Bases\TRAFFIC\STGQTY.dwg

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: DOUGLAS W. FISCHER

SIGNATURE:

DATE: 5/31/16 REG. NO. 20235

DRAWN BY: RB DATE 5/31/16

DESIGN BY: RB DATE 5/31/16

CHECKED BY: JR DATE 5/31/16

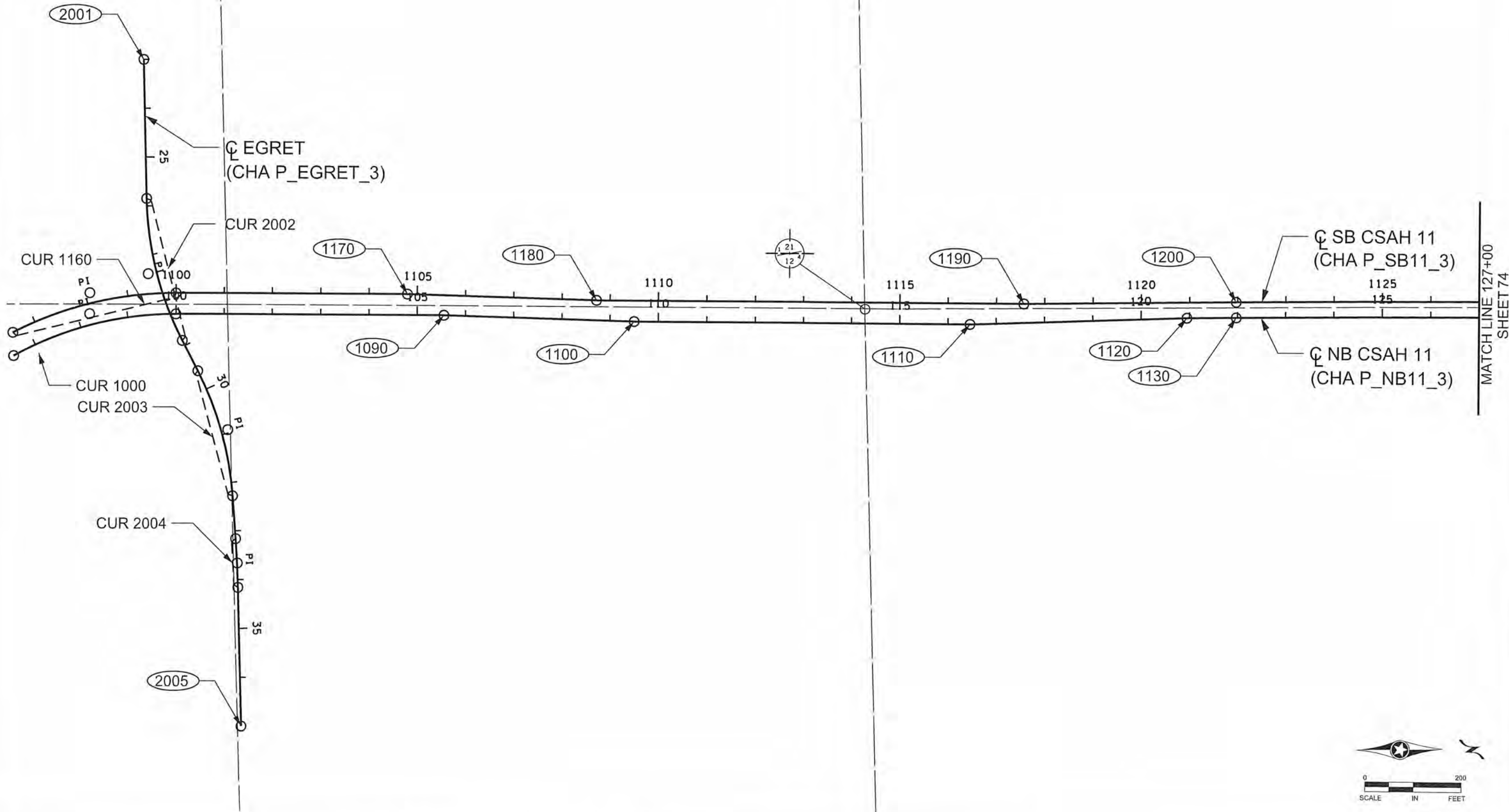


ANOKA COUNTY
HIGHWAY DEPT.

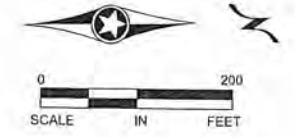
STATE PROJECT NO. 002-611-034
STATE PROJECT NO. 114-020-050
CITY PROJECT NO. 13-10
COUNTY PROJECT NO.

STAGING
SIGN QUANTITIES

Sheet 72 of 298 Sheets



MATCHLINE 127+00
SHEET 74



1 OF 3

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_AL_P1.dgn 05/13/2016 12:57:00 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

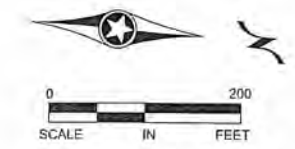
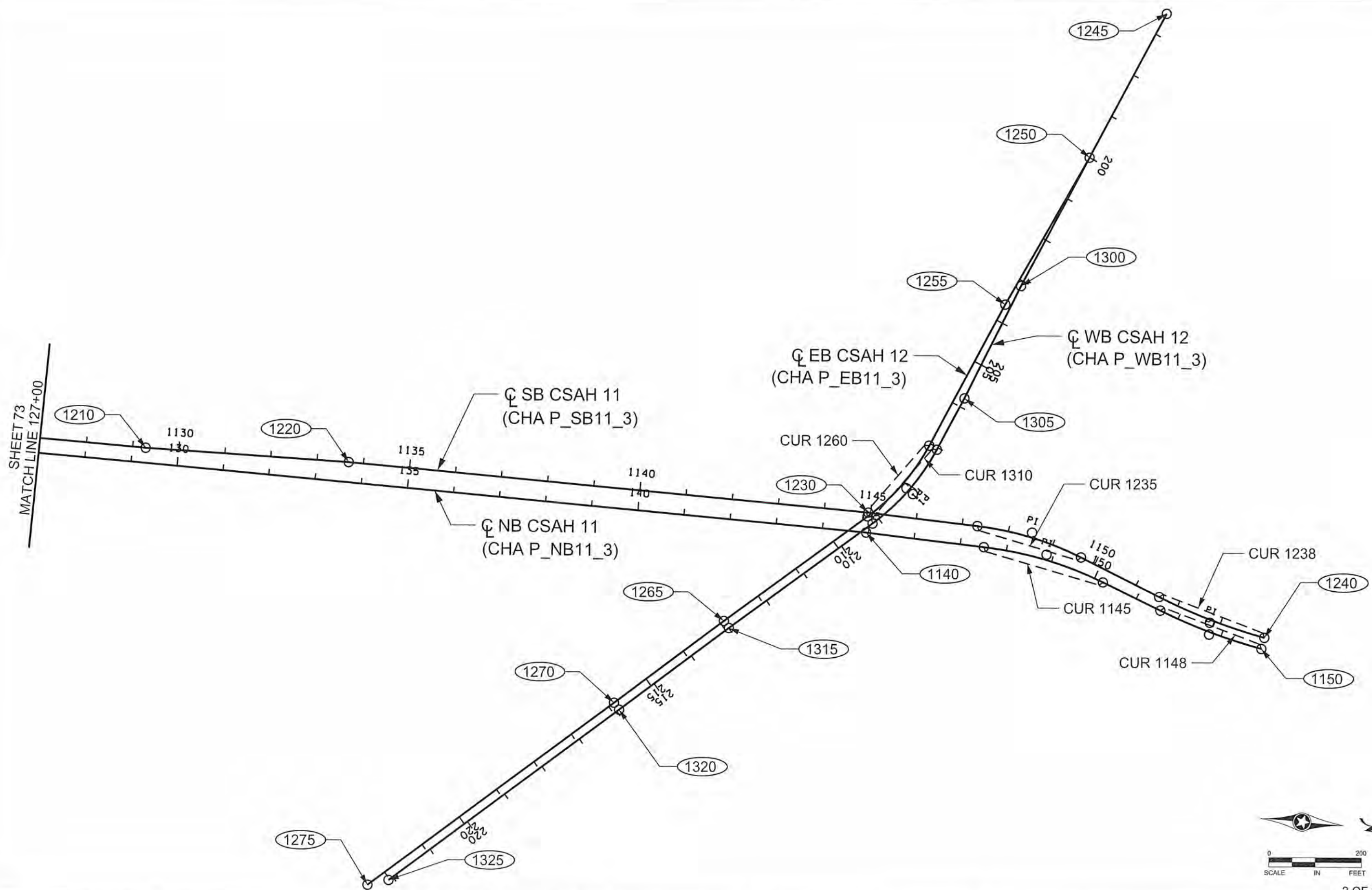
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 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



**ANOKA COUNTY
HIGHWAY DEPT.**

SP 002-611-034
 SP 114-020-050
 CP 13-10

ALIGNMENT PLAN
 STA 96+50 TO 127+00
 Sheet 73 of 298 Sheets



2 OF 3

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\02-611-034_AL_P2.dgn 05/13/2016 12:57:00 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *GINA M. PIZZO*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



ANOKA COUNTY
 HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

ALIGNMENT PLAN
 STA 127+00 TO 153+91.40
 Sheet 74 of 298 Sheets

ALIGNMENT TABULATION

POINT NUMBER	POINT	ALIGNMENT	CIRCULAR CURVE DATA					COORDINATES		AZIMUTH	
			DELTA	DEGREE	RADIUS	TANGENT	LENGTH	E	N		
		C C.S.A.H. 11 <P_NB11_3>									
	PC	C.C.S.A.H. 11	96+50.000						497,516.5190	146,604.4244	N 27° 58' 13.25" E
1000	PI		98+28.772	28° 46' 16.30" RT	8° 13' 13.23"	697.000'	178.772'	350.000'	497,432.6721	146,762.3145	PI
	CC								498,132.1027	146,931.3276	
	PT		100+00.000						497,435.1708	146,941.0695	N 0° 48' 03.04" E
1090	POT		105+55.543						497,442.9356	147,496.5584	N 2° 16' 11.07" E
1100	POT		109+50.173						497,458.5646	147,890.8786	N 0° 50' 15.52" E
1110	POT		116+45.672						497,468.7322	148,586.3038	N 1° 11' 06.00" E
1120	POT		120+94.707						497,459.4458	149,035.2422	N 0° 05' 56.54" E
1130	POT		121+97.266						497,459.6231	149,137.8016	N 0° 15' 26.14" E
1140	POT		144+94.962						497,469.9398	151,435.4741	N 1° 41' 21.68" E
	PC		147+51.125						497,477.4917	151,691.5259	N 1° 41' 21.68" E
1145	PI		148+86.781	19° 03' 39.97" RT	7° 05' 27.82"	808.000'	135.656'	268.805'	497,481.4909	151,827.1227	PI
	CC								498,285.1405	151,667.7056	
	PT		150+19.930						497,529.5535	151,953.9788	N 20° 45' 01.66" E
	PC		151+57.741						497,578.3796	152,082.8501	N 20° 45' 01.66" E
1148	PI		152+74.909	10° 41' 23.02" LT	4° 34' 29.90"	1,252.374'	117.168'	233.657'	497,619.8922	152,192.4180	PI
	CC								496,407.2442	152,526.5644	
1150	PT		153+91.397						497,640.3606	152,307.7847	N 10° 03' 38.63" E

ALIGNMENT TABULATION

POINT NUMBER	POINT	ALIGNMENT	CIRCULAR CURVE DATA					COORDINATES		AZIMUTH	
			DELTA	DEGREE	RADIUS	TANGENT	LENGTH	E	N		
		C C.S.A.H. 12 <P_EB12_3>									
1245	POT	C.C.S.A.H. 12	196+50.000								
1250	POT		200+00.000						496,306.9109	151,979.6754	S 66° 49' 45.42" E
1255	POT		203+61.899						496,628.6787	151,841.9602	S 65° 06' 30.19" E
	PC		207+04.607						496,956.9590	151,689.6359	S 66° 46' 43.81" E
1260	PI		208+08.382	25° 45' 02.71" RT	12° 37' 12.78"	454.000'	103.775'	204.044'	497,271.9048	151,554.5122	S 66° 46' 43.81" E
	CC								497,367.2726	151,513.5959	PI
	PT		209+08.651						497,092.9011	151,137.2909	
1265	POT		212+91.180						497,435.3932	151,435.3096	S 41° 01' 41.10" E
1270	POT		215+85.874						497,686.4959	151,146.7348	S 41° 01' 41.10" E
1275	POT		222+45.564						497,881.9010	150,926.1414	S 41° 32' 06.12" E
									498,317.0244	150,430.2996	S 41° 16' 06.23" E

ALIGNMENT TABULATION

POINT NUMBER	POINT	ALIGNMENT	CIRCULAR CURVE DATA					COORDINATES		AZIMUTH	
			DELTA	DEGREE	RADIUS	TANGENT	LENGTH	E	N		
		C C.S.A.H. 12 <P_WB12_3>									
1250	POT	C.C.S.A.H. 12	200+00.000								
1300	POT		203+11.060						496,628.6787	151,841.9602	S 66° 49' 45.42" E
1305	POT		205+79.811						496,914.6473	151,719.5668	S 68° 24' 56.28" E
	PC		207+04.563						497,164.5531	151,620.7010	S 66° 46' 43.81" E
1310	PI		208+12.567	25° 45' 02.71" RT	12° 07' 33.93"	472.500'	108.003'	212.358'	497,279.1990	151,571.5135	S 66° 46' 43.81" E
	CC								497,378.4529	151,528.9299	PI
	PT		209+16.922						497,092.9011	151,137.2909	
1315	POT		212+99.450						497,449.3494	151,447.4535	S 41° 01' 41.10" E
1320	POT		215+94.066						497,700.4521	151,158.8787	S 41° 01' 41.10" E
1325	POT		222+11.070						497,895.8061	150,938.3438	S 41° 32' 06.48" E
									498,302.7734	150,747.5870	S 41° 16' 06.23" E

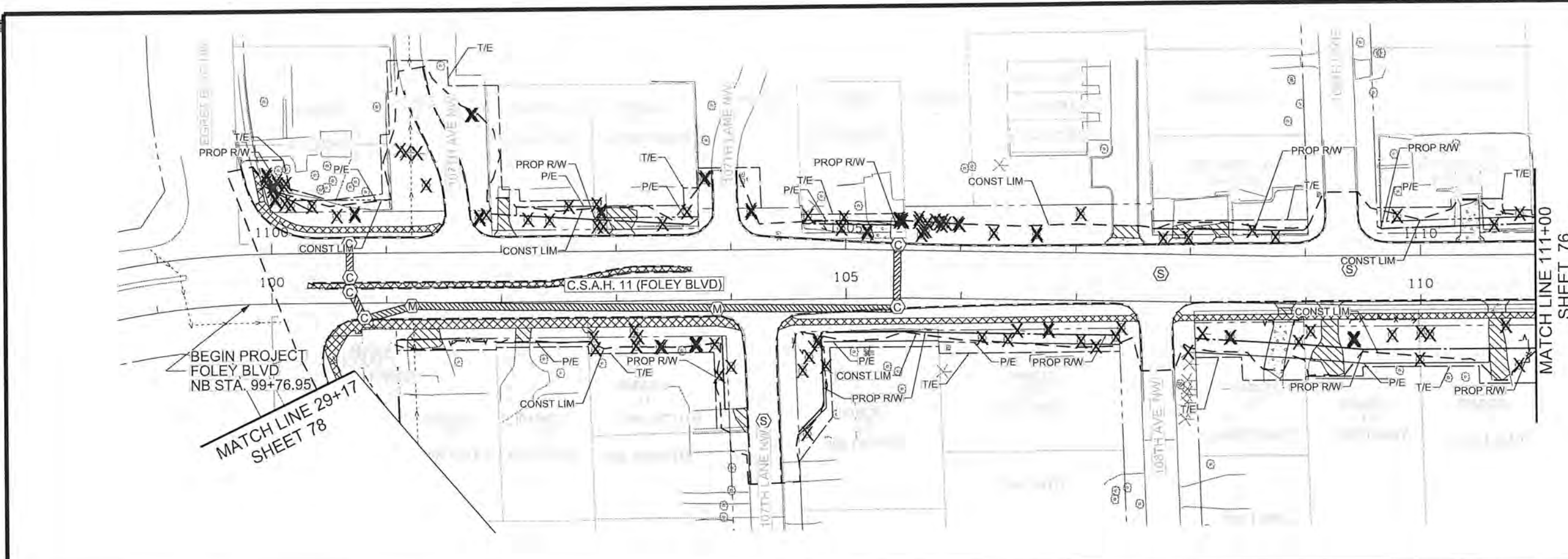
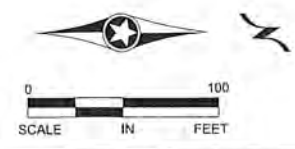
ALIGNMENT TABULATION

POINT NUMBER	POINT	ALIGNMENT	CIRCULAR CURVE DATA					COORDINATES		AZIMUTH	
			DELTA	DEGREE	RADIUS	TANGENT	LENGTH	E	N		
		C C.S.A.H. 11 <P_SB11_3>									
	PC	C.C.S.A.H. 11	1096+50.000						497,468.6942	146,603.4721	N 26° 17' 54.64" E
1160	PI		1098+28.337	27° 05' 57.69" RT	7° 44' 33.62"	740.000'	178.337'	350.000'	497,389.6824	146,763.3509	PI
	CC								498,132.1027	146,931.3276	
	PT		1100+00.000						497,392.1750	146,941.6705	N 0° 48' 03.04" E
1170	POT		1104+79.468						497,398.8765	147,421.0915	N 2° 16' 11.07" E
1180	POT		1108+72.142						497,414.4280	147,813.4579	N 0° 50' 15.52" E
1190	POT		1117+56.798						497,427.3609	148,698.0191	N 0° 02' 03.29" E
1200	POT		1121+96.841						497,427.6239	149,138.0623	N 0° 15' 26.14" E
1210	POT		1129+28.046						497,430.9071	149,869.2594	N 1° 18' 29.36" E
1220	POT		1133+68.188						497,421.8828	150,309.3088	N 0° 1' 26.14" E
1230	POT		1144+95.095						497,426.9427	151,436.2046	N 1° 41' 21.68" E
	PC		1147+32.532						497,433.9424	151,673.5383	N 1° 41' 21.68" E
1235	PI		1148+50.803	19° 45' 42.21" RT	8° 26' 17.73"	679.000'	118.271'	234.192'	497,437.4291	151,791.7577	PI
	CC								498,112.6473	151,653.5210	
	PT		1149+66.724						497,480.6816	151,901.8359	N 21° 27' 03.90" E
	PC		1151+55.614						497,549.7600	152,077.6418	N 21° 27' 03.90" E
1238	PI		1152+78.035	11° 23' 25.26" LT	4° 40' 02.99"	1,227.548'	122.421'	244.035'	497,594.5303	152,191.5829	PI
	CC								496,407.2442	152,526.5644	
1240	PT		1153+99.650						497,615.9163	152,312.1217	N 10° 03' 38.63" E

ALIGNMENT TABULATION

POINT NUMBER	POINT	ALIGNMENT	CIRCULAR CURVE DATA					COORDINATES		AZIMUTH	
			DELTA	DEGREE	RADIUS	TANGENT	LENGTH	E	N		
		C EGRET <P_EGRET_3>									
2001	POT	C.EGRET	23+00.000								
	PC		25+84.645						496,913.1851	146,879.3120	
2002	PI		27+38.830	26° 01' 55.02" LT	8° 35' 24.26"	667.000'	154.185'	303.047'	497,197.8099	146,882.6891	N 89° 19' 12.73" E
	CC								497,351.9841	146,884.5184	PI
	PT		28+87.692						497,189.8964	147,549.6422	
	PC		29+58.027						497,489.7143	146,953.8249	N 63° 17' 17.71" E
2003	PI		30+94.244	23° 05' 05.57" RT	8° 35' 24.26"	667.000'	136.217'	268.739'	497,552.5429	146,985.4405	N 63° 17' 17.71" E
	CC								497,674.2229	147,046.6705	PI
	PT		32+26.766						497,852.3608	146,389.6233	
	PC		33+15.009						497,810.1674	147,055.2874	N 86° 22' 23.27" E
2004	PI		33+65.006	2° 53' 35.18" RT	2° 53' 38.15"	1,979.860'	49.996'	99.971'	497,898.2344	147,060.8696	N 86° 22' 23.27" E
	CC								497,948.1306	147,064.0323	PI
	PT		34+14.981						498,023.4774	145,084.9749	
2005	POT		36+99.089						497,998.1228	147,064.6725	N 89° 15' 58.45" E
									498,282.2077	147,068.3109	

LEGEND	
	REMOVE BITUMINOUS PAVEMENT / MAINLINE
	REMOVE BITUMINOUS DRIVEWAY PAVEMENT
	REMOVE CONCRETE DRIVEWAY PAVEMENT
	REMOVE SIDEWALK / CONC. MEDIAN
	TREE REMOVAL
	REMOVE CB
	REMOVE STORM MH
	ADJUST SAN MH
	REMOVE C & G
	REMOVE STORM PIPE
	REMOVE RETAINING WALL
	SAWING BIT / CONC
	EXISTING R/W
	PROPOSED R/W
	TEMP. EASEMENT
	PERM. EASEMENT
	CONSTRUCTION LIMIT



REMOVAL NOTES

REFER TO TRAFFIC SIGNAL PLANS FOR TRAFFIC SIGNAL REMOVALS.

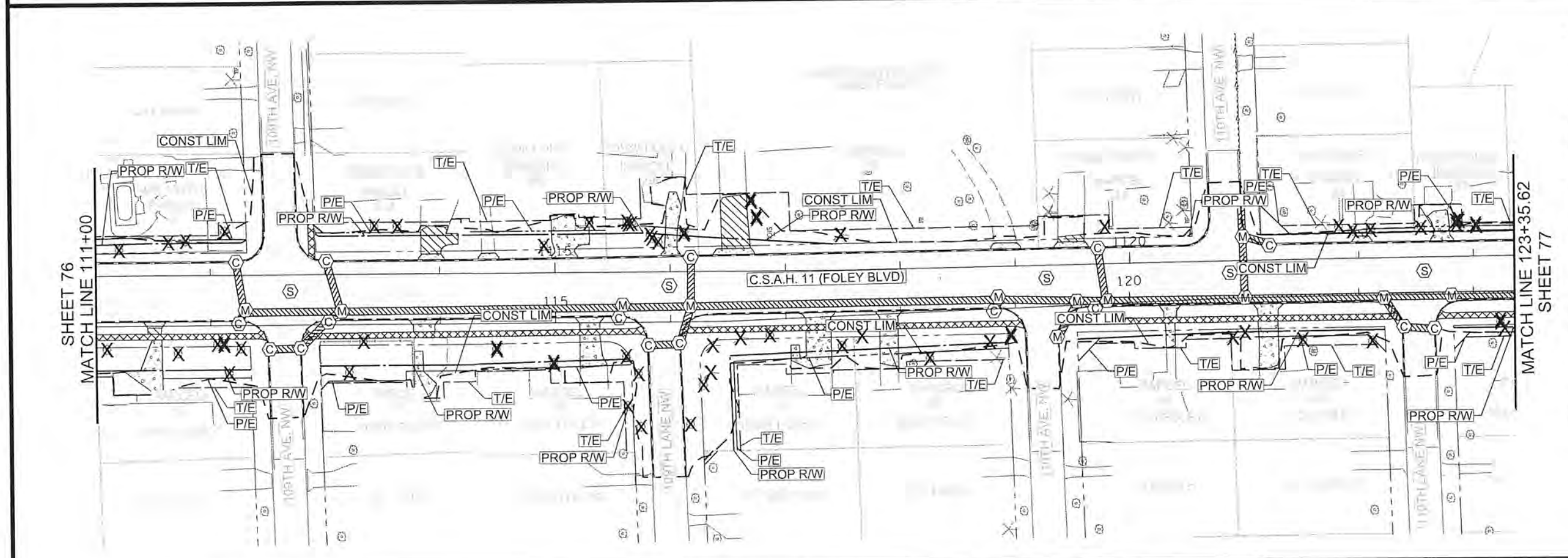
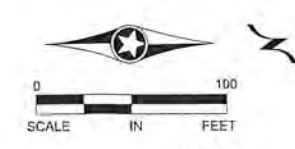
THE CONTRACTOR SHALL PERFORM ALL CLEARING AND GRUBBING AS DIRECTED AND MARKED IN THE FIELD BY THE ENGINEER. THE CONTRACTOR SHALL OTHERWISE PROTECT ALL EXISTING TREES NOT SPECIFICALLY MARKED FOR REMOVAL.

ALL MANHOLES AND CATCH BASINS WILL BE PAID FOR AS "REMOVE DRAINAGE STRUCTURE" ITEM 2104.509 CALLED OUT IN REMOVAL PLANS AS MH AND CB. FOR INFORMATION PURPOSES ONLY.

ALL PRIVATE UTILITIES TO BE RELOCATED BY OTHERS AS REQUIRED. SEE IN PLACE UTILITY TABULATION FOR MORE INFORMATION.

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SEE CITY WATERMAIN AND SEWERS PLANS FOR WATERMAIN, HYDRANT, AND SEWER REMOVAL ITEMS.



NO	DATE	BY	CKD	APPR	REVISION

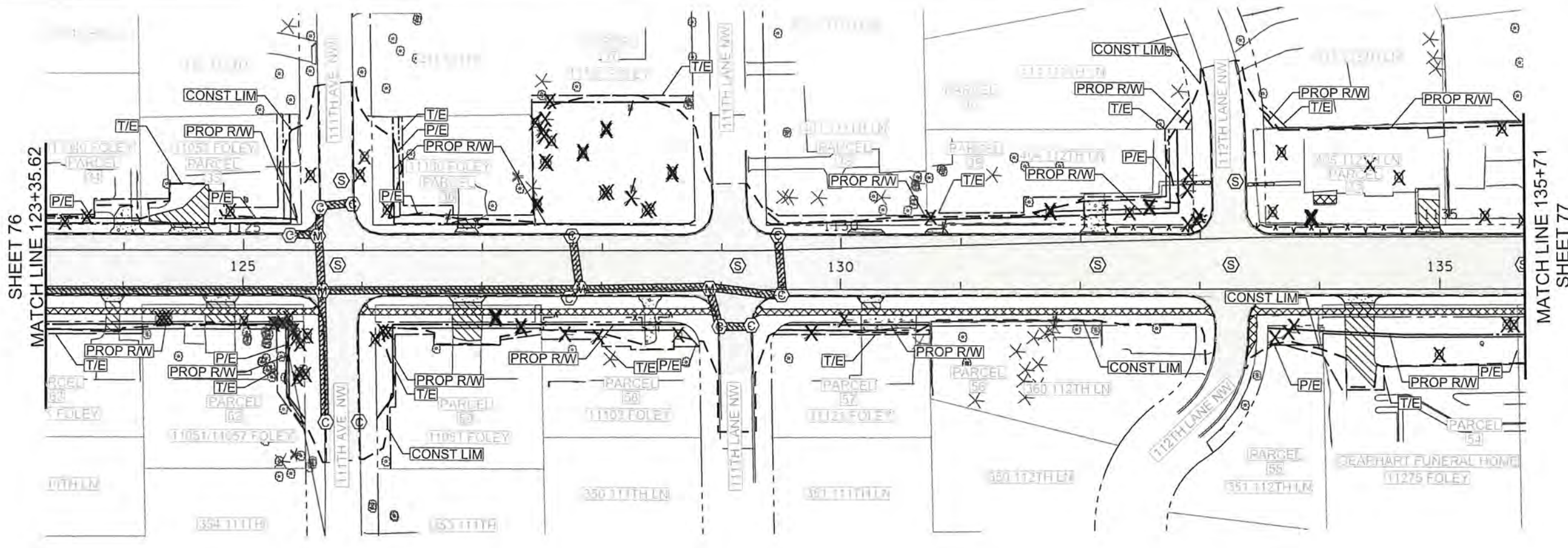
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *GINA M. PIZZO*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

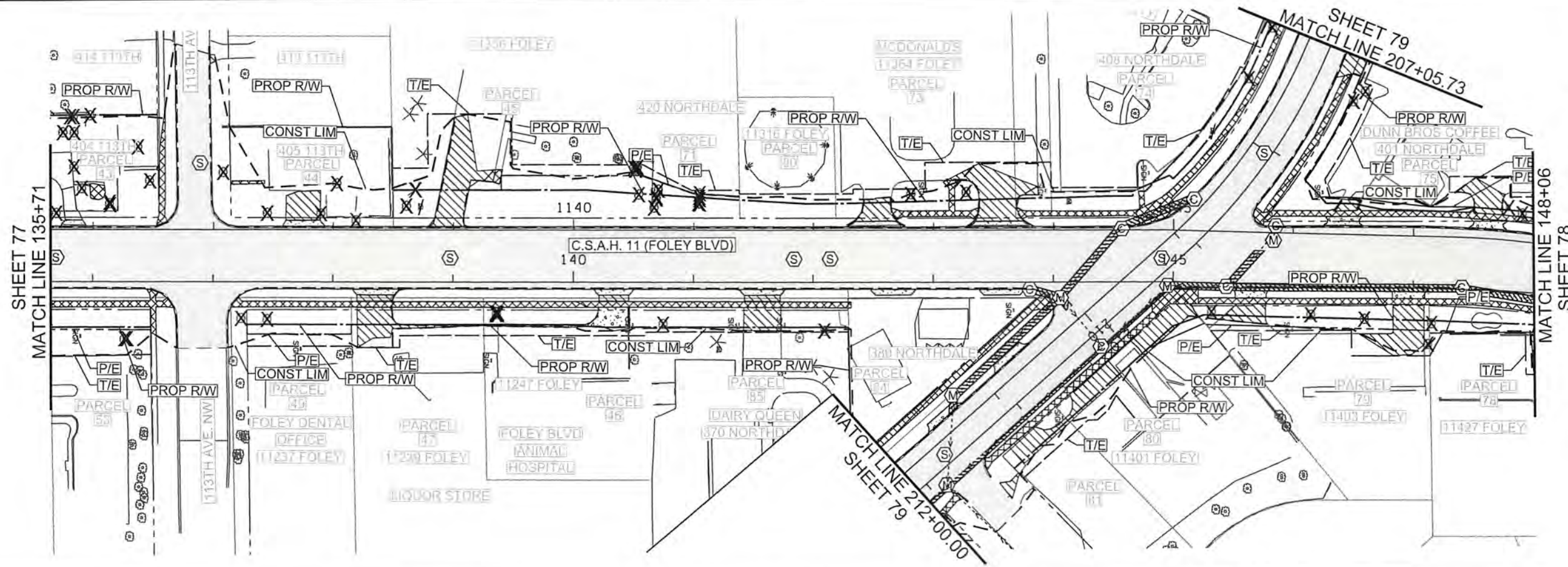
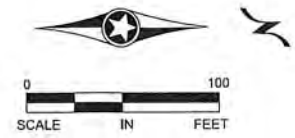
SP 002-611-034
 SP 114-020-050
 CP 13-10

REMOVALS
 FOLEY BLVD.
 STA 99+76.95 TO 111+00.00
 Sheet 76 of 298 Sheets



LEGEND

- REMOVE BITUMINOUS PAVEMENT / MAINLINE
- REMOVE BITUMINOUS DRIVEWAY PAVEMENT
- REMOVE CONCRETE DRIVEWAY PAVEMENT
- REMOVE SIDEWALK / CONC. MEDIAN
- TREE REMOVAL
- REMOVE CB
- REMOVE STORM MH
- ADJUST SAN MH
- REMOVE C & G
- REMOVE STORM PIPE
- REMOVE RETAINING WALL
- SAWING BIT / CONC
- EXISTING R/W
- PROPOSED R/W
- TEMP. EASEMENT
- PERM. EASEMENT
- CONSTRUCTION LIMIT



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2 OF 4

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_RM_P2.dgn 05/20/2016 7:28:41 AM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *Gina M. Pizzo*

DATE: 5-20-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

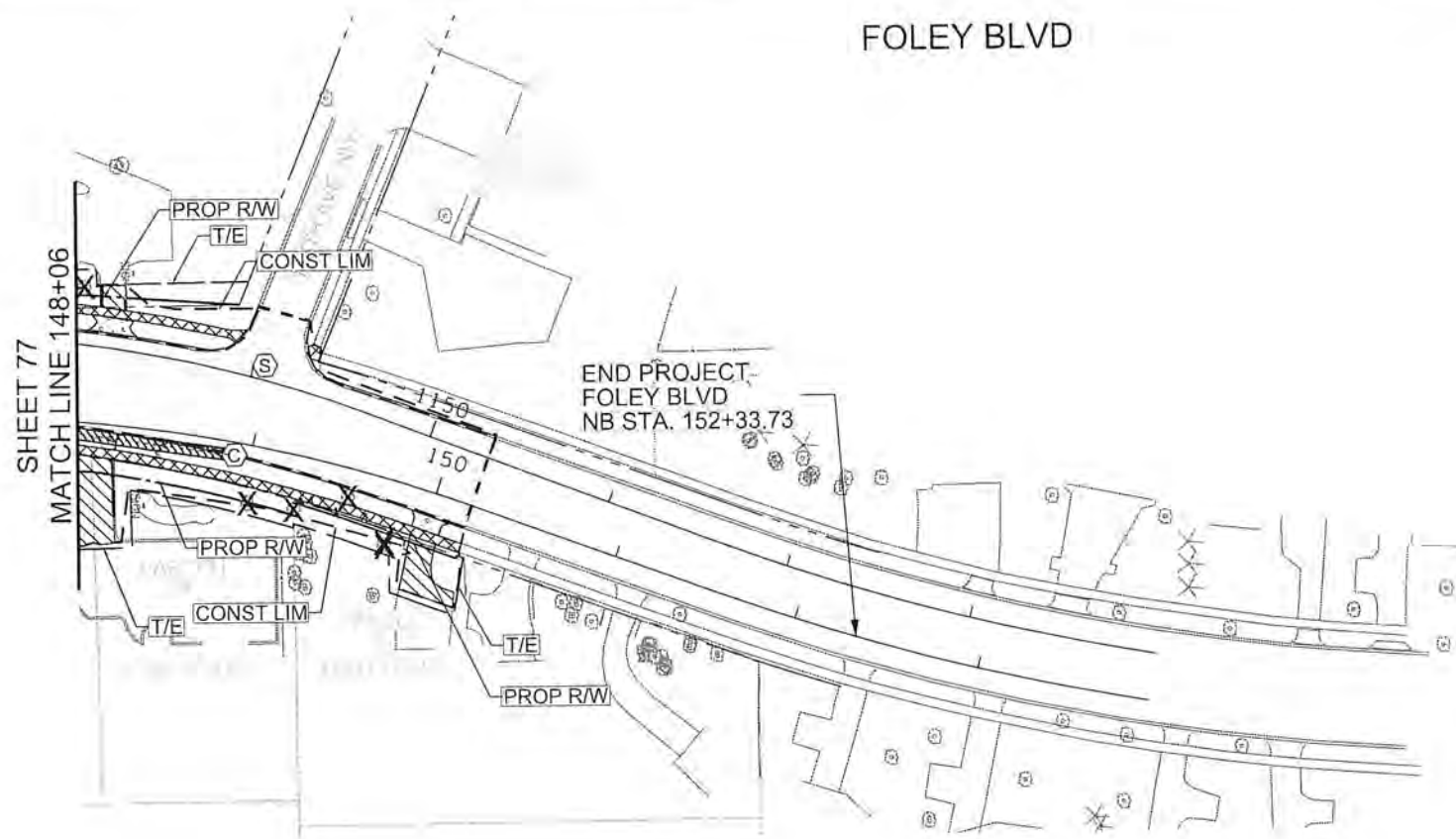
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SP 114-020-050
CP 13-10

REMOVALS

STA 123+36 TO 148+06

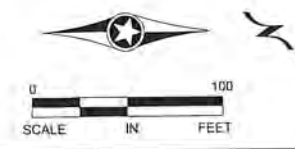
Sheet 77 of 298 Sheets

FOLEY BLVD

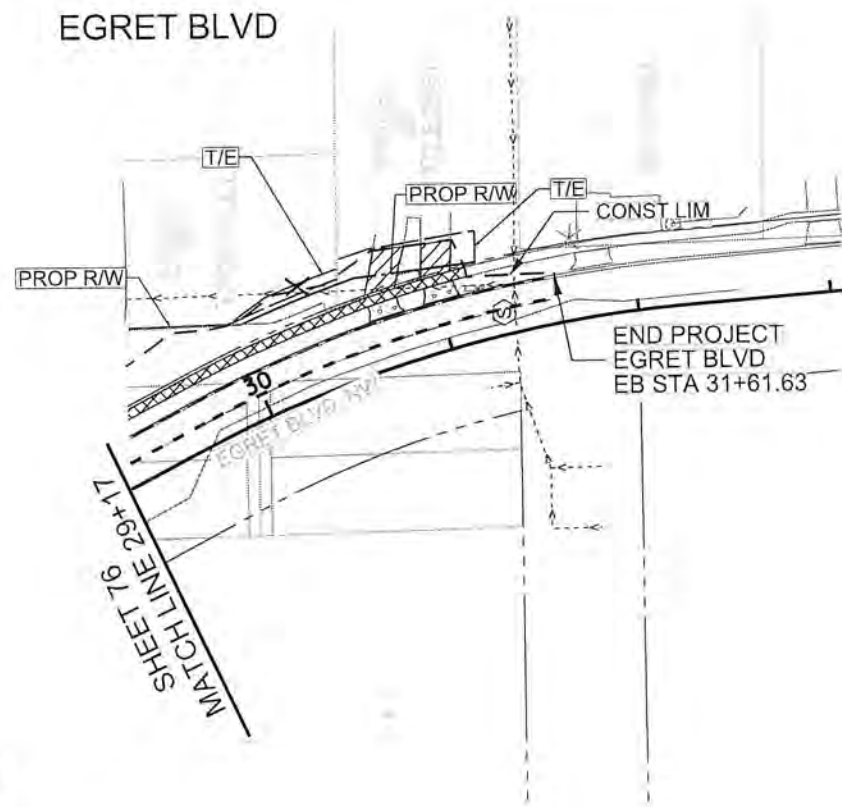


LEGEND

[Symbol]	REMOVE BITUMINOUS PAVEMENT / MAINLINE
[Symbol]	REMOVE BITUMINOUS DRIVEWAY PAVEMENT
[Symbol]	REMOVE CONCRETE DRIVEWAY PAVEMENT
[Symbol]	REMOVE SIDEWALK / CONC. MEDIAN
[Symbol]	CLEARING AND GRUBBING (ACRE)
[Symbol]	TREE REMOVAL
[Symbol]	REMOVE CB
[Symbol]	REMOVE STORM MH
[Symbol]	ADJUST SAN MH
[Symbol]	REMOVE C & G
[Symbol]	REMOVE STORM PIPE
[Symbol]	REMOVE RETAINING WALL
[Symbol]	SAWING BIT / CONC
[Symbol]	EXISTING R/W
[Symbol]	PROPOSED R/W
[Symbol]	TEMP. EASEMENT
[Symbol]	PERM. EASEMENT
[Symbol]	CONSTRUCTION LIMIT



EGRET BLVD



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NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_RM_P3.dgn 05/13/2016 12:57:08 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

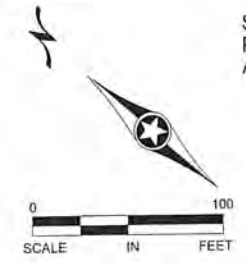
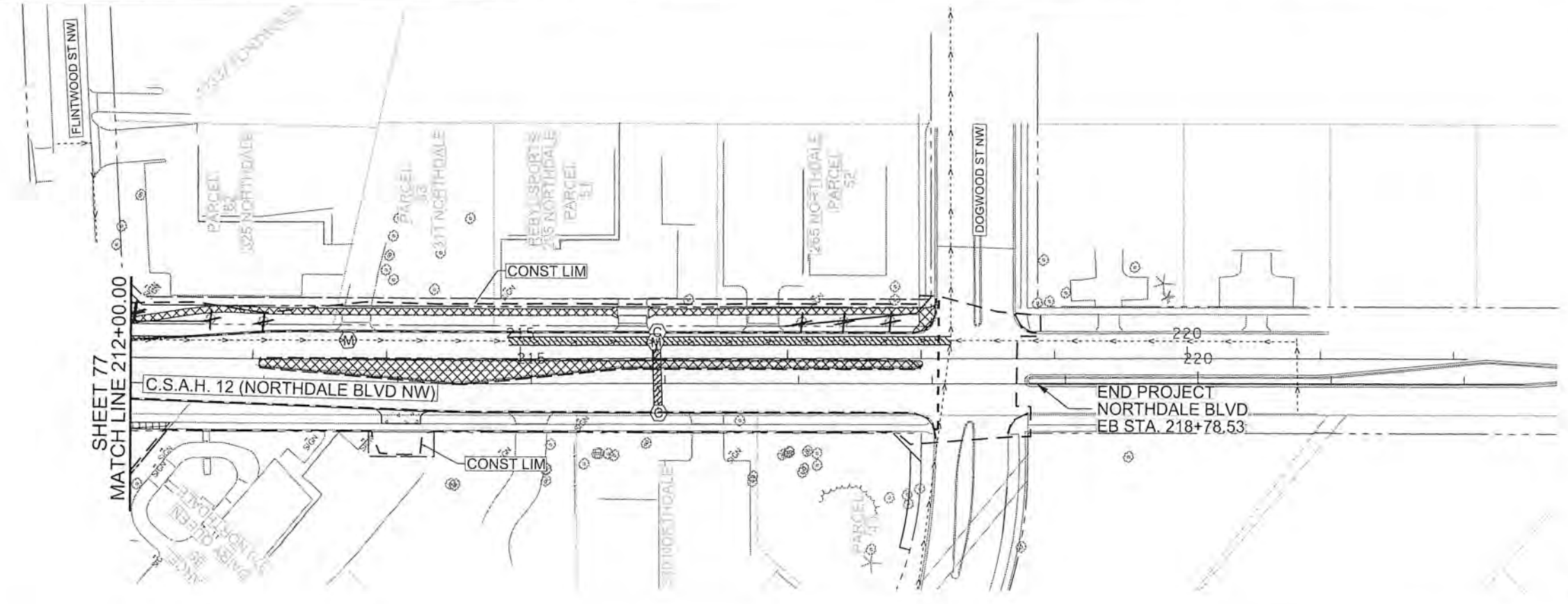
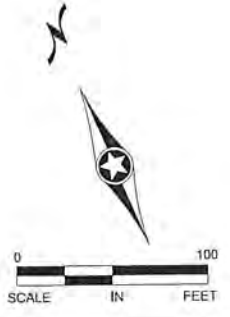
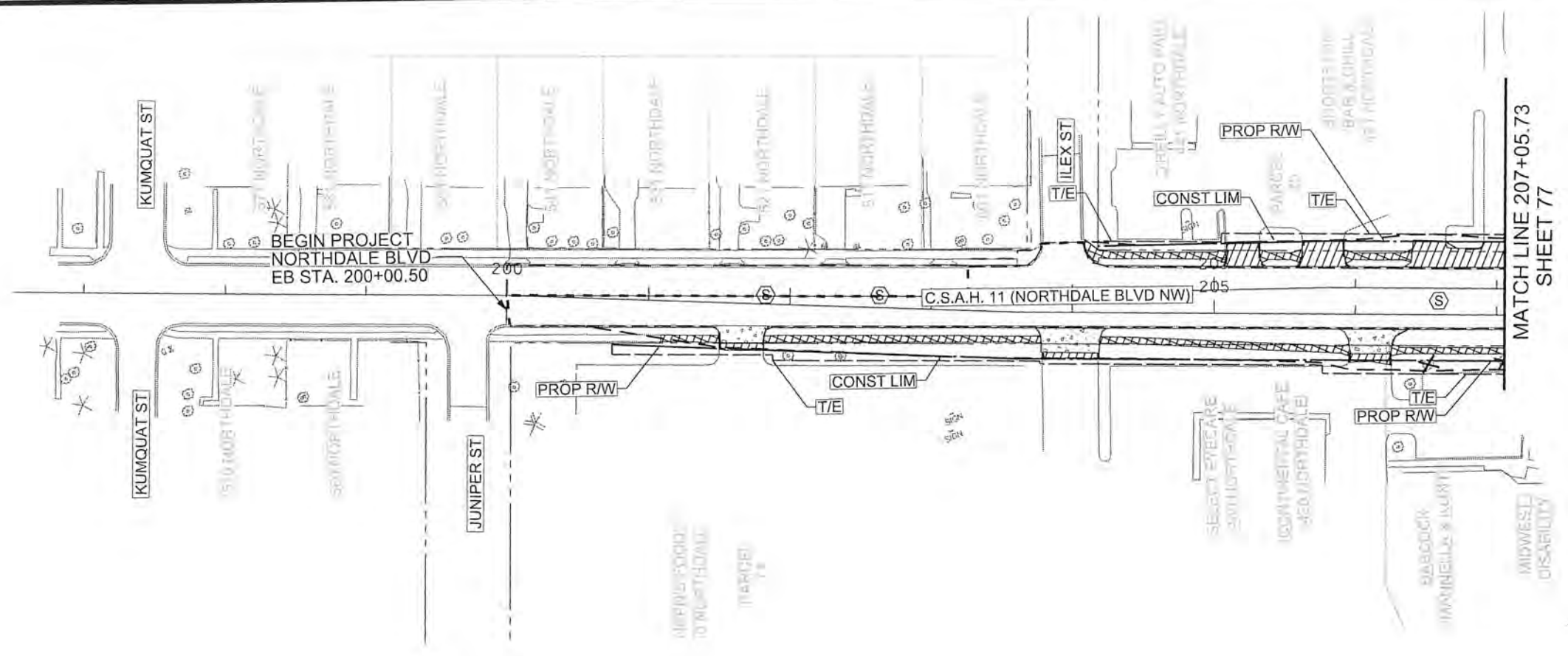
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

REMOVALS
 FOLEY BLVD & EGRET BLVD

Sheet 78 of 298 Sheets

LEGEND	
	REMOVE BITUMINOUS PAVEMENT / MAINLINE
	REMOVE BITUMINOUS DRIVEWAY PAVEMENT
	REMOVE CONCRETE DRIVEWAY PAVEMENT
	REMOVE SIDEWALK / CONC. MEDIAN
	TREE REMOVAL
	REMOVE CB
	REMOVE STORM MH
	ADJUST SAN MH
	REMOVE C & G
	REMOVE STORM PIPE
	REMOVE RETAINING WALL
	SAWING BIT / CONC
	EXISTING R/W
	PROPOSED R/W
	TEMP. EASEMENT
	PERM. EASEMENT
	CONSTRUCTION LIMIT



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NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_RM_P4.dgn 05/13/2016 12:57:10 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *Gina M. Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

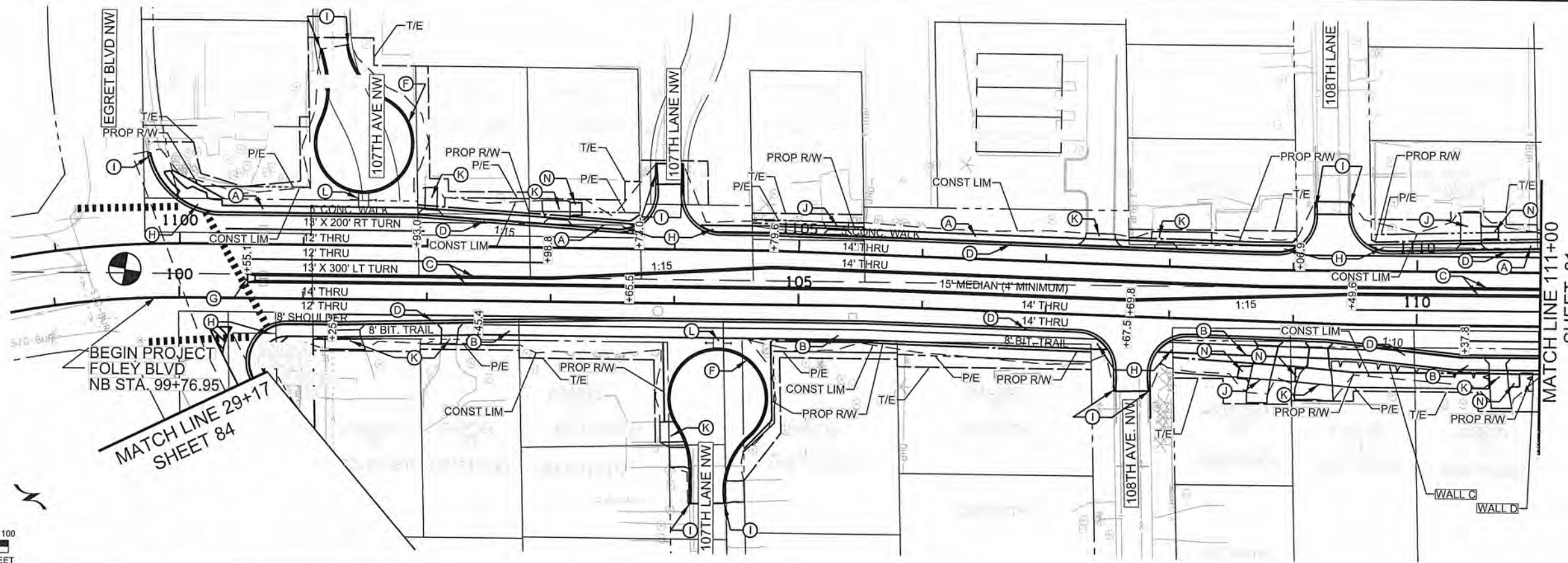
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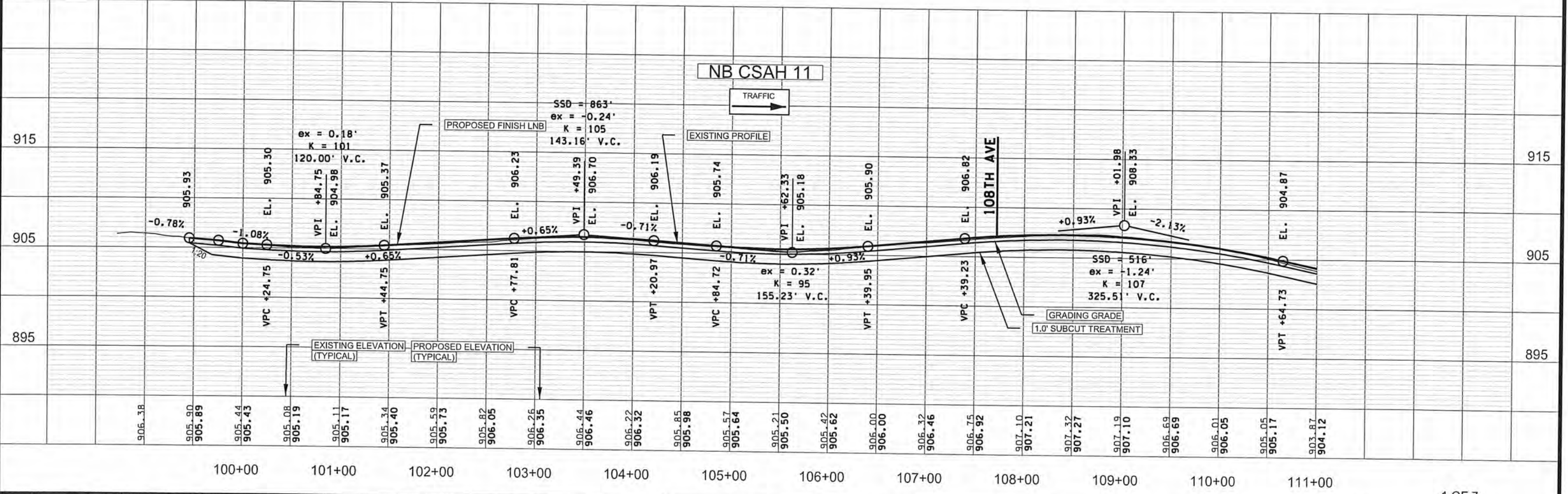
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

REMOVALS
NORTHDALE BLVD.
STA 196+50 TO 218+04.77
Sheet 79 of 298 Sheets



- CONSTRUCTION NOTES:**
- (A) CONCRETE WALK
 - (B) BITUMINOUS TRAIL
 - (C) B418 CURB & GUTTER
 - (D) B424 CURB & GUTTER
 - (E) B618 CURB & GUTTER
 - (F) S518 CURB & GUTTER
 - (G) CONCRETE APPROACH NOSE STD. PLATE 7113
 - (H) PEDESTRIAN CURB RAMP
 - (I) 10' CURB TRANSITION
 - (J) CONCRETE DRIVEWAY
 - (K) BITUMINOUS DRIVEWAY
 - (L) GATE
 - (N) TURNAROUND
 - (P) B612 CURB & GUTTER
 - ▬ MODULAR BLOCK RET. WALL
 - ▬ CHAIN LINK FENCE
- ALL DIMENSIONS ARE TO FACE OF CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- SEE SHEETS 90 - 104 FOR INTERSECTION DETAILS.
- SIGNAL SYSTEM



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_PP_P1.dgn
05/17/2016 11:34:31 AM

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PRINT NAME: GINA M. PIZZO
SIGNATURE: *[Signature]*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
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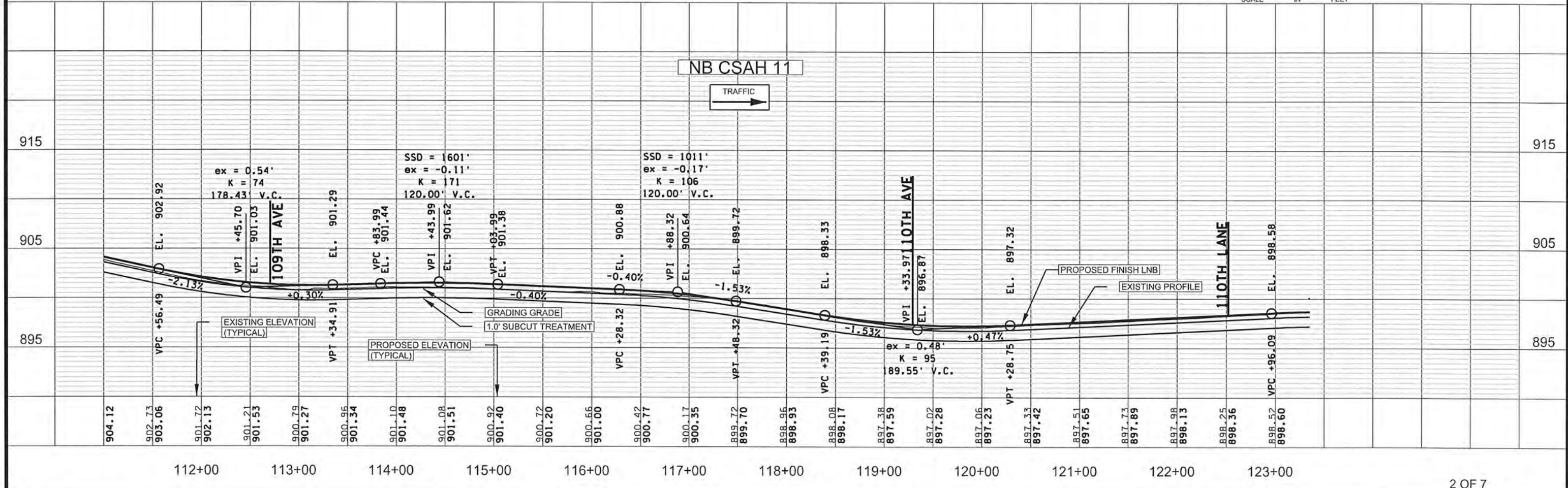
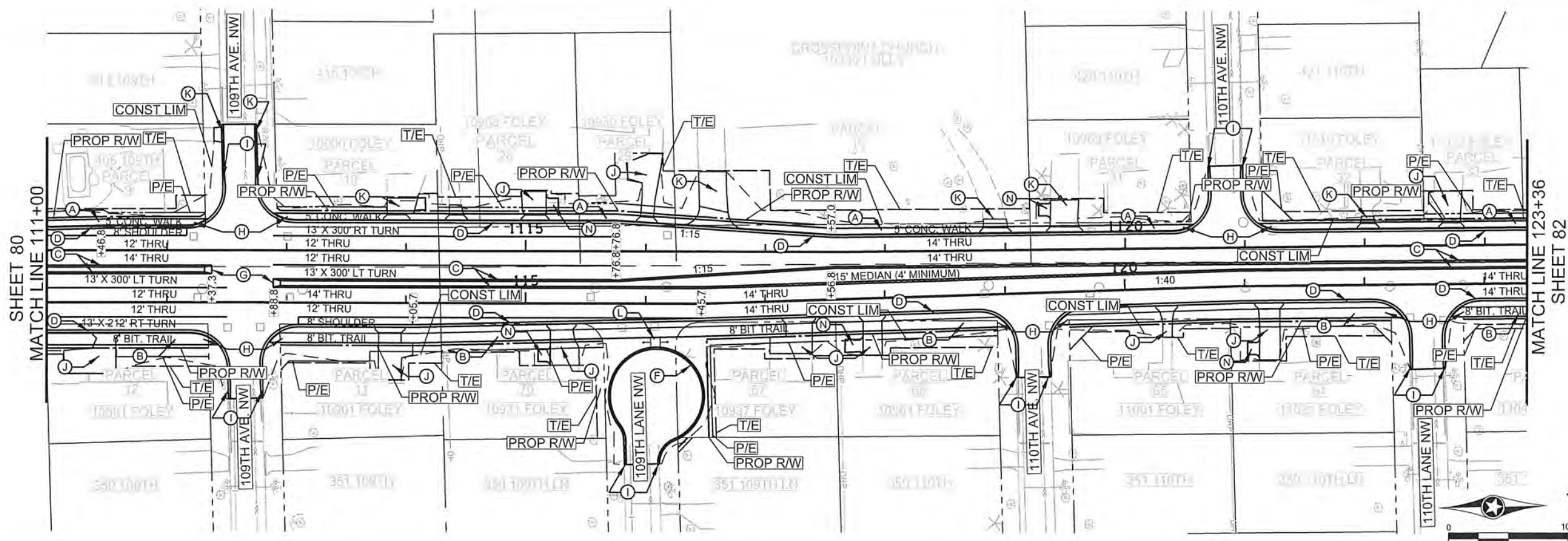
**ANOKA COUNTY
HIGHWAY DEPT.**

SP 002-611-034
SP 114-020-050
CP 13-10

CONSTRUCTION PLAN
STA 99+50 TO 111+00
Sheet 80 of 298 Sheets

CONSTRUCTION NOTES:

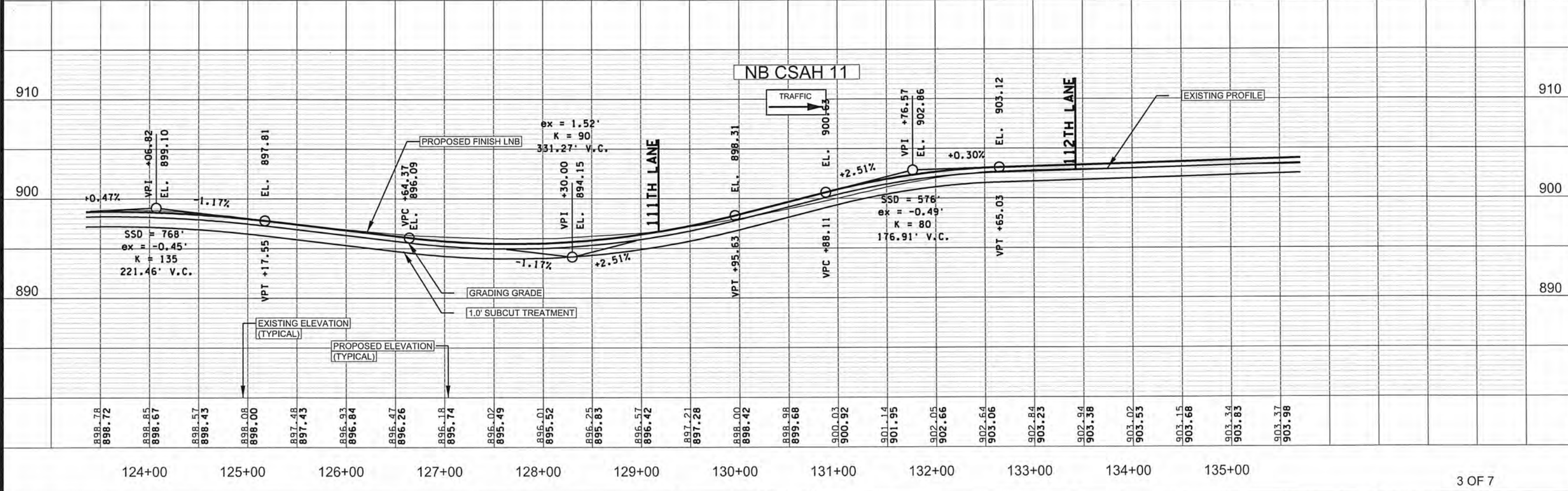
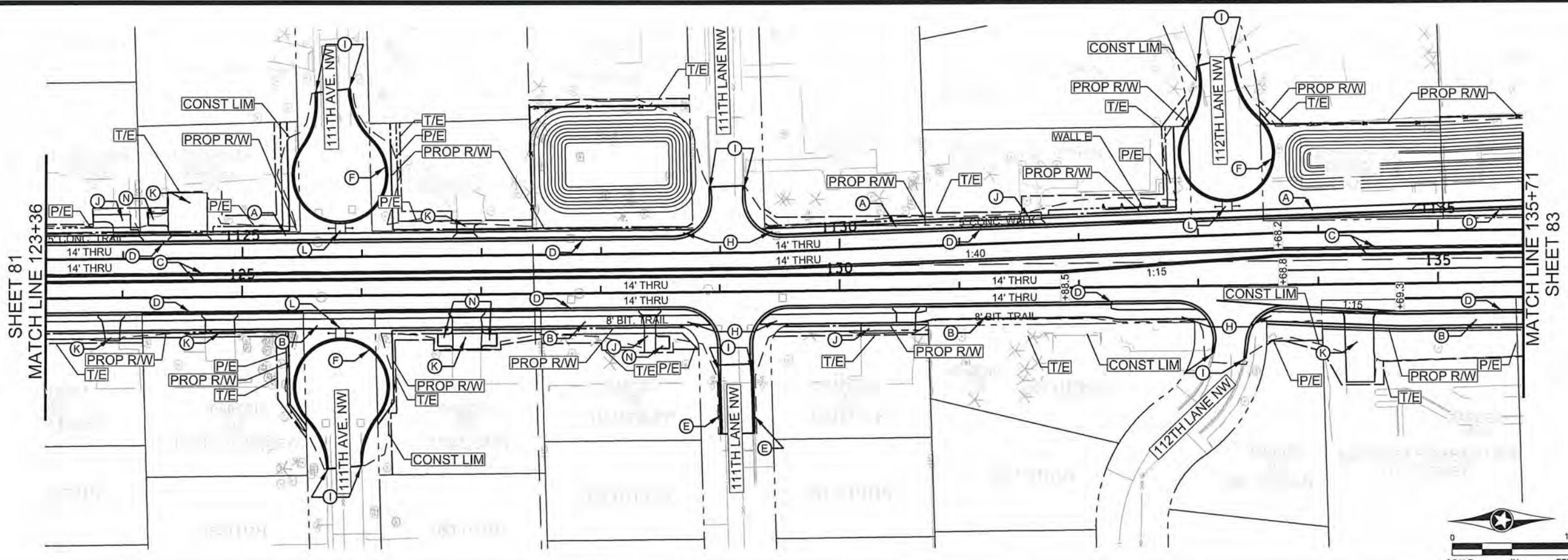
- (A) CONCRETE WALK
 - (B) BITUMINOUS TRAIL
 - (C) B418 CURB & GUTTER
 - (D) B424 CURB & GUTTER
 - (E) B618 CURB & GUTTER
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 - (G) CONCRETE APPROACH NOSE STD. PLATE 7113
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- SIGNAL SYSTEM



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NO. DATE BY CKD APPR REVISION NAME: P:\02-611-34\Plan\002-611-034_PP_P2.dgn 05/13/2016 12:57:20 PM					

CONSTRUCTION NOTES:

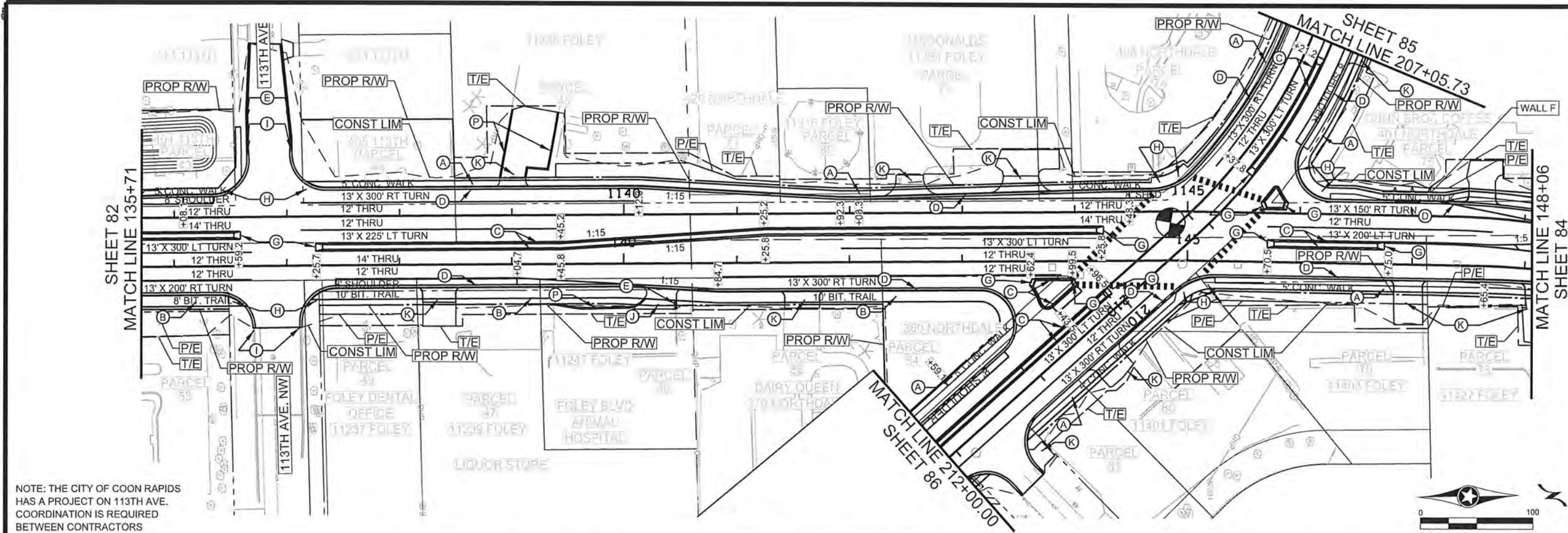
- (A) CONCRETE WALK
 - (B) BITUMINOUS TRAIL
 - (C) B418 CURB & GUTTER
 - (D) B424 CURB & GUTTER
 - (E) B618 CURB & GUTTER
 - (F) S518 CURB & GUTTER
 - (G) CONCRETE APPROACH NOSE STD. PLATE 7113
 - (H) PEDESTRIAN CURB RAMP
 - (I) 10' CURB TRANSITION
 - (J) CONCRETE DRIVEWAY
 - (K) BITUMINOUS DRIVEWAY
 - (L) GATE
 - (N) TURNAROUND
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 - CHAIN LINK FENCE
- ALL DIMENSIONS ARE TO FACE OF CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- SEE SHEETS 90 - 104 FOR INTERSECTION DETAILS.
- SIGNAL SYSTEM



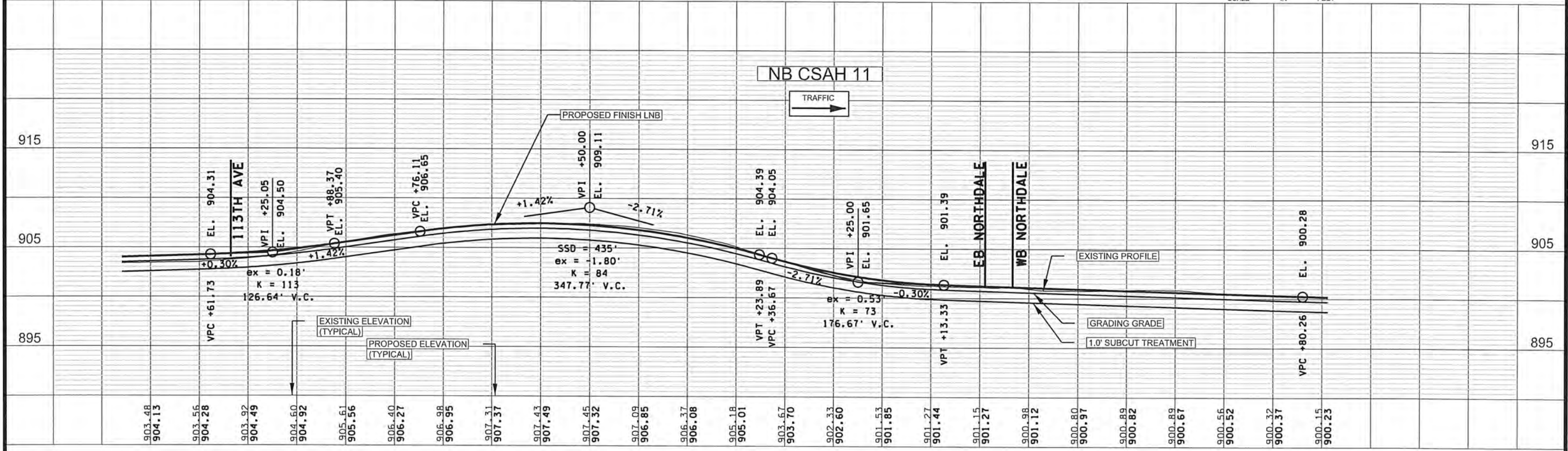
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NO	DATE	BY	CKD	APPR	REVISION					
NAME: P:\02-611-34\Plan\002-611-034_PP_P3.dgn						05/17/2016	11:34:32 AM			

CONSTRUCTION NOTES:

- (A) CONCRETE WALK
 - (B) BITUMINOUS TRAIL
 - (C) B418 CURB & GUTTER
 - (D) B424 CURB & GUTTER
 - (E) B618 CURB & GUTTER
 - (F) S518 CURB & GUTTER
 - (G) CONCRETE APPROACH NOSE STD. PLATE 7113
 - (H) PEDESTRIAN CURB RAMP
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-  MODULAR BLOCK RET. WALL
 CHAIN LINK FENCE
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-  SIGNAL SYSTEM



NOTE: THE CITY OF COON RAPIDS HAS A PROJECT ON 113TH AVE. COORDINATION IS REQUIRED BETWEEN CONTRACTORS



903.48 904.13	903.56 904.28	903.92 904.49	904.60 904.92	905.61 905.56	906.40 906.27	906.98 906.95	907.31 907.37	907.43 907.49	907.45 907.32	907.09 906.85	906.37 906.08	905.18 905.01	903.67 903.70	902.33 902.60	901.53 901.85	901.27 901.44	901.15 901.27	900.98 901.12	900.80 900.97	900.89 900.82	900.89 900.67	900.56 900.52	900.32 900.37	900.15 900.23
136+00	137+00	138+00	139+00	140+00	141+00	142+00	143+00	144+00	145+00	146+00	147+00	148+00												

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_PP_P4.dgn 05/13/2016 12:57:23 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *Gina M. Pizzo*


DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

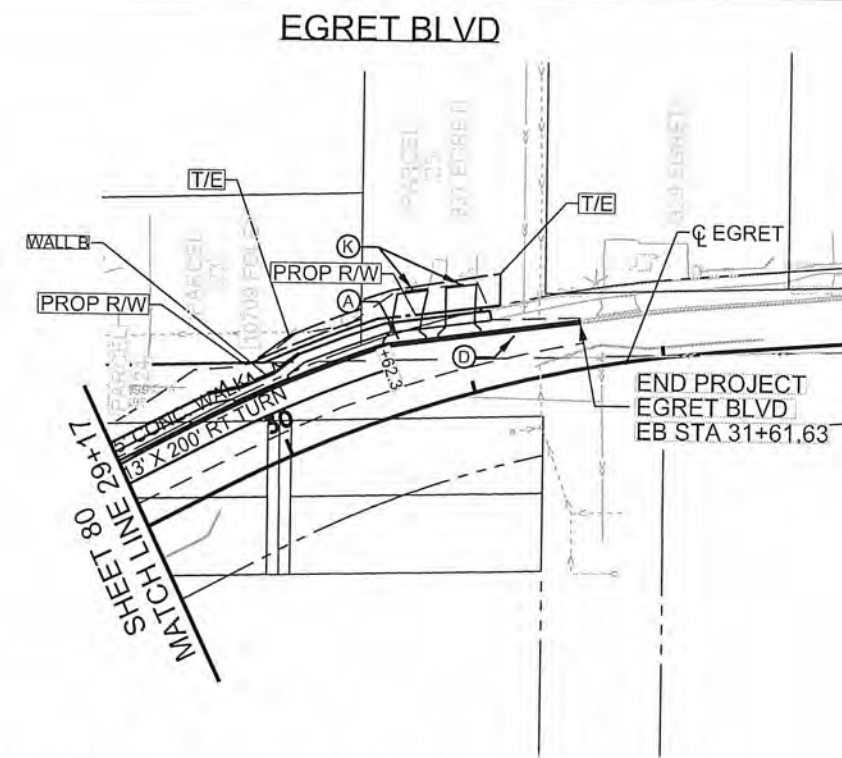
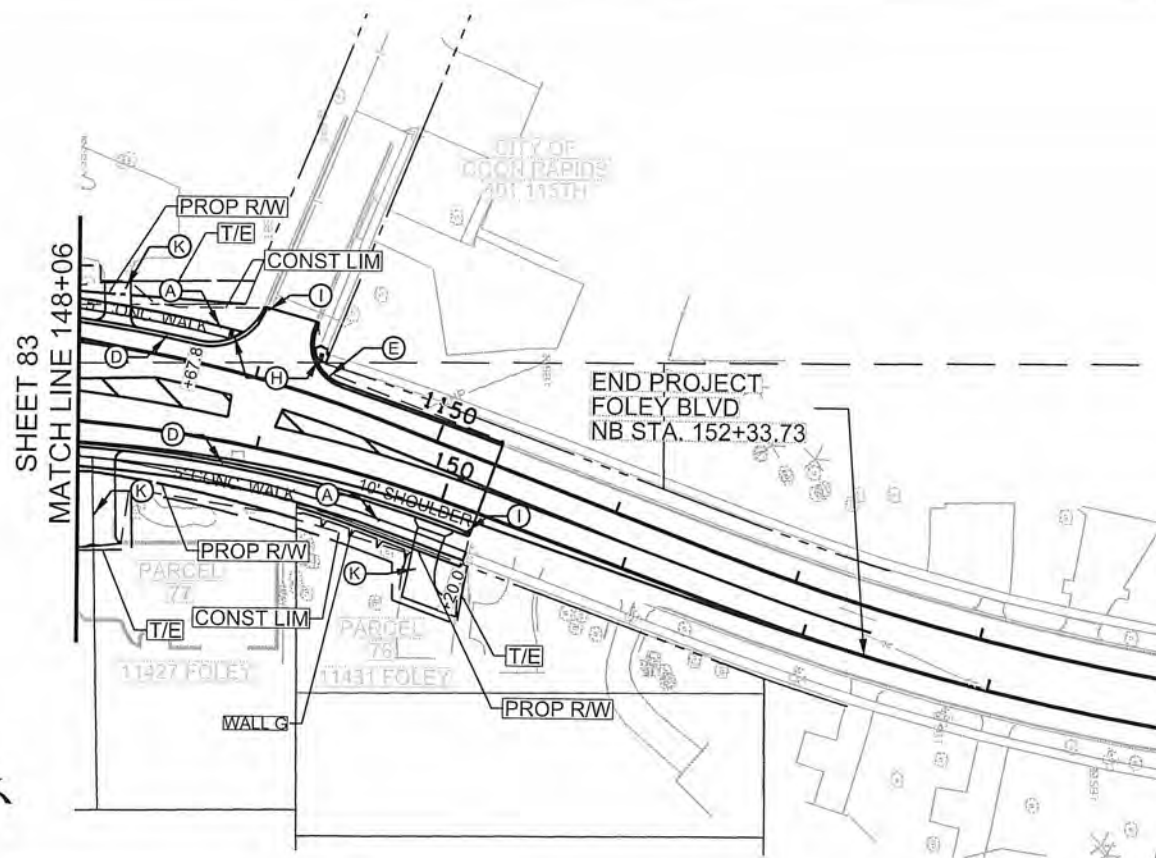


SP 002-611-034
SP 114-020-050
CP 13-10

CONSTRUCTION PLAN

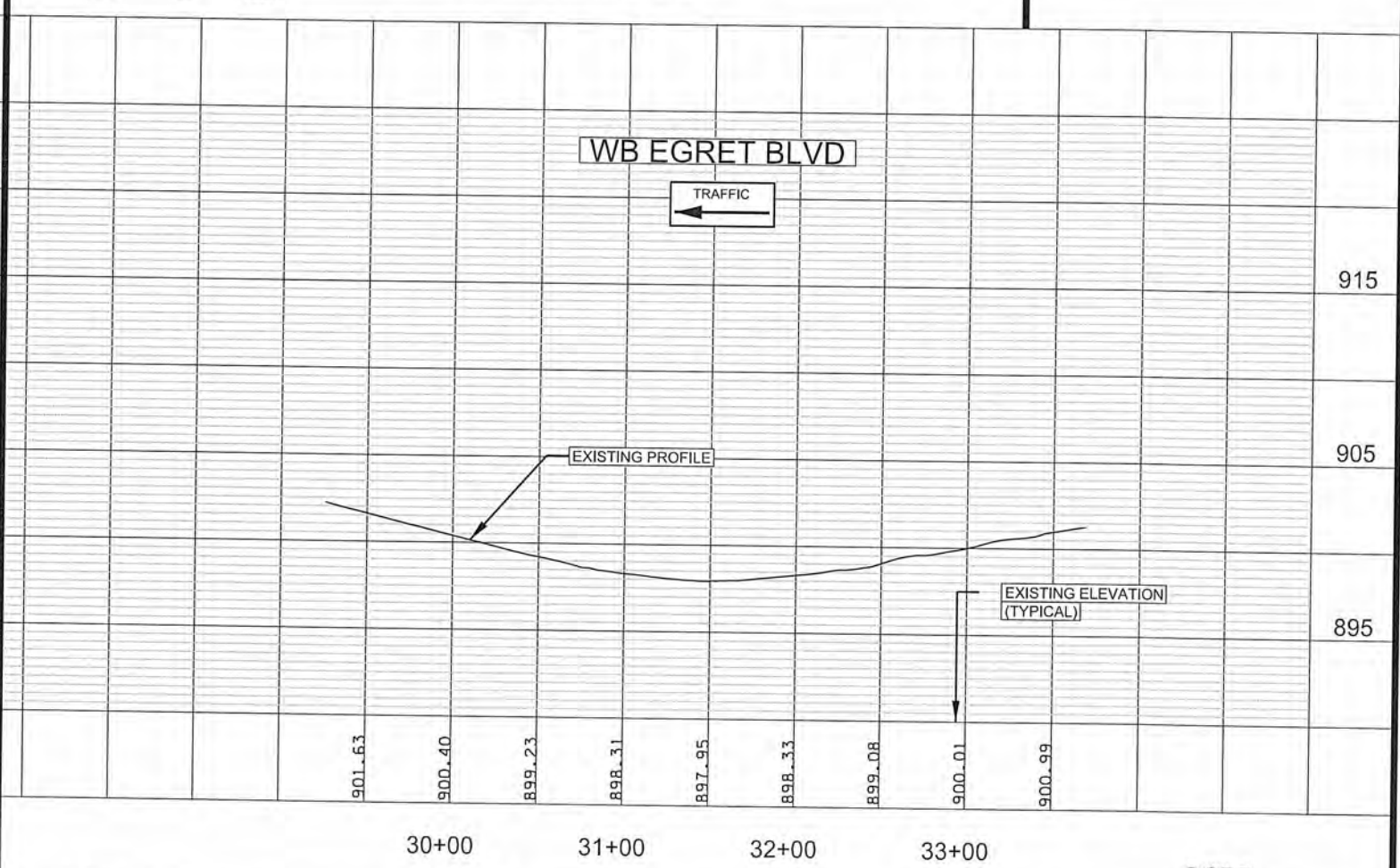
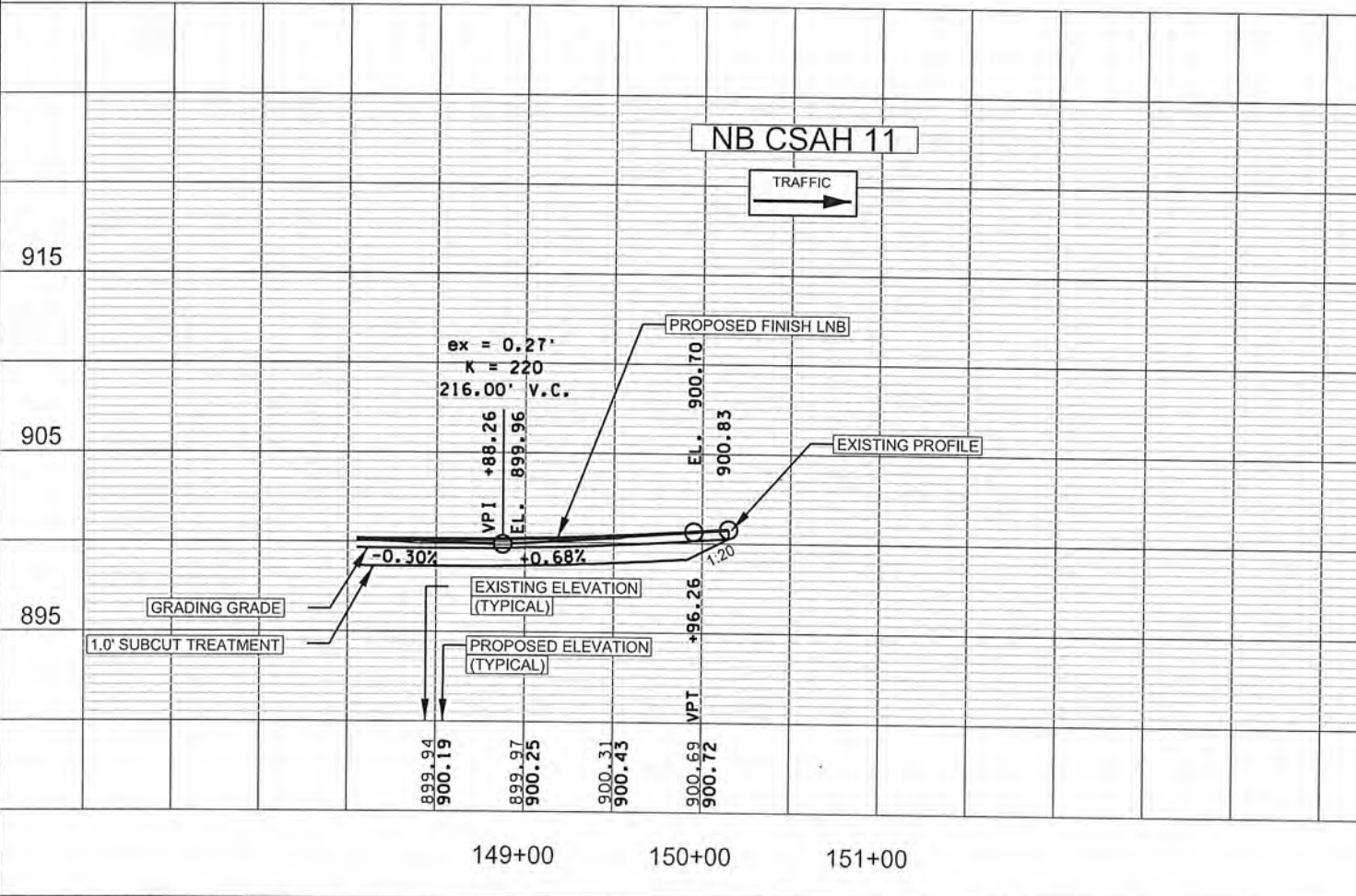
STA 135+71 TO 148+06

Sheet 83 of 298 Sheets



- CONSTRUCTION NOTES:**
- (A) CONCRETE WALK
 - (B) BITUMINOUS TRAIL
 - (C) B418 CURB & GUTTER
 - (D) B424 CURB & GUTTER
 - (E) B618 CURB & GUTTER
 - (F) S518 CURB & GUTTER
 - (G) CONCRETE APPROACH NOSE STD. PLATE 7113
 - (H) PEDESTRIAN CURB RAMP
 - (I) 10' CURB TRANSITION
 - (J) CONCRETE DRIVEWAY
 - (K) BITUMINOUS DRIVEWAY
 - (L) GATE
 - (N) TURNAROUND
 - (P) B612 CURB & GUTTER
- MODULAR BLOCK RET. WALL
 CHAIN LINK FENCE
- ALL DIMENSIONS ARE TO FACE OF CURB OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- SEE SHEETS 90 - 104 FOR INTERSECTION DETAILS.
- SIGNAL SYSTEM

NOTE: THE CITY OF COON RAPIDS HAS A PROJECT ON EGRET BLVD. COORDINATION IS REQUIRED BETWEEN CONTRACTORS



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_PP_P5.dgn 05/13/2016 12:57:24 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

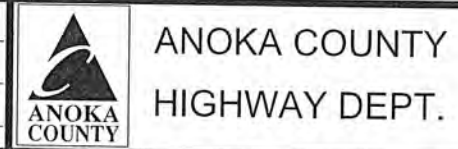
SIGNATURE: *Gina Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16



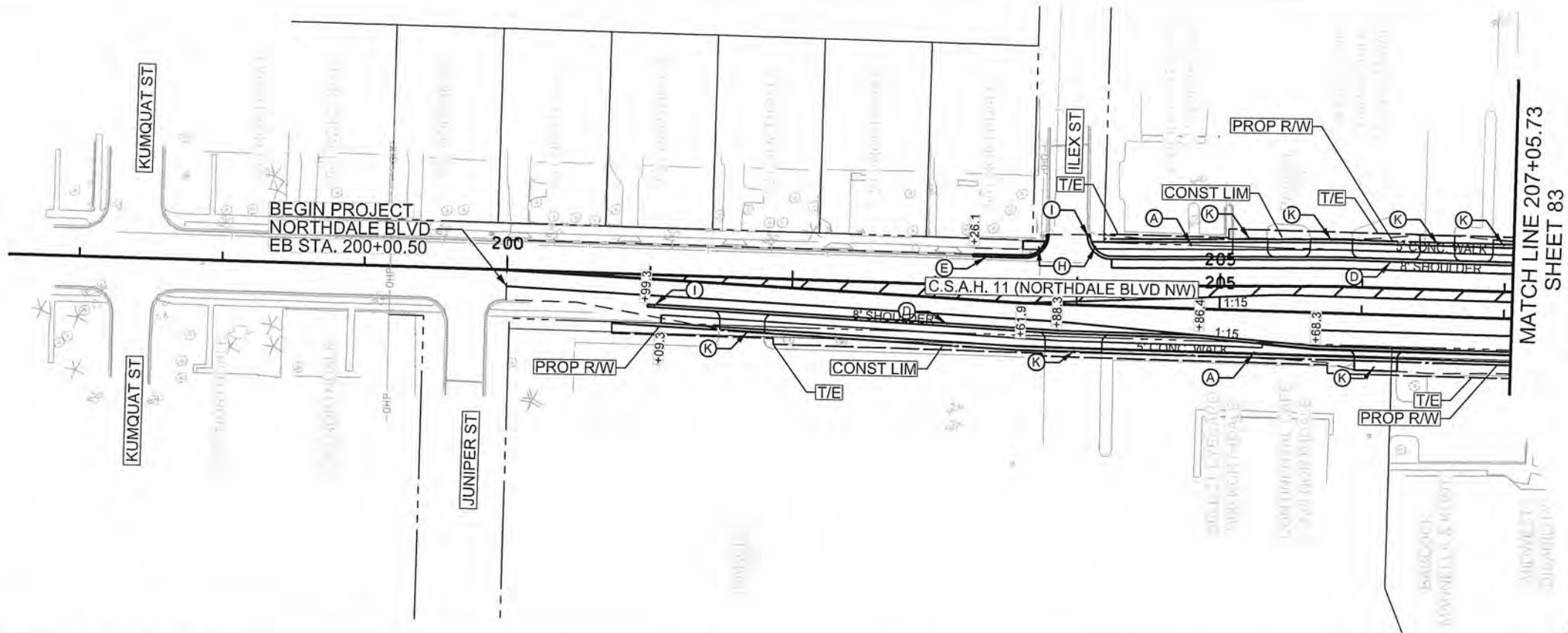
SP 002-611-034
 SP 114-020-050
 CP 13-10

5 OF 7

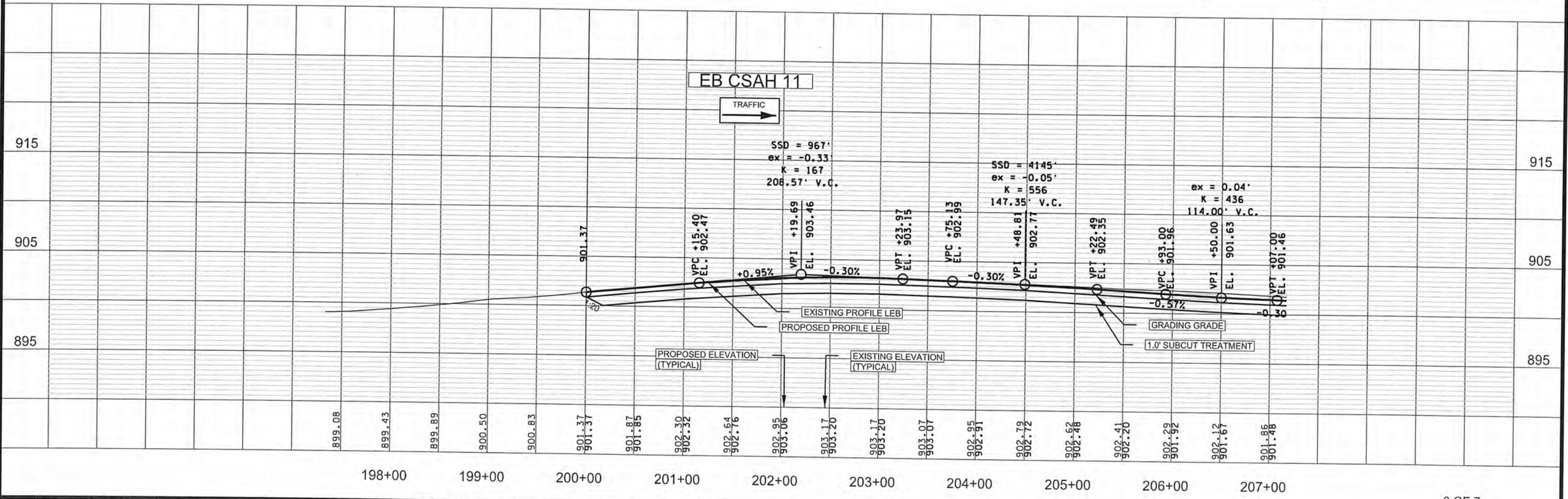
CONSTRUCTION PLAN

STA 148+06 TO _____

Sheet 84 of 298 Sheets



- CONSTRUCTION NOTES:**
- (A) CONCRETE WALK
 - (B) BITUMINOUS TRAIL
 - (C) B418 CURB & GUTTER
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 - (E) B618 CURB & GUTTER
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 CHAIN LINK FENCE
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- SEE SHEETS 90 - 104 FOR INTERSECTION DETAILS.
- SIGNAL SYSTEM



NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_PP_P6.dgn
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *Gina M. Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16



ANOKA COUNTY
HIGHWAY DEPT.

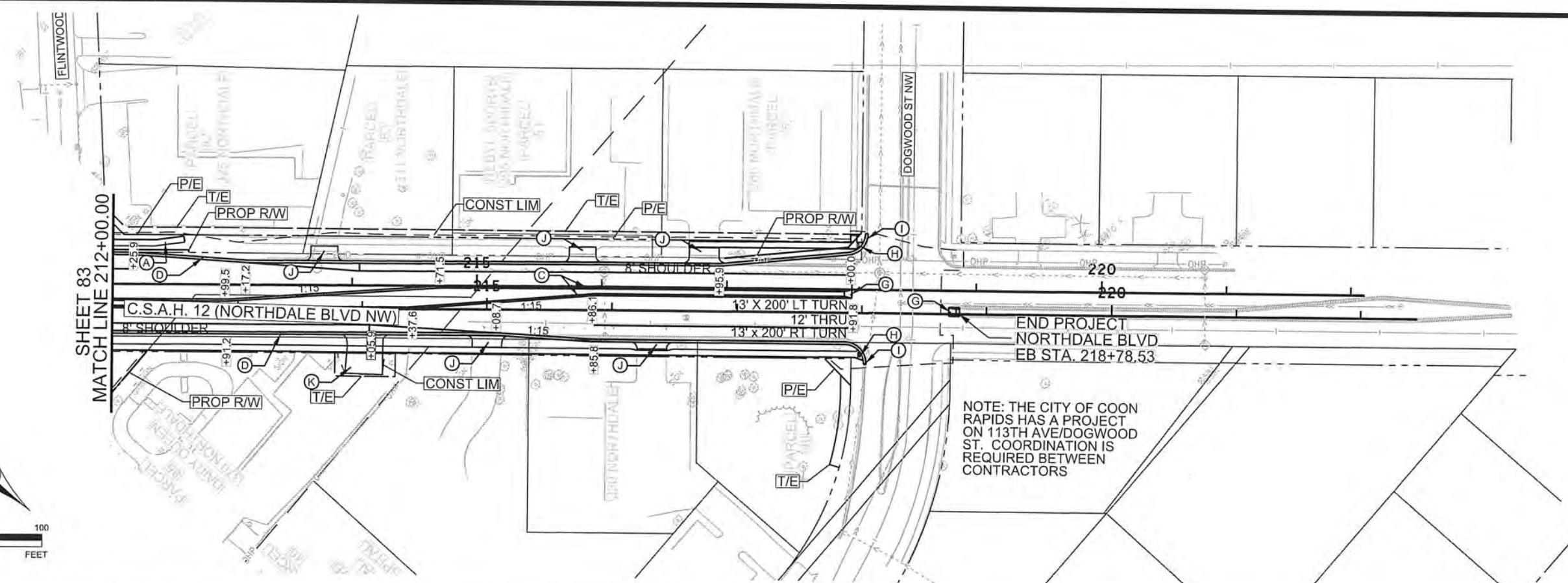
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SP 114-020-050
CP 13-10

6 OF 7

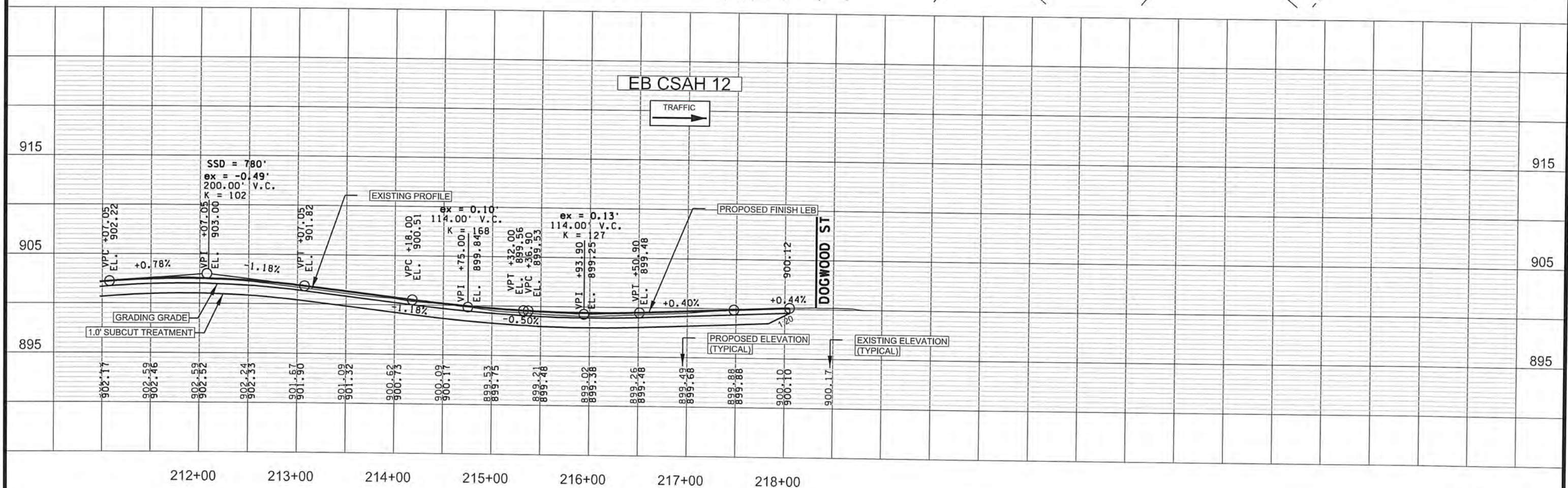
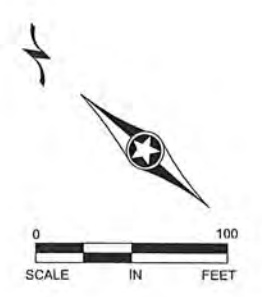
CONSTRUCTION PLAN

STA 196+50 TO 207+05.73

Sheet 85 of 298 Sheets



- CONSTRUCTION NOTES:**
- (A) CONCRETE WALK
 - (B) BITUMINOUS TRAIL
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- SEE SHEETS 90 - 104 FOR INTERSECTION DETAILS.
- SIGNAL SYSTEM



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_PP_P7.dgn 05/13/2016 12:57:26 PM

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PRINT NAME: GINA M. PIZZO

SIGNATURE: *Gina M. Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

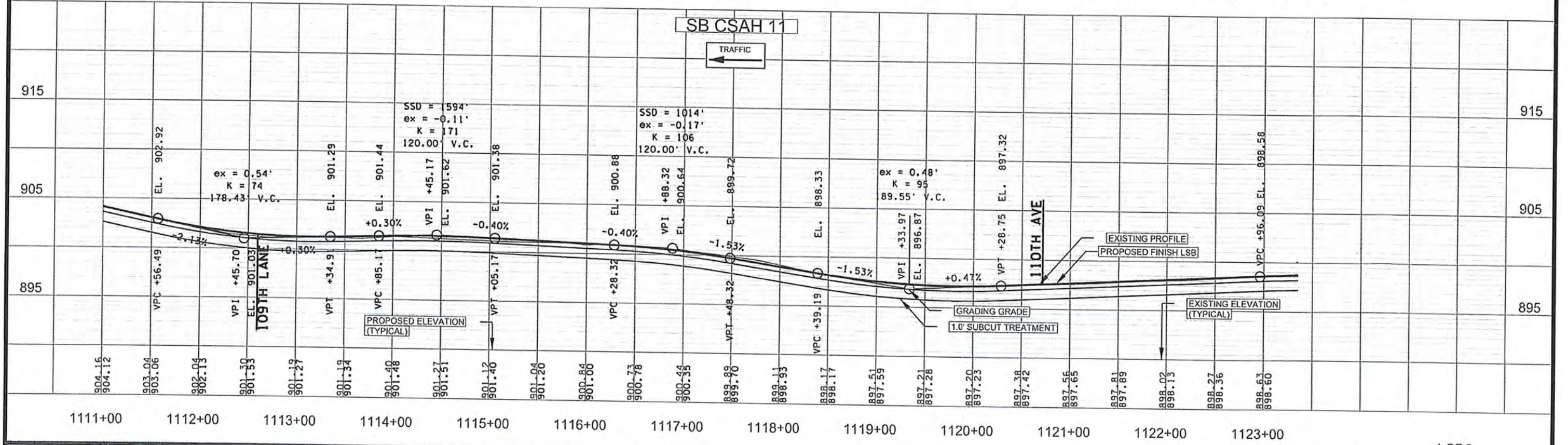
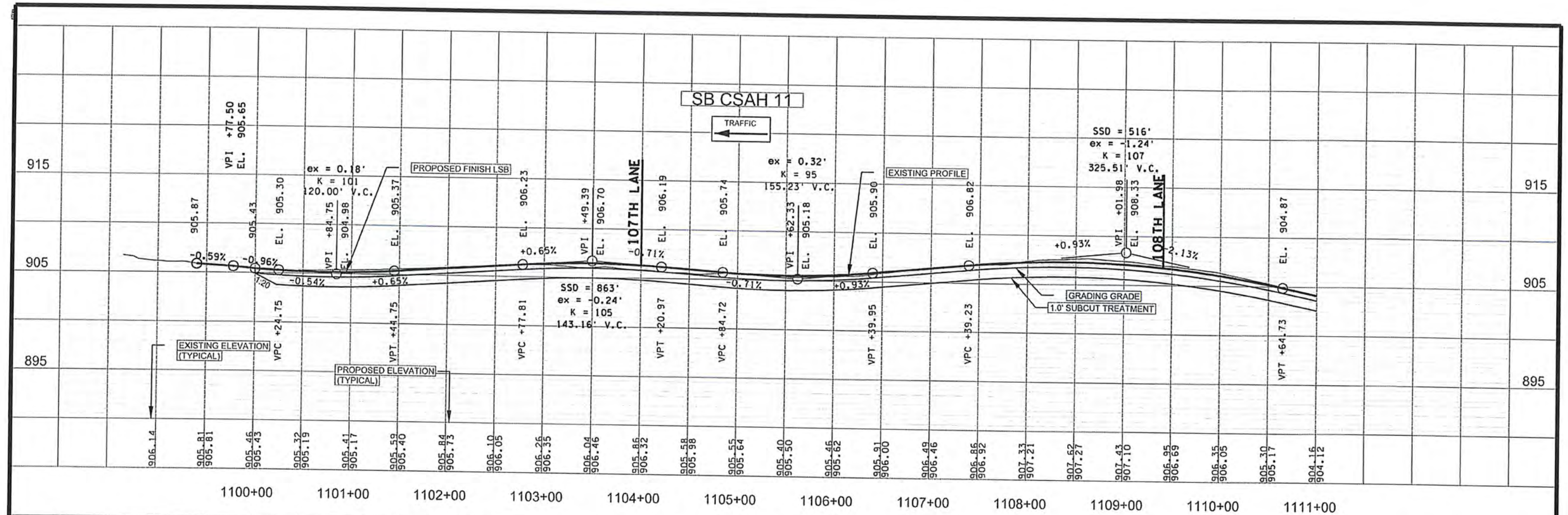
DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16



ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10



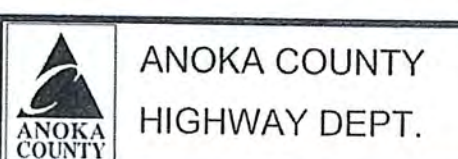
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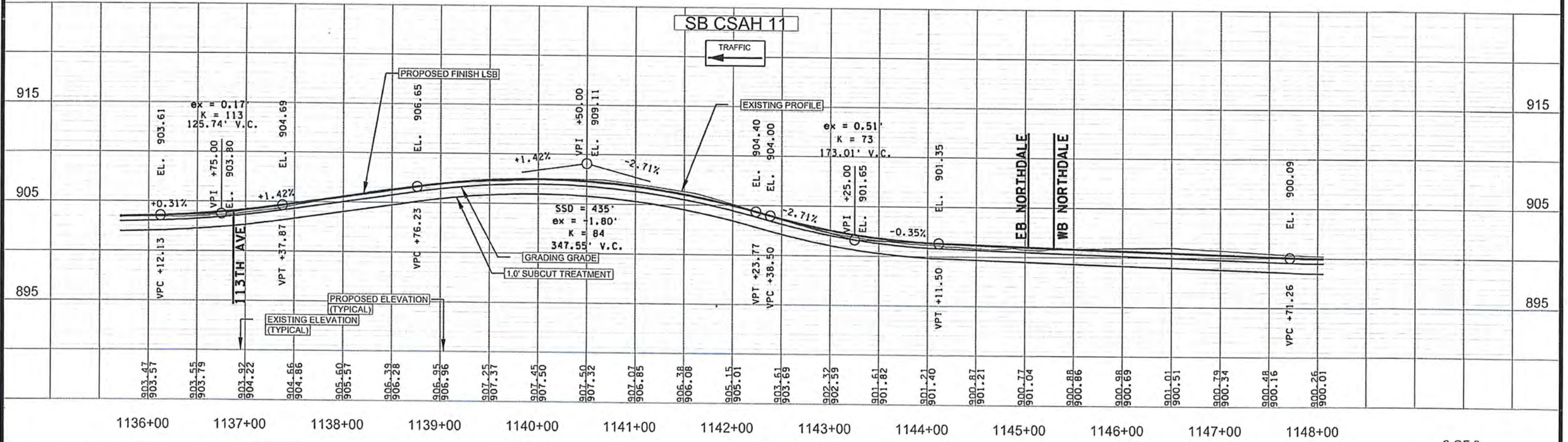
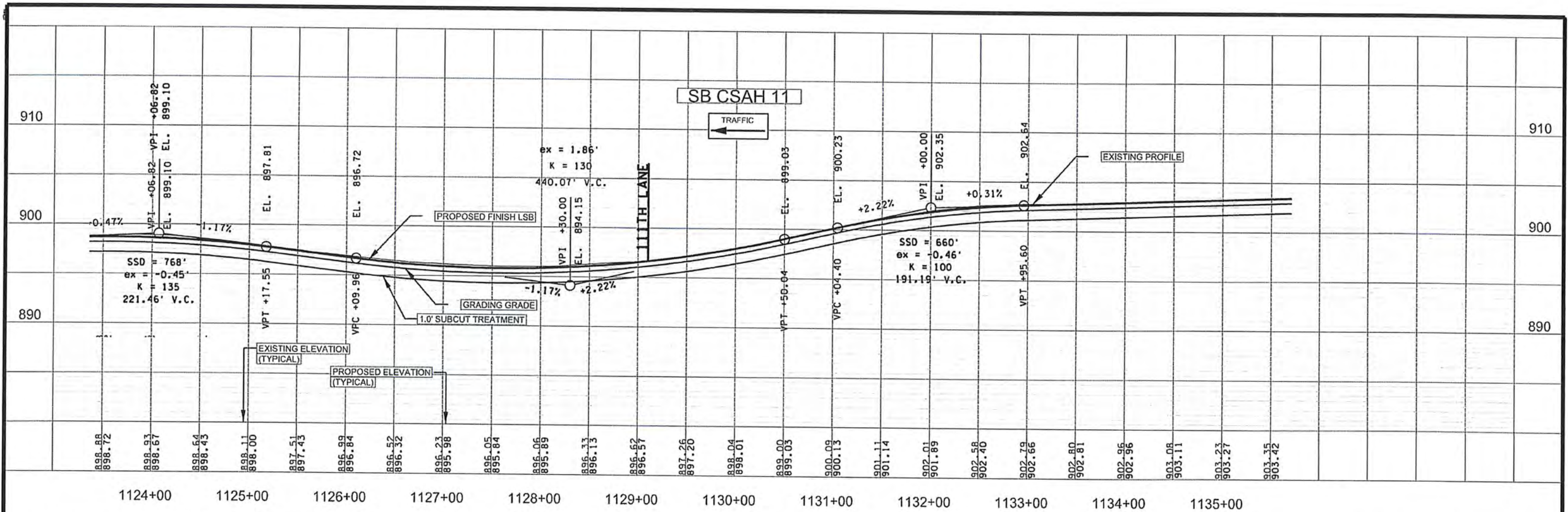
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 6/1/16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



SP 002-611-034
 SP 114-020-050
 CP 13-10



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:02-611-34\Fan\02-611-034_PR_P2.dgn 05/01/2016 8:04:11 AM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

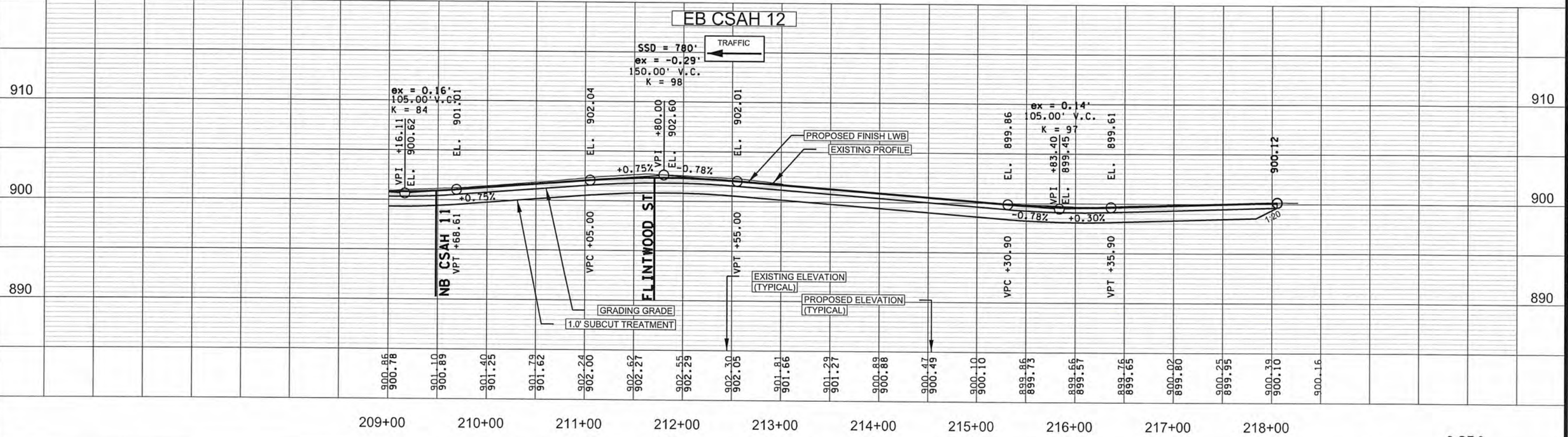
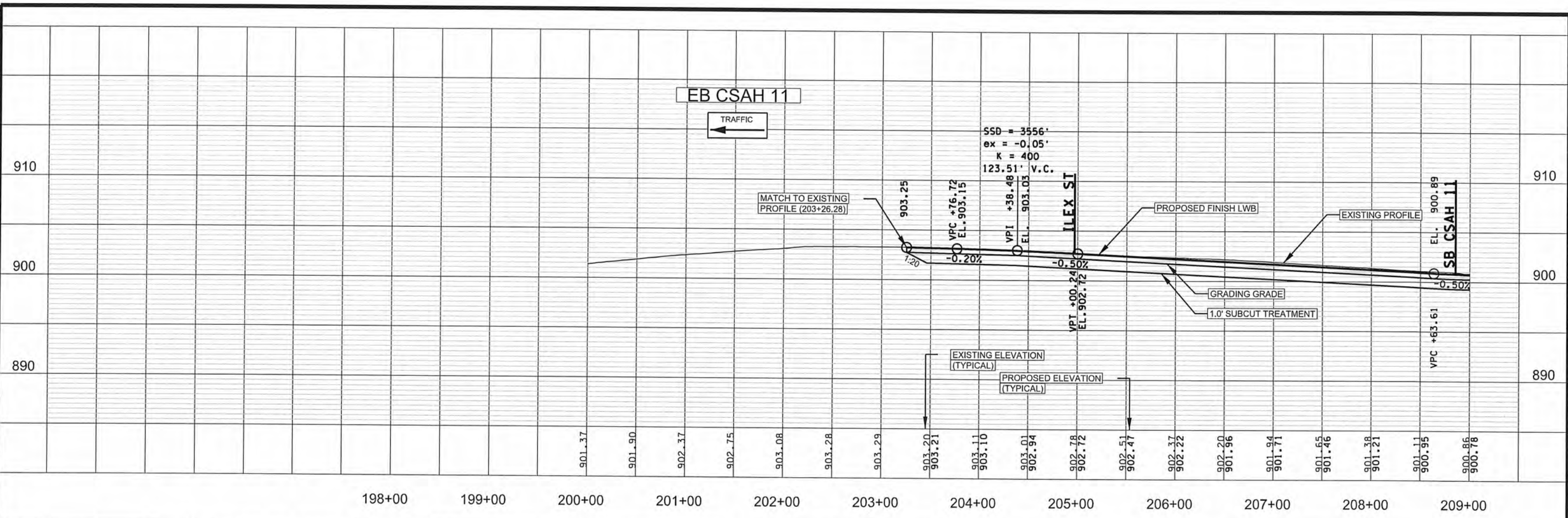
PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina Pizzo*
 DATE: 6/11/16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



ANOKA COUNTY
 HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_PR_P3.dgn 05/17/2016 9:16:16 AM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

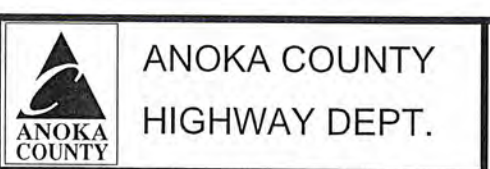
SIGNATURE: *Gina Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16









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CHECKED BY: JEO DATE: 02/27/16



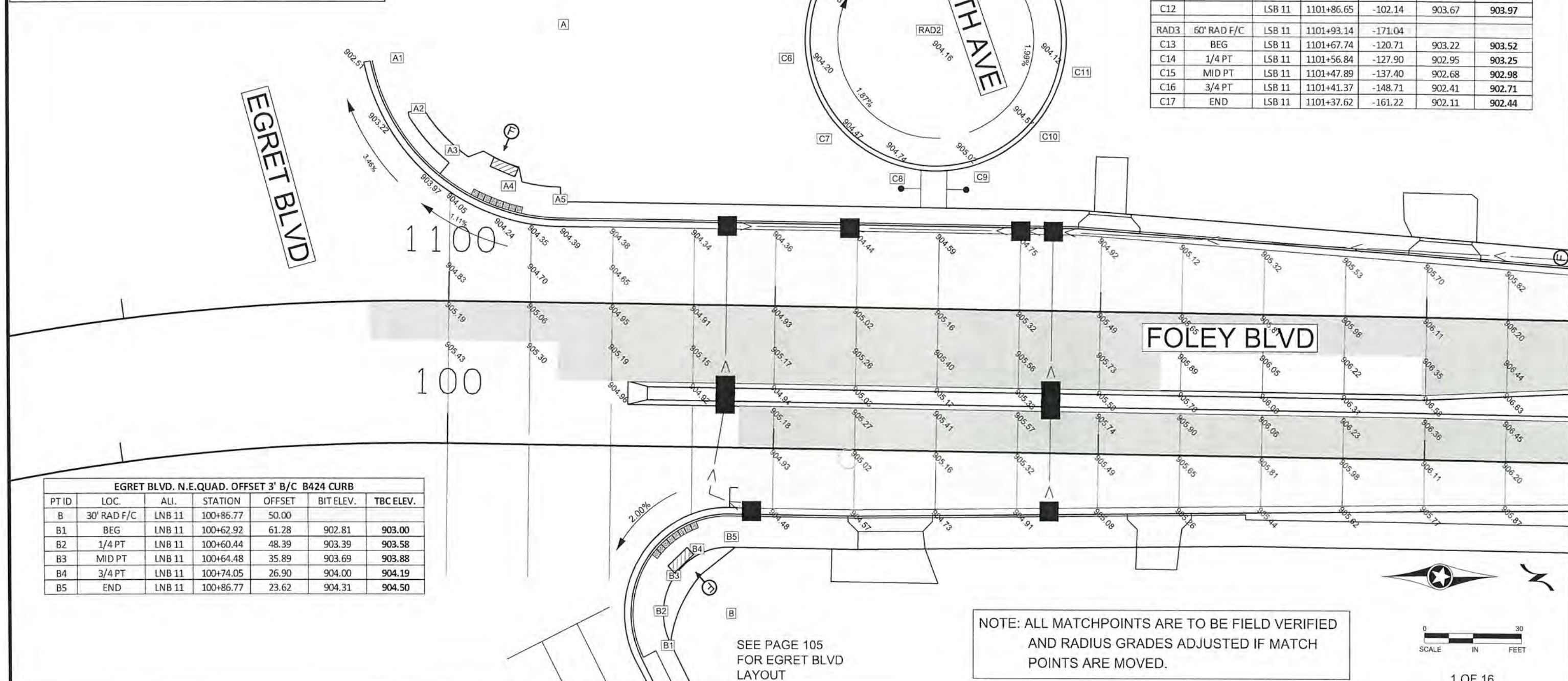
SP 002-611-034
 SP 114-020-050
 CP 13-10

LEGEND

-  PROPOSED SIGNAL POLE
-  CONTROL POINTS ON C&G ALONG THE GUTTER LIP/
CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
-  TRUNCATED DOMES (SEE STANDARD PLATE 7038)
-  CONSTRUCT CONCRETE CURB & GUTTER
-  LANDING AREA - 4' X 4' MIN. DIMENSIONS
AND MAX 2.0% SLOPE IN ALL DIRECTIONS
-  INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS
THAN 5.0% IN THE DIRECTION SHOWN AND CROSS
SLOPE SHALL NOT EXCEED 2.0%
- 999.99 PROPOSED SPOT ELEVATION AT TOP OF FINISHED
PAVEMENT
-  2.00% CROSS SLOPE
-  2.50% CROSS SLOPE

EGRET BLVD. N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
A	60' RAD F/C	LSB 11	1100+33.77	-85.00		
A1	BEG	LSB 11	1099+80.56	-73.51	902.51	902.70
A2	1/4 PT	LSB 11	1099+86.27	-56.47	903.22	903.41
A3	MID PT	LSB 11	1099+98.40	-41.17	903.97	904.16
A4	3/4 PT	LSB 11	1100+17.07	-31.15	904.24	904.43
A5	END	LSB 11	1100+33.77	-28.62	904.39	904.58

107TH AVE CUL-DE-SAC (LSB) OFFSET 3' B/C S518 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
RAD1	60' RAD F/C	LSB 11	1100+57.79	-124.75		
C1	BEG	LSB 11	1101+12.39	-138.80	902.34	902.64
C2	1/4 PT	LSB 11	1101+14.00	-129.14	902.74	903.04
C3	MID PT	LSB 11	1101+13.91	-119.34	903.14	903.44
C4	3/4 PT	LSB 11	1101+12.13	-109.70	903.54	903.84
C5	END	LSB 11	1101+08.70	-100.51	903.93	904.23
RAD2	40' RAD F/C	LSB 11	1101+48.08	-81.77		
C6		LSB 11	1101+04.93	-75.38	904.20	904.50
C7		LSB 11	1101+15.82	-52.41	904.47	904.77
C8		LSB 11	1101+36.67	-39.67	904.74	905.04
C9	MID PT	LSB 11	1101+61.11	-40.14	905.02	905.32
C10		LSB 11	1101+81.46	-53.68	904.57	904.87
C11		LSB 11	1101+91.33	-76.04	904.12	904.42
C12		LSB 11	1101+86.65	-102.14	903.67	903.97
RAD3	60' RAD F/C	LSB 11	1101+93.14	-171.04		
C13	BEG	LSB 11	1101+67.74	-120.71	903.22	903.52
C14	1/4 PT	LSB 11	1101+56.84	-127.90	902.95	903.25
C15	MID PT	LSB 11	1101+47.89	-137.40	902.68	902.98
C16	3/4 PT	LSB 11	1101+41.37	-148.71	902.41	902.71
C17	END	LSB 11	1101+37.62	-161.22	902.11	902.44



EGRET BLVD. N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
B	30' RAD F/C	LNB 11	100+86.77	50.00		
B1	BEG	LNB 11	100+62.92	61.28	902.81	903.00
B2	1/4 PT	LNB 11	100+60.44	48.39	903.39	903.58
B3	MID PT	LNB 11	100+64.48	35.89	903.69	903.88
B4	3/4 PT	LNB 11	100+74.05	26.90	904.00	904.19
B5	END	LNB 11	100+86.77	23.62	904.31	904.50

SEE PAGE 105
FOR EGRET BLVD
LAYOUT


NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED
AND RADIUS GRADES ADJUSTED IF MATCH
POINTS ARE MOVED.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P1.dgn 05/13/2016 12:57:29 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME
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THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *Gina M. Pizzo*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY DFF DATE 02/27/16
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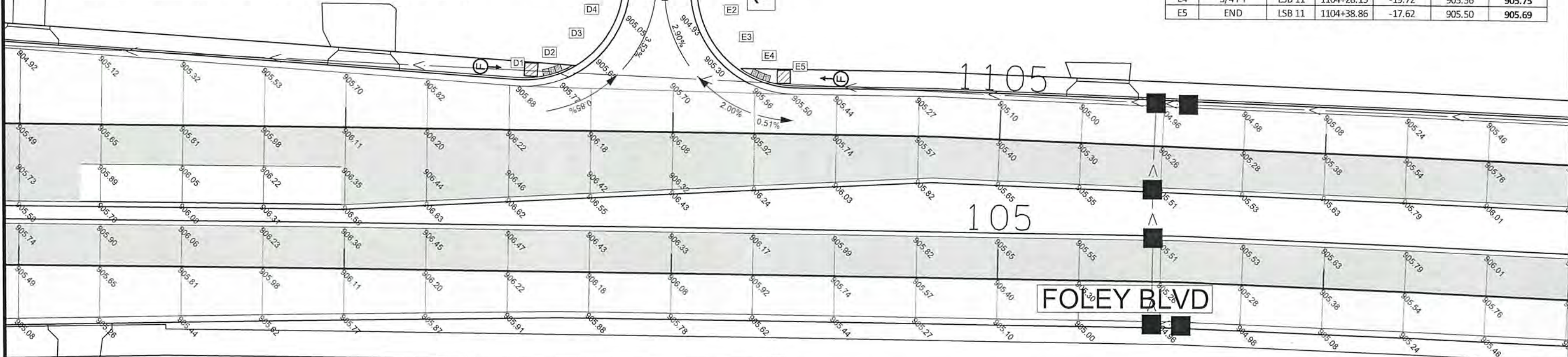
 ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
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CP 13-10

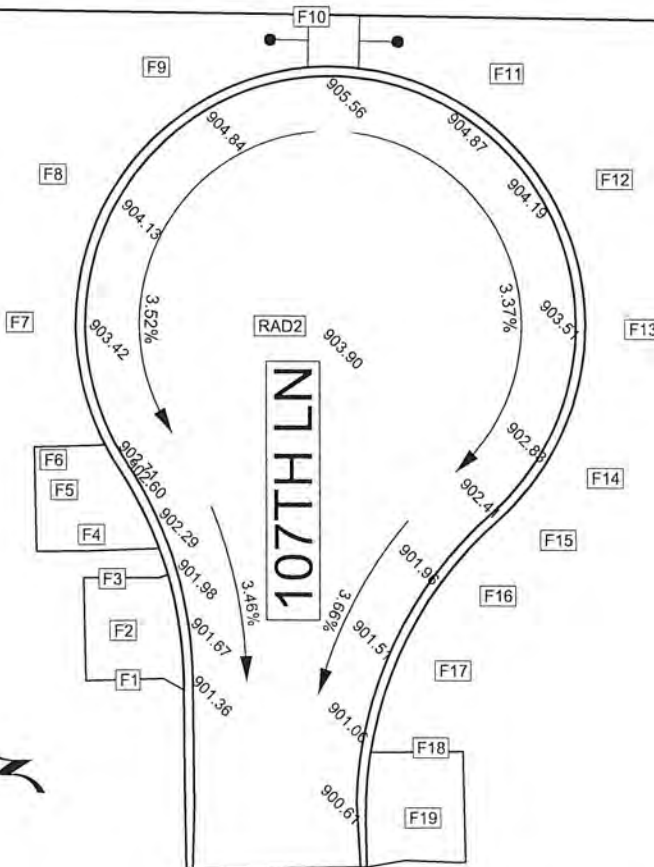
INTERSECTION / CUL-DE-SAC
LAYOUT
EGRET BLVD / 107TH AVE
Sheet 90 of 298 Sheets

107TH LN S.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
D	30' RAD F/C	LSB 11	1103+54.38	-45.20		
D1	BEG	LSB 11	1103+52.77	-18.87	905.88	906.07
D2	1/4 PT	LSB 11	1103+63.47	-20.43	905.77	905.96
D3	MID PT	LSB 11	1103+72.64	-26.15	905.65	905.84
D4	3/4 PT	LSB 11	1103+78.74	-35.08	905.05	905.24
D5	END	LSB 11	1103+80.76	-45.70	904.70	904.89

107TH LN N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
E	30' RAD F/C	LSB 11	1104+38.86	-46.00		
E1	BEG	LSB 11	1104+10.49	-45.37	904.58	904.77
E2	1/4 PT	LSB 11	1104+12.83	-34.71	904.93	905.12
E3	MID PT	LSB 11	1104+19.02	-25.71	905.30	905.49
E4	3/4 PT	LSB 11	1104+28.15	-19.72	905.56	905.75
E5	END	LSB 11	1104+38.86	-17.62	905.50	905.69



107TH LN CUL-DE-SAC (LNB) OFFSET 3' B/C S518 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
RAD1	60' RAD F/C	LNB 11	103+53.77	131.77		
F1	BEG	LNB 11	104+10.13	130.14	901.36	901.66
F2	1/4 PT	LNB 11	104+09.31	122.09	901.67	901.97
F3	MID PT	LNB 11	104+07.36	114.24	901.98	902.28
F4	3/4 PT	LNB 11	104+04.29	106.75	902.29	902.59
F5	END	LNB 11	104+00.18	99.76	902.60	902.90
RAD2	40' RAD F/C	LNB 11	104+36.09	75.00		
F6		LNB 11	103+98.31	96.81	902.71	903.01
F7		LNB 11	103+92.47	75.00	903.42	903.72
F8		LNB 11	103+98.31	53.19	904.13	904.43
F9		LNB 11	104+14.28	37.22	904.84	905.14
F10	MID PT	LNB 11	104+36.09	31.38	905.56	905.86
F11		LNB 11	104+57.90	37.22	904.87	905.17
F12		LNB 11	104+73.87	53.19	904.19	904.49
F13		LNB 11	107+79.71	75.00	903.51	903.81
F14		LNB 11	104+73.87	96.81	902.83	903.13
RAD3	60' RAD F/C	LNB 11	105+02.64	149.65		
F15	BEG	LNB 11	104+65.12	107.55	902.41	902.71
F16	1/4 PT	LNB 11	104+56.93	116.63	901.96	902.26
F17	MID PT	LNB 11	104+50.89	127.27	901.51	901.81
F18	3/4 PT	LNB 11	104+47.28	138.95	901.06	901.36
F19	END	LNB 11	104+46.28	151.14	900.61	900.91



LEGEND

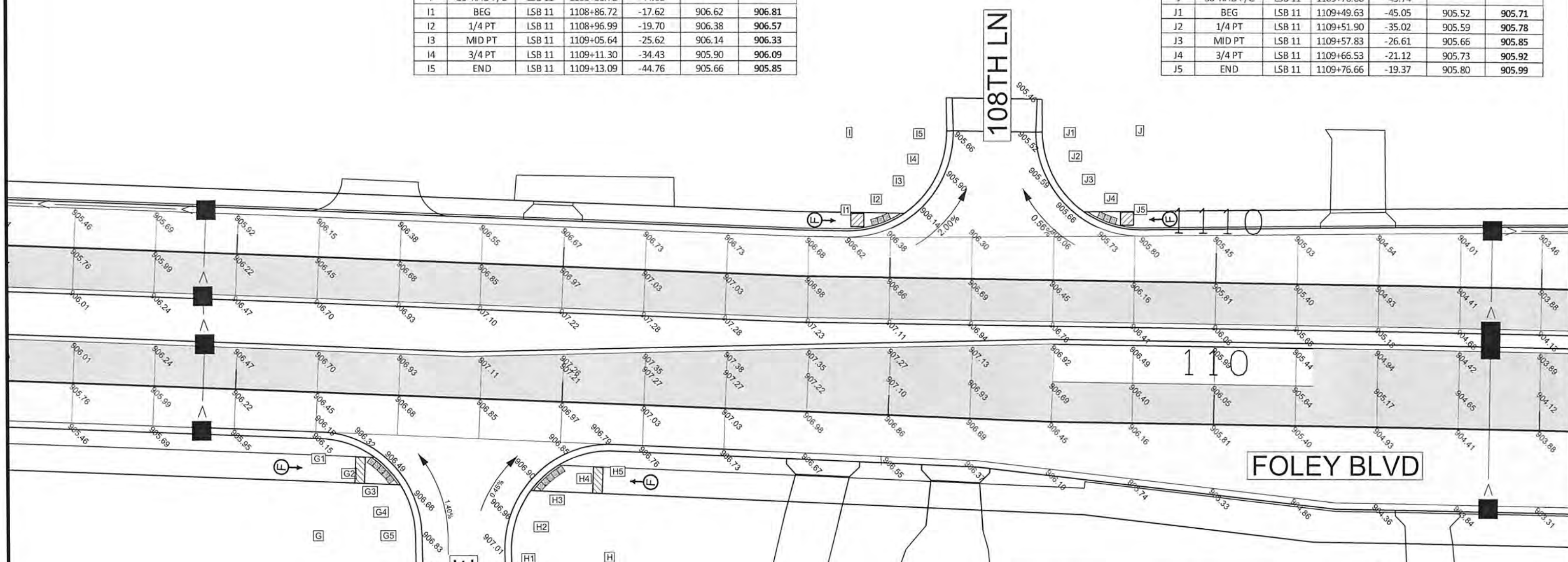
- PROPOSED SIGNAL POLE
- CONTROL POINTS ON C&G ALONG THE GUTTER LIP/
CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
- TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- CONSTRUCT CONCRETE CURB & GUTTER
- LANDING AREA - 4' X 4' MIN. DIMENSIONS
AND MAX 2.0% SLOPE IN ALL DIRECTIONS
- INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS
THAN 5.0% IN THE DIRECTION SHOWN AND CROSS
SLOPE SHALL NOT EXCEED 2.0%
- 999.99 PROPOSED SPOT ELEVATION AT TOP OF FINISHED
PAVEMENT
- 2.00% CROSS SLOPE
- 2.50% CROSS SLOPE

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POINTS ARE MOVED.

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: GINA M. PIZZO SIGNATURE: <i>[Signature]</i> DATE: 5-19-16 LICENSE NO. 22713		DRAWN BY: DFF DATE: 02/27/16 DESIGN BY: DFF DATE: 02/27/16 CHECKED BY: JEO DATE: 02/27/16	ANOKA COUNTY HIGHWAY DEPT.	SP 002-611-034 SP 114-020-050 CP 13-10	INTERSECTION / CUL-DE-SAC LAYOUT 107TH LN Sheet 91 of 298 Sheets
NO. DATE BY CKD APPR REVISION NAME: P:\02-611-34\plan\002-611-034_INC_P2.dgn 05/13/2016 12:57:31 PM					

108TH LN S.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
I	30' RAD F/C	LSB 11	1108+86.72	-44.00		
I1	BEG	LSB 11	1108+86.72	-17.62	906.62	906.81
I2	1/4 PT	LSB 11	1108+96.99	-19.70	906.38	906.57
I3	MID PT	LSB 11	1109+05.64	-25.62	906.14	906.33
I4	3/4 PT	LSB 11	1109+11.30	-34.43	905.90	906.09
I5	END	LSB 11	1109+13.09	-44.76	905.66	905.85

108TH LN N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
J	30' RAD F/C	LSB 11	1109+76.00	-45.74		
J1	BEG	LSB 11	1109+49.63	-45.05	905.52	905.71
J2	1/4 PT	LSB 11	1109+51.90	-35.02	905.59	905.78
J3	MID PT	LSB 11	1109+57.83	-26.61	905.66	905.85
J4	3/4 PT	LSB 11	1109+66.53	-21.12	905.73	905.92
J5	END	LSB 11	1109+76.66	-19.37	905.80	905.99



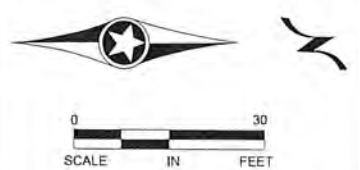
108TH AVE S.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
G	30' RAD F/C	LNB 11	107+26.71	44.00		
G1	BEG	LNB 11	107+26.71	17.62	906.15	906.34
G2	1/4 PT	LNB 11	107+36.49	19.49	906.32	906.51
G3	MID PT	LNB 11	107+44.87	24.85	906.49	906.68
G4	3/4 PT	LNB 11	107+50.67	32.94	906.66	906.85
G5	END	LNB 11	107+53.06	42.61	906.83	907.02

108TH AVE N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
H	30' RAD F/C	LNB 11	108+16.09	44.25		
H1	BEG	LNB 11	107+89.75	45.68	907.01	907.20
H2	1/4 PT	LNB 11	107+91.34	35.12	906.96	907.15
H3	MID PT	LNB 11	107+96.97	26.07	906.90	907.09
H4	3/4 PT	LNB 11	108+05.72	19.99	906.85	907.04
H5	END	LNB 11	108+05.72	16.16	906.79	906.98

LEGEND

- PROPOSED SIGNAL POLE
- CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
- TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- CONSTRUCT CONCRETE CURB & GUTTER
- LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
- INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- PROPOSED SPOT ELEVATION AT TOP OF FINISHED PAVEMENT
- 2.00% CROSS SLOPE
- 2.50% CROSS SLOPE

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED AND RADIUS GRADES ADJUSTED IF MATCH POINTS ARE MOVED.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P3.dgn 05/13/2016 12:57:34 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

INTERSECTION / CUL-DE-SAC LAYOUT
 108TH AVE / 108TH LN
 Sheet 92 of 298 Sheets

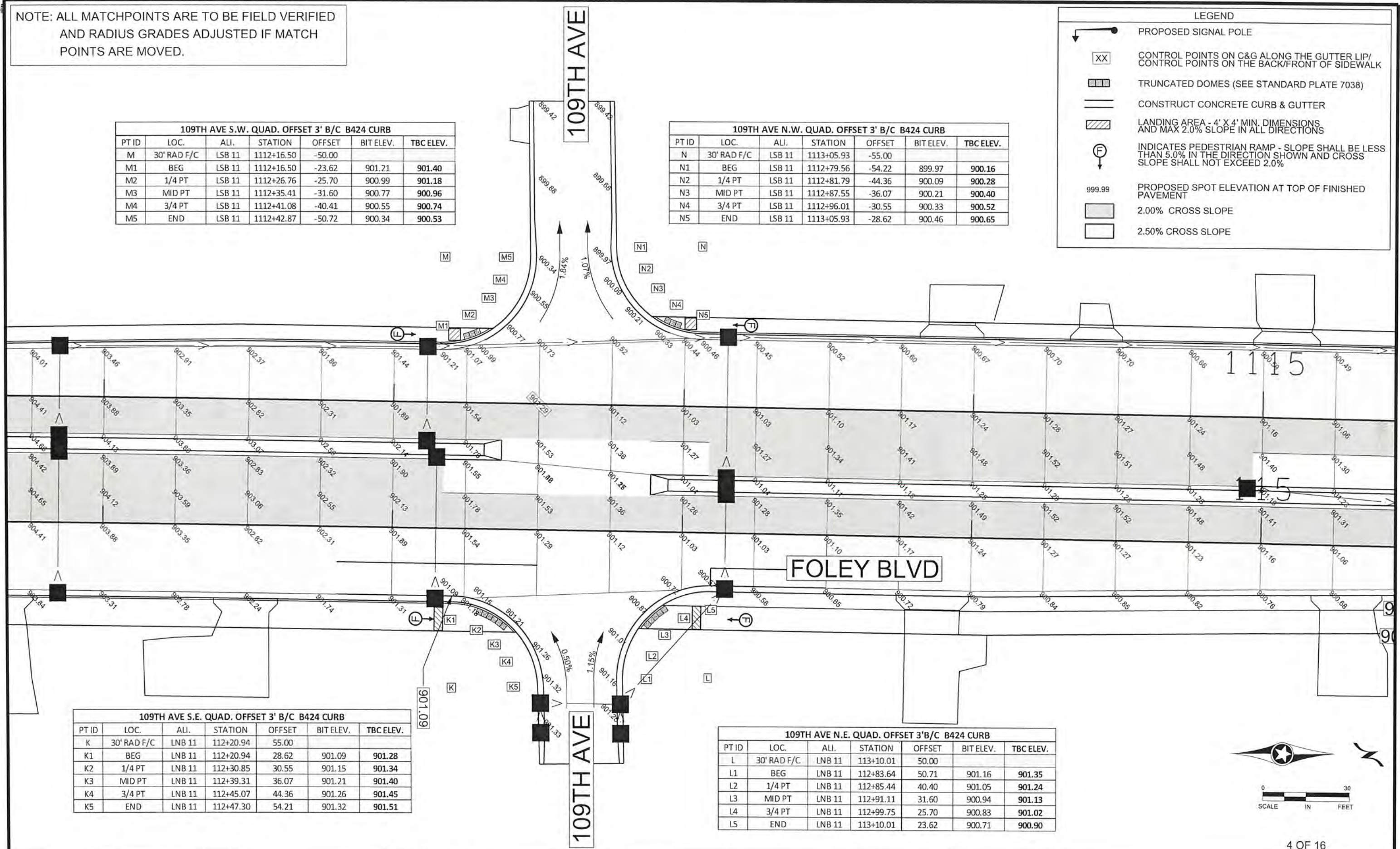
NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED AND RADIUS GRADES ADJUSTED IF MATCH POINTS ARE MOVED.

109TH AVE S.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
M	30' RAD F/C	LSB 11	1112+16.50	-50.00		
M1	BEG	LSB 11	1112+16.50	-23.62	901.21	901.40
M2	1/4 PT	LSB 11	1112+26.76	-25.70	900.99	901.18
M3	MID PT	LSB 11	1112+35.41	-31.60	900.77	900.96
M4	3/4 PT	LSB 11	1112+41.08	-40.41	900.55	900.74
M5	END	LSB 11	1112+42.87	-50.72	900.34	900.53

109TH AVE N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
N	30' RAD F/C	LSB 11	1113+05.93	-55.00		
N1	BEG	LSB 11	1112+79.56	-54.22	899.97	900.16
N2	1/4 PT	LSB 11	1112+81.79	-44.36	900.09	900.28
N3	MID PT	LSB 11	1112+87.55	-36.07	900.21	900.40
N4	3/4 PT	LSB 11	1112+96.01	-30.55	900.33	900.52
N5	END	LSB 11	1113+05.93	-28.62	900.46	900.65

LEGEND

- PROPOSED SIGNAL POLE
- XX CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
- TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- CONSTRUCT CONCRETE CURB & GUTTER
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- 999.99 PROPOSED SPOT ELEVATION AT TOP OF FINISHED PAVEMENT
- 2.00% CROSS SLOPE
- 2.50% CROSS SLOPE



109TH AVE S.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
K	30' RAD F/C	LNB 11	112+20.94	55.00		
K1	BEG	LNB 11	112+20.94	28.62	901.09	901.28
K2	1/4 PT	LNB 11	112+30.85	30.55	901.15	901.34
K3	MID PT	LNB 11	112+39.31	36.07	901.21	901.40
K4	3/4 PT	LNB 11	112+45.07	44.36	901.26	901.45
K5	END	LNB 11	112+47.30	54.21	901.32	901.51

109TH AVE N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
L	30' RAD F/C	LNB 11	113+10.01	50.00		
L1	BEG	LNB 11	112+83.64	50.71	901.16	901.35
L2	1/4 PT	LNB 11	112+85.44	40.40	901.05	901.24
L3	MID PT	LNB 11	112+91.11	31.60	900.94	901.13
L4	3/4 PT	LNB 11	112+99.75	25.70	900.83	901.02
L5	END	LNB 11	113+10.01	23.62	900.71	900.90

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P4.dgn 05/13/2016 12:57:35 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *Gina M. Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

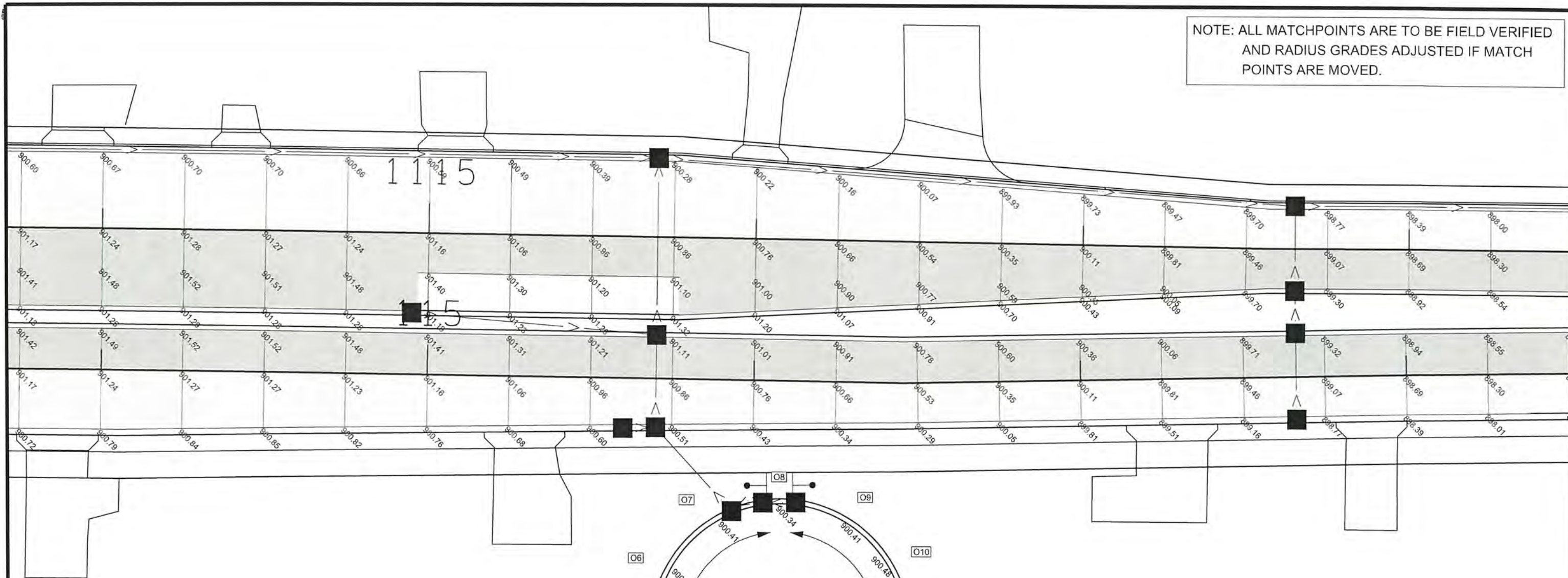
DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY HIGHWAY DEPT.

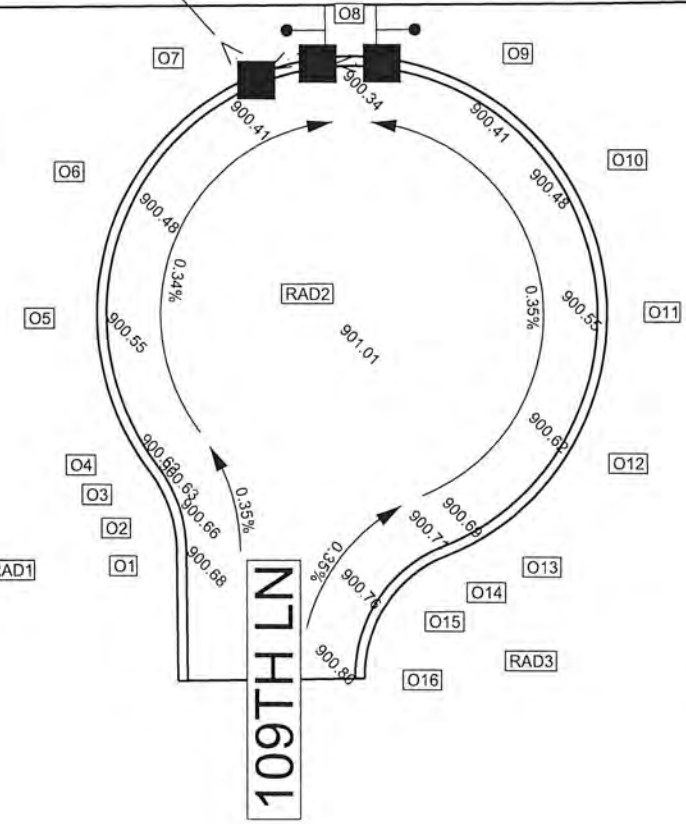
SP 002-611-034
 SP 114-020-050
 CP 13-10

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED AND RADIUS GRADES ADJUSTED IF MATCH POINTS ARE MOVED.

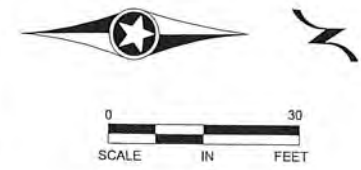


LEGEND

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- 2.00% CROSS SLOPE
- 2.50% CROSS SLOPE



109TH LN CUL-DE-SAC (LNB) OFFSET 3' B/C S518 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
RAD1	20' RAD F/C	LNB 11	115+65.25	113.27		
O1	BEG	LNB 11	115+78.62	113.01	900.68	900.98
O2	MID PT	LNB 11	115+77.67	107.75	900.66	900.96
O3	END	LNB 11	115+75.07	103.08	900.63	900.93
RAD2	40' RAD F/C	LNB 11	116+09.21	75.93		
O4		LNB 11	115+71.99	98.67	900.62	900.92
O5		LNB 11	115+65.60	77.02	900.55	900.85
O6		LNB 11	115+70.90	55.07	900.48	900.78
O7		LNB 11	115+86.46	38.71	900.41	900.71
O8	MID PT	LNB 11	116+08.12	32.32	900.34	900.64
O9		LNB 11	116+30.07	37.62	900.41	900.71
O10		LNB 11	116+45.67	53.18	900.48	900.78
O11		LNB 11	116+50.17	75.04	900.55	900.85
O12		LNB 11	116+45.67	96.80	900.62	900.92
O13		LNB 11	116+31.96	113.15	900.69	900.99
RAD3	20' RAD F/C	LNB 11	116+31.96	131.45		
O14	BEG	LNB 11	116+25.75	116.29	900.71	901.01
O15	MID PT	LNB 11	116+20.30	119.91	900.76	901.06
O16	END	LNB 11	116+15.58	131.91	900.80	901.10



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P5.dgn 05/13/2016 12:57:37 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *[Signature]*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

INTERSECTION / CUL-DE-SAC LAYOUT
109TH LN

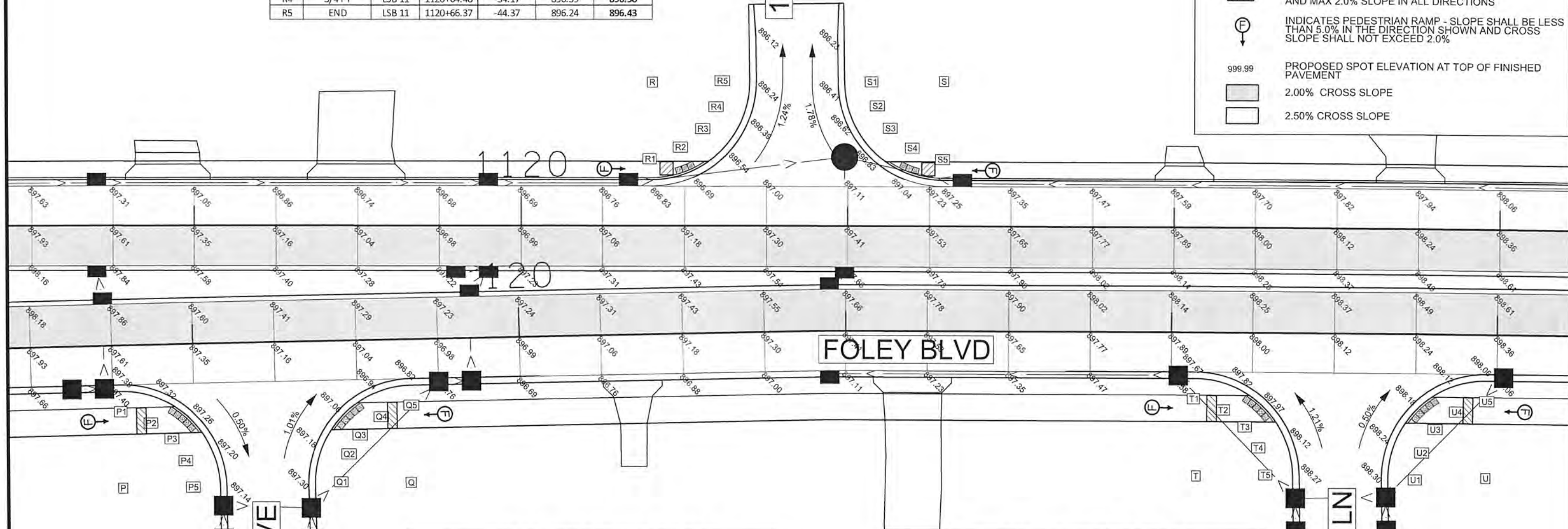
Sheet 94 of 298 Sheets

110TH AVE S.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
R	30' RAD F/C	LSB 11	1120+40.00	-44.00		
R1	BEG	LSB 11	1120+40.00	-17.62	896.83	897.02
R2	1/4 PT	LSB 11	1120+50.18	-19.67	896.69	896.88
R3	MID PT	LSB 11	1120+58.79	-25.48	896.54	896.73
R4	3/4 PT	LSB 11	1120+64.48	-34.17	896.39	896.58
R5	END	LSB 11	1120+66.37	-44.37	896.24	896.43

110TH AVE N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
S	30' RAD F/C	LSB 11	1121+29.03	-44.00		
S1	BEG	LSB 11	1121+02.65	-43.69	896.41	896.60
S2	1/4 PT	LSB 11	1121+04.74	-33.69	896.62	896.81
S3	MID PT	LSB 11	1121+10.48	-25.24	896.83	897.02
S4	3/4 PT	LSB 11	1121+19.00	-19.60	897.04	897.23
S5	END	LSB 11	1121+29.03	-17.62	897.25	897.44

LEGEND

- PROPOSED SIGNAL POLE
- XX CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/Front OF SIDEWALK
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- 2.00% CROSS SLOPE
- 2.50% CROSS SLOPE



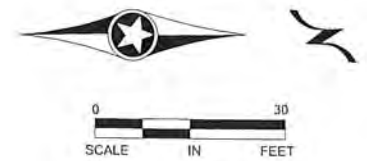
110TH AVE S.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
P	30' RAD F/C	LNB 11	118+77.52	44.00		
P1	BEG	LNB 11	118+77.52	17.62	897.38	897.57
P2	1/4 PT	LNB 11	118+87.67	19.64	897.32	897.51
P3	MID PT	LNB 11	118+96.26	25.42	897.26	897.45
P4	3/4 PT	LNB 11	119+01.96	34.06	897.20	897.39
P5	END	LNB 11	119+03.90	44.22	897.14	897.33

110TH LN S.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
T	30' RAD F/C	LNB 11	122+07.49	44.00		
T1	BEG	LNB 11	122+07.49	17.62	897.67	897.86
T2	1/4 PT	LNB 11	122+17.48	19.58	897.82	898.01
T3	MID PT	LNB 11	122+25.98	25.18	897.97	898.16
T4	3/4 PT	LNB 11	122+31.73	33.59	898.12	898.31
T5	END	LNB 11	122+33.87	43.54	898.27	898.46

110TH AVE N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
Q	30' RAD F/C	LNB 11	119+66.19	44.00		
Q1	BEG	LNB 11	119+39.83	42.80	897.30	897.49
Q2	1/4 PT	LNB 11	119+42.17	33.07	897.18	897.37
Q3	MID PT	LNB 11	119+47.96	24.92	897.06	897.25
Q4	3/4 PT	LNB 11	119+56.37	19.51	896.94	897.13
Q5	END	LNB 11	119+66.19	17.62	896.82	897.01

110TH LN N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
U	30' RAD F/C	LNB 11	122+96.62	44.00		
U1	BEG	LNB 11	122+70.24	44.46	898.30	898.49
U2	1/4 PT	LNB 11	122+72.12	34.23	898.24	898.43
U3	MID PT	LNB 11	122+77.80	25.51	898.18	898.37
U4	3/4 PT	LNB 11	122+86.42	19.67	898.12	898.31
U5	END	LNB 11	122+96.62	17.62	898.06	898.25

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED AND RADIUS GRADES ADJUSTED IF MATCH POINTS ARE MOVED.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P6.dgn 05/13/2016 12:57:39 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-19-14 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

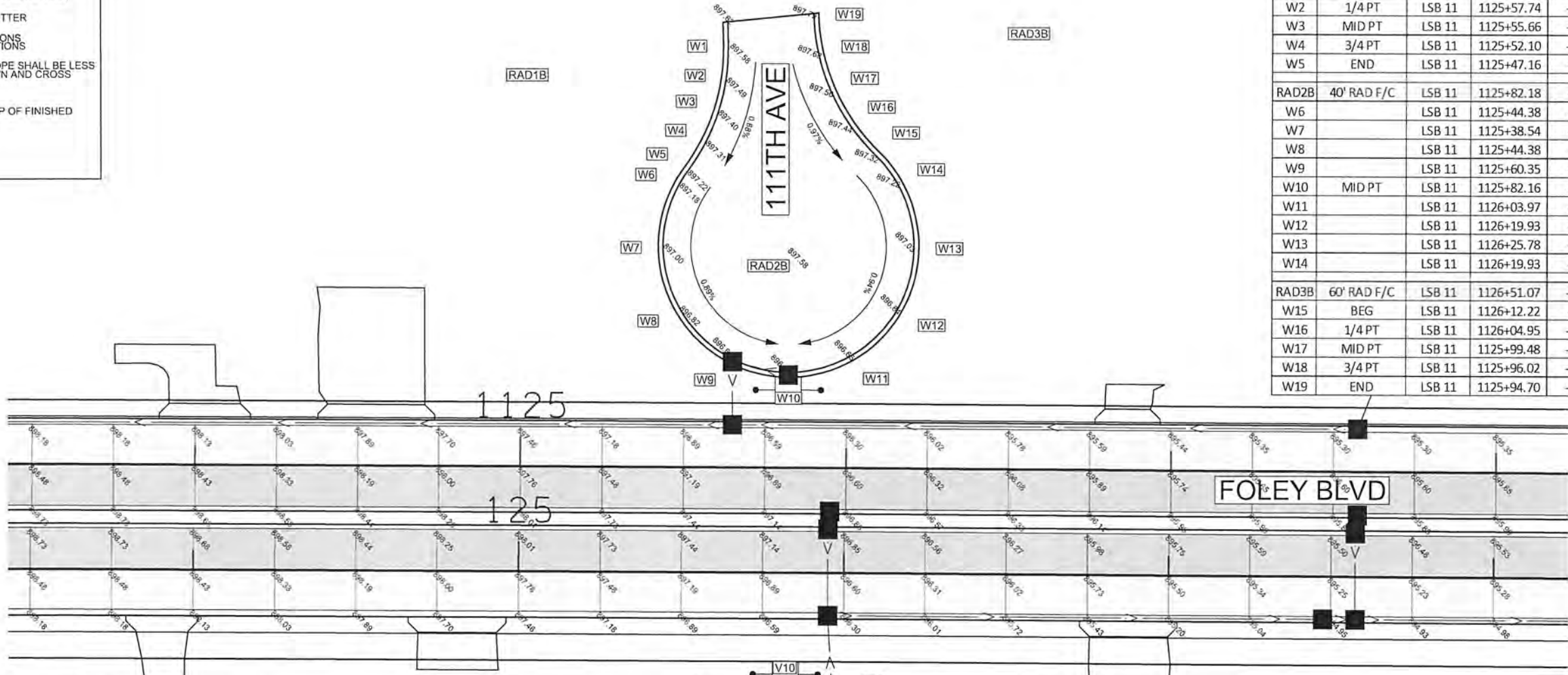
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

INTERSECTION / CUL-DE-SAC
 LAYOUT
 110TH AVE / 110TH LN
 Sheet 95 of 298 Sheets

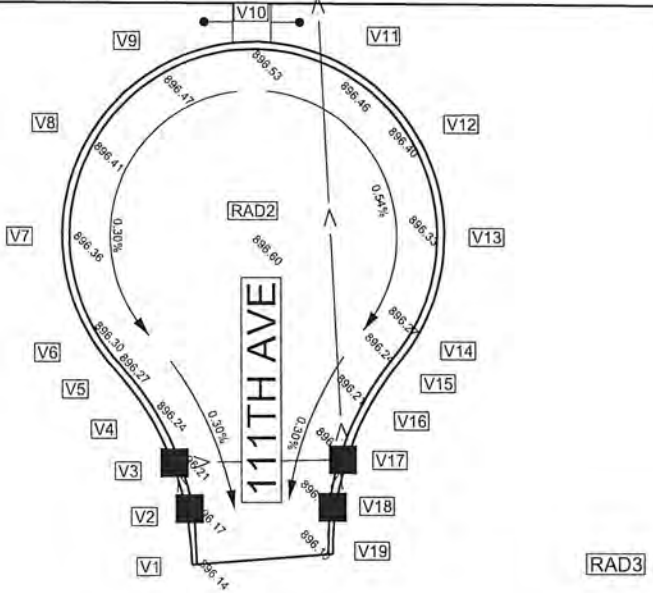
LEGEND

- PROPOSED SIGNAL POLE
- XX CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
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- 2.50% CROSS SLOPE

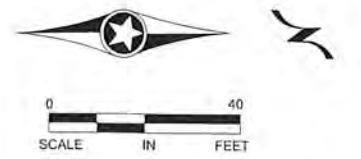


111TH AVE CUL-DE-SAC (LSB) OFFSET 3' B/C S518 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
RAD1B	60' RAD F/C	LSB 11	1125+01.92	-127.28		
W1	BEG	LSB 11	1125+58.28	-128.72	897.58	897.88
W2	1/4 PT	LSB 11	1125+57.74	-119.37	597.49	597.79
W3	MID PT	LSB 11	1125+55.66	-110.24	897.40	897.70
W4	3/4 PT	LSB 11	1125+52.10	-101.59	897.31	897.61
W5	END	LSB 11	1125+47.16	-93.64	897.22	897.52
RAD2B	40' RAD F/C	LSB 11	1125+82.18	-67.60		
W6		LSB 11	1125+44.38	-89.41	897.18	897.48
W7		LSB 11	1125+38.54	-67.60	897.00	897.30
W8		LSB 11	1125+44.38	-45.79	896.82	897.12
W9		LSB 11	1125+60.35	-29.83	896.64	896.94
W10	MID PT	LSB 11	1125+82.16	-23.98	896.46	896.76
W11		LSB 11	1126+03.97	-29.83	896.65	896.95
W12		LSB 11	1126+19.93	-45.79	896.84	897.14
W13		LSB 11	1126+25.78	-67.60	897.03	897.33
W14		LSB 11	1126+19.93	-89.41	897.22	897.52
RAD3B	60' RAD F/C	LSB 11	1126+51.07	-140.07		
W15	BEG	LSB 11	1126+12.22	-99.21	897.32	897.62
W16	1/4 PT	LSB 11	1126+04.95	-107.64	897.44	897.74
W17	MID PT	LSB 11	1125+99.48	-117.32	897.56	897.86
W18	3/4 PT	LSB 11	1125+96.02	-127.89	897.68	897.98
W19	END	LSB 11	1125+94.70	-138.94	897.79	898.09

111TH AVE CUL-DE-SAC (LNB) OFFSET 3' B/C S518 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
RAD1	60' RAD F/C	LNB 11	125+09.80	143.56		
V1	BEG	LNB 11	125+66.17	142.66	896.14	896.44
V2	1/4 PT	LNB 11	125+65.05	132.31	896.17	896.47
V3	MID PT	LNB 11	125+62.04	122.35	896.21	896.51
V4	3/4 PT	LNB 11	125+57.25	113.11	896.24	896.54
V5	END	LNB 11	125+50.84	104.90	896.27	896.57
RAD2	40' RAD F/C	LNB 11	125+82.08	75.00		
V6		LNB 11	125+44.82	96.81	896.30	896.60
V7		LNB 11	125+38.98	75.00	896.36	896.66
V8		LNB 11	125+44.82	53.13	896.41	896.71
V9		LNB 11	125+60.79	37.22	896.47	896.77
V10	MID PT	LNB 11	125+82.60	31.38	896.53	896.83
V11		LNB 11	126+04.41	37.22	896.46	896.76
V12		LNB 11	126+20.37	53.13	896.40	896.70
V13		LNB 11	126+26.22	75.00	896.33	896.63
V14		LNB 11	126+20.37	96.81	896.27	896.57
RAD3	60' RAD F/C	LNB 11	126+59.35	139.10		
V15	BEG	LNB 11	126+16.08	102.96	896.24	896.54
V16	1/4 PT	LNB 11	126+10.35	111.21	896.21	896.51
V17	MID PT	LNB 11	126+06.18	120.35	896.17	896.47
V18	3/4 PT	LNB 11	126+03.70	130.09	896.14	896.44
V19	END	LNB 11	126+02.98	140.11	896.10	896.40



NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED AND RADIUS GRADES ADJUSTED IF MATCH POINTS ARE MOVED.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P7.dgn 05/13/2016 12:57:41 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina Pizzo*
 DATE: 5-19-16 LICENSE NO. _____

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

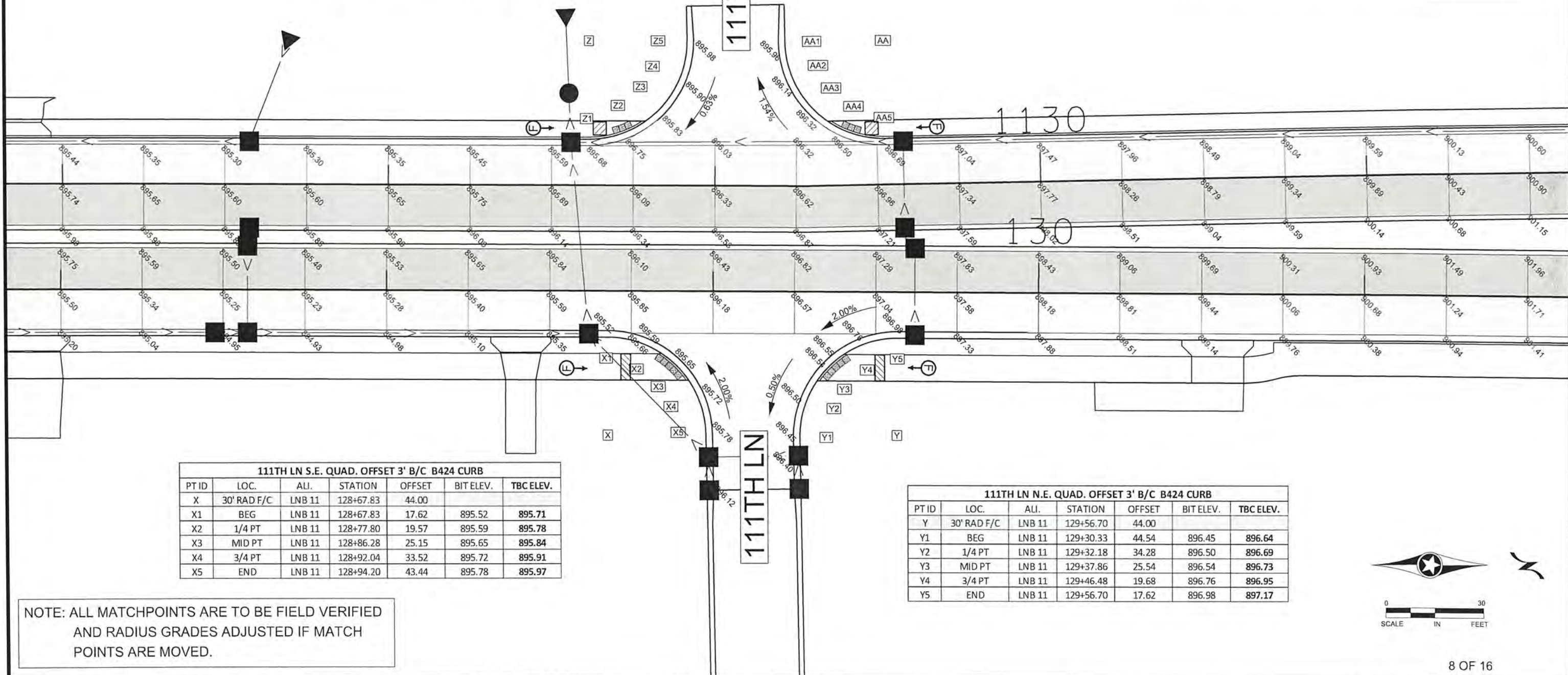
INTERSECTION / CUL-DE-SAC LAYOUT
 111TH AVE
 Sheet 96 of 298 Sheets

111TH LN S.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
Z	30' RAD F/C	LSB 11	1128+61.48	-44.00		
Z1	BEG	LSB 11	1128+61.48	-17.62	895.68	895.87
Z2	1/4 PT	LSB 11	1128+71.70	-19.68	895.75	895.94
Z3	MID PT	LSB 11	1128+80.32	-25.54	895.83	896.02
Z4	3/4 PT	LSB 11	1128+86.00	-34.28	895.90	896.09
Z5	END	LSB 11	1128+87.86	-44.54	895.98	896.17

111TH LN N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
AA	30' RAD F/C	LSB 11	1129+52.46	-44.00		
AA1	BEG	LSB 11	1129+24.98	-43.98	895.96	896.15
AA2	1/4 PT	LSB 11	1129+28.60	-34.00	896.14	896.33
AA3	MID PT	LSB 11	1129+33.79	-25.36	896.32	896.51
AA4	3/4 PT	LSB 11	1129+42.36	-19.63	896.50	896.69
AA5	END	LSB 11	1129+52.46	-17.62	896.69	896.88

LEGEND

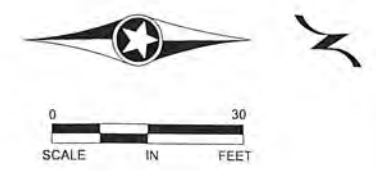
- PROPOSED SIGNAL POLE
- CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
- TRUNCATED DOMES (SEE STANDARD PLATE 7038)
- CONSTRUCT CONCRETE CURB & GUTTER
- LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
- INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
- 999.99 PROPOSED SPOT ELEVATION AT TOP OF FINISHED PAVEMENT
- 2.00% CROSS SLOPE
- 2.50% CROSS SLOPE



111TH LN S.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
X	30' RAD F/C	LNB 11	128+67.83	44.00		
X1	BEG	LNB 11	128+67.83	17.62	895.52	895.71
X2	1/4 PT	LNB 11	128+77.80	19.57	895.59	895.78
X3	MID PT	LNB 11	128+86.28	25.15	895.65	895.84
X4	3/4 PT	LNB 11	128+92.04	33.52	895.72	895.91
X5	END	LNB 11	128+94.20	43.44	895.78	895.97

111TH LN N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
Y	30' RAD F/C	LNB 11	129+56.70	44.00		
Y1	BEG	LNB 11	129+30.33	44.54	896.45	896.64
Y2	1/4 PT	LNB 11	129+32.18	34.28	896.50	896.69
Y3	MID PT	LNB 11	129+37.86	25.54	896.54	896.73
Y4	3/4 PT	LNB 11	129+46.48	19.68	896.76	896.95
Y5	END	LNB 11	129+56.70	17.62	896.98	897.17

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED AND RADIUS GRADES ADJUSTED IF MATCH POINTS ARE MOVED.

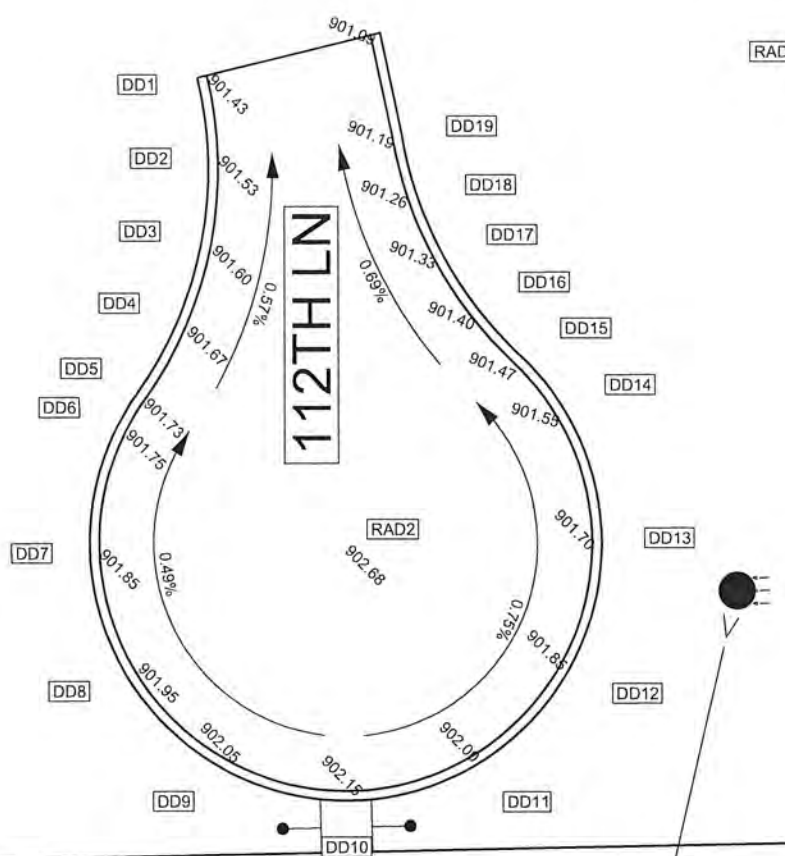


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NO	DATE	BY	CKD	APPR	REVISION										

LEGEND

- PROPOSED SIGNAL POLE
- XX CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
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- 2.00% CROSS SLOPE
- 2.50% CROSS SLOPE

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED AND RADIUS GRADES ADJUSTED IF MATCH POINTS ARE MOVED.



112TH LN CUL-DE-SAC (LSB) OFFSET 3' B/C S518 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
RAD1	60' RAD F/C	LSB 11	1132+45.98	-127.48		
DD1	BEG	LSB 11	1133+01.03	-139.65	901.43	901.73
DD2	1/4 PT	LSB 11	1133+02.36	-127.63	901.53	901.83
DD3	MID PT	LSB 11	1133+01.10	-115.60	901.60	901.90
DD4	3/4 PT	LSB 11	1132+97.30	-104.12	901.67	901.97
DD5	END	LSB 11	1132+90.92	-93.43	901.73	902.03
RAD2	40' RAD F/C	LSB 11	1133+26.06	-67.58		
DD6		LSB 11	1132+88.29	-89.39	901.75	902.05
DD7		LSB 11	1132+82.44	-67.58	901.85	902.15
DD8		LSB 11	1132+88.29	-45.77	901.95	902.25
DD9		LSB 11	1133+04.25	-29.81	902.05	902.35
DD10	MID PT	LSB 11	1133+26.06	-26.06	902.15	902.45
DD11		LSB 11	1133+47.87	-29.81	902.00	902.30
DD12		LSB 11	1133+63.84	-45.77	901.85	902.15
DD13		LSB 11	1133+68.19	-67.60	901.70	902.00
DD14		LSB 11	1133+63.93	-89.24	901.55	901.85
RAD3	60' RAD F/C	LSB 11	1133+92.23	-139.97		
DD15	BEG	LSB 11	1133+56.45	-98.88	901.47	901.77
DD16	1/4 PT	LSB 11	1133+50.34	-105.88	901.40	901.70
DD17	MID PT	LSB 11	1133+45.46	-113.79	901.33	901.63
DD18	3/4 PT	LSB 11	1133+41.95	-122.39	901.26	901.56
DD19	END	LSB 11	1133+39.90	-131.45	901.19	901.49

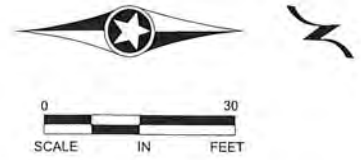


112TH LN S.E. QUAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
BB	30' RAD F/C	LNB 11	132+82.88	44.00		
BB1	BEG	LNB 11	132+82.88	17.62	902.74	902.93
BB2	1/4 PT	LNB 11	132+94.43	20.28	902.84	903.03
BB3	MID PT	LNB 11	133+03.64	27.73	902.93	903.12
BB4	3/4 PT	LNB 11	133+08.67	38.47	903.03	903.22
BB5	END	LNB 11	133+08.49	50.32	903.12	903.31

112TH LN N.E. QUAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
CC	30' RAD F/C	LNB 11	133+70.94	49.05		
CC1	BEG	LNB 11	133+44.74	45.96	903.18	903.37
CC2	1/4 PT	LNB 11	133+47.76	36.47	903.12	903.31
CC3	MID PT	LNB 11	133+54.07	28.77	903.05	903.24
CC4	3/4 PT	LNB 11	133+62.80	23.96	902.99	903.18
CC5	END	LNB 11	133+72.68	22.73	902.92	903.11

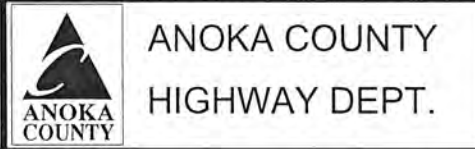


NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P9.dgn 05/16/2016 11:57:11 AM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



SP 002-611-034
 SP 114-020-050
 CP 13-10

PROPOSED SIGNAL POLE

XX CONTROL POINTS ON C&G ALONG THE GUTTER LIP/
CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK

TRUNCATED DOMES (SEE STANDARD PLATE 7038)

CONSTRUCT CONCRETE CURB & GUTTER

LANDING AREA - 4' X 4' MIN. DIMENSIONS
AND MAX 2.0% SLOPE IN ALL DIRECTIONS

INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS
THAN 5.0% IN THE DIRECTION SHOWN AND CROSS
SLOPE SHALL NOT EXCEED 2.0%

999.99 PROPOSED SPOT ELEVATION AT TOP OF FINISHED
PAVEMENT

2.00% CROSS SLOPE

2.50% CROSS SLOPE

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED
AND RADIUS GRADES ADJUSTED IF MATCH
POINTS ARE MOVED.

113TH AVE S.W. QUAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
GG	30' RAD F/C	LSB 11	1136+34.77	-50.00		
GG1	BEG	LSB 11	1136+34.77	-23.62	903.01	903.20
GG2	1/4 PT	LSB 11	1136+44.94	-25.66	902.78	902.97
GG3	MID PT	LSB 11	1136+53.53	-31.46	902.55	902.74
GG4	3/4 PT	LSB 11	1136+59.23	-40.12	902.31	902.50
GG5	END	LSB 11	1136+61.15	-50.31	902.01	902.20

113TH AVE N.W. QUAD. OFFSET 3' B/C B424 CURB

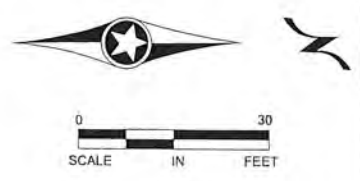
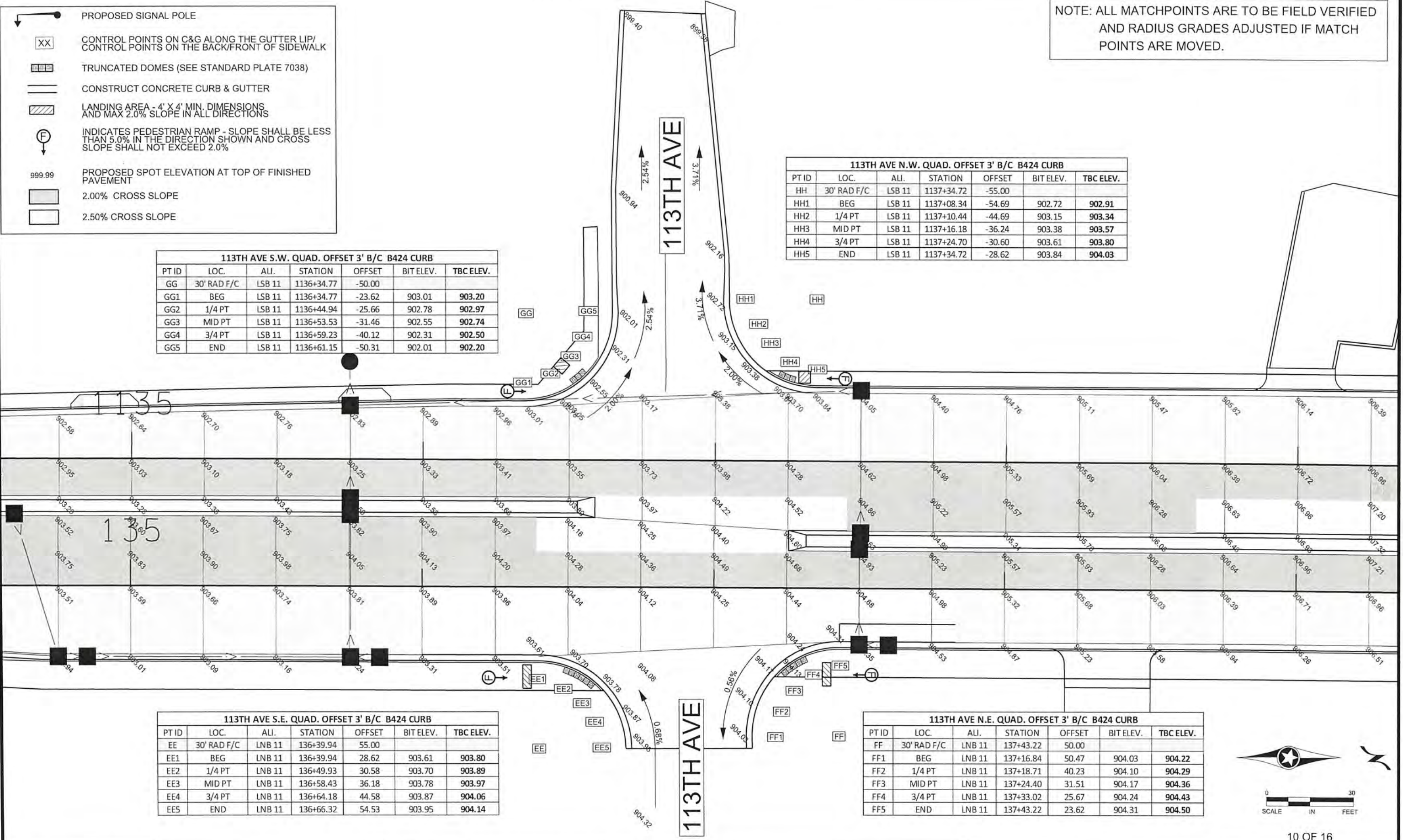
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
HH	30' RAD F/C	LSB 11	1137+34.72	-55.00		
HH1	BEG	LSB 11	1137+08.34	-54.69	902.72	902.91
HH2	1/4 PT	LSB 11	1137+10.44	-44.69	903.15	903.34
HH3	MID PT	LSB 11	1137+16.18	-36.24	903.38	903.57
HH4	3/4 PT	LSB 11	1137+24.70	-30.60	903.61	903.80
HH5	END	LSB 11	1137+34.72	-28.62	903.84	904.03

113TH AVE S.E. QUAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
EE	30' RAD F/C	LNB 11	136+39.94	55.00		
EE1	BEG	LNB 11	136+39.94	28.62	903.61	903.80
EE2	1/4 PT	LNB 11	136+49.93	30.58	903.70	903.89
EE3	MID PT	LNB 11	136+58.43	36.18	903.78	903.97
EE4	3/4 PT	LNB 11	136+64.18	44.58	903.87	904.06
EE5	END	LNB 11	136+66.32	54.53	903.95	904.14

113TH AVE N.E. QUAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
FF	30' RAD F/C	LNB 11	137+43.22	50.00		
FF1	BEG	LNB 11	137+16.84	50.47	904.03	904.22
FF2	1/4 PT	LNB 11	137+18.71	40.23	904.10	904.29
FF3	MID PT	LNB 11	137+24.40	31.51	904.17	904.36
FF4	3/4 PT	LNB 11	137+33.02	25.67	904.24	904.43
FF5	END	LNB 11	137+43.22	23.62	904.31	904.50



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:02-611-34\Plan\002-611-034_INC_P10.dgn 05/13/2016 12:57:47 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *Gina M. Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

INTERSECTION / CUL-DE-SAC
LAYOUT
113TH AVE

Sheet 99 of 298 Sheets

LEGEND

- PROPOSED SIGNAL POLE
- CONTROL POINTS ON C&G ALONG THE GUTTER LIP/
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PAVEMENT
- 2.00% CROSS SLOPE
- 2.50% CROSS SLOPE

CSAH 12 S.W. QUAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
MM	50' RAD F/C	LSB 11	1144+68.01	-70.00		
MM1	BEG	LSB 11	1144+68.01	-23.62	900.84	901.03
MM2	1/4 PT	LSB 11	1144+77.26	-24.55	900.80	900.99
MM3	MID PT	LSB 11	1144+86.13	-27.31	900.76	900.95
MM4	3/4 PT	LSB 11	1144+94.28	-31.77	900.72	900.91
MM5	END	LSB 11	1145+00.43	-37.92	900.68	900.87

CSAH 12 N.W. QUAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
NN	30' RAD F/C	LSB 11	1146+27.06	-43.00		
NN1	BEG	LSB 11	1146+04.84	-57.21	900.95	901.14
NN2	1/4 PT	LSB 11	1146+00.70	-43.90	900.79	900.98
NN3	MID PT	LSB 11	1146+03.92	-30.33	900.64	900.83
NN4	3/4 PT	LSB 11	1146+13.62	-20.31	900.48	900.67
NN5	END	LSB 11	1146+27.06	-16.62	900.33	900.52

CSAH 12 S.E. QUAD. SHLDR RAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
ii	30' RAD F/C	LNB 11	143+16.13	55.00		
ii1	BEG	LNB 11	143+16.13	28.62	901.63	901.82
ii3	1/4 PT	LNB 11	143+31.13	33.31	901.69	901.88
ii5	MID PT	LNB 11	143+40.81	45.70	901.75	901.94
ii7	3/4 PT	LNB 11	143+41.72	61.40	901.80	901.99
ii9	END	LNB 11	143+33.53	74.82	901.85	902.04

CSAH 12 S.E. QUAD. PORKCHOP RAD. OFFSET 1.38 B/C B418 CURB

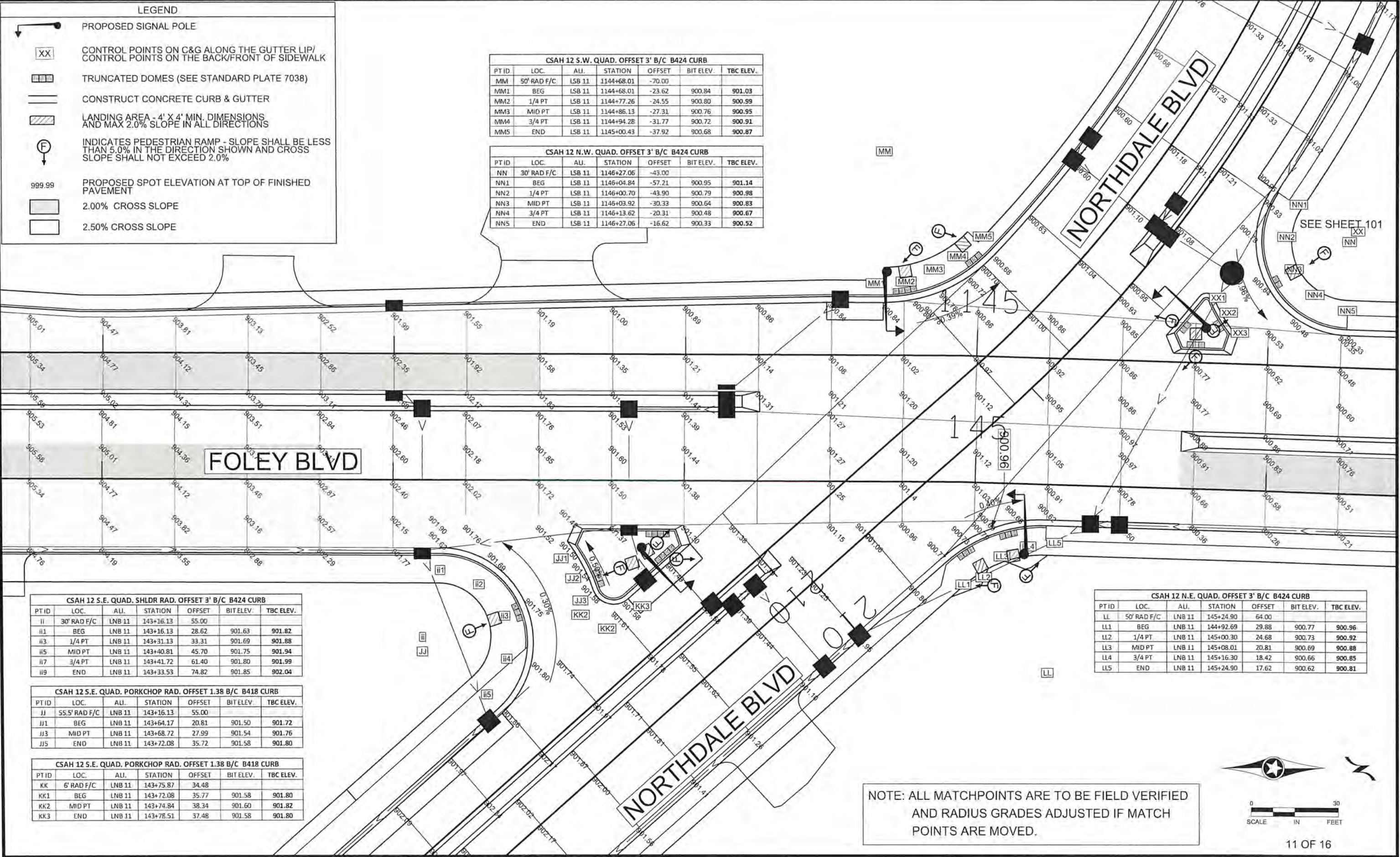
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
JJ	55.5' RAD F/C	LNB 11	143+16.13	55.00		
JJ1	BEG	LNB 11	143+64.17	20.81	901.50	901.72
JJ3	MID PT	LNB 11	143+68.72	27.99	901.54	901.76
JJ5	END	LNB 11	143+72.08	35.72	901.58	901.80

CSAH 12 S.E. QUAD. PORKCHOP RAD. OFFSET 1.38 B/C B418 CURB

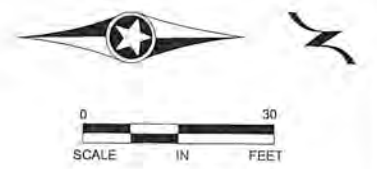
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
KK	6' RAD F/C	LNB 11	143+75.87	34.48		
KK1	BEG	LNB 11	143+72.08	35.77	901.58	901.80
KK2	MID PT	LNB 11	143+74.84	38.34	901.60	901.82
KK3	END	LNB 11	143+75.51	37.48	901.58	901.80

CSAH 12 N.E. QUAD. OFFSET 3' B/C B424 CURB

PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
LL	50' RAD F/C	LNB 11	145+24.90	64.00		
LL1	BEG	LNB 11	144+92.69	29.88	900.77	900.96
LL2	1/4 PT	LNB 11	145+00.30	24.68	900.73	900.92
LL3	MID PT	LNB 11	145+08.01	20.81	900.69	900.88
LL4	3/4 PT	LNB 11	145+16.30	18.42	900.66	900.85
LL5	END	LNB 11	145+24.90	17.62	900.62	900.81



NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED
AND RADIUS GRADES ADJUSTED IF MATCH
POINTS ARE MOVED.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:02-611-341Plan002-611-034_INC_P11.dgn 05/13/2016 12:57:49 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME
OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY
LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF
THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *[Signature]*
DATE: 5-19-14 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
DESIGN BY: DFF DATE: 02/27/16
CHECKED BY: JEO DATE: 02/27/16

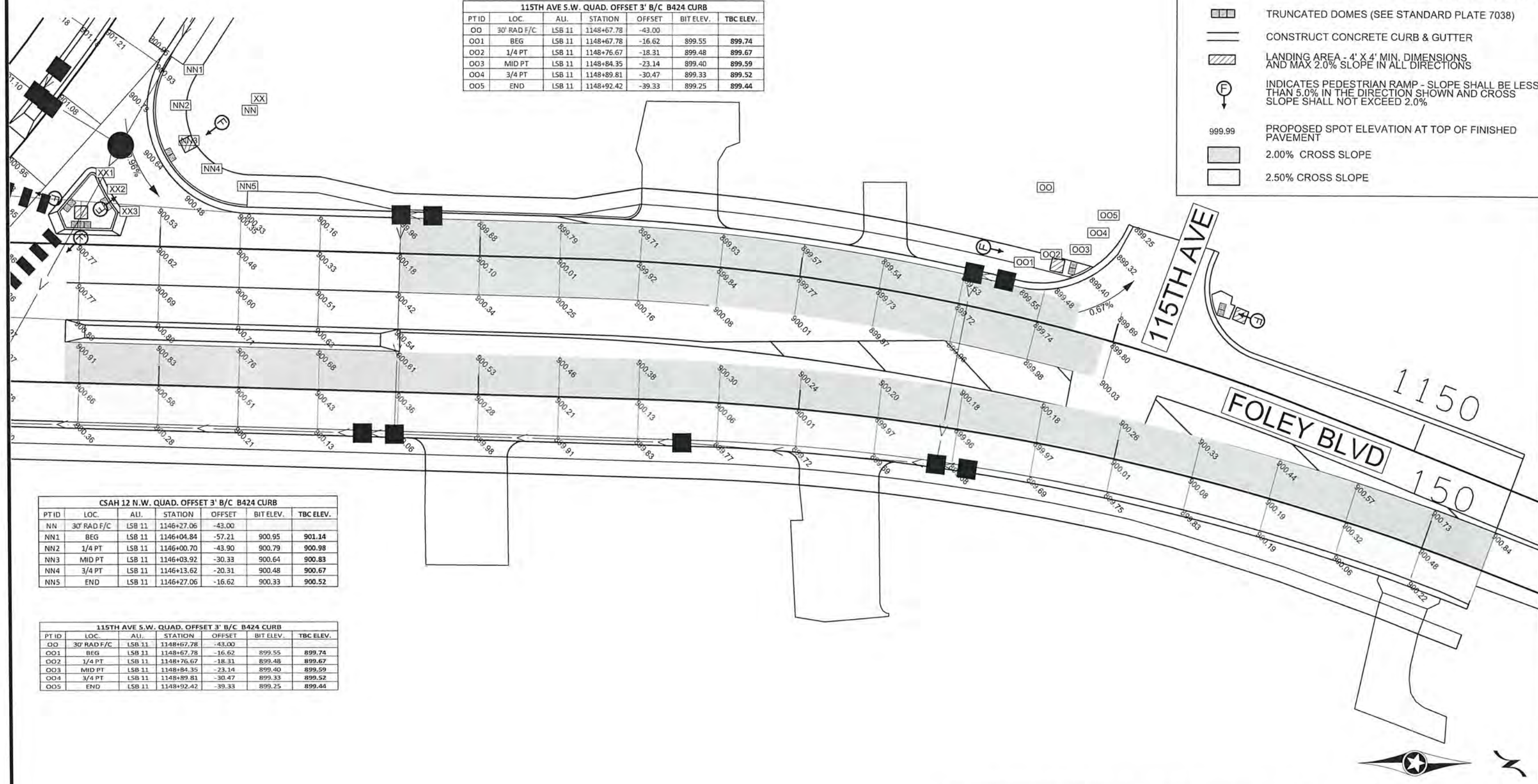
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

INTERSECTION / CUL-DE-SAC
LAYOUT
FOLEY / NORTHDAL BLVD
Sheet 100 of 298 Sheets

115TH AVE S.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	AU.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
OO	30' RAD F/C	LSB 11	1148+67.78	-43.00		
OO1	BEG	LSB 11	1148+67.78	-16.62	899.55	899.74
OO2	1/4 PT	LSB 11	1148+76.67	-18.31	899.48	899.67
OO3	MID PT	LSB 11	1148+84.35	-23.14	899.40	899.59
OO4	3/4 PT	LSB 11	1148+89.81	-30.47	899.33	899.52
OO5	END	LSB 11	1148+92.42	-39.33	899.25	899.44

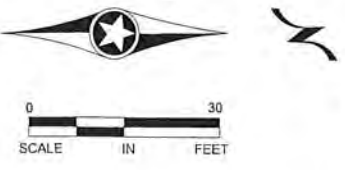
LEGEND	
	PROPOSED SIGNAL POLE
	CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
	TRUNCATED DOMES (SEE STANDARD PLATE 7038)
	CONSTRUCT CONCRETE CURB & GUTTER
	LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
999.99	PROPOSED SPOT ELEVATION AT TOP OF FINISHED PAVEMENT
	2.00% CROSS SLOPE
	2.50% CROSS SLOPE



CSAH 12 N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	AU.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
NN	30' RAD F/C	LSB 11	1146+27.06	-43.00		
NN1	BEG	LSB 11	1146+04.84	-57.21	900.95	901.14
NN2	1/4 PT	LSB 11	1146+00.70	-43.90	900.79	900.98
NN3	MID PT	LSB 11	1146+03.92	-30.33	900.64	900.83
NN4	3/4 PT	LSB 11	1146+13.62	-20.31	900.48	900.67
NN5	END	LSB 11	1146+27.06	-16.62	900.33	900.52

115TH AVE S.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	AU.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
OO	30' RAD F/C	LSB 11	1148+67.78	-43.00		
OO1	BEG	LSB 11	1148+67.78	-16.62	899.55	899.74
OO2	1/4 PT	LSB 11	1148+76.67	-18.31	899.48	899.67
OO3	MID PT	LSB 11	1148+84.35	-23.14	899.40	899.59
OO4	3/4 PT	LSB 11	1148+89.81	-30.47	899.33	899.52
OO5	END	LSB 11	1148+92.42	-39.33	899.25	899.44

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED
AND RADIUS GRADES ADJUSTED IF MATCH
POINTS ARE MOVED.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P12.dgn 05/13/2016 12:57:51 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME
OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY
LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF
THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *[Signature]*
DATE: 5-19-14 LICENSE NO. 22713










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DESIGN BY: DFF DATE: 02/27/16
CHECKED BY: JEO DATE: 02/27/16

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HIGHWAY DEPT.

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SP 114-020-050
CP 13-10

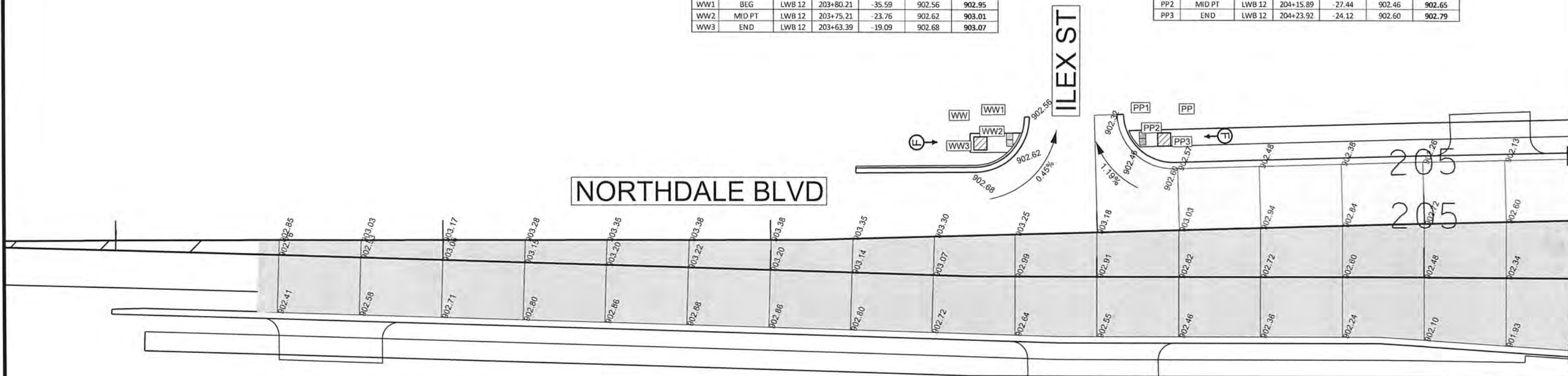
INTERSECTION / CUL-DE-SAC
LAYOUT
115TH AVE
Sheet 101 of 298 Sheets

LEGEND

-  PROPOSED SIGNAL POLE
-  CONTROL POINTS ON C&G ALONG THE GUTTER LIP/
CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
-  TRUNCATED DOMES (SEE STANDARD PLATE 7038)
-  CONSTRUCT CONCRETE CURB & GUTTER
-  LANDING AREA - 4' X 4' MIN. DIMENSIONS
AND MAX 2.0% SLOPE IN ALL DIRECTIONS
-  INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS
THAN 5.0% IN THE DIRECTION SHOWN AND CROSS
SLOPE SHALL NOT EXCEED 2.0%
-  999.99 PROPOSED SPOT ELEVATION AT TOP OF FINISHED
PAVEMENT
-  2.00% CROSS SLOPE
-  2.50% CROSS SLOPE

ILEX ST. N.W. QUAD. OFFSET 3' B/C B618 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
WW	15' RAD F/C	LWB 12	203+63.71	-35.59		
WW1	BEG	LWB 12	203+80.21	-35.59	902.56	902.95
WW2	MID PT	LWB 12	203+75.21	-23.76	902.62	903.01
WW3	END	LWB 12	203+63.39	-19.09	902.68	903.07

ILEX ST. N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
PP	15' RAD F/C	LWB 12	204+23.92	-35.50		
PP1	BEG	LWB 12	204+12.54	-35.47	902.32	902.51
PP2	MID PT	LWB 12	204+15.89	-27.44	902.46	902.65
PP3	END	LWB 12	204+23.92	-24.12	902.60	902.79




NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED
AND RADIUS GRADES ADJUSTED IF MATCH
POINTS ARE MOVED.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P13.dgn 05/13/2016 12:57:52 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME
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THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *[Signature]*
DATE: 5-19-16 LICENSE NO. 22713









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DESIGN BY: DFF DATE: 02/27/16
CHECKED BY: JEO DATE: 02/27/16

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CP 13-10

INTERSECTION / CUL-DE-SAC
LAYOUT
ILEX ST
Sheet 102 of 298 Sheets

LEGEND

-  PROPOSED SIGNAL POLE
-  CONTROL POINTS ON C&G ALONG THE GUTTER LIP/
CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
-  TRUNCATED DOMES (SEE STANDARD PLATE 7038)
-  CONSTRUCT CONCRETE CURB & GUTTER
-  LANDING AREA - 4' X 4' MIN. DIMENSIONS
AND MAX 2.0% SLOPE IN ALL DIRECTIONS
-  INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS
THAN 5.0% IN THE DIRECTION SHOWN AND CROSS
SLOPE SHALL NOT EXCEED 2.0%
- 999.99 PROPOSED SPOT ELEVATION AT TOP OF FINISHED
PAVEMENT
-  2.00% CROSS SLOPE
-  2.50% CROSS SLOPE

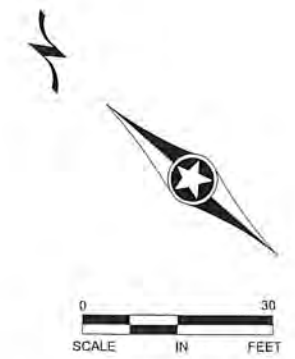
FLINTWOOD ST N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
QQ	30' RAD F/C	LWB 12	211+24.65	-55.00		
QQ1	BEG	LWB 12	211+24.68	-28.62	901.69	901.88
QQ2	1/4 PT	LWB 12	211+35.05	-30.76	901.76	901.95
QQ3	MID PT	LWB 12	211+43.77	-36.82	901.84	902.03
QQ4	3/4 PT	LWB 12	211+49.39	-45.83	901.75	901.94
QQ5	END	LWB 12	211+51.00	-56.33	901.63	901.82

FLINTWOOD ST S.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
RR	30' RAD F/C	LWB 12	212+13.27	-55.00		
RR1	BEG	LWB 12	211+86.93	-53.57	901.60	901.79
RR2	1/4 PT	LWB 12	211+89.33	-43.92	901.74	901.93
RR3	MID PT	LWB 12	211+95.13	-35.85	901.81	902.00
RR4	3/4 PT	LWB 12	212+03.51	-30.49	901.75	901.94
RR5	END	LWB 12	212+13.27	-28.62	901.69	901.88

SEE SHEET 100

SEE SHEET 100

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED
AND RADIUS GRADES ADJUSTED IF MATCH
POINTS ARE MOVED.



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_INC_P14.dgn 05/13/2016 12:57:53 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME
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LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF
THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *Gina M. Pizzo*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
DESIGN BY: DFF DATE: 02/27/16
CHECKED BY: JEO DATE: 02/27/16

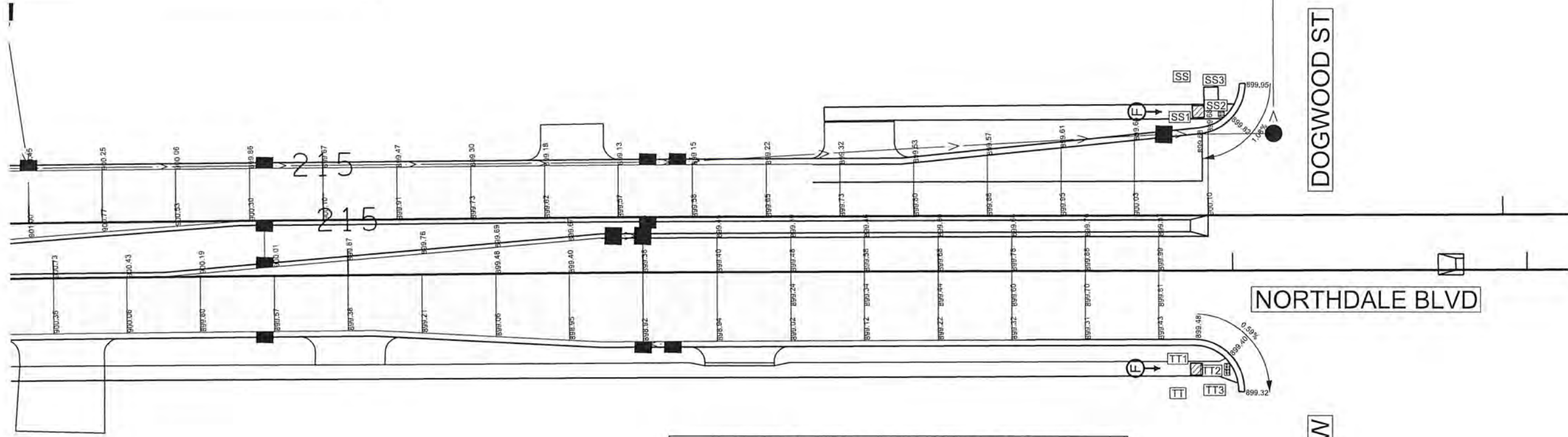
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

INTERSECTION / CUL-DE-SAC
LAYOUT
FLINTWOOD ST
Sheet 103 of 298 Sheets

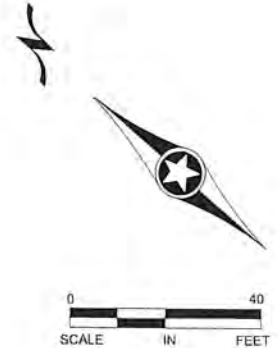
LEGEND	
	PROPOSED SIGNAL POLE
	CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
	TRUNCATED DOMES (SEE STANDARD PLATE 7038)
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	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
999.99	PROPOSED SPOT ELEVATION AT TOP OF FINISHED PAVEMENT
	2.00% CROSS SLOPE
	2.50% CROSS SLOPE

DOGWOOD ST. N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
SS	15' RAD F/C	LWB 12	217+95.73	-44.39		
SS1	BEG	LWB 12	217+96.84	-33.06	899.68	899.87
SS2	MID PT	LWB 12	218+04.16	-36.74	899.82	900.01
SS3	END	LWB 12	218+07.11	-44.38	899.95	900.14



DOGWOOD ST./113TH AVE N.W. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
TT	15' RAD F/C	LEB 12	217+86.96	40.00		
TT1	BEG	LEB 12	217+86.96	28.62	899.48	899.67
TT2	MID PT	LEB 12	217+95.14	32.09	899.40	899.59
TT3	END	LEB 12	217+98.33	40.38	899.32	899.51

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED
AND RADIUS GRADES ADJUSTED IF MATCH
POINTS ARE MOVED.



NO	DATE	BY	CKD	APPR	REVISION

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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME
OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY
LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF
THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *Gina M. Pizzo*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
DESIGN BY: DFF DATE: 02/27/16
CHECKED BY: JEO DATE: 02/27/16

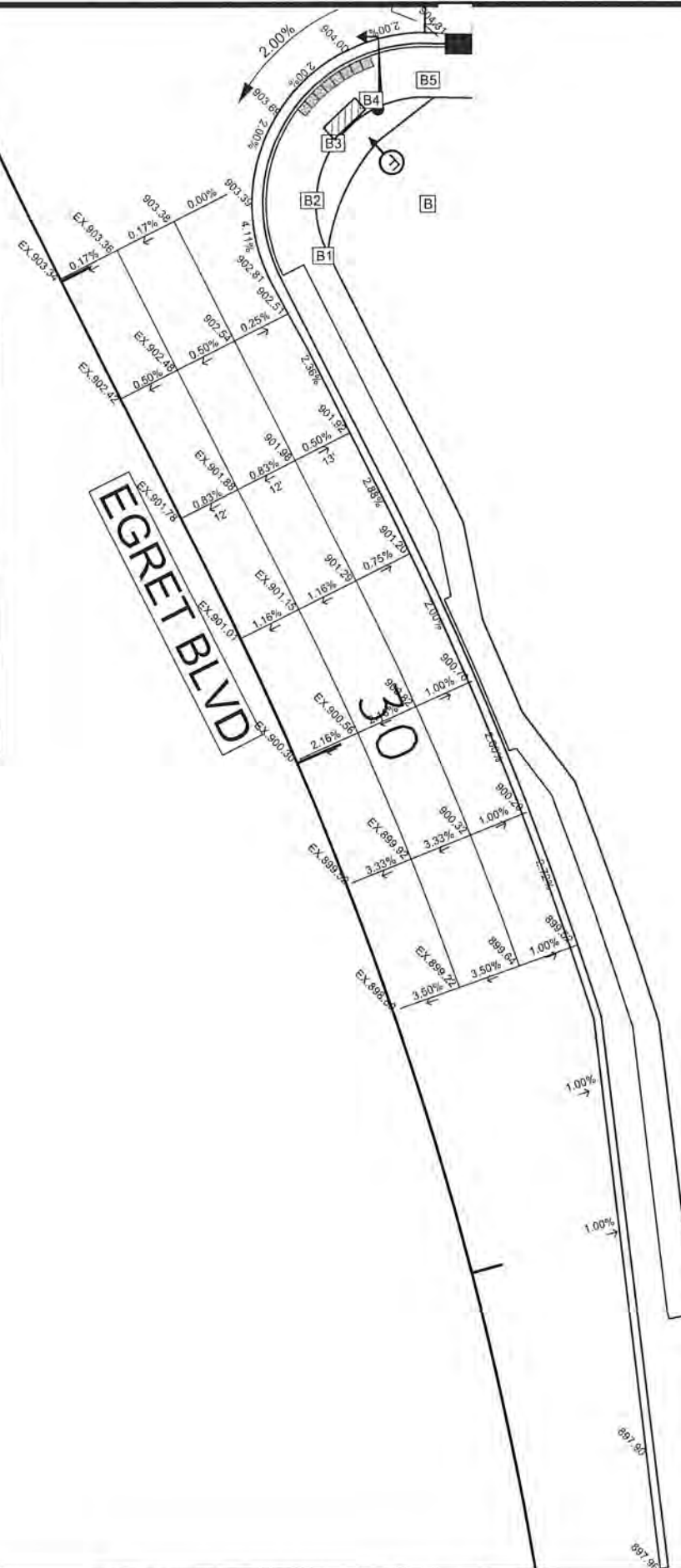


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HIGHWAY DEPT.

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SP 114-020-050
CP 13-10

INTERSECTION / CUL-DE-SAC
LAYOUT
113TH AVE/DOGWOOD ST
Sheet 104 of 298 Sheets

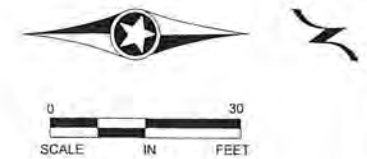
FOLEY BLVD



LEGEND	
	PROPOSED SIGNAL POLE
	CONTROL POINTS ON C&G ALONG THE GUTTER LIP/ CONTROL POINTS ON THE BACK/FRONT OF SIDEWALK
	TRUNCATED DOMES (SEE STANDARD PLATE 7038)
	CONSTRUCT CONCRETE CURB & GUTTER
	LANDING AREA - 4' X 4' MIN. DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS
	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%
999.99	PROPOSED SPOT ELEVATION AT TOP OF FINISHED PAVEMENT
	2.00% CROSS SLOPE
	2.50% CROSS SLOPE

EGRET BLVD. N.E. QUAD. OFFSET 3' B/C B424 CURB						
PT ID	LOC.	ALI.	STATION	OFFSET	BIT ELEV.	TBC ELEV.
B	30' RAD F/C	LNB 11	100+86.77	50.00		
B1	BEG	LNB 11	100+62.92	61.28	902.81	903.00
B2	1/4 PT	LNB 11	100+60.44	48.39	903.39	903.58
B3	MID PT	LNB 11	100+64.48	35.89	903.69	903.88
B4	3/4 PT	LNB 11	100+74.05	26.90	904.00	904.19
B5	END	LNB 11	100+86.77	23.62	904.31	904.50

NOTE: ALL MATCHPOINTS ARE TO BE FIELD VERIFIED
AND RADIUS GRADES ADJUSTED IF MATCH
POINTS ARE MOVED.



NO	DATE	BY	CKD	APPR	REVISION

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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME
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LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF
THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *Gina M. Pizzo*
DATE: 5-19-16 LICENSE NO. 22713


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
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

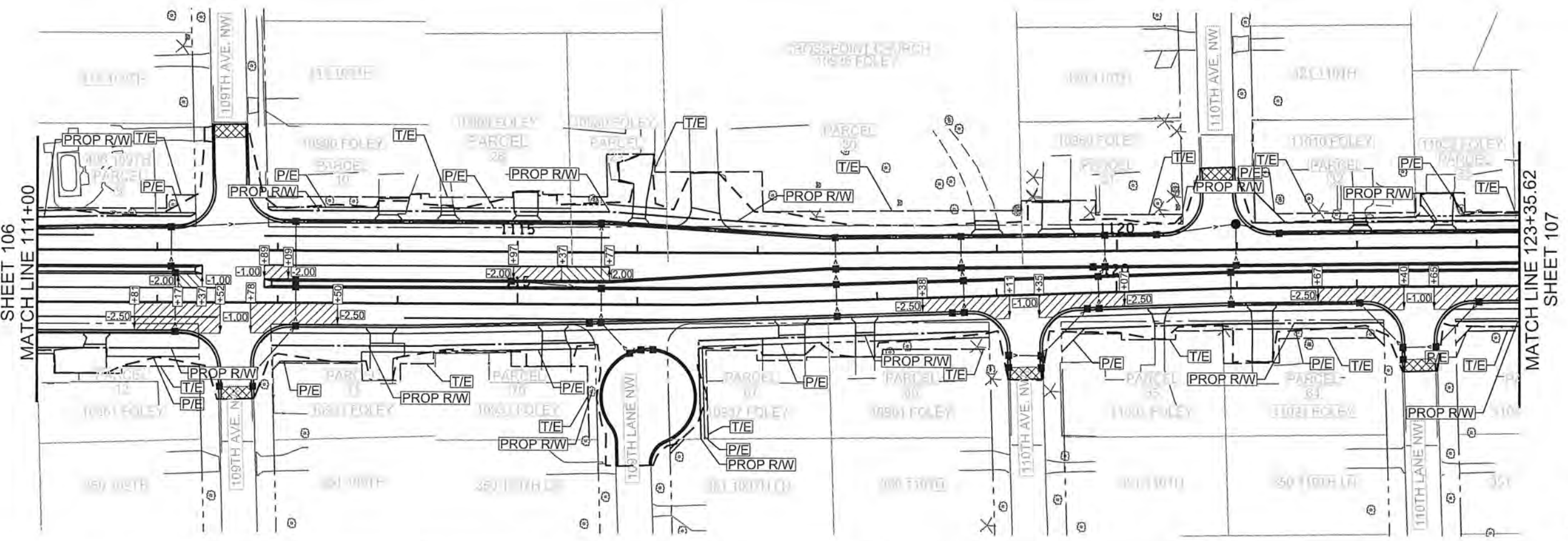
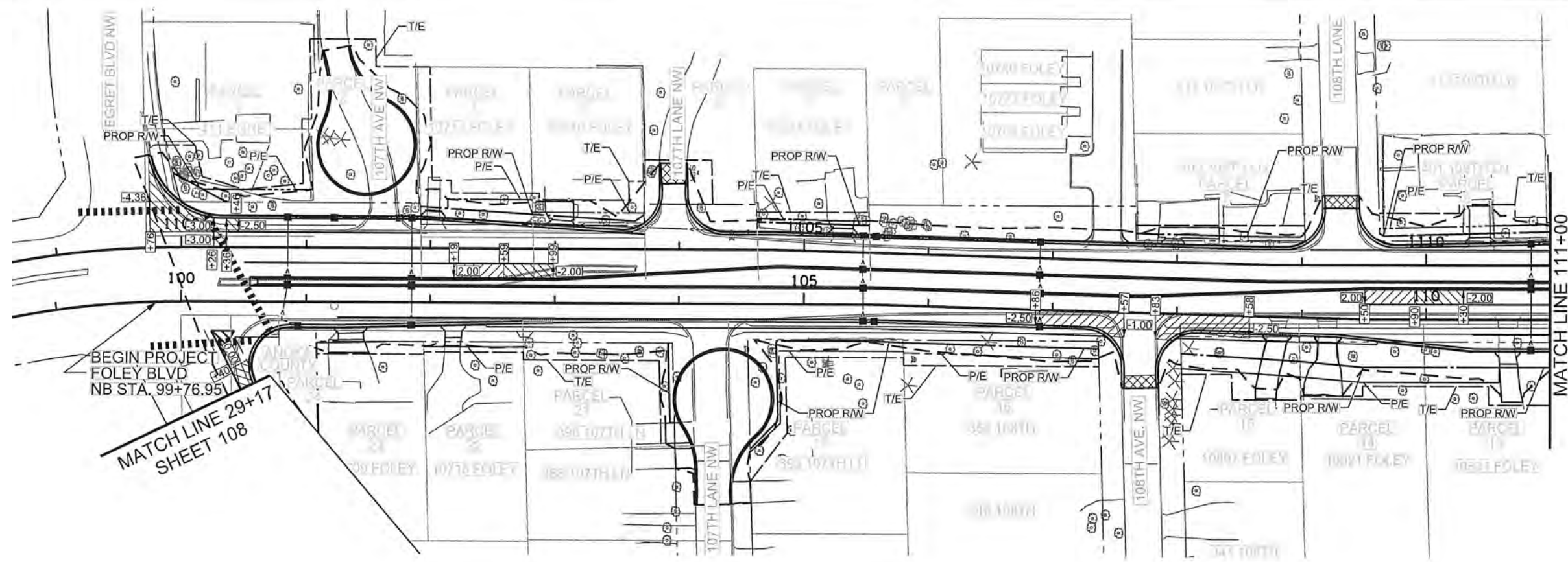
INTERSECTION / CUL-DE-SAC
LAYOUT
EGRET BLVD / 107TH AVE
Sheet 105 of 298 Sheets

LEGEND

 SUPERELEVATION TRANSITION

 MATCH TO EXISTING

SEE SHEETS 90 - 105 FOR INTERSECTION DETAILS AND SIDE STREET CROSS SLOPES.



NO	DATE	BY	CKD	APPR	REVISION
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *Gina Pizzo*

DATE: 5-19-14 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16





**ANOKA COUNTY
HIGHWAY DEPT.**

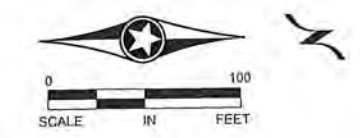
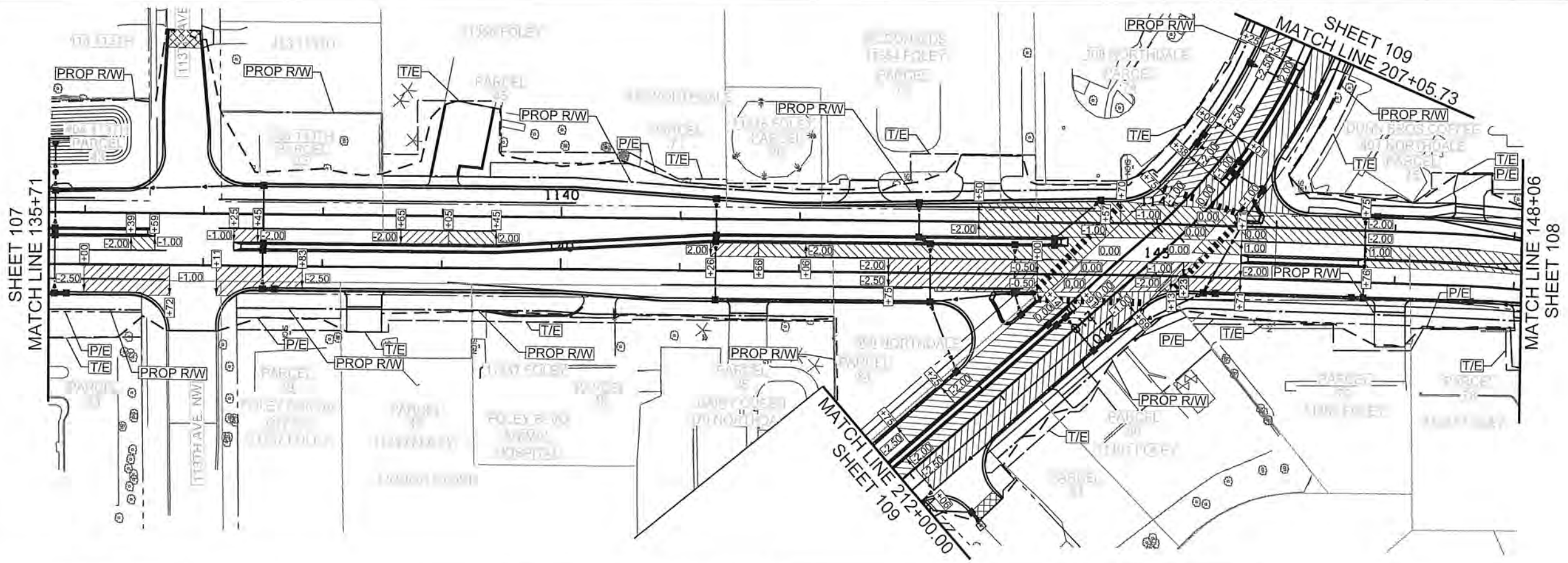
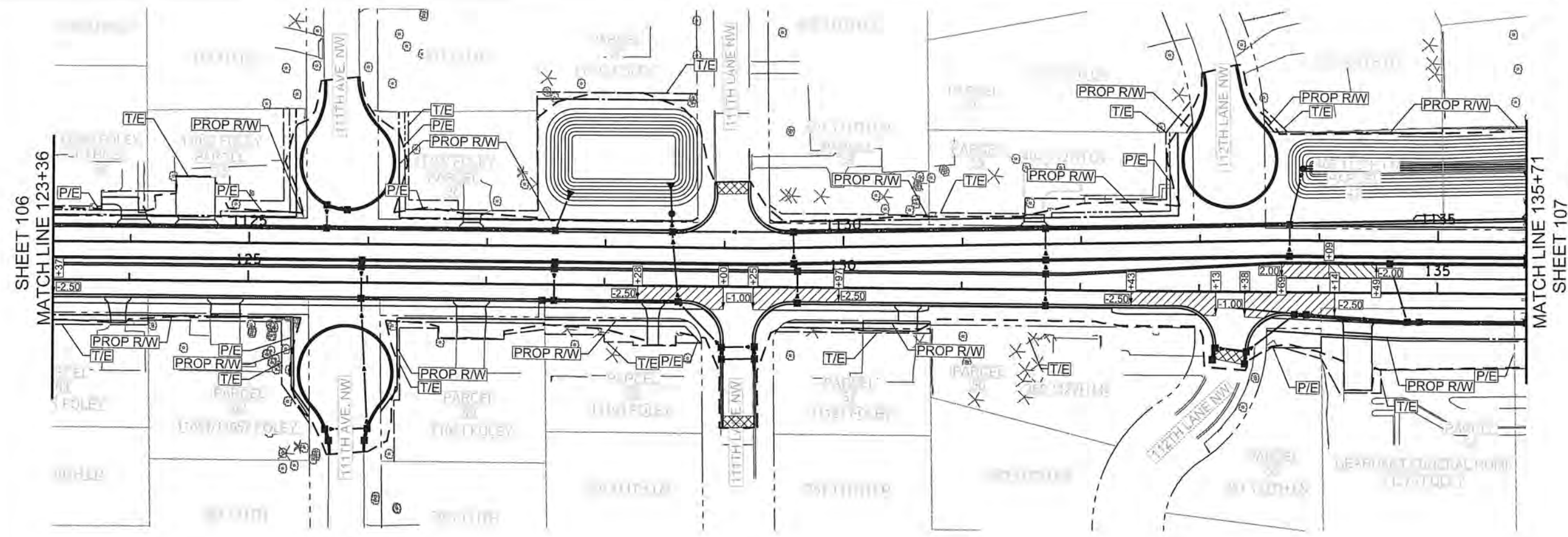
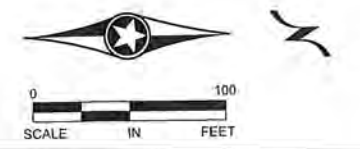
SP 002-611-034
SP 114-020-050
CP 13-10

**SUPERELEVATION PLAN
FOLEY BLVD.**
STA 99+76.95 TO 111+00.00
Sheet 106 of 298 Sheets

LEGEND

-  SUPERELEVATION TRANSITION
-  MATCH TO EXISTING

SEE SHEETS 90 - 105 FOR INTERSECTION DETAILS AND SIDE STREET CROSS SLOPES.



NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_SE_P2.dgn					
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-14 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
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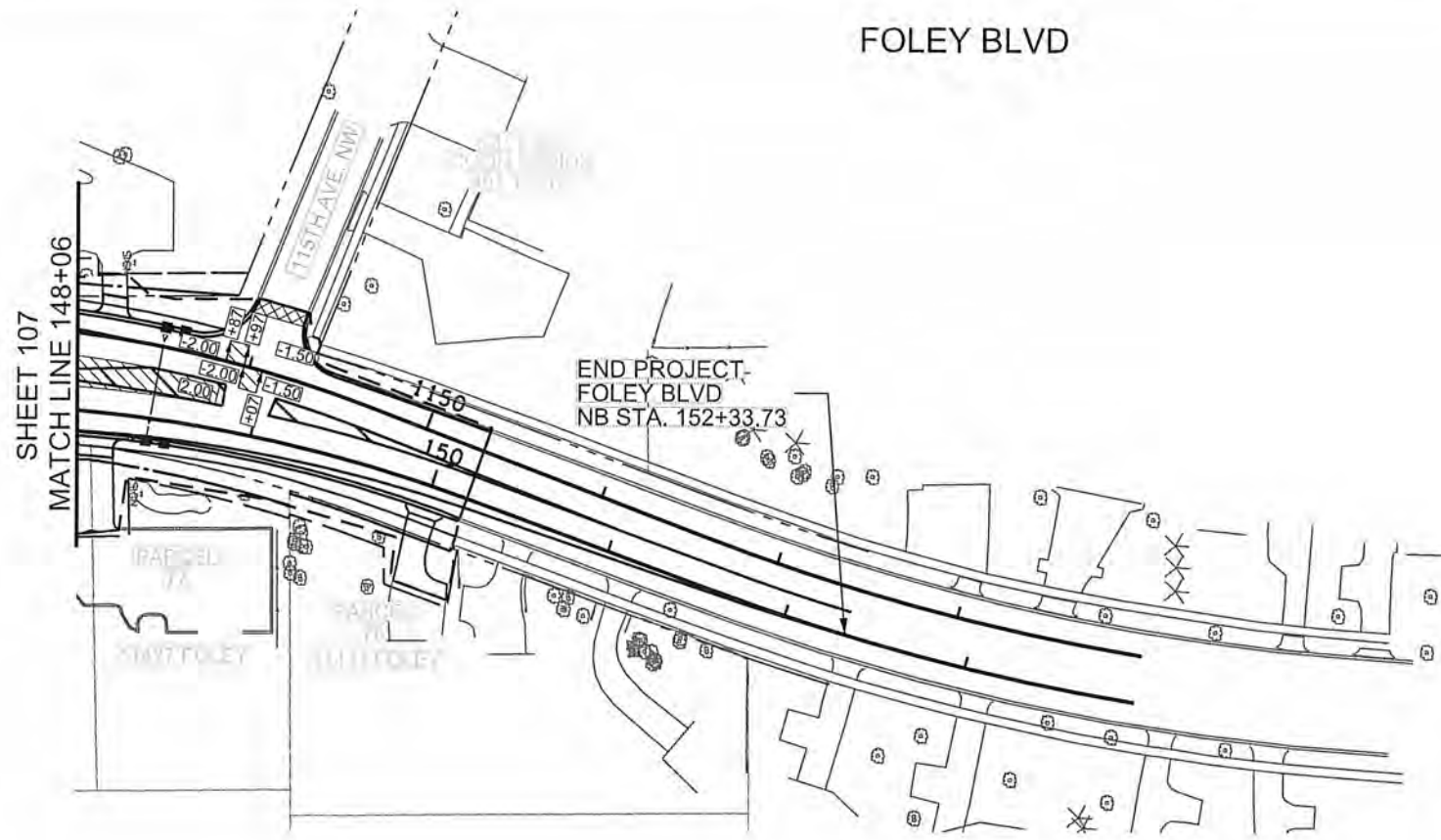


ANOKA COUNTY
 HIGHWAY DEPT.



SP 002-611-034
 SP 114-020-050
 CP 13-10

SUPERELEVATION PLAN
 FOLEY BLVD
 STA 123+36 TO 148+06
 Sheet 107 of 298 Sheets

FOLEY BLVD

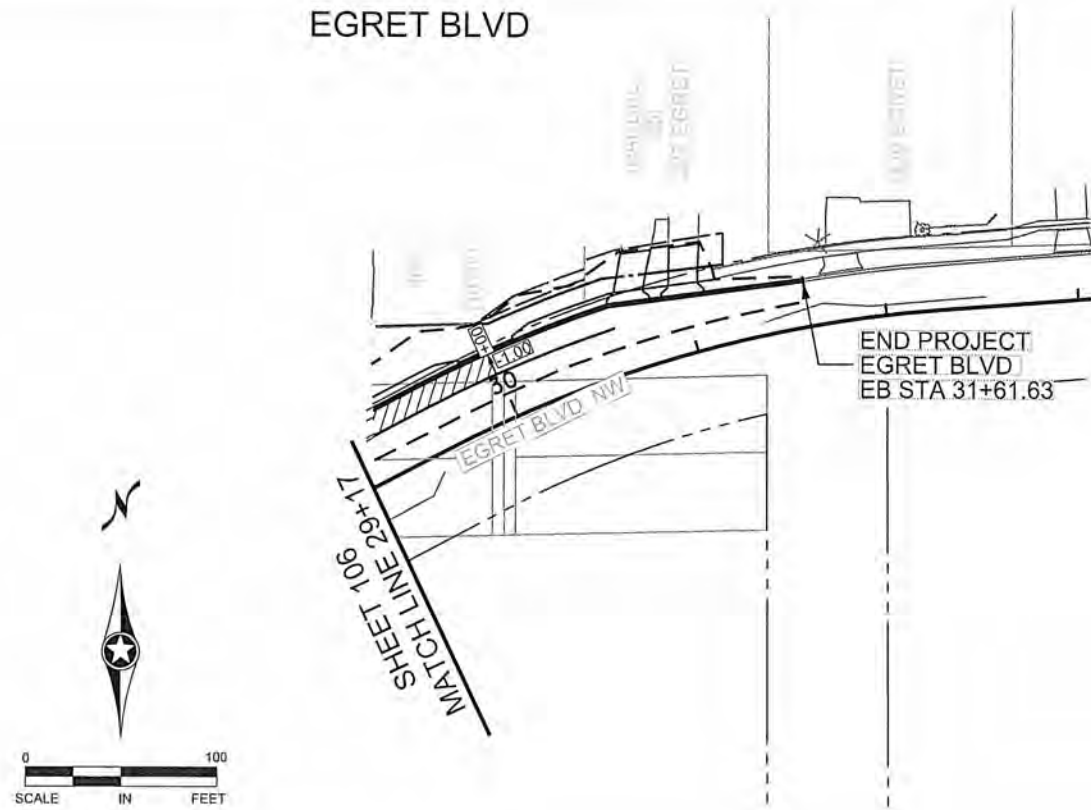


LEGEND

-  SUPERELEVATION TRANSITION
-  MATCH TO EXISTING

SEE SHEETS 90 - 105 FOR INTERSECTION DETAILS AND SIDE STREET CROSS SLOPES.

EGRET BLVD



NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_SE_P3.dgn 05/16/2016 1:18:07 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO


SIGNATURE: *Gina Pizzo*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

DESIGN BY: DFF DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16





ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

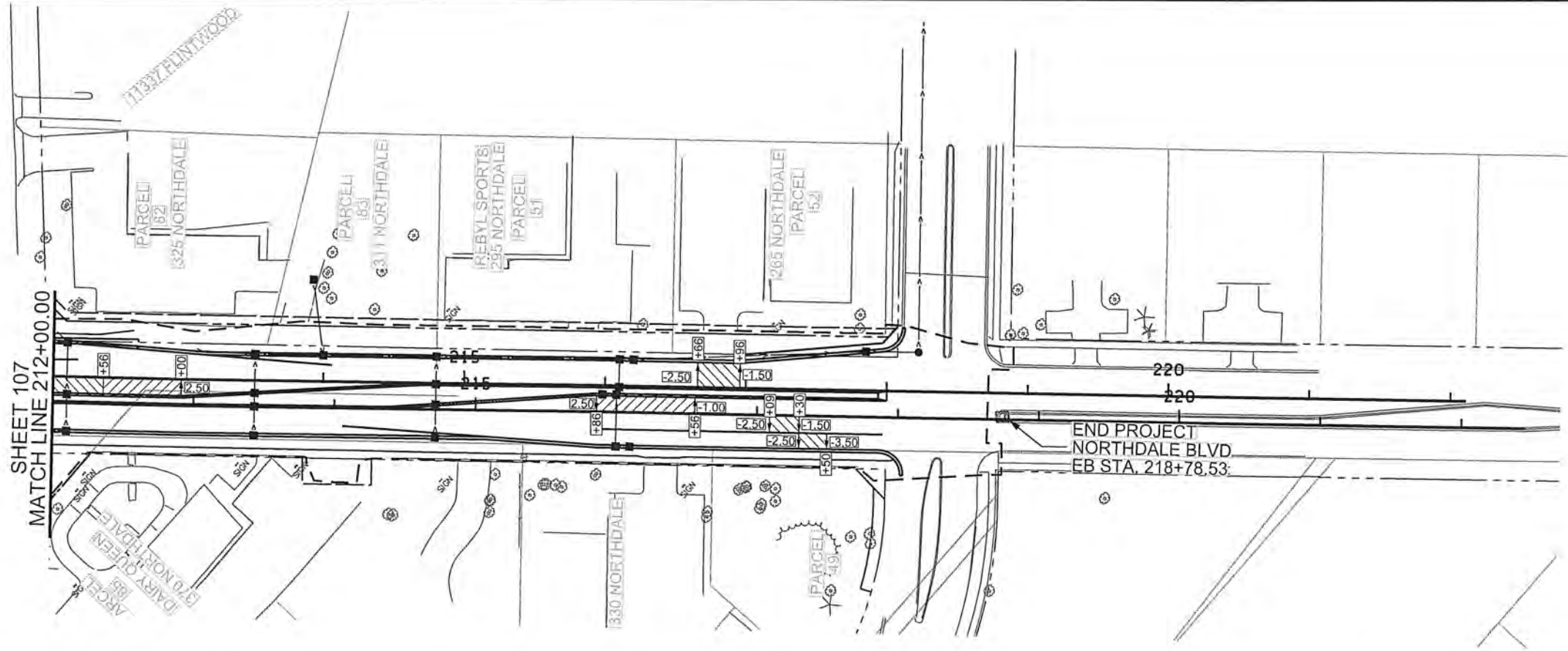
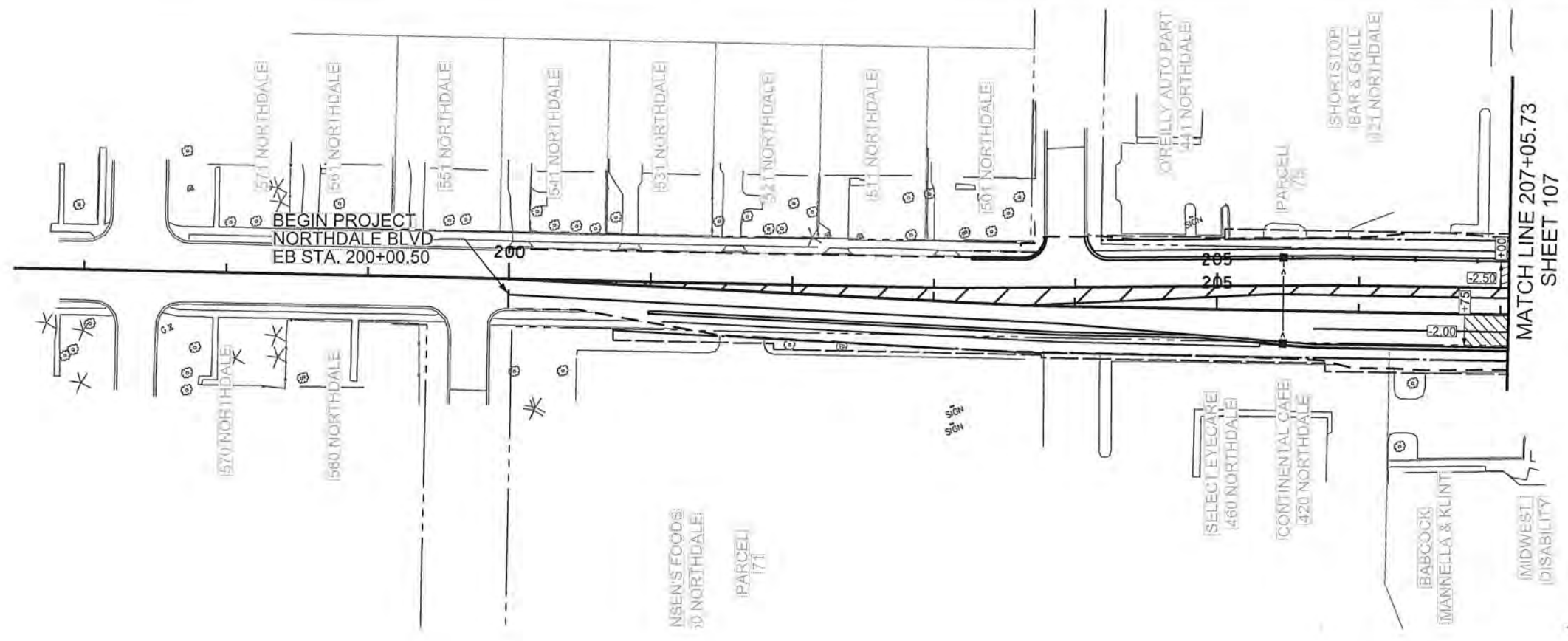
SUPERELEVATION PLAN
FOLEY BLVD & EGRET BLVD

Sheet 108 of 298 Sheets

LEGEND

-  SUPERELEVATION TRANSITION
-  MATCH TO EXISTING

SEE SHEETS 90 - 105 FOR INTERSECTION DETAILS AND SIDE STREET CROSS SLOPES.



NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_SE_P4.dgn					
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
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 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

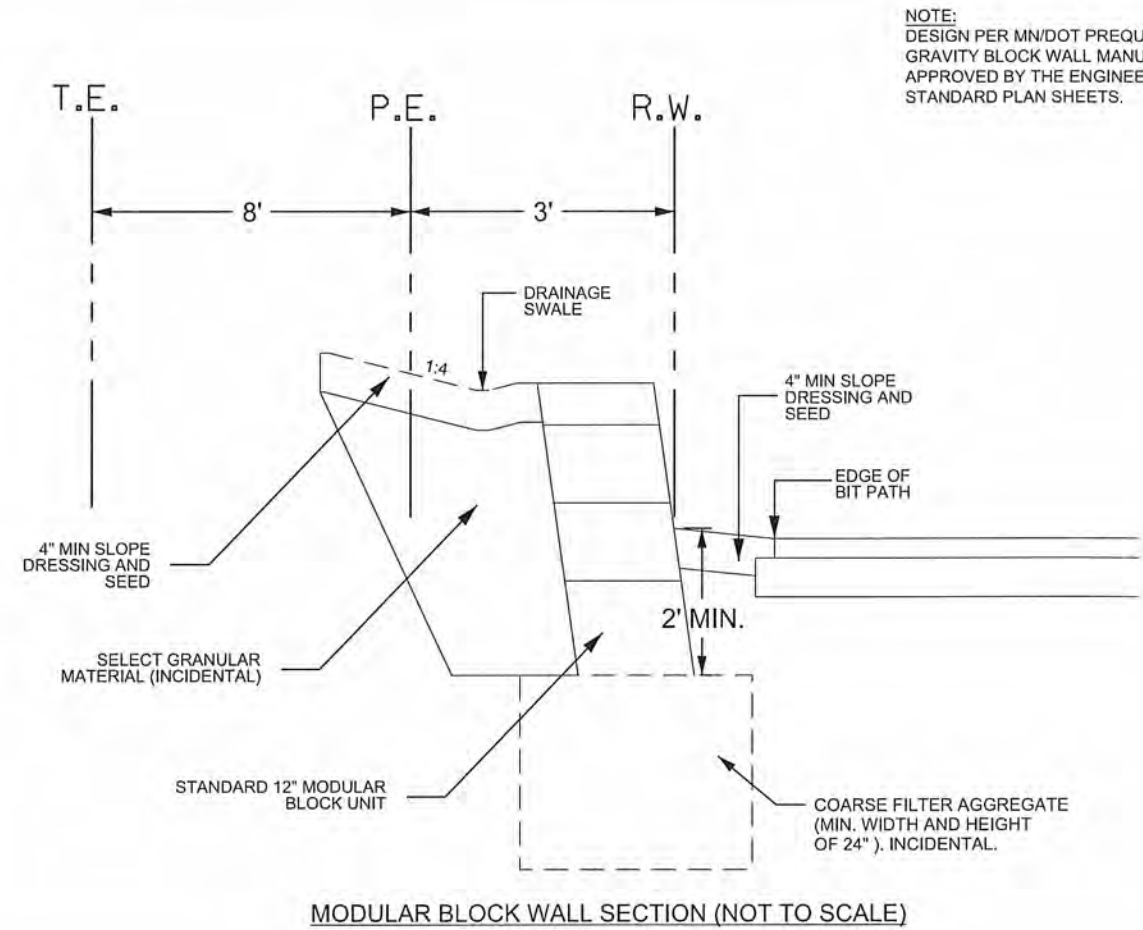
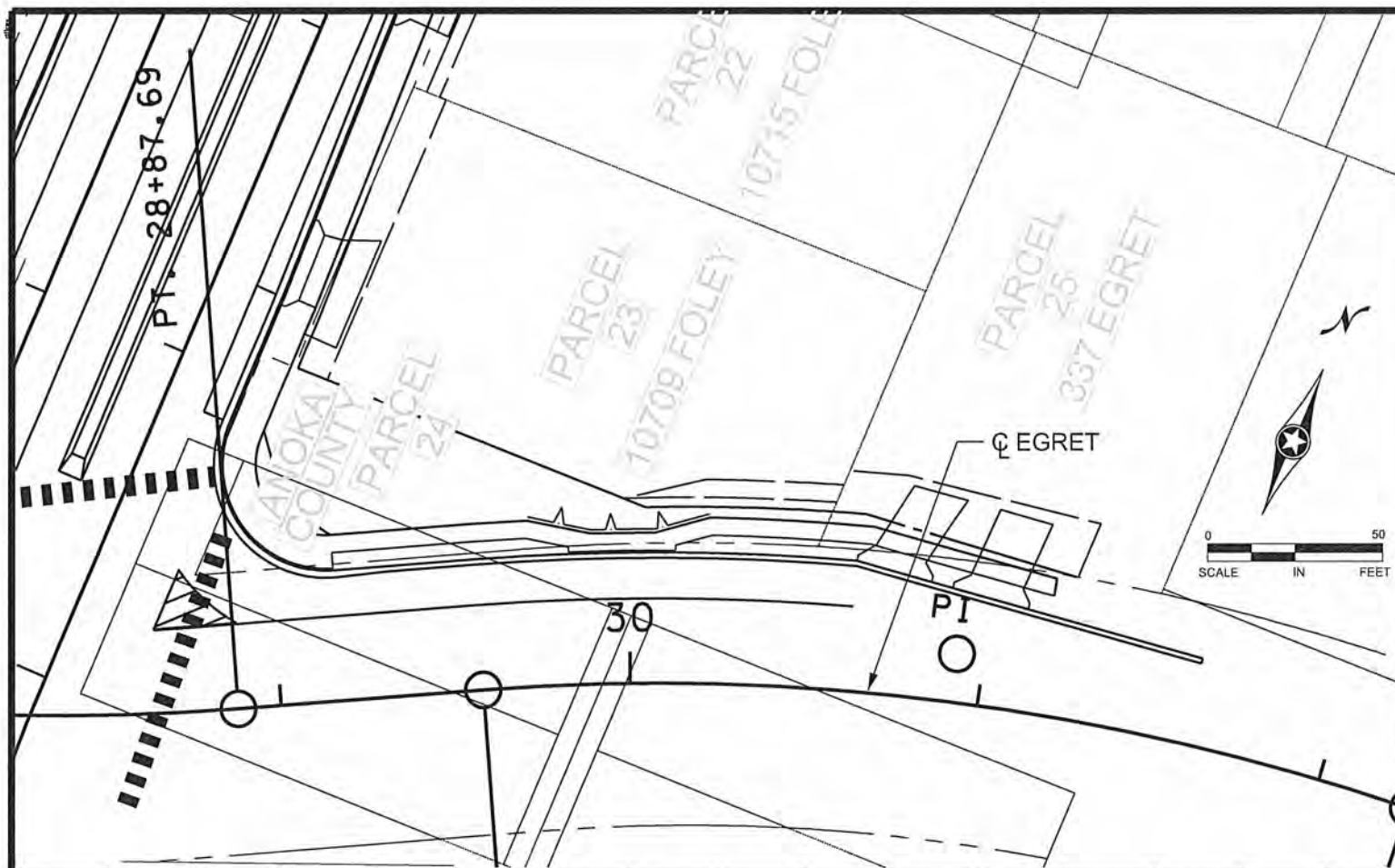
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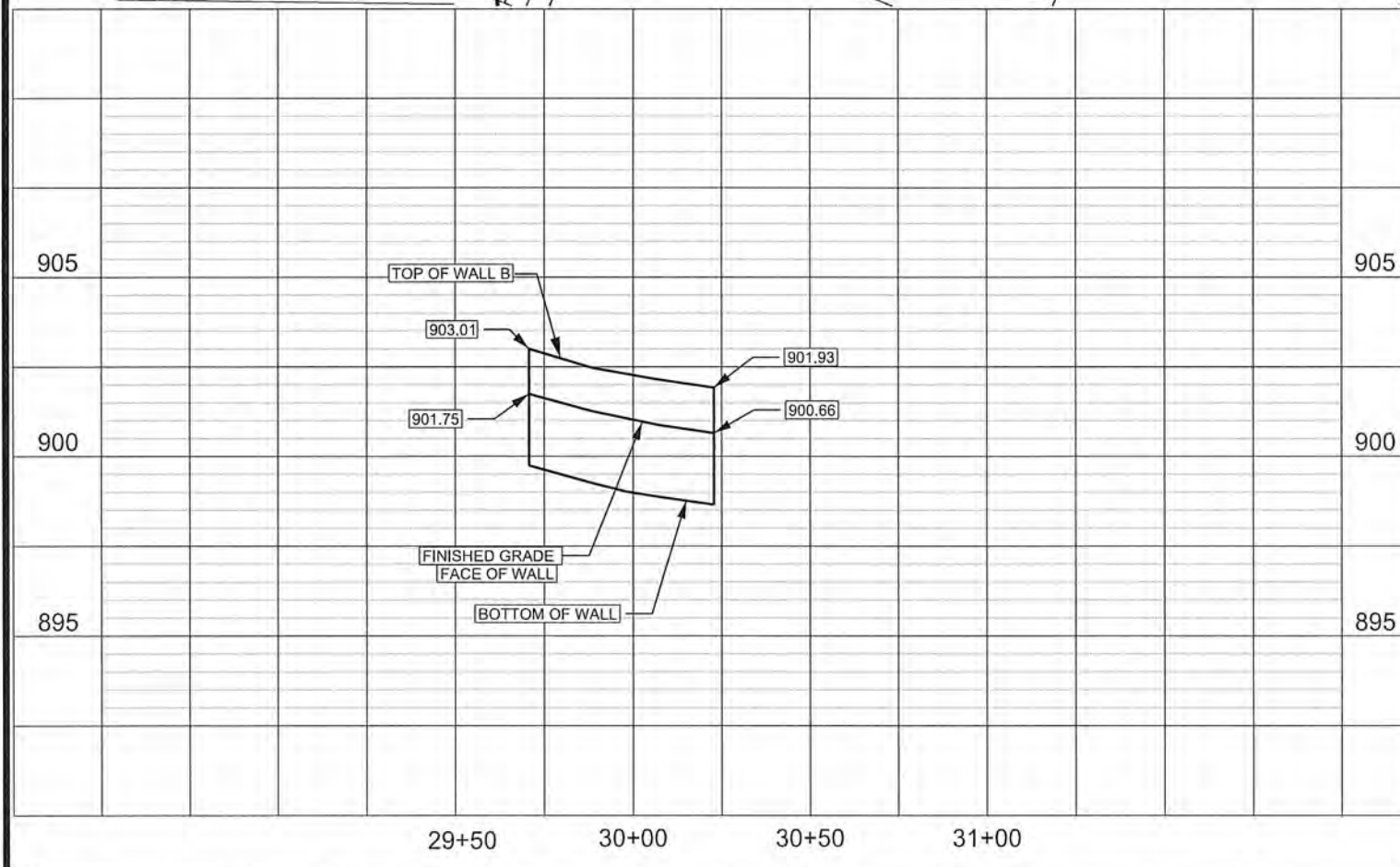
ANOKA COUNTY
 HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

SUPERELEVATION PLAN
 NORTHDALE BLVD.
 STA 196+50 TO 218+04.77
 Sheet 109 of 298 Sheets



NOTE:
DESIGN PER MN/DOT PREQUALIFIED MODULAR
GRAVITY BLOCK WALL MANUFACTURER AND TO BE
APPROVED BY THE ENGINEER. SEE RETAINING WALL
STANDARD PLAN SHEETS.



MODULAR BLOCK WALL DATA - WALL B								
STATION	OFFSET	ALIGN	FINISHED GRADE (FRONT FACE)	TOP OF WALL	BOTTOM OF WALL (1)	HEIGHT	AREA (SQ FT)	
29+74.80	47.97	EGRET	901.75	903.01	899.75	3.26'		
29+90.56	43.61	EGRET	901.27	902.48	899.27	3.21'	51	
30+08.22	43.61	EGRET	900.88	902.15	898.88	3.27'	57	
30+21.87	47.96	EGRET	900.66	901.93	898.66	3.27'	101	
RETAINING WALL TOTAL AREA								209

NOTES: (1) BOTTOM OF WALL 2.0' BELOW GROUND ELEVATION

NO	DATE	BY	CKD	APPR	REVISION

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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
SIGNATURE: *[Signature]*
DATE: 5-19-16 LICENSE NO. 22713

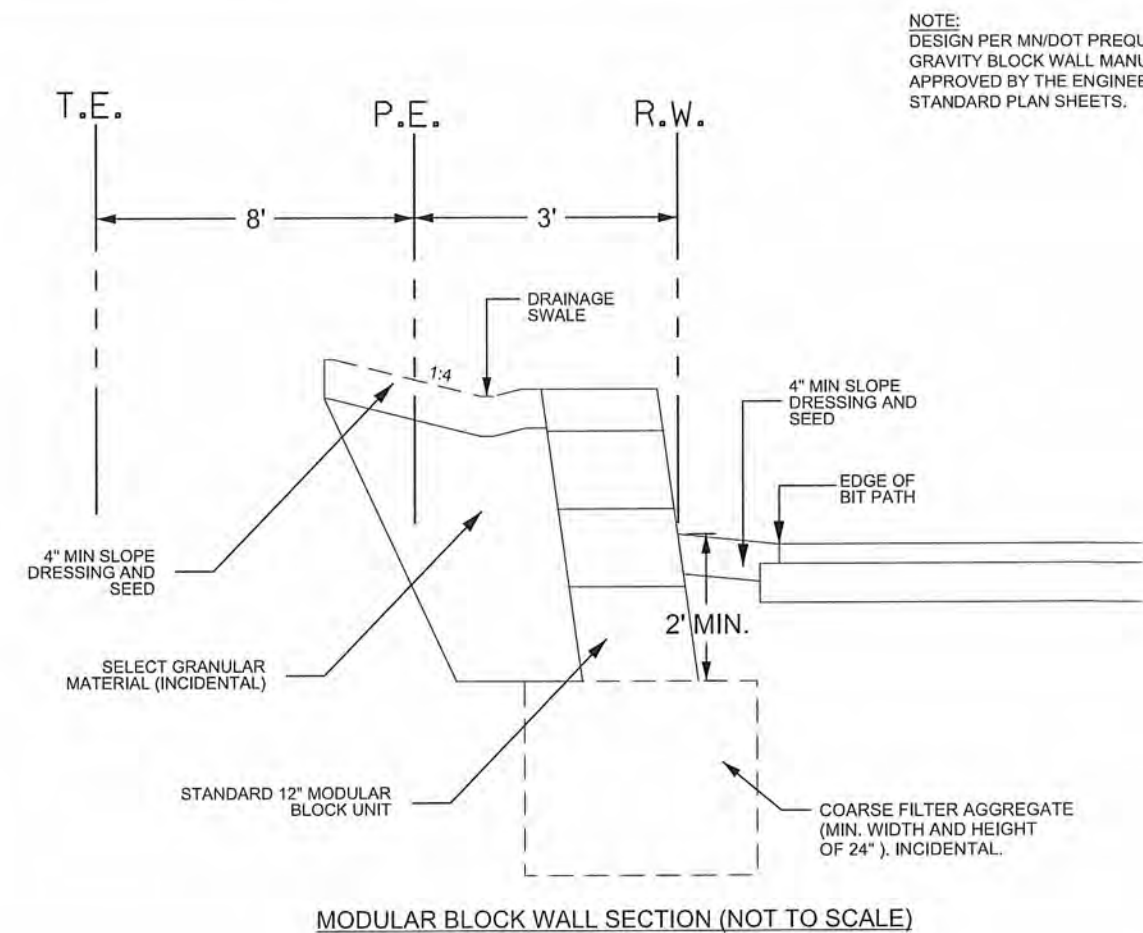
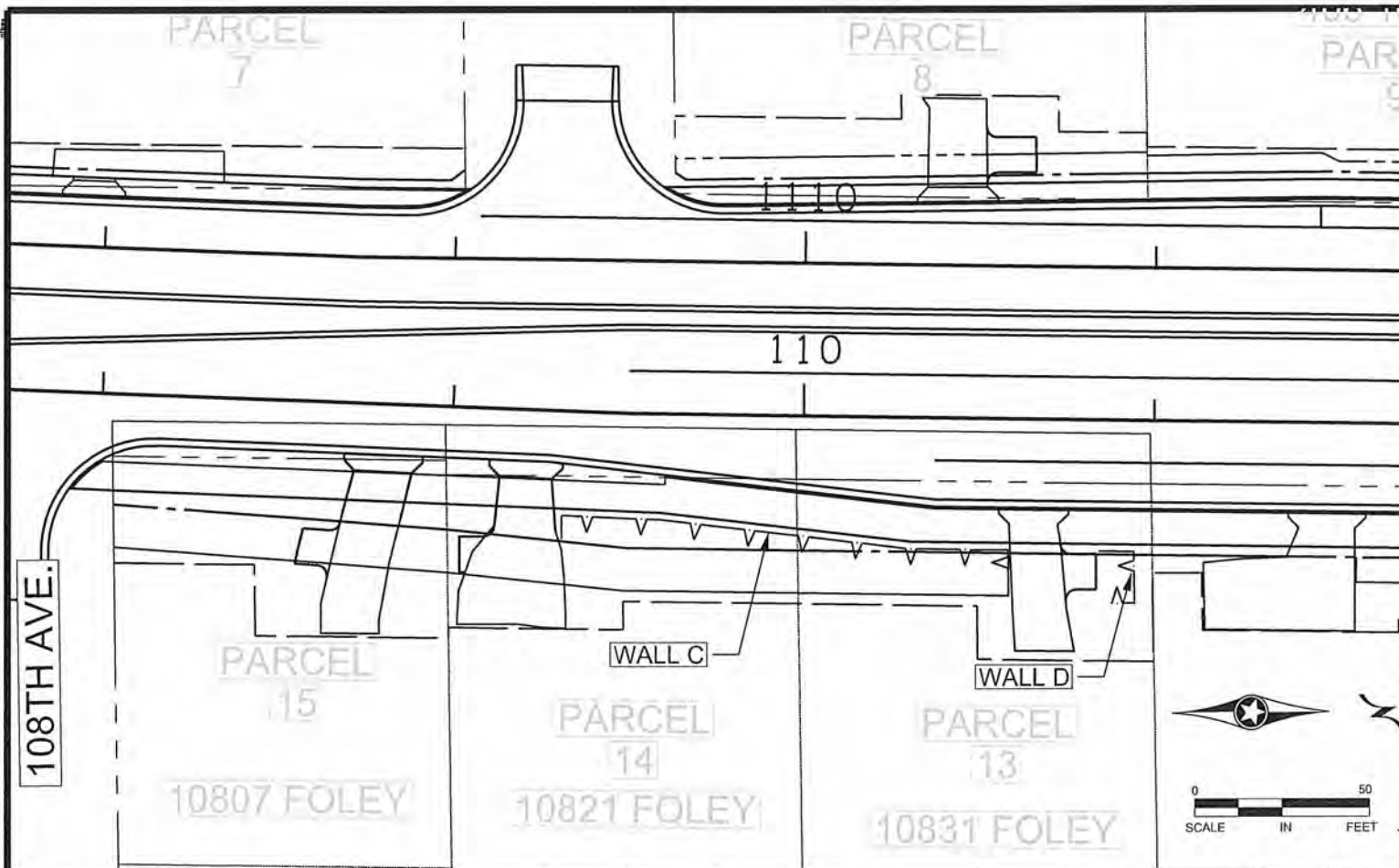
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DESIGN BY DEF DATE 02/27/16
CHECKED BY JEO DATE 02/27/16



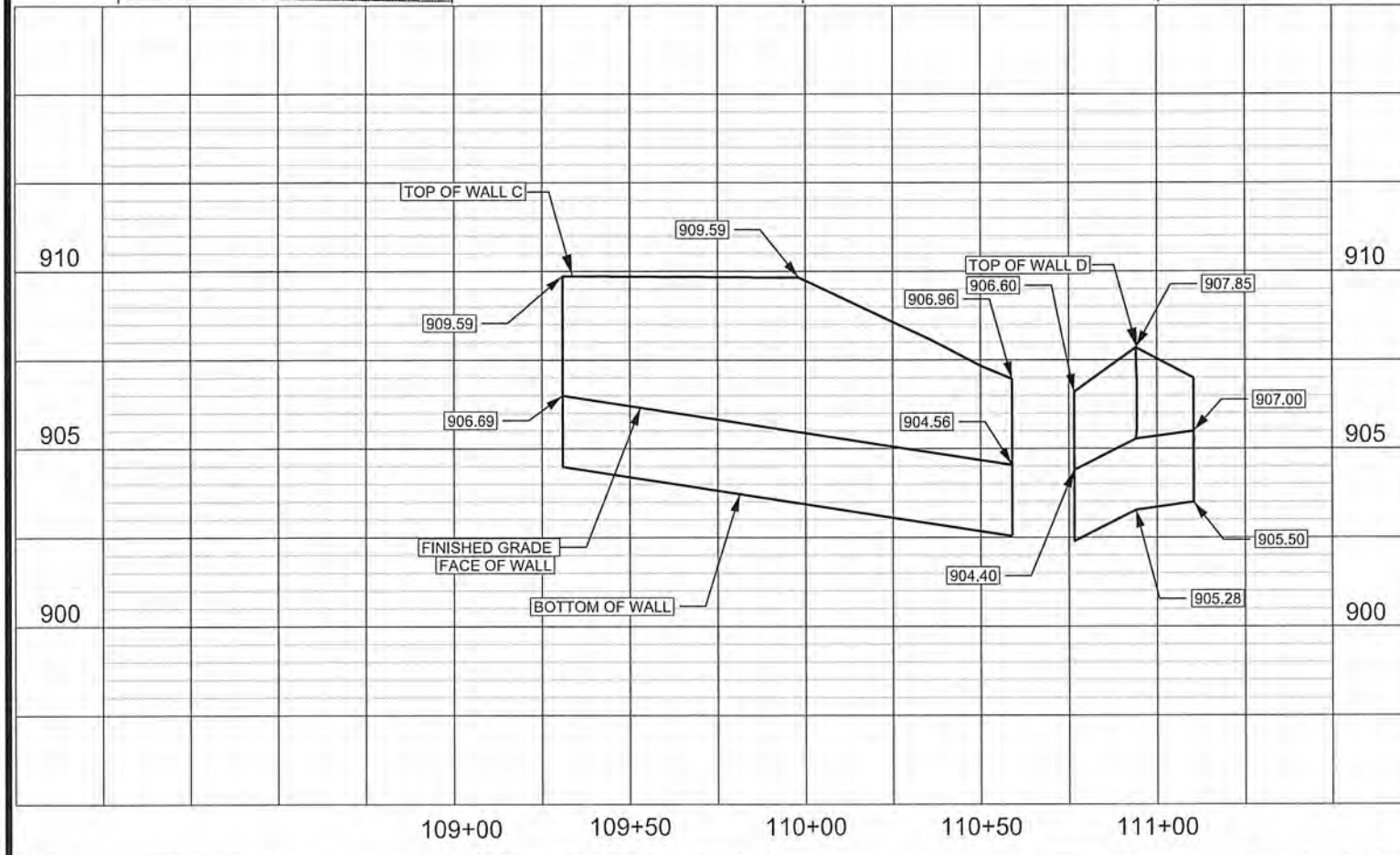
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

RETAINING WALL DETAILS
WALL B
Sheet 110 of 298 Sheets



NOTE:
DESIGN PER MN/DOT PREQUALIFIED MODULAR
GRAVITY BLOCK WALL MANUFACTURER AND TO BE
APPROVED BY THE ENGINEER. SEE RETAINING WALL
STANDARD PLAN SHEETS.



MODULAR BLOCK WALL DATA - WALL C							
STATION	OFFSET	ALIGN	FINISHED GRADE (FRONT FACE)	TOP OF WALL	BOTTOM OF WALL (1)	HEIGHT	AREA (SQ FT)
109+32.45	36.04	NB	906.69	909.59	904.69	4.90'	
109+50.00	36.97	NB	906.47	909.59	904.47	5.12'	88
109+98.19	37.00	NB	905.66	909.59	903.66	5.93'	266
110+36.00	37.00	NB	904.96	908.07	902.96	5.11'	209
110+50.00	37.00	NB	904.69	907.34	902.69	4.65'	68
110+59.52	37.00	NB	904.56	906.96	902.56	4.40'	43
RETAINING WALL TOTAL AREA							674

MODULAR BLOCK WALL DATA - WALL D							
STATION	OFFSET	ALIGN	FINISHED GRADE (FRONT FACE)	TOP OF WALL	BOTTOM OF WALL (1)	HEIGHT	AREA (SQ FT)
110+77.05	37.00	SB	904.40	906.60	902.40	4.20'	
110+95.14	37.00	SB	905.28	907.85	903.28	4.57'	79
110+95.14	53.07	SB	905.50	907.00	903.50	3.50'	73
RETAINING WALL TOTAL AREA							152

NOTES: (1) BOTTOM OF WALL 2.0' BELOW GROUND ELEVATION
(2) INSTALL CHAIN LINK FENCE WHERE HEIGHT OF WALL IS 5' OR HIGHER

NO	DATE	BY	CHKD	APPR	REVISION

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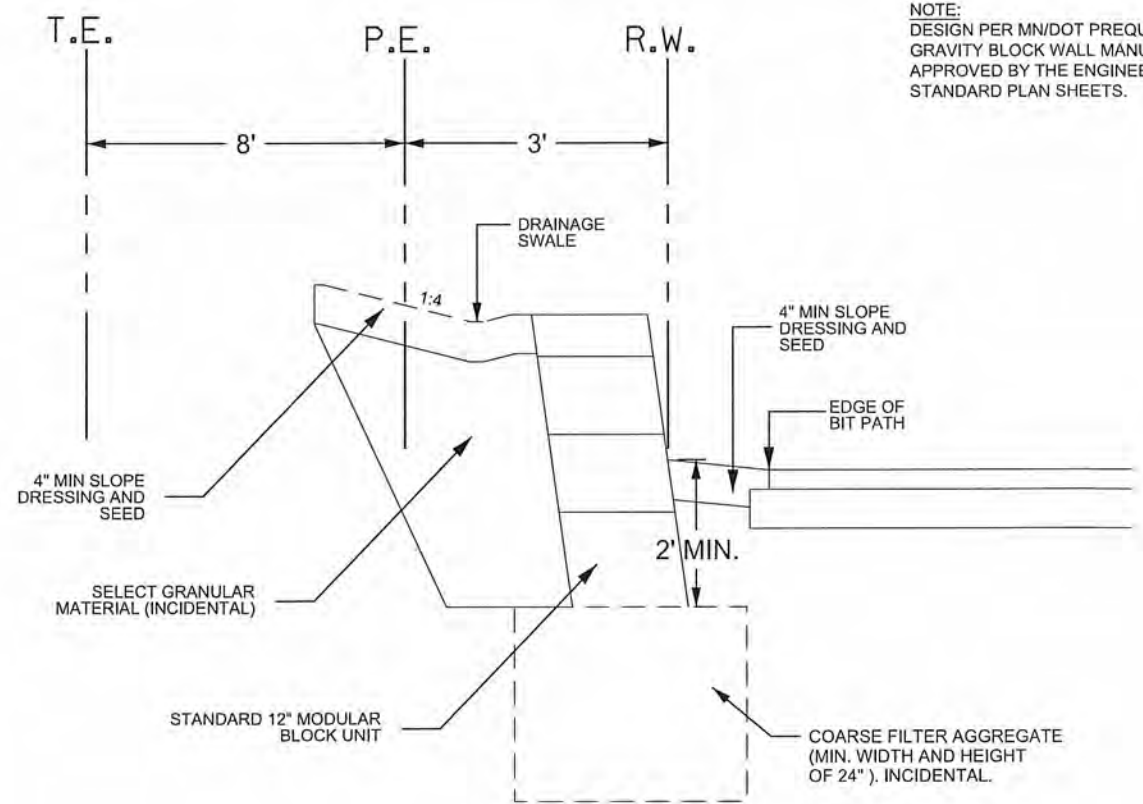
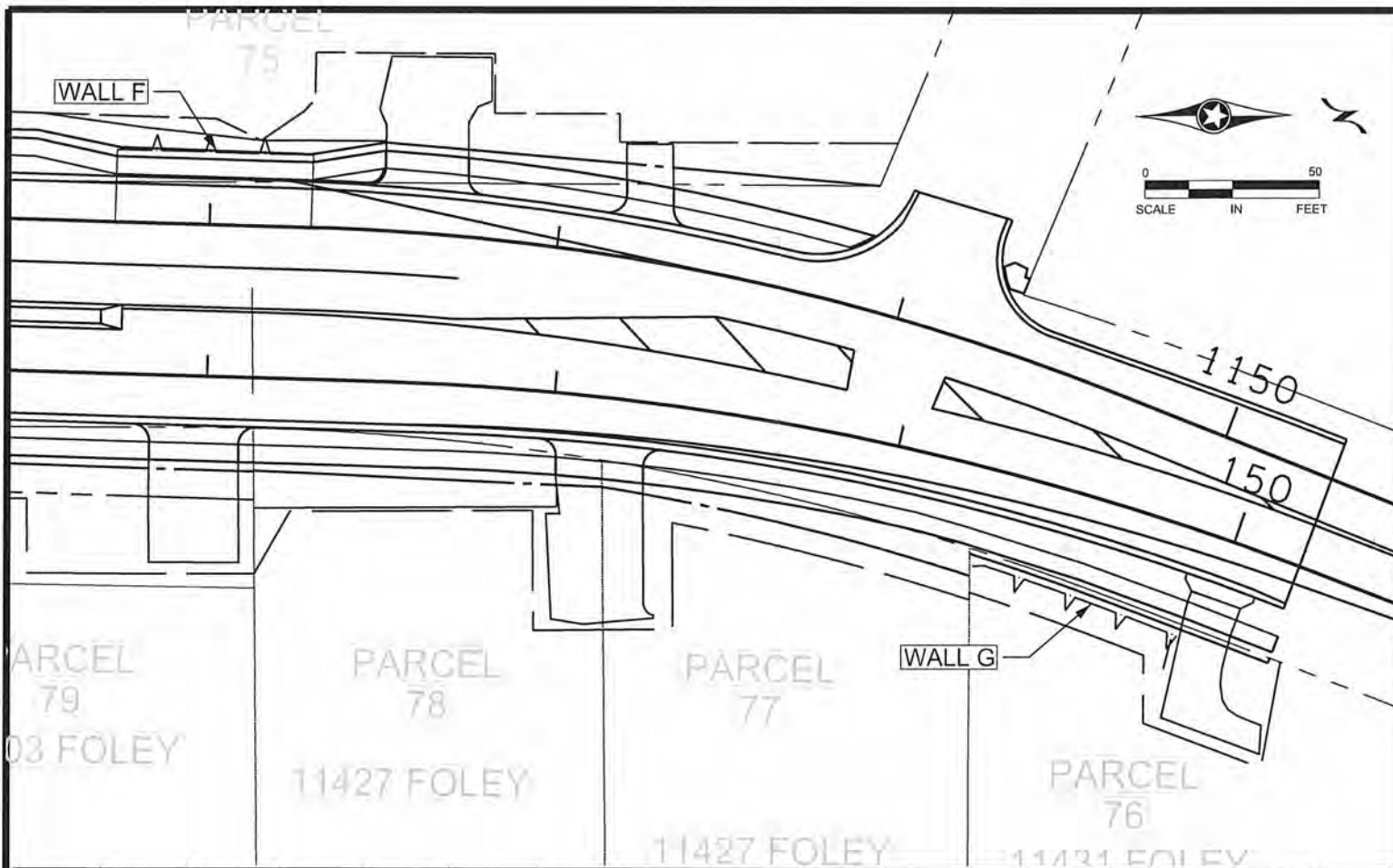
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PRINT NAME: GINA M. PIZZO
SIGNATURE: *[Signature]*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DEF DATE: 02/27/16
DESIGN BY: DEF DATE: 02/27/16
CHECKED BY: JEQ DATE: 02/27/16

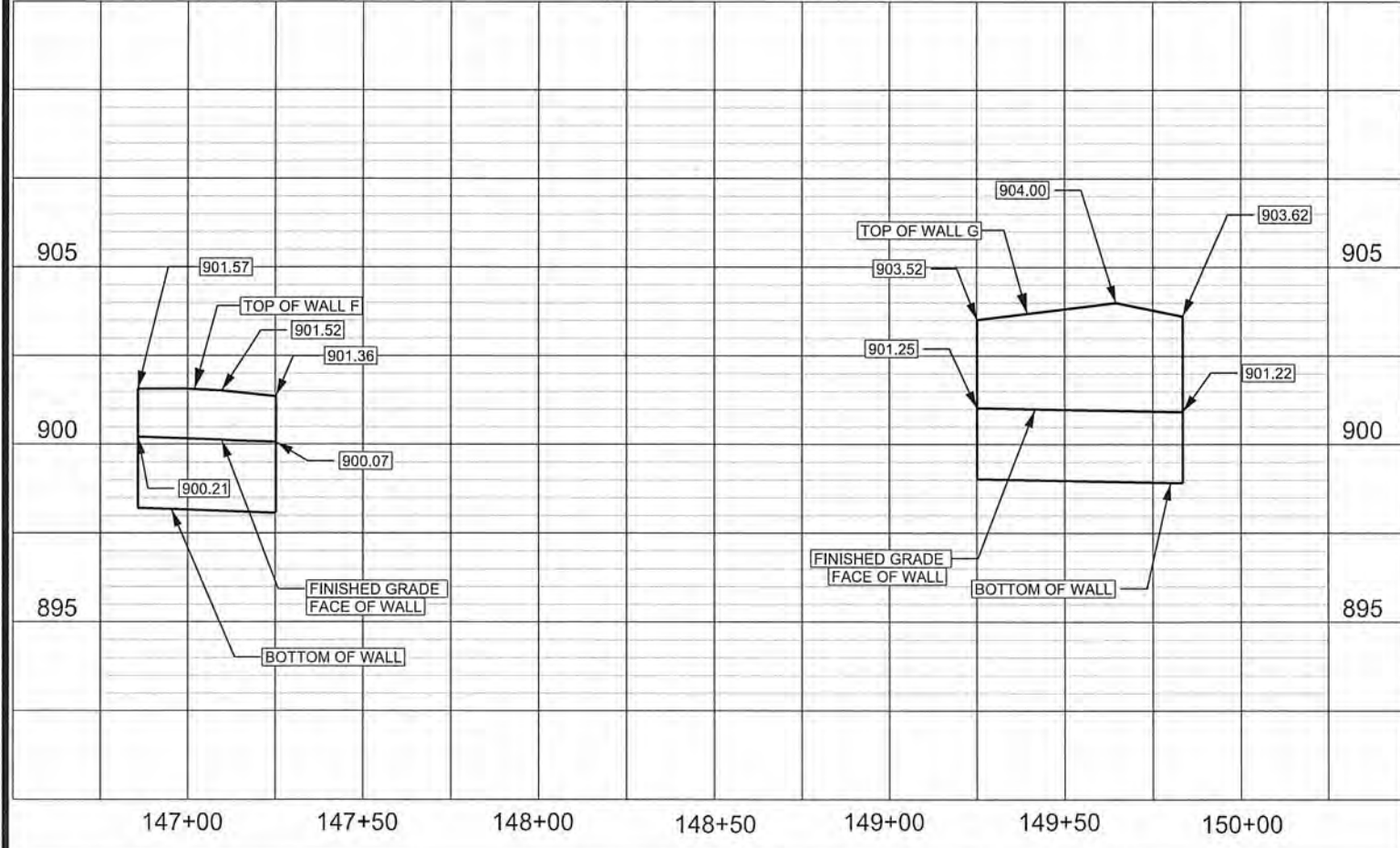


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CP 13-10



NOTE:
DESIGN PER MNDOT PREQUALIFIED MODULAR GRAVITY BLOCK WALL MANUFACTURER AND TO BE APPROVED BY THE ENGINEER. SEE RETAINING WALL STANDARD PLAN SHEETS.

MODULAR BLOCK WALL SECTION (NOT TO SCALE)



MODULAR BLOCK WALL DATA - WALL F								
STATION	OFFSET	ALIGN	FINISHED GRADE (FRONT FACE)	TOP OF WALL	BOTTOM OF WALL (1)	HEIGHT	AREA (SQ FT)	
1146+85.67	-20.61	SB	900.21	901.57	898.21	3.36'		
1147+00.67	-20.61	SB	900.16	901.57	898.16	3.41'	51	
1147+09.67	-20.61	SB	900.13	901.52	898.13	3.39'	31	
1147+25.17	-20.61	SB	900.07	901.36	898.07	3.29'	52	
RETAINING WALL TOTAL AREA								134

MODULAR BLOCK WALL DATA - WALL G								
STATION	OFFSET	ALIGN	FINISHED GRADE (FRONT FACE)	TOP OF WALL	BOTTOM OF WALL (1)	HEIGHT	AREA (SQ FT)	
149+32.36	27.84	NB	901.25	903.52	899.25	4.27'		
149+71.86	29.92	NB	901.23	904.00	899.23	4.77'	179	
149+90.87	30.71	NB	901.22	903.62	899.22	4.40'	87	
RETAINING WALL TOTAL AREA								266

NOTES: (1) BOTTOM OF WALL 2.0' BELOW GROUND ELEVATION

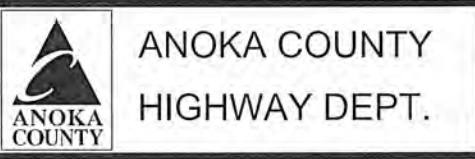
NO	DATE	BY	CKD	APPR	REVISION

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PRINT NAME: GINA M. PIZZO
SIGNATURE: *Gina Pizzo*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DEF DATE: 02/27/16
DESIGN BY: DEF DATE: 02/27/16
CHECKED BY: JEQ DATE: 02/27/16



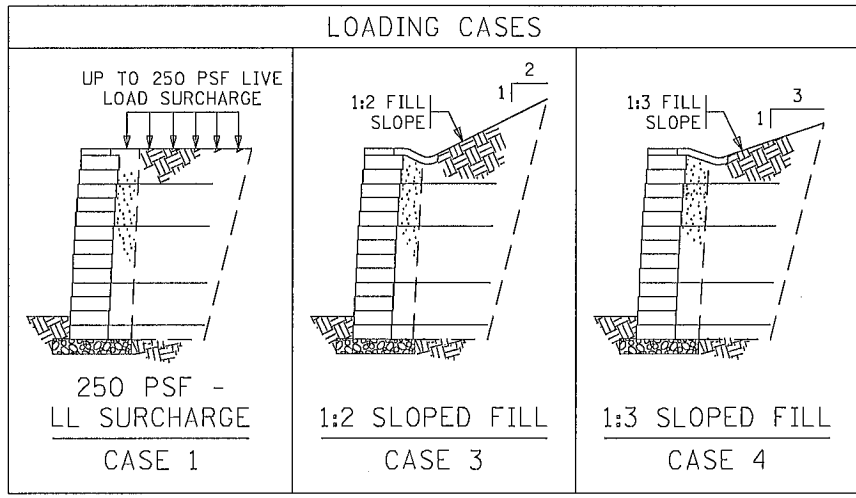
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RETAINING WALL DETAILS
WALL F & G
Sheet 113 of 298 Sheets

PLOTTED/REVISED:
05/13/2016

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CASE 2 IS OMITTED INTENTIONALLY

NOTES TO CONTRACTOR:

- APPROVED COMBINATIONS OF MODULAR BLOCK UNIT AND SOIL REINFORCEMENT PRODUCTS LIST WITH MBW REINFORCEMENT CLASS NOTED ARE HELD AND MAINTAINED BY THE FOUNDATIONS UNIT, AND POSTED AT <http://www.mn.dot.state.mn.us/geotechnical/foundations/foundations.asp> UNDER FOUNDATIONS UNIT. ONLY APPROVED PRODUCT COMBINATIONS, INCLUDING BLOCK PRODUCED FROM APPROVED SOURCES MEETING DURABILITY AND QUALITY CONTROL REQUIREMENTS, MAY BE USED IN STANDARD DESIGNS.
- PROVIDE DETAILED DRAWINGS FOR CONSTRUCTION CONTAINING:
 - SUBMIT, WITH THE DETAILED DRAWINGS, A COPY OF Mn/DOT STANDARD SHEETS FOR LOADING CASE(S) USED WITH OPTIONS USED MARKED IN THE TABLE.
 - ELEVATION VIEW WITH REINFORCEMENT PLACEMENT REQUIREMENTS, WALL FACING LAYOUT, AND GEOMETRIC INFORMATION. TOP OF WALL MAY EXTEND UP TO 4" ABOVE PLAN TOP OF WALL ELEVATION.
 - PLAN VIEW WITH BOTTOM AND TOP OF WALL ALIGNMENT, AND PLAN LIMITS OF WALL ALIGNMENT.
 - CROSS SECTIONS DETAILING BATTER, REINFORCEMENT, VERTICAL SPACING, REINFORCEMENT LENGTHS, SUBSURFACE DRAINAGE, SURFACE DRAINAGE, AND WATER RUNOFF COLLECTION ABOVE WALL.
 - REINFORCEMENT LAYOUT; REINFORCEMENT SHALL BE PLACED AT 100% COVERAGE RATIO. REINFORCEMENT ELEVATIONS SHALL BE CONSISTENT ACROSS LENGTH OF WALL STRUCTURE.
 - NOTE BLOCK, REINFORCEMENT, AND FILL PLACEMENT METHODS AND REQUIREMENTS.
 - DETAIL ALL WALL FILL PENETRATIONS AND WALL FACE PENETRATIONS. DETAIL REINFORCEMENT AND/OR WALL FACING UNIT PLACEMENT AROUND PENETRATIONS.
 - DETAILS THAT ARE SPECIFIC TO VENDOR PRODUCTS AND THEIR INTERACTION WITH OTHER PROJECT COMPONENTS.
 - LIST INFORMATION ON APPROVED COMBINATION OF MBW UNIT AND GEOSYNTHETIC REINFORCEMENT, INCLUDING Mn/DOT CLASSIFICATION CODE, NOMINAL BLOCK WIDTH, PROPERTIES FOR FIELD IDENTIFICATION, AND INSTALLATION INSTRUCTIONS.
 - DETAILS OF CAP UNITS AND INSTALLATION/FASTENING INSTRUCTIONS FOR THE CAPS. CAP UNITS SHALL BE SET IN A BED OF ADHESIVE DESIGNED TO WITHSTAND MOISTURE AND TEMPERATURE EXTREMES, REMAIN FLEXIBLE, AND SHALL BE SPECIFICALLY FORMULATED FOR BONDING MASONRY TO MASONRY.
 - CERTIFICATION BY PROFESSIONAL ENGINEER THAT THE CONSTRUCTION LAYOUT MEETS THE REQUIREMENTS OF PLANS AND Mn/DOT MSEW STANDARDS. DEVIATION FROM STANDARD DESIGN TABLES ARE PERMITTED BY VALUE ENGINEERING SUBMITTAL ONLY ON PROJECTS WITH OVER 5000 SQ. FT. OF WALL.

DEFINITION OF TERMS	
MBW	MODULAR BLOCK WALL
LL	LIVE LOAD
C.I.P.	CAST-IN-PLACE
H	WALL HEIGHT
S	VERTICAL REINFORCEMENT SPACING
REINFORCEMENT COVERAGE RATIO	WIDTH OF SOIL REINFORCEMENTS TO HORIZONTAL SPACING (100% COVERAGE RATIO REQUIRED)

DESIGN CRITERIA

DESIGN CRITERIA FOLLOWS THE AASHTO SPECIFICATION FOR HIGHWAY BRIDGES (16TH EDITION WITH 1998 INTERIMS) EXCEPT FOR THE DEVIATIONS NOTED BELOW. DESIGN CRITERIA ARE IN ACCORDANCE WITH Mn/DOT POLICY, AS RECORDED IN THE Mn/DOT ROAD DESIGN MANUAL.

- THE MINIMUM REINFORCEMENT LENGTH IS 4 FT. OR 0.7H, WHICHEVER IS GREATER.
- THE REINFORCEMENT FILL FRICTION ANGLE IS 35°.
- THE ALLOWABLE CONNECTION LOAD, AT A GIVEN NORMAL LOAD, IS COMPUTED AS THE ULTIMATE CONNECTION STRENGTH REDUCED BY A SAFETY FACTOR EQUAL TO 2.0.
- THE LATERAL EARTH PRESSURE COMPUTATION FOR EXTERNAL STABILITY CALCULATIONS USES AN INTERFACE ANGLE SET EQUAL TO THE RETAINED BACKFILL ANGLE.
- THE LATERAL EARTH PRESSURE COMPUTATION FOR INTERNAL STABILITY CALCULATIONS INCORPORATES THE EFFECTS OF WALL FACE BATTER.

MINIMUM FACTORS OF SAFETY:
 OVERTURNING: 2.0
 SLIDING: 1.5
 ECCENTRICITY: $e < L/6$
 BEARING CAPACITY: 2.5
 DEEP SEATED STABILITY: 1.3

BEARING:

- SEE FOUNDATION REPORT FOR ALLOWABLE SOIL BEARING PRESSURE.
- CASES 1 AND 4 - ALLOWABLE SOIL BEARING CAPACITY (ULTIMATE BEARING CAPACITY REDUCED BY A SAFETY FACTOR OF 2.5) OF 2000 PSF IS REQUIRED FOR WALLS UP TO 10 FT. IN HEIGHT. FOR WALLS GREATER THAN 10 FT. IN HEIGHT, THE REQUIRED ALLOWABLE BEARING CAPACITY IS EQUAL TO: $2000 \text{ PSF} + (H-10)(625 \text{ PSF})$ WITH H IN FEET.
- CASE 3 - ALLOWABLE SOIL BEARING CAPACITY (ULTIMATE BEARING CAPACITY REDUCED BY A SAFETY FACTOR OF 2.5) OF 2500 PSF IS REQUIRED FOR WALLS UP TO 10 FT. IN HEIGHT. FOR WALLS GREATER THAN 10 FT. IN HEIGHT, THE REQUIRED ALLOWABLE BEARING CAPACITY IS EQUAL TO: $2500 \text{ PSF} + (H-10)(850 \text{ PSF})$ WITH H IN FEET.

REINFORCED WALL FILL CHARACTERISTICS:

- SELECT GRANULAR MATERIAL MODIFIED FOLLOWING SPEC. 3149.2B.2. MODIFICATION: SELECT GRANULAR MATERIAL MODIFIED, FOR SPECIAL USE IN EMBANKMENT OR BACKFILL CONSTRUCTION OR OTHER SPECIFIED PURPOSES, MAY BE ANY PIT-RUN OR CRUSHER-RUN MATERIAL THAT IS GRADED FROM COARSE TO FINE, SUCH THAT 100% OF THE MATERIAL MUST PASS THE 2" SIEVE, AND THAT THE RATIO OF THE PORTION PASSING THE #200 SIEVE DIVIDED BY THE PORTION PASSING THE 1" SIEVE MAY NOT EXCEED 10% BY MASS (THAT IS: #200/1" RATIO)
- INTERNAL ANGLE OF FRICTION (Φ_p) = 35°
- COHESION (C) = 0
- MOIST UNIT WEIGHT (γ_p) = 125 PSF

COARSE FILTER AGGREGATE CHARACTERISTICS:

- COARSE FILTER AGGREGATE TO MEET SPEC. 3149.2H. INCIDENTAL, NO DIRECT PAYMENT WILL BE MADE.

RETAINED BACKFILL CHARACTERISTICS:

- INTERNAL ANGLE OF FRICTION (Φ_b) = 30°
- COHESION (C) = 0
- MOIST UNIT WEIGHT (γ_b) = 120 PSF

FOUNDATION SOILS CHARACTERISTICS:

- INTERNAL ANGLE OF FRICTION (Φ_f) = 30°
- COHESION (C) = 0
- UNIT WEIGHT (γ_f) = 120 PSF

SAMPLE ESTIMATED QUANTITIES FOR MODULAR BLOCK WALLS		
	UNIT	QUANTITY
STRUCTURE EXCAVATION CLASS ---	CU. YD.	---
SELECT GRANULAR MATERIAL MODIFIED (CV)	CU. YD.	---
STRUCTURAL CONCRETE (1A43)	CU. YD.	---
MODULAR BLOCK RETAINING WALL	SQ. FT.	---
TYPE I GEOTEXTILE FABRIC	SQ. YD.	---

- VERTICAL FACE AREA OF MODULAR BLOCK AS MEASURED FROM PLAN TOP OF WALL TO 2 FT. BELOW FINISHED GRADE AT BOTTOM OF WALL.
- PAY ITEM FOR MBW WALLS SHALL BE 2411.
- REFER TO TABULATIONS / ESTIMATE SHEETS FOR QUANTITIES.

NOTES TO DESIGNER:

HEIGHT AND LOCATION RESTRICTIONS FOR ISSUES SUCH AS FREEZE-THAW DURABILITY ARE GOVERNED BY APPROPRIATE TECHNICAL MEMORANDUMS. CURRENT GOVERNING TECH. MEMO. NO.: 14-03-MAT-01.

IN ADDITION TO THE STANDARD SHEETS, PLAN AND FRONT ELEVATION VIEWS OF THE MODULAR BLOCK RETAINING WALLS SHALL BE INCLUDED IN THE PLANS. THE PLAN VIEW MUST SHOW ALIGNMENT BASELINE, LIMITS OF BOTTOM OF WALL ALIGNMENT, AND LIMITS OF TOP OF WALL ALIGNMENT AS ALIGNMENTS VARY WITH BATTER OF WALL SYSTEM ACTUALLY SUPPLIED. THE FRONT ELEVATION MUST IDENTIFY BOTTOM AND TOP OF WALL ELEVATIONS, EXISTING GRADES, AND FINISHED GRADES.

IF THE WALL IS CURVED, THE RADIUS AT THE BOTTOM AND THE TOP OF EACH WALL SEGMENT AND THE P.C. AND P.T. STATION POINTS OFF OF BASELINE AND LIMITS OF BOTTOM AND TOP OF WALL ALIGNMENT MUST BE SHOWN.

REFERENCE STANDARD PLATES AND PROVIDE DETAILS FOR TRAFFIC BARRIERS, CURB AND GUTTER, HANDRAILS AND FENCING AS REQUIRED BY PROJECT CONDITIONS. SEE AASHTO AND Mn/DOT DESIGN MANUALS, STANDARD PLATES AND DETAILS FOR REQUIREMENTS.

SURFACE DRAINAGE PATTERNS SHALL BE SHOWN IN THE PLAN VIEW. PROVIDE DIMENSIONS FOR WIDTH AND DEPTH OF THE DRAINAGE SWALE AS WELL AS THE TYPE OF IMPERVIOUS LINER MATERIAL. SURFACE WATER RUNOFF SHOULD BE COLLECTED ABOVE AND DIVERTED AROUND WALL FACE.

DETAIL LINES AND GRADES OF THE INTERNAL DRAINAGE COLLECTION PIPE. DETAIL OR NOTE THE DESTINATION OF INTERNAL WALL DRAINS AS WELL AS THE METHOD OF TERMINATION (DAYLIGHT END OF PIPE OR CONNECTION INTO HYDRAULIC STRUCTURE). THE SPACING FOR DRAIN PIPE OUTLET SHALL NOT BE MORE THAN 250 FT.

SOFT SOILS AND/OR HIGH WATER CONDITIONS (DEFINED AS GROUNDWATER WITHIN A DEPTH EQUAL TO THE WALL HEIGHT H) MAY NOT BE SUITABLE FOR APPLICATION OF STANDARD DESIGNS AND REQUIRE SPECIAL CONSIDERATION BY THE FOUNDATIONS UNIT.

STANDARD DESIGN CHARTS ARE NOT APPLICABLE TO:

- PROJECT/SITES WHERE FOUNDATION SOILS SHEAR STRENGTH AND/OR BEARING CAPACITY DO NOT MEET OR EXCEED VALUES USED IN THE DEVELOPMENT OF STANDARD DESIGN CHARTS.
- PROJECTS WITH A LARGE QUANTITY OF FACE AREA WHERE PROJECT SPECIFIC DESIGNS ARE RECOMMENDED, AS DEFINED IN Mn/DOT ROAD DESIGN MANUAL.
- WHERE SLOPES IN FRONT OF WALL ARE STEEPER THAN 1:3.
- WHERE MAXIMUM WALL HEIGHT EXCEEDS 12 FT.
- WHERE WALLS ARE TIERED.
- WALLS WITH NOISE WALLS.

IF USING CONCRETE RAILING, INCLUDE STANDARD BRIDGE DETAIL "CONCRETE RAILING (TYPE F)" IN PLAN SET.

PROVIDE PROJECT SPECIFIC AESTHETIC REQUIREMENTS INCLUDING COLOR AND FASCIA SURFACING IN THE SPECIAL PROVISIONS.

CHAPTER 9 OF THE Mn/DOT "ROAD DESIGN MANUAL" CONTAINS GUIDELINES, TRAFFIC SAFETY AND OTHER ASPECTS.

GENERAL NOTES:

UTILITIES:
 EXISTING AND PROPOSED UTILITIES ARE SHOWN IN THE GRADING PLANS. THE CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING FACILITIES AND SHALL EXERCISE CARE IN ADJACENT CONSTRUCTION.

EXCAVATION AND EARTHWORK:
 ALL EXCAVATION AND EMBANKMENT WORK SHALL CONFORM TO Mn/DOT 2451.

CAST-IN-PLACE CONCRETE:
 ALL CONCRETE SHALL CONFORM TO Mn/DOT 2461, EXCEPT AS NOTED.

CONSTRUCTION:
 CONSTRUCTION SHALL BE IN ACCORDANCE WITH Mn/DOT 2411, EXCEPT AS NOTED.

GEOMETRICS AND GRADES:
 DATA FOR BASELINE GEOMETRY IS TABULATED FOR WALL ALIGNMENT, SEE LAYOUT SHEETS. WALL ALIGNMENT REFERENCE IS ALONG FRONT FACE OF WALL.

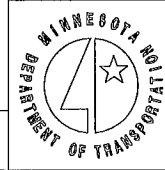
THE FILL SLOPE CONVENTION OF 1 VERTICAL TO HORIZONTAL IS USED IN THIS PLAN.

COMPACTION REQUIREMENTS:
 COMPACT REINFORCED WALL FILL IN ACCORDANCE WITH Mn/DOT SPEC. 2105.3F1 UNLESS RECOMMENDED OTHERWISE BY THE SOILS ENGINEER.

REVISION:
 APPROVED: DECEMBER 1, 2014

 DIRECTOR, OFFICE OF MATERIALS AND ROAD RESEARCH

SP 002-611-034
 SP 114-020-050
 CP 13-10



REVISOR:

 APPROVED: 12-1-2014
 STATE DESIGN ENGINEER

MODULAR BLOCK RETAINING WALL
 GENERAL NOTES
 STANDARD PLAN 5-297.640 114 OF 298

MODULAR BLOCK WALL REINFORCEMENT LAYOUT

CASE 1 - LEVEL BACKFILL WITH 250 PSF SURCHARGE

MBW REINFORCEMENT CLASS	STRENGTH OF SOIL REINF. (PLF)		① MINIMUM REINFORCEMENT LENGTH, L (FT.)	MAXIMUM WALL HEIGHT (FT.)	② NOMINAL BLOCK WIDTH (IN.)	WALL BATTER RANGE (DEGREES)		③ MAXIMUM UNREINFORCED WALL HT, A (IN.)	ZONE 1		ZONE 2		ZONE 3	
	LG. TERM (T _d)	DESIGN (T _d)				<	>		H1 (FT.)	S1 _{MAX} (IN.)	H2 (FT.)	S2 _{MAX} (IN.)	H3 (FT.)	S3 _{MAX} (IN.)
MBW-700	1050	700	0.7 H	12.0	12	0	3	15	7.9	24	4.1	16		
						3	7	16	9.8	24	2.2	16		
						7	10	18	11.5	24	0.5	16		
						10	15	18	12.0	24				
						0	3	32	4.9	32	3.0	24	4.1	16
						3	7	32	4.9	32	4.9	24	2.2	16
						7	10	32	5.9	32	6.1	24		
						10	15	32	7.2	32	4.8	24		
MBW-1050	1575	1050	0.7 H	12.0	12	0	3	15	12.0	24				
						3	7	16	12.0	24				
						7	10	18	12.0	24				
						10	15	18	12.0	24				
						0	3	36	5.9	42	4.9	32	1.2	24
						3	7	40	8.5	42	3.5	32		
						7	10	42	9.8	42	2.2	32		
						10	15	42	9.8	42	2.2	32		
MBW-1400	2100	1400	0.7 H	12.0	12	0	3	15	12.0	24				
						3	7	16	12.0	24				
						7	10	18	12.0	24				
						10	15	18	12.0	24				
						0	3	36	6.6	48	3.3	42	2.1	32
						3	7	40	8.2	48	3.8	42		
					7	10	48	9.8	48	2.2	42			
					10	15	48	9.8	48	2.2	42			

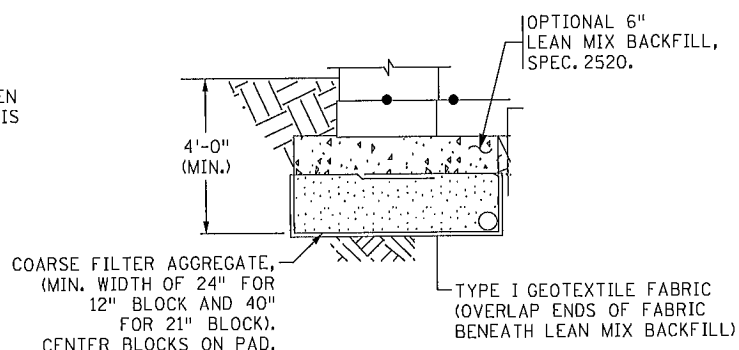
INSTRUCTIONS TO CONTRACTOR:

USE AS MANY ZONES AS WALL HEIGHT REQUIRES, STARTING WITH ZONE 1 AND ADDING ADDITIONAL ZONES TO THE BOTTOM OF THE WALL AS NEEDED TO MAKE UP THE TOTAL WALL HEIGHT (H) NEEDED.

REINFORCEMENT CLASS, NOMINAL BLOCK WIDTH AND WALL BATTER ARE GENERALLY THE CONTRACTOR'S OPTION TO SELECT FROM Mn/DOT APPROVED PRODUCTS LISTS LOCATED AT www.mrr.dot.state.mn.us/geotechnical/foundations/foundations.asp.

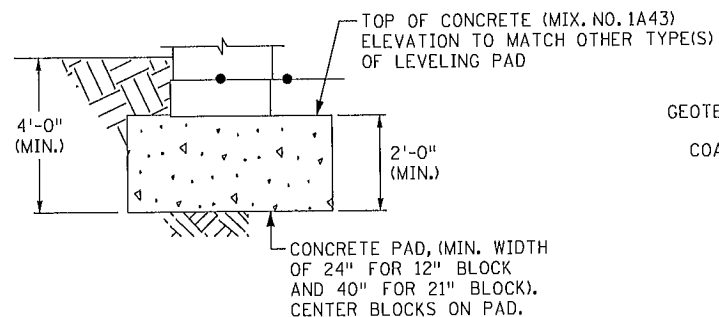
NOTES TO CONTRACTOR:

- OR 4 FT. MINIMUM, WHICHEVER IS GREATER.
- WIDTH - AS MEASURED FROM FRONT TO BACK FACE OF BLOCK UNIT.
- MAXIMUM DISTANCE FROM TOP OF WALL TO FIRST REINFORCEMENT LAYER. UNREINFORCED WALLS ARE NOT INCLUDED IN THIS STANDARD BUT MAY BE CONSTRUCTED UP TO AT LEAST THE HEIGHT GIVEN IN THE TABLE FOR A GIVEN NOMINAL BLOCK WIDTH AND THE SPECIFIED FILL MATERIALS CONTAINED IN THIS STANDARD.
- PAY LIMITS OF STRUCTURAL EXCAVATION. ACTUAL EXCAVATION SLOPE IS DETERMINED BY OSHA REGULATIONS AND IN-SITU SOILS; EXCAVATION BEYOND "LIMITS OF STRUCTURAL EXCAVATION" AT CONTRACTOR'S EXPENSE.
- THE WRAP LENGTH FOR GEOTEXTILE FABRIC SHALL NOT BE MORE THAN 6".
- INSPECT EXCAVATION SLOPES FOR ACTIVE SEEPAGE AND PLACE ADDITIONAL DRAINS WHERE SEEPAGE OCCURS AS DIRECTED BY THE ENGINEER.
- PLACE DRAIN AT BOTTOM OF REINFORCED SOIL IF PIPE CAN BE SLOPED TO OUTLET. DO NOT OUTLET ONTO A SIDEWALK.
- IF PIPE AT THIS ELEVATION CANNOT BE SLOPED TO DRAIN, OMIT DRAIN AND USE "CONCRETE PAD WITHOUT DRAIN" DETAIL.
- 4" THERMOPLASTIC PERFORATED PIPE, SPEC. 3245, WRAP WITH TYPE I GEOTEXTILE, SPEC. 3733 (TYP.) INSTALLATION AS PER SPEC. 2502, WITH PRECAST CONCRETE HEAD WALL AT OUTLET.
- $S_{MAX} = 0.5 S1_{MAX}$ IF THE WALL HEIGHT IS WITHIN ZONE 1.
 $S_{MAX} = 0.5 S2_{MAX}$ IF THE WALL HEIGHT IS WITHIN ZONE 2.
 $S_{MAX} = 0.5 S3_{MAX}$ IF THE WALL HEIGHT IS WITHIN ZONE 3.
- THE REINFORCED WALL FILL DRAIN MAY BE CONNECTED INTO FOOTING DRAIN, INSTEAD OF OUT LETTING THROUGH THE WALL, IF CAPACITY IS ADEQUATE TO TRANSMIT THE FLOW.



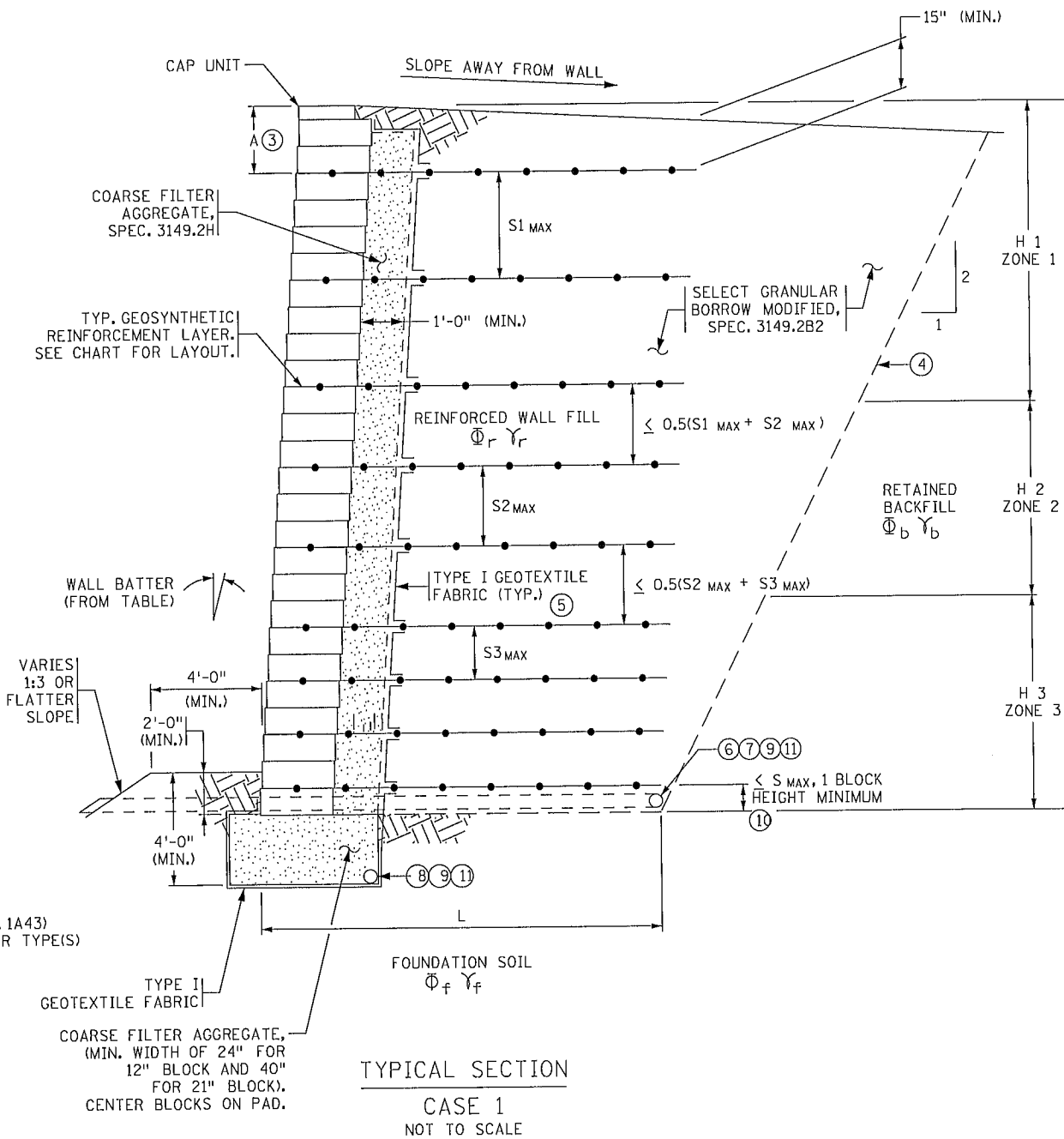
OPTIONAL CONCRETE LEVELING PAD

NOT TO SCALE



CONCRETE PAD WITHOUT DRAIN

NOT TO SCALE



TYPICAL SECTION

CASE 1 NOT TO SCALE

PLOTTED/REVISED: 05/18/2016

DISTRICT #: USER NAME: dffrey PATH & FILENAME: P:\02-611-34\PlanStandard_Plan\5641J_spdn.dgn

FILE NAME: 5641J_spdn.dgn

REVISION:
APPROVED: 8-6-2014
DIRECTOR, OFFICE OF MATERIALS AND ROAD RESEARCH

SP 002-611-034 CP 13-10
SP 114-020-050

REVISOR: APPROVED: 8-6-2014
STATE DESIGN ENGINEER

MODULAR BLOCK RETAINING WALL
SOIL REINFORCEMENT FOR LEVEL FILL, CASE 1
STANDARD PLAN 5-297.641 115 OF 298

PLOTTED/REVISED:
05/13/2016

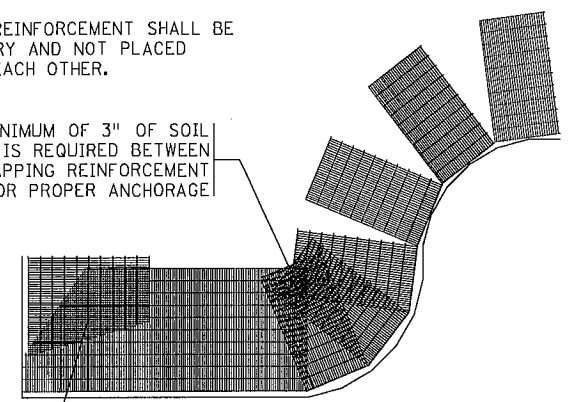
NOTES:

CORRECT ORIENTATION OF GEOSYNTHETIC TO OBTAIN PROPER STRENGTH SHALL BE DETAILED ON CONTRACTOR DRAWINGS.

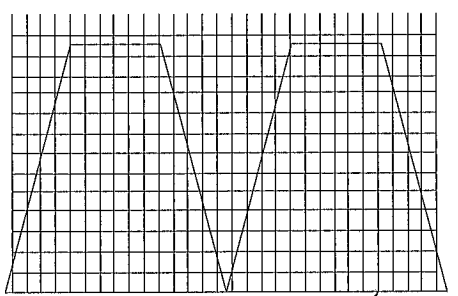
ADJACENT WIDTHS OF REINFORCEMENT SHALL BE EXTENDED AS NECESSARY AND NOT PLACED DIRECTLY ON TOP OF EACH OTHER.

MINIMUM OF 3" OF SOIL FILL IS REQUIRED BETWEEN OVERLAPPING REINFORCEMENT FOR PROPER ANCHORAGE

STAGGER REINFORCEMENT BY ONE BLOCK HEIGHT. REINFORCEMENTS SHALL NOT BE PLACED DIRECTLY ON TOP OF EACH OTHER.

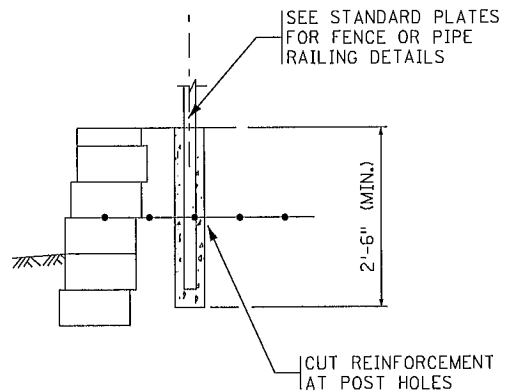


REINFORCEMENT PLACEMENT AROUND CURVES AND CORNERS

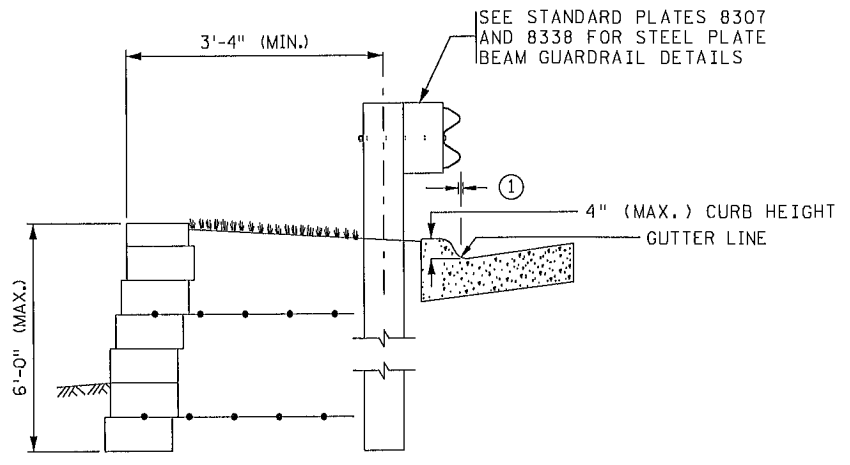


REINFORCEMENT IS TO BE PLACED ON LEVEL BACKFILL AND EXTENDED TO FRONT FACE OF OVERLYING BLOCKS. PLACE NEXT UNIT, PULL REINFORCEMENT TAUT AND BACKFILL AS REQUIRED.

REINFORCEMENT PLACEMENT BETWEEN BLOCK UNITS

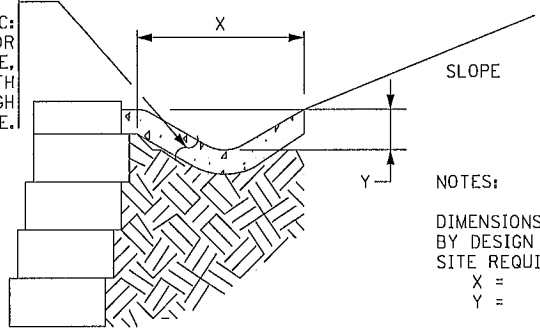


POST DETAIL
TYPICAL HANDRAIL AND/OR FENCE POST



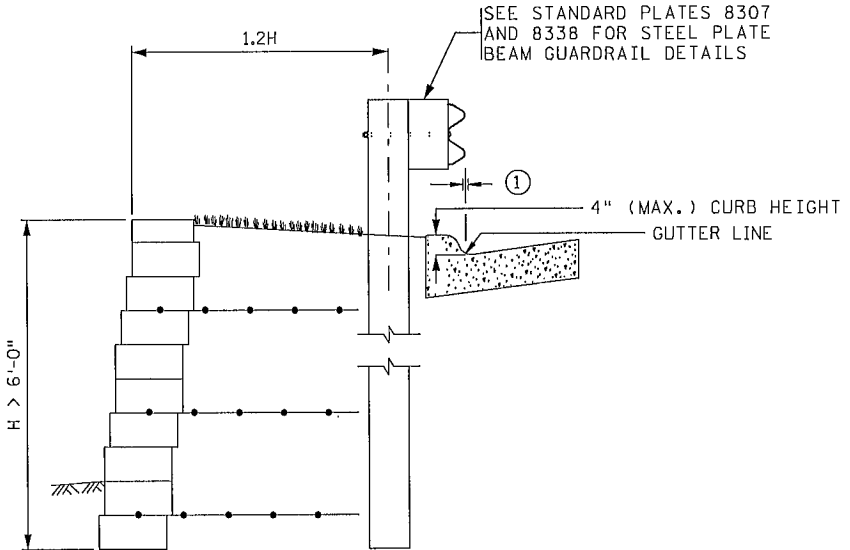
STEEL PLATE BEAM GUARDRAIL DETAIL 1

- OPTION A: 4" CONCRETE
- OPTION B: 6" CLAY OR CLAY LOAM, TOPSOIL AND SOD.
- OPTION C: IMPERVIOUS 20 mil OR THICKER GEOMEMBRANE, TOPSOIL AND SOD WITH NO STAKES THROUGH GEOMEMBRANE.



TYPICAL DRAIN SWALE DETAIL

NOTES:
DIMENSIONS TO BE DETERMINED BY DESIGN ENGINEER BASED ON SITE REQUIREMENTS.
X =
Y =
SEE PLAN VIEW FOR SURFACE DRAINAGE PATTERNS.



STEEL PLATE BEAM GUARDRAIL DETAIL 2

(AADT SHALL BE LESS THAN 5000)
STEEL PLATE BEAM GUARDRAIL SHOWN.

NOTES:

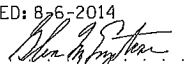
① USE CAUTION WHEN PLACING CURB WITH GUARDRAIL. CURBS ADVERSELY AFFECT THE PERFORMANCE OF THE GUARDRAIL. GENERALLY PLACE CURB DIRECTLY BELOW GUARDRAIL. SEE PLANS OR REFER TO STANDARD PLAN 5-297.601 (2). FOR CURB LOCATIONS ON NCHRP REPORT NO. 350 APPROVED BRIDGE TRANSITIONS, SEE STANDARD PLANS 5-297.603, .605, .606 ETC..

DISTRICT #: USER NAME: dffrey PATH & FILENAME: P:\02-611-34\Plan\Standard_Plan\5645_spn.dgn

FILE NAME: s645_spn.dgn

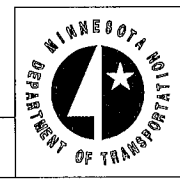
REVISION:

APPROVED: 8-6-2014



DIRECTOR, OFFICE OF MATERIALS AND ROAD RESEARCH

SP 002-611-034 CP 13-10
SP 114-020-050



REVISOR:
APPROVED: 8-6-2014
STATE DESIGN ENGINEER

MODULAR BLOCK RETAINING WALL
DETAILS

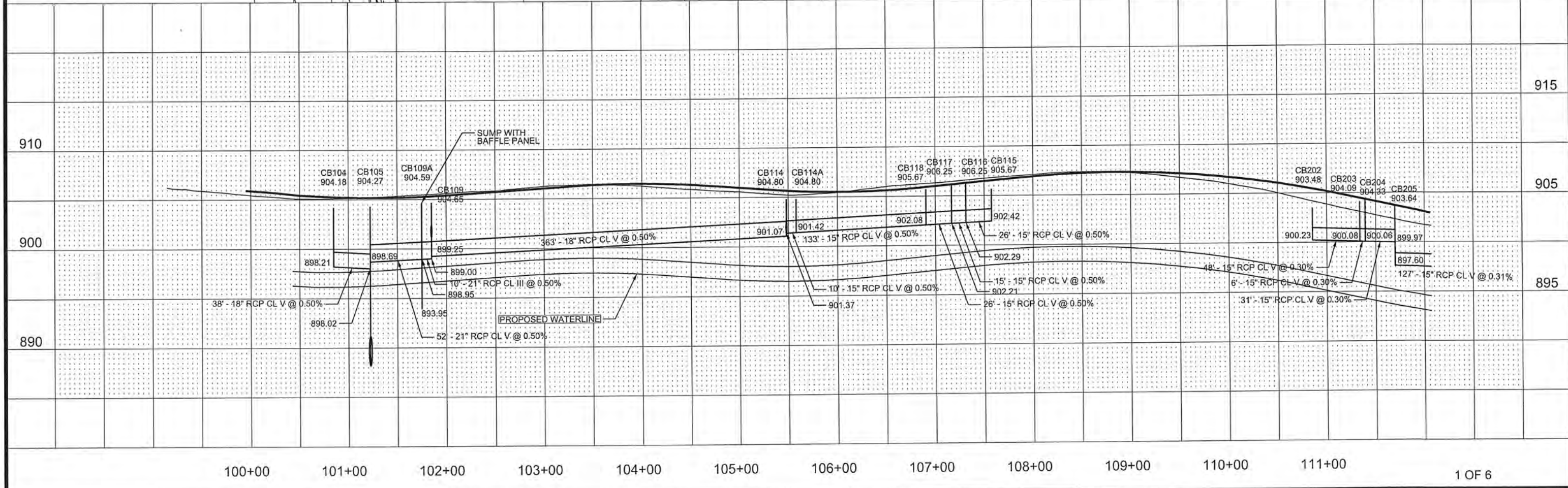
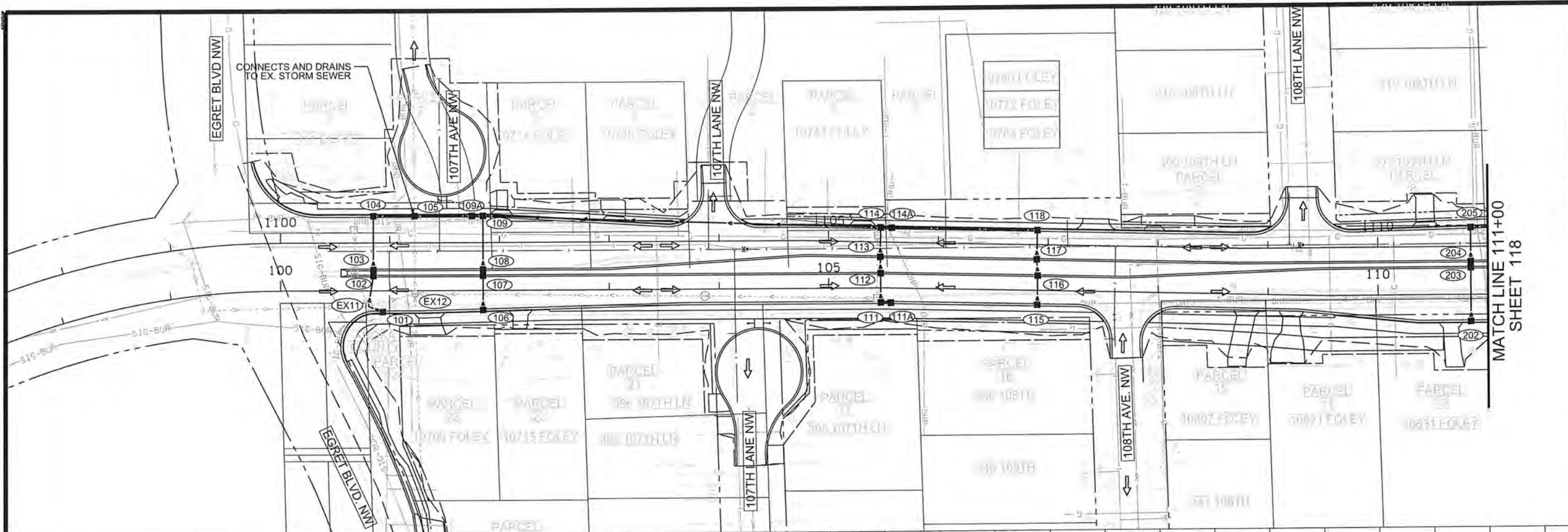
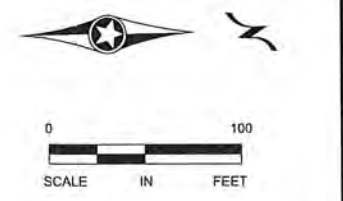
STANDARD PLAN 5-297.645 116 OF 298

LEGEND

- PROPOSED CATCH BASIN
- INPLACE CATCH BASIN
- PROPOSED MANHOLE
- INPLACE MANHOLE
- PROPOSED STORM SEWER
- PROPOSED WATERMAIN
- INPLACE STORM SEWER
- SURFACE FLOW ARROW

NOTE: WATERMAIN IS SHOWN FOR INFORMATIONAL PURPOSES. SEE WATERMAIN PLANS FOR CONSTRUCTION INFORMATION

109A IS A SUMP STRUCTURE WITH BAFFLE PANEL. SEE DETAILS ON SHEET 132.



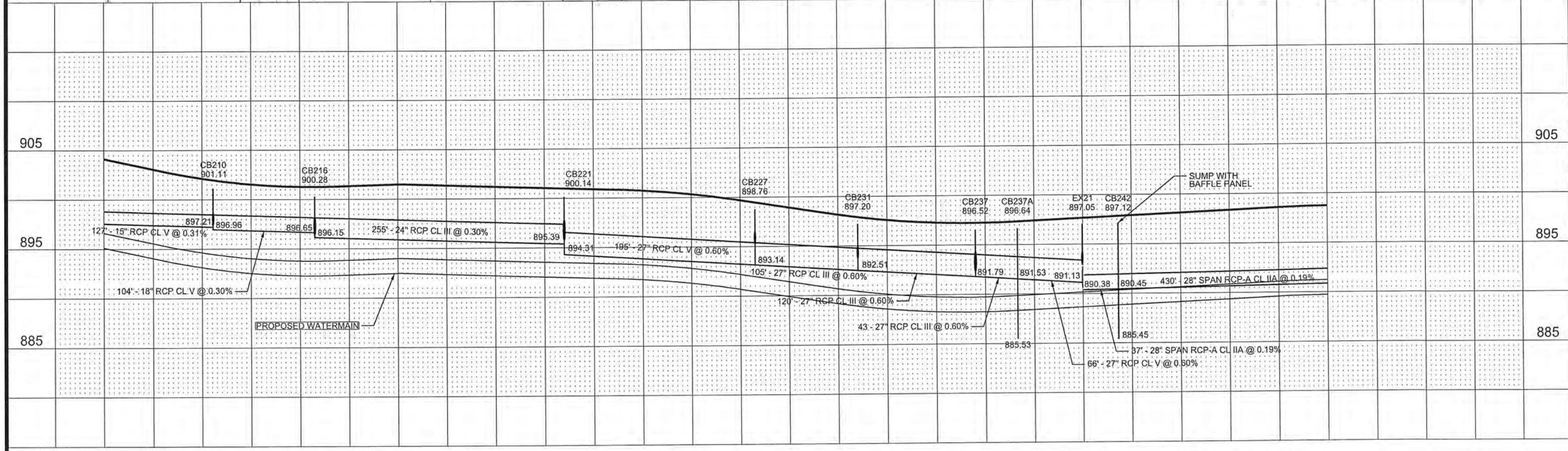
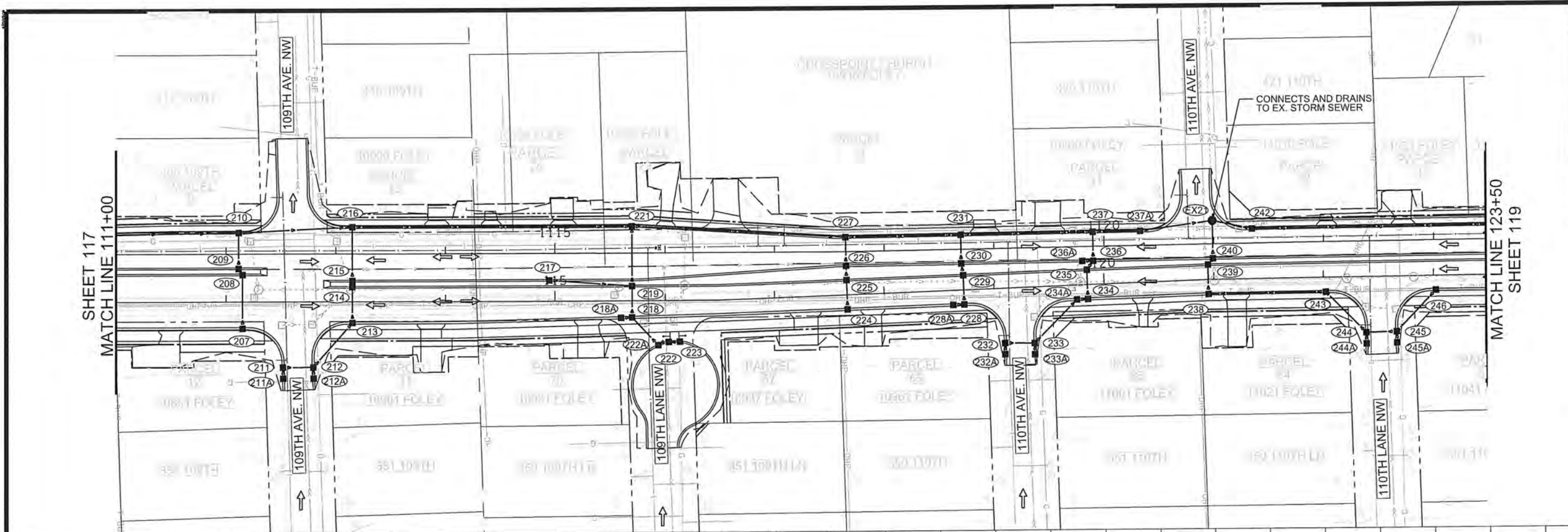
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LEGEND

- PROPOSED CATCH BASIN
- INPLACE CATCH BASIN
- PROPOSED MANHOLE
- INPLACE MANHOLE
- PROPOSED STORM SEWER
- PROPOSED WATERMAIN
- INPLACE STORM SEWER
- SURFACE FLOW ARROW

NOTE: WATERMAIN IS SHOWN FOR INFORMATIONAL PURPOSES. SEE WATERMAIN PLANS FOR CONSTRUCTION INFORMATION

242 IS A SUMP STRUCTURE WITH BAFFLE PANEL. SEE DETAILS ON SHEET 132.



111+00 112+00 113+00 114+00 115+00 116+00 117+00 118+00 119+00 120+00 121+00 122+00 123+00

2 OF 6

NO	DATE	BY	CKD	APPR	REVISION

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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *GINA M. PIZZO*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: EM DATE: 02/27/16

DESIGN BY: EM DATE: 02/27/16

CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

DRAINAGE PLAN & PROFILE

STA 111+00 TO 123+00

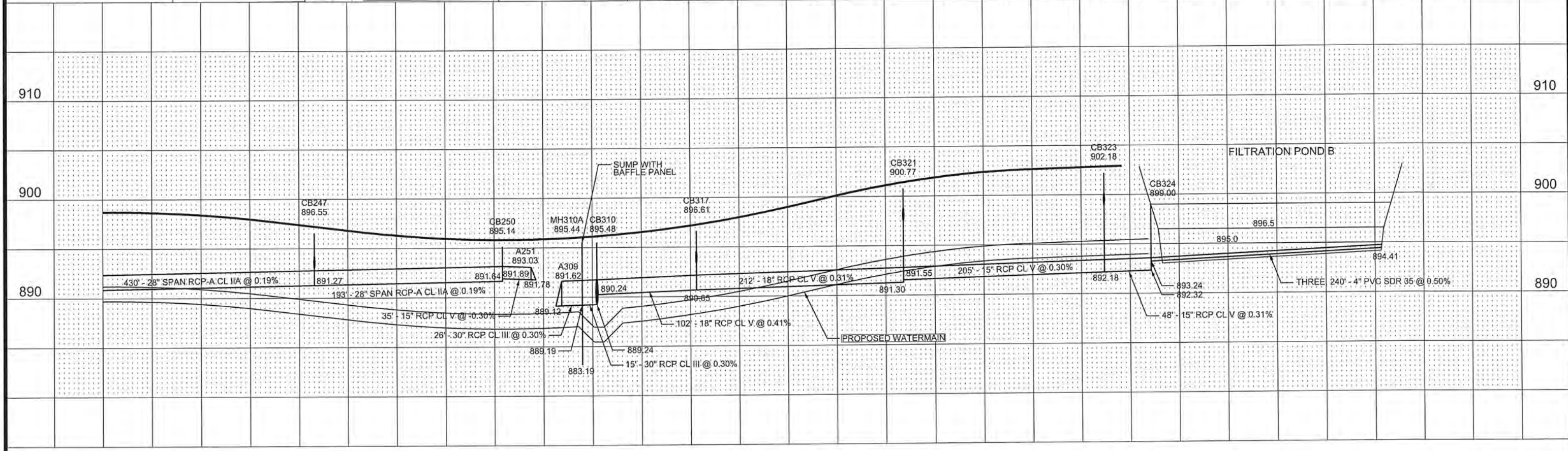
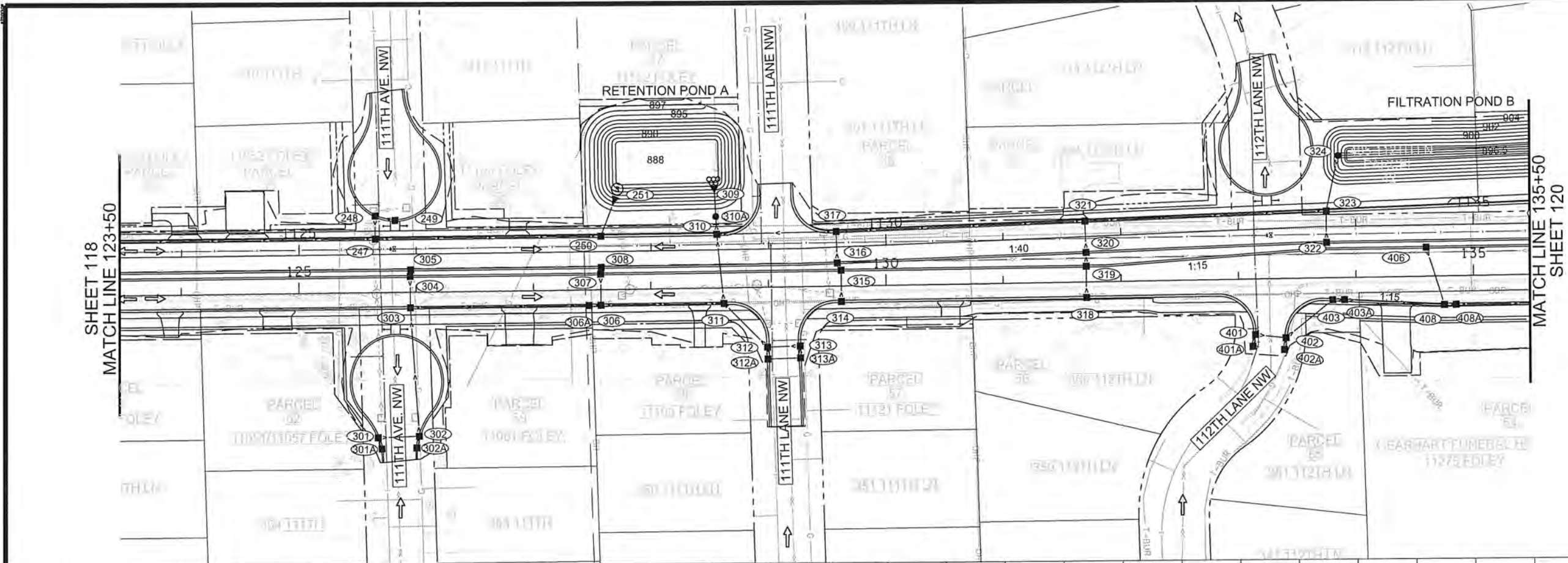
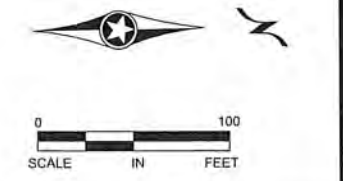
Sheet 118 of 298 Sheets

- LEGEND**
- PROPOSED CATCH BASIN
 - INPLACE CATCH BASIN
 - PROPOSED MANHOLE
 - INPLACE MANHOLE
 - - - PROPOSED STORM SEWER
 - - - PROPOSED WATERMAIN
 - - - INPLACE STORM SEWER
 - SURFACE FLOW ARROW
 - ▲ PROPOSED APRON
 - ⊙ SOD AT APRON INLET

NOTE: WATERMAIN IS SHOWN FOR INFORMATIONAL PURPOSES. SEE WATERMAIN PLANS FOR CONSTRUCTION INFORMATION

SEE SHEET 127 FOR POND GRADING PLAN AND DESIGN REQUIREMENTS

310A IS A SUMP STRUCTURE WITH BAFFLE PANEL. SEE DETAILS ON SHEET 132.



124+00 125+00 126+00 127+00 128+00 129+00 130+00 131+00 132+00 133+00 134+00 135+00

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PRINT NAME: GINA M. PIZZO

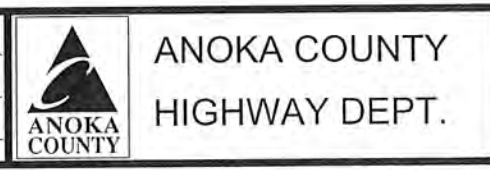
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DATE: 5-19-16 LICENSE NO. 22713

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CHECKED BY: JEO DATE: 02/27/16

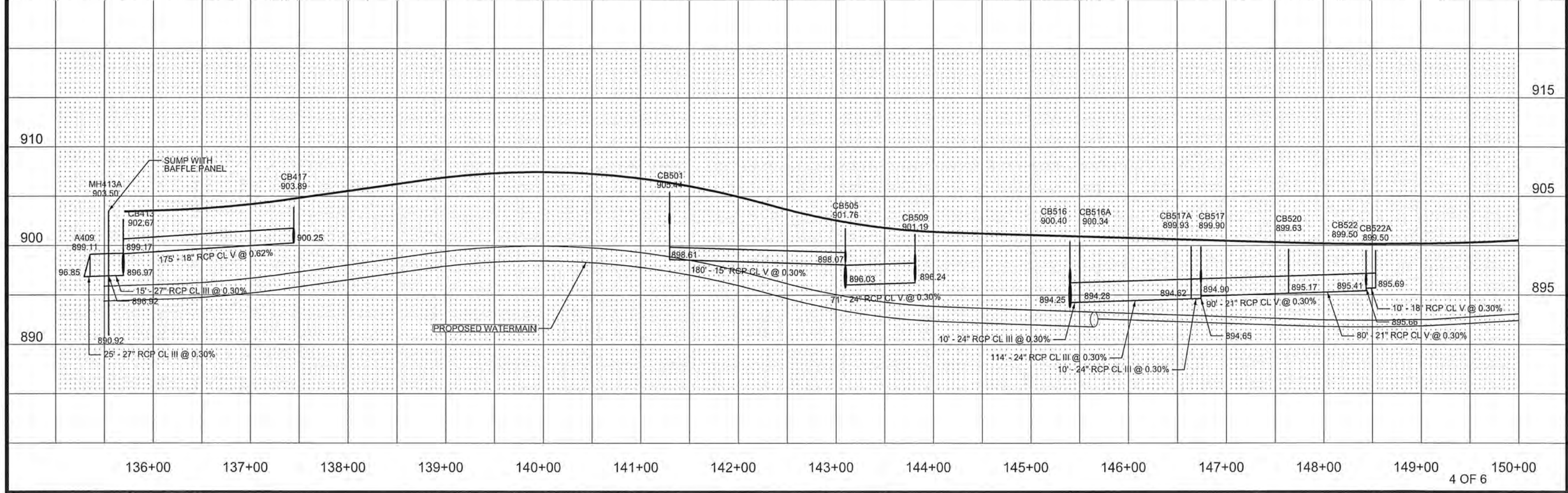
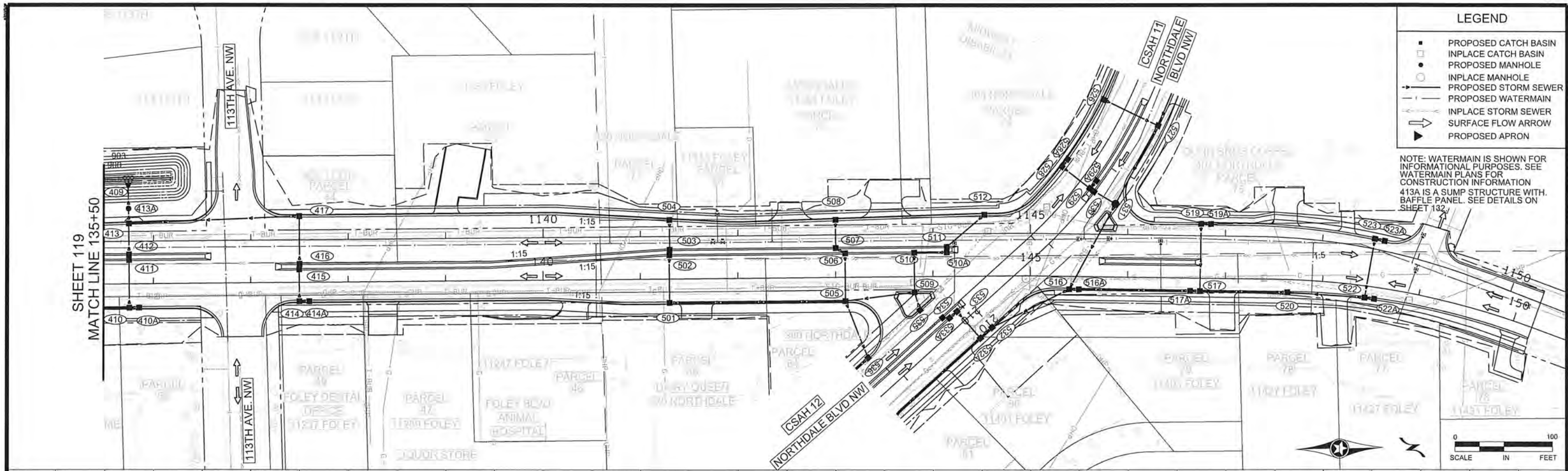


SP 002-611-034
 SP 114-020-050
 CP 13-10

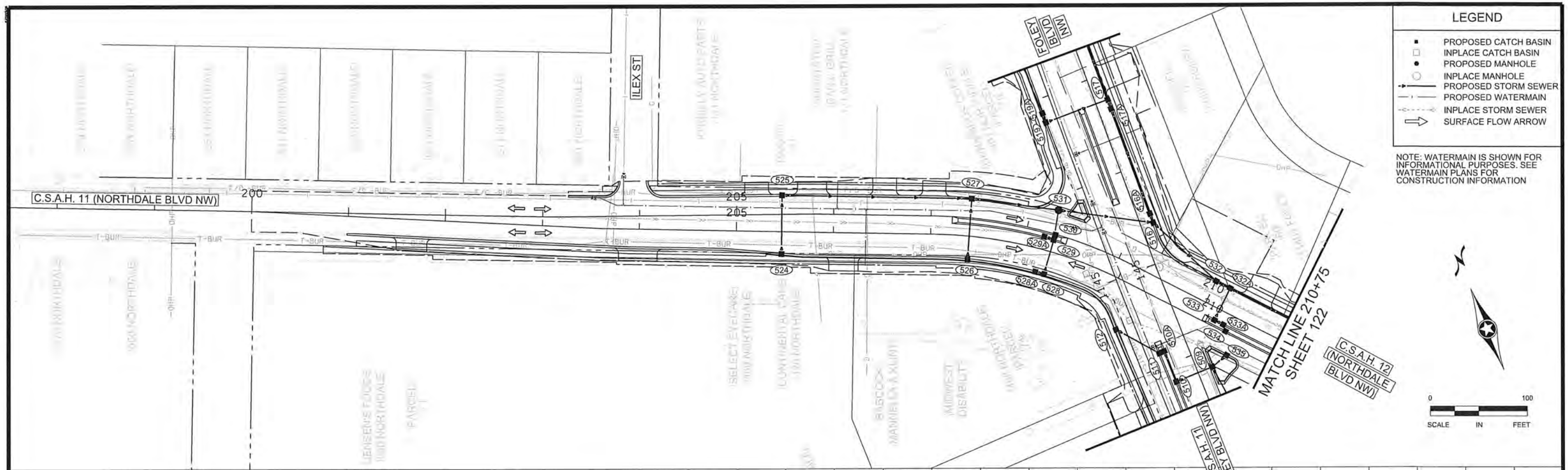
DRAINAGE PLAN & PROFILE

STA 123+50 TO 135+50

Sheet 119 of 298 Sheets



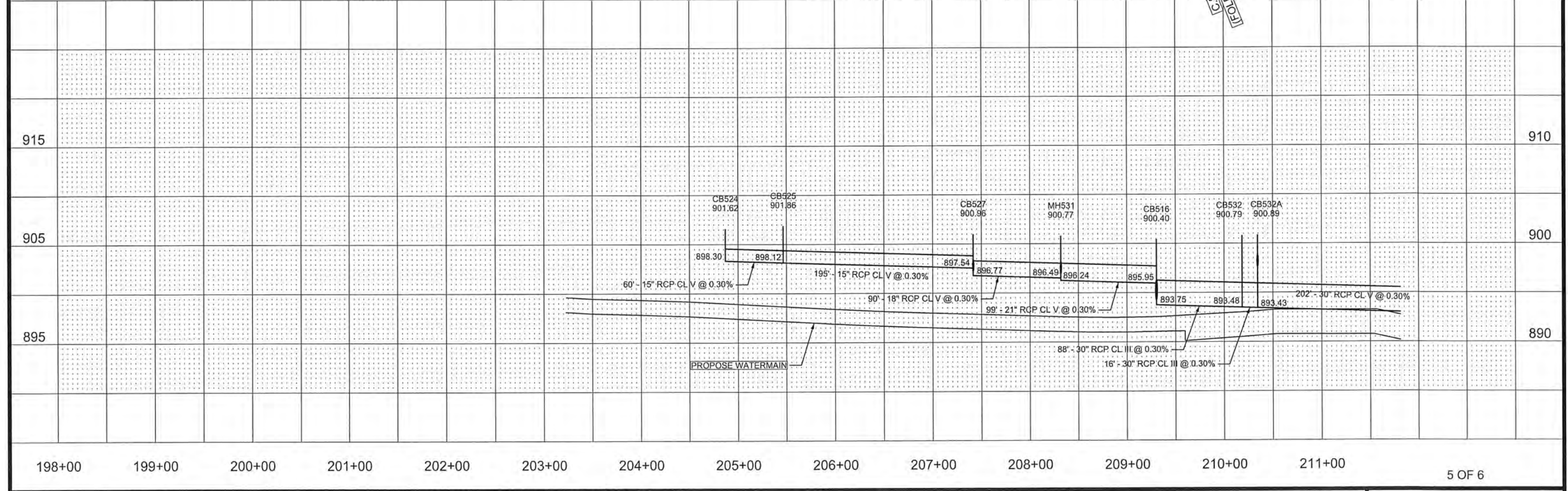
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NO	DATE	BY	CKD	APPR	REVISION											



LEGEND

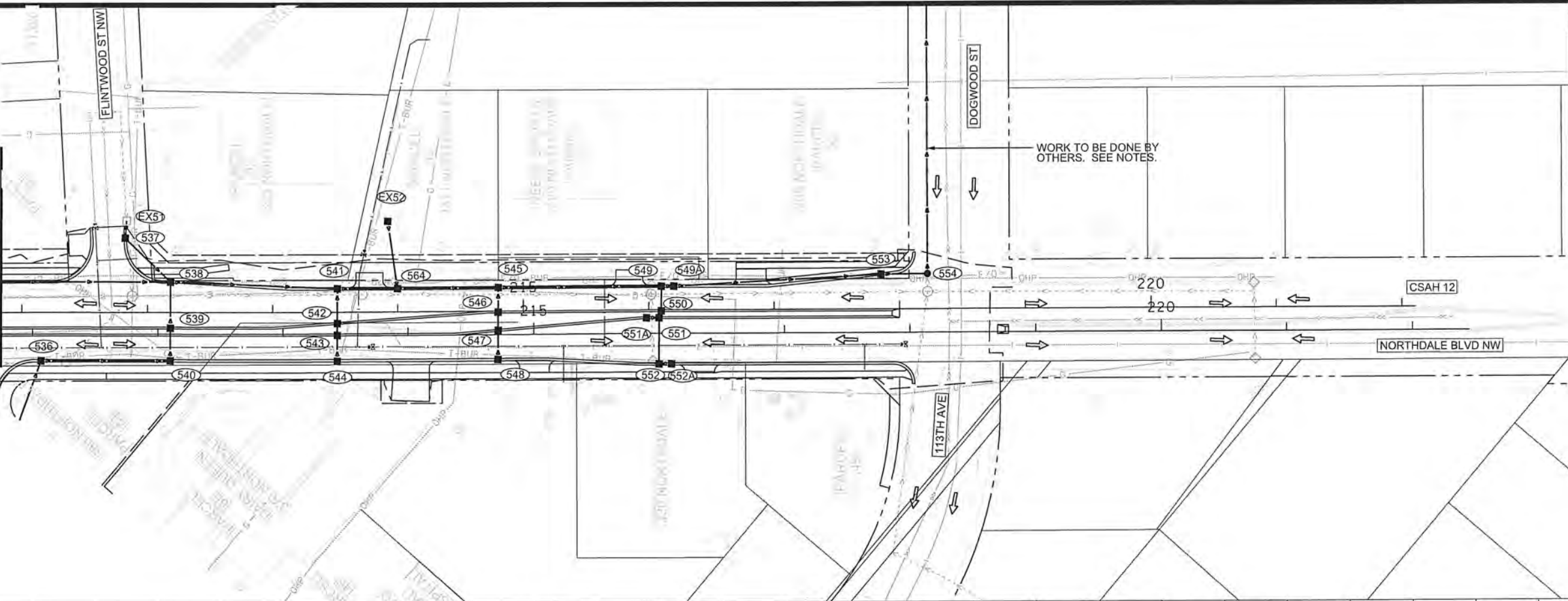
- PROPOSED CATCH BASIN
- INPLACE CATCH BASIN
- PROPOSED MANHOLE
- INPLACE MANHOLE
- PROPOSED STORM SEWER
- - - PROPOSED WATERMAIN
- - - INPLACE STORM SEWER
- SURFACE FLOW ARROW

NOTE: WATERMAIN IS SHOWN FOR INFORMATIONAL PURPOSES. SEE WATERMAIN PLANS FOR CONSTRUCTION INFORMATION



<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>NO</th> <th>DATE</th> <th>BY</th> <th>CKD</th> <th>APPR</th> <th>REVISION</th> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </table> <p>NAME: P:102-611-34\Plan\102-611-034_DR_P5.dgn 05/19/2016 12:46:01 PM</p>	NO	DATE	BY	CKD	APPR	REVISION							<p>I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.</p> <p>PRINT NAME: GINA M. PIZZO</p> <p>SIGNATURE: <i>[Signature]</i></p> <p>DATE: 5-19-16 LICENSE NO. 22713</p>	<p>DRAWN BY: EM DATE: 02/27/16</p> <p>DESIGN BY: EM DATE: 02/27/16</p> <p>CHECKED BY: JEO DATE: 02/27/16</p>	<p style="text-align: center;">ANOKA COUNTY HIGHWAY DEPT.</p> <p>SP 002-611-034 SP 114-020-050 CP 13-10</p>	<p style="text-align: center;">DRAINAGE PLAN & PROFILE</p> <p>STA 196+50 TO 210+75</p> <p>Sheet 121 of 298 Sheets</p>
NO	DATE	BY	CKD	APPR	REVISION											

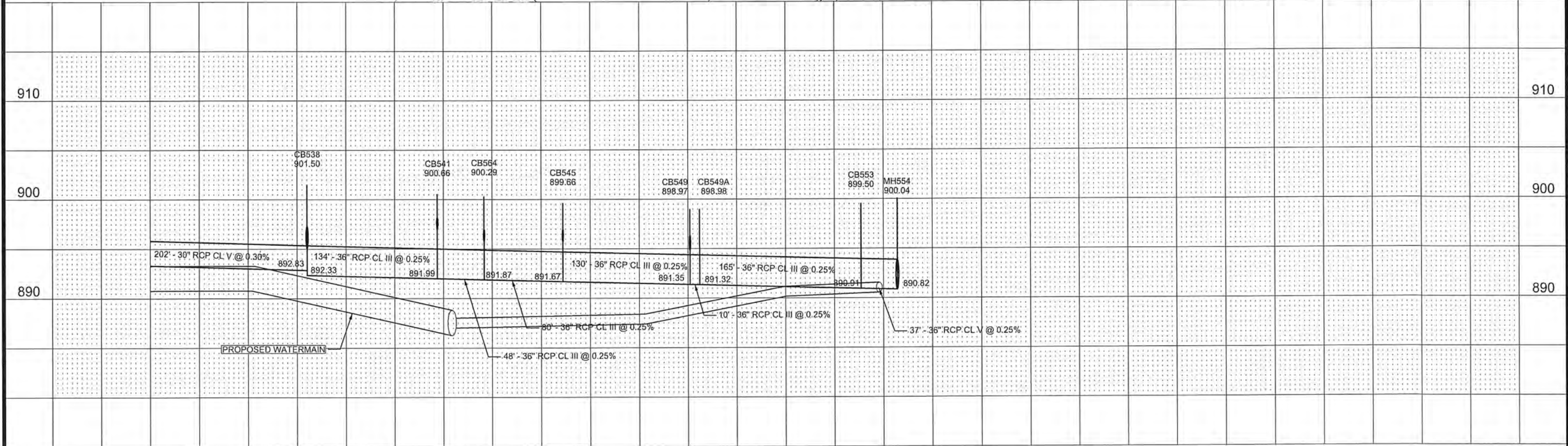
SHEET 121
MATCH LINE 210+75



LEGEND

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- INPLACE CATCH BASIN
- PROPOSED MANHOLE
- INPLACE MANHOLE
- PROPOSED STORM SEWER
- - - PROPOSED WATERMAIN
- - - INPLACE STORM SEWER
- SURFACE FLOW ARROW

NOTES:
WATERMAIN IS SHOWN FOR INFORMATIONAL PURPOSES. SEE WATERMAIN PLANS FOR CONSTRUCTION INFORMATION.
STORM SEWER NETWORK BEYOND MANHOLE 554 SHOWN FOR INFORMATION ONLY. WORK TO BE DONE BY OTHERS.



210+00 211+00 212+00 213+00 214+00 215+00 216+00 217+00 218+00

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_DR_P6.dgn 05/19/2016 12:46:05 PM

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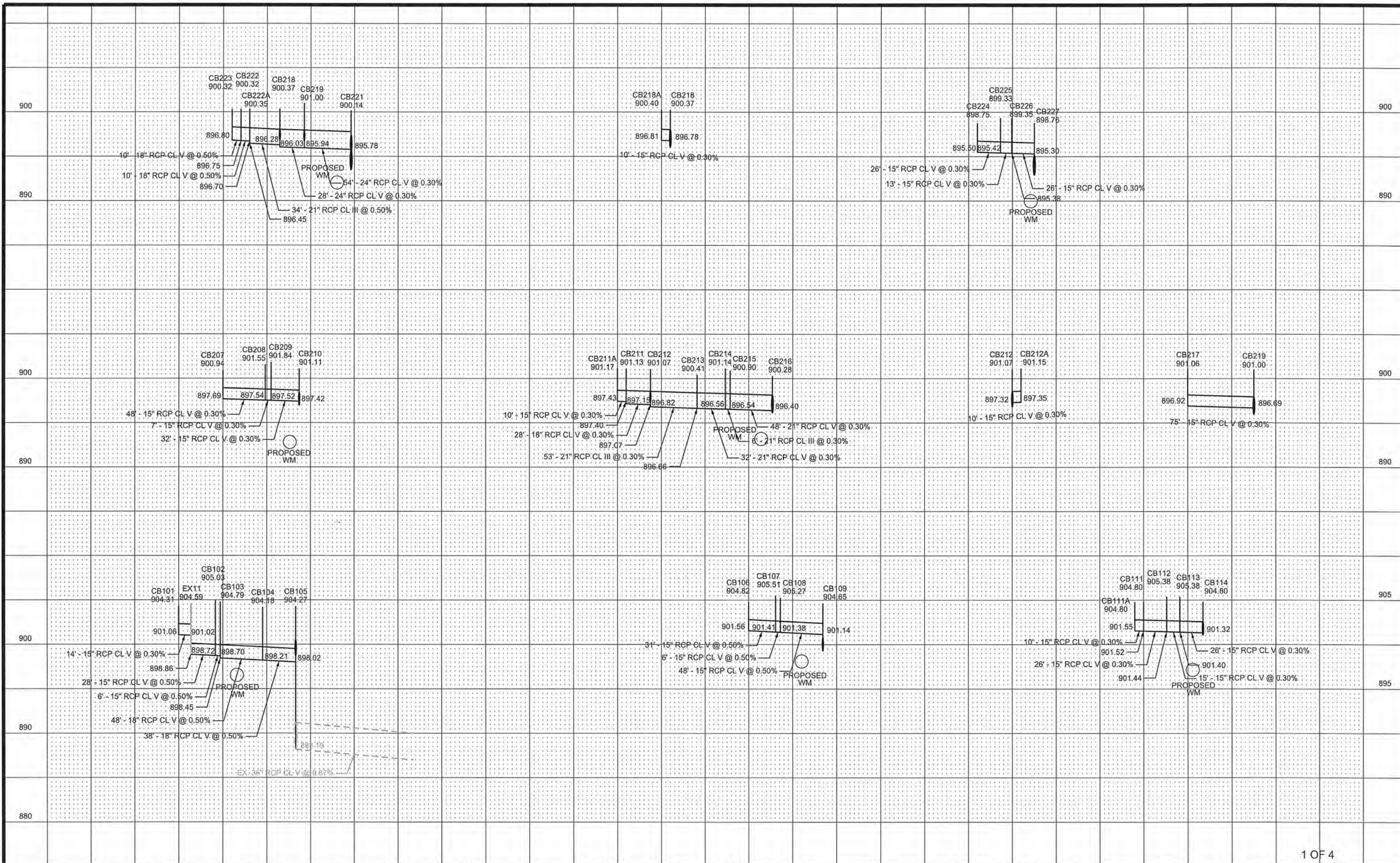
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SIGNATURE: *Gina M. Pizzo*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: EM DATE: 02/27/16
DESIGN BY: EM DATE: 02/27/16
CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

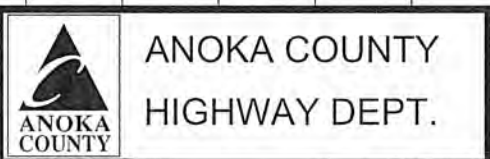
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STA 210+75 TO 218+04.77
Sheet 122 of 298 Sheets



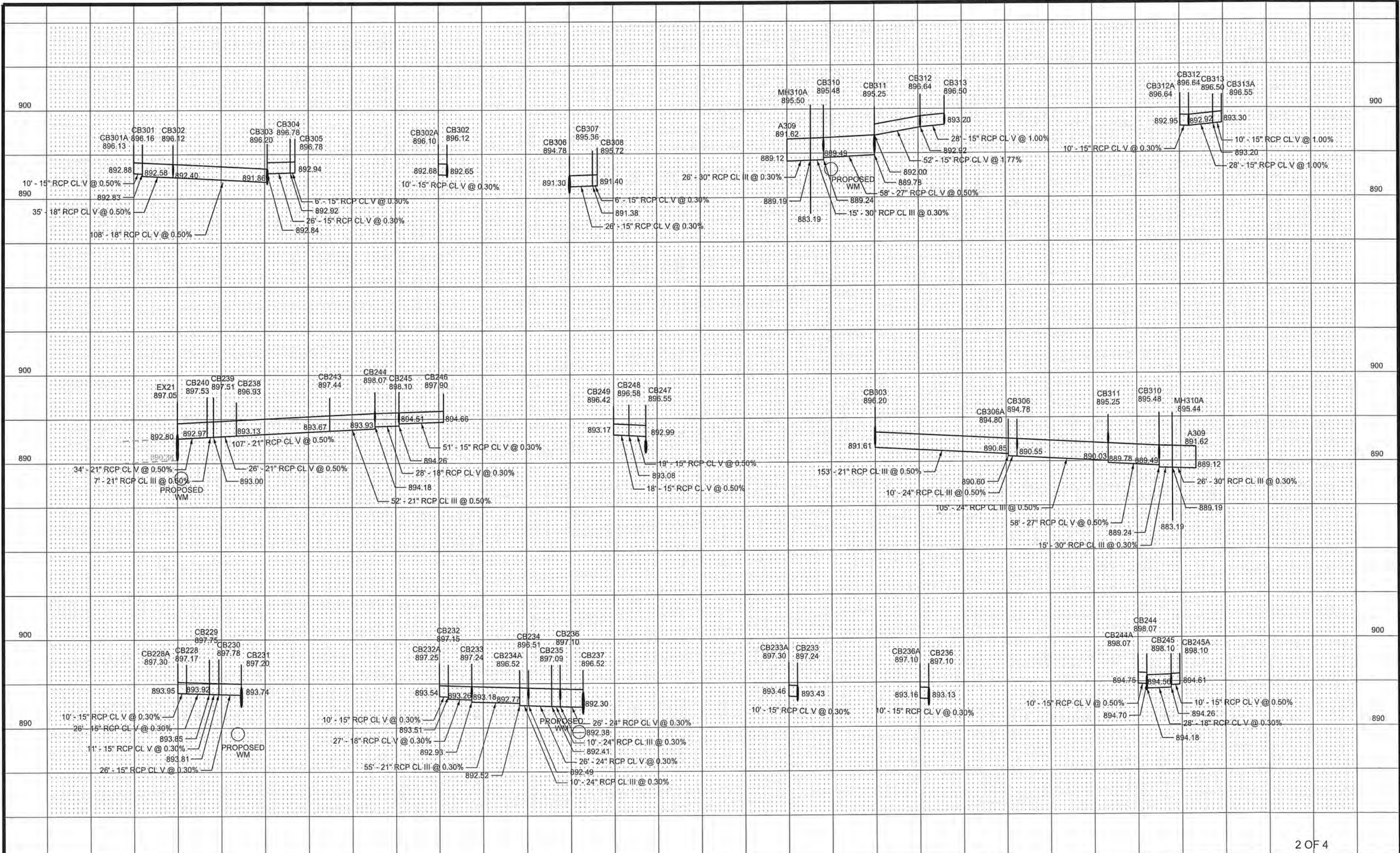
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 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY EM DATE 10/27/14
 DESIGN BY EM DATE 10/27/14
 CHECKED BY JEO DATE 10/27/14



SP 002-611-034
 SP 114-020-050
 CP 13-10



NO	DATE	BY	CKD	APPR	REVISION
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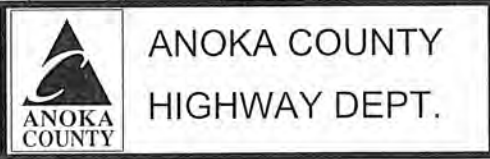
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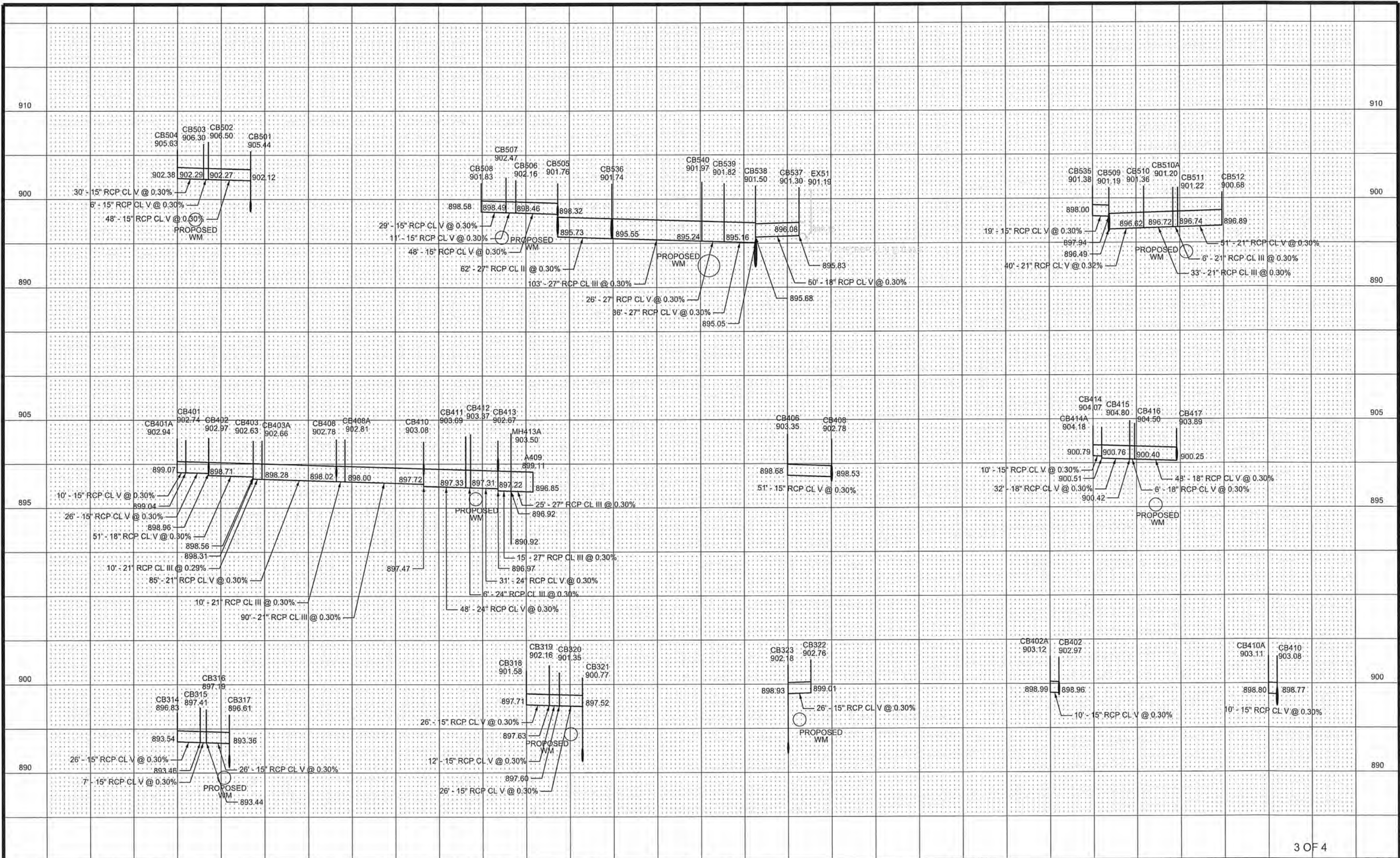
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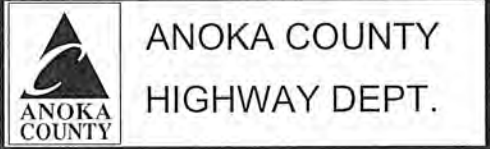
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 CP 13-10



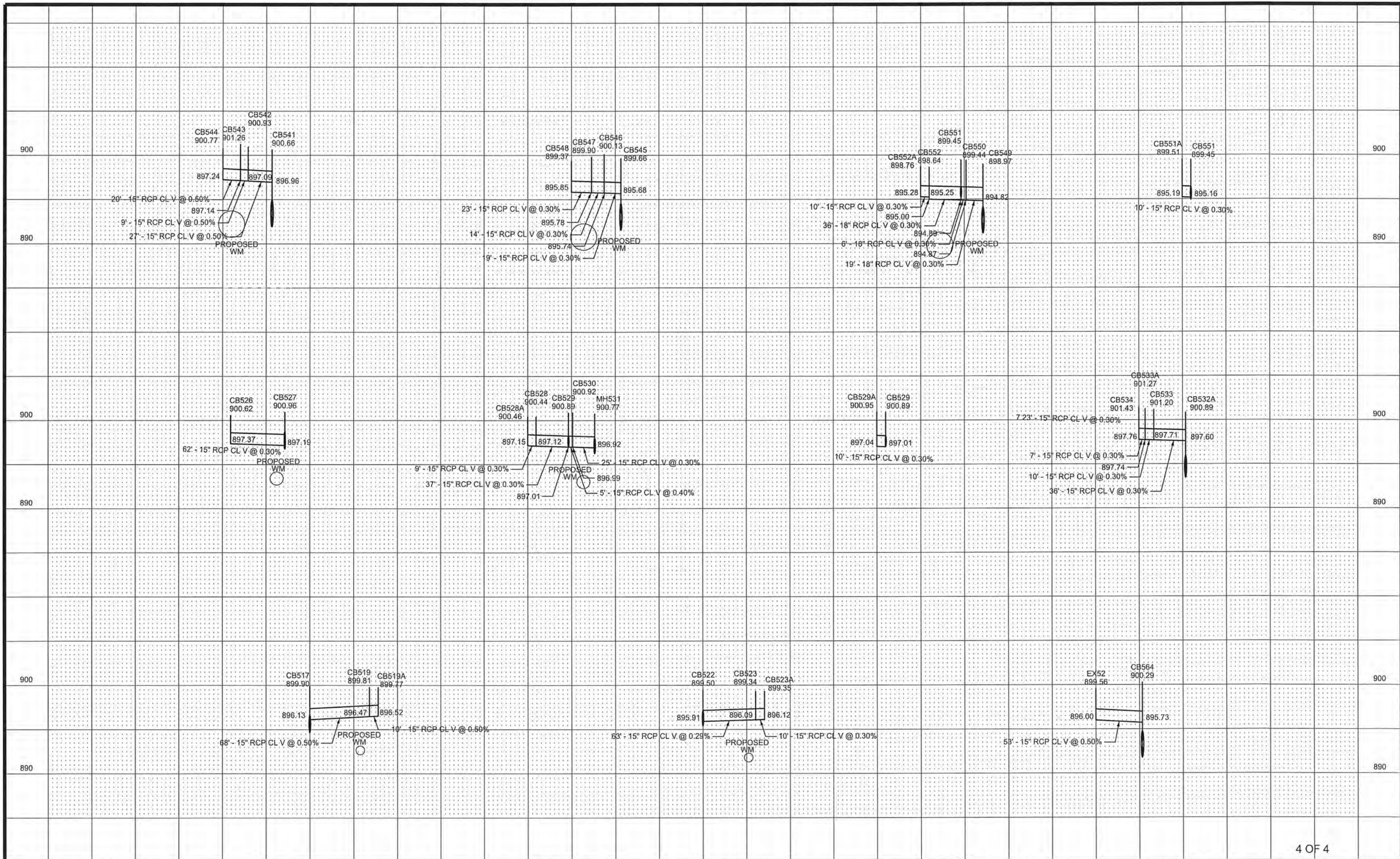
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA
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 SIGNATURE: *GINA M. PIZZO*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY EM DATE 10/27/14
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SP 002-611-034
 SP 114-020-050
 CP 13-10

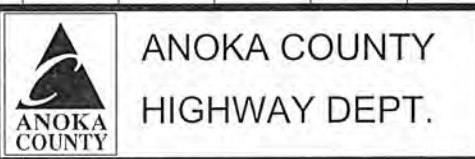


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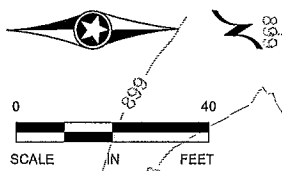
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
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DRAWN BY EM DATE 10/27/14
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 CHECKED BY JEO DATE 10/27/14



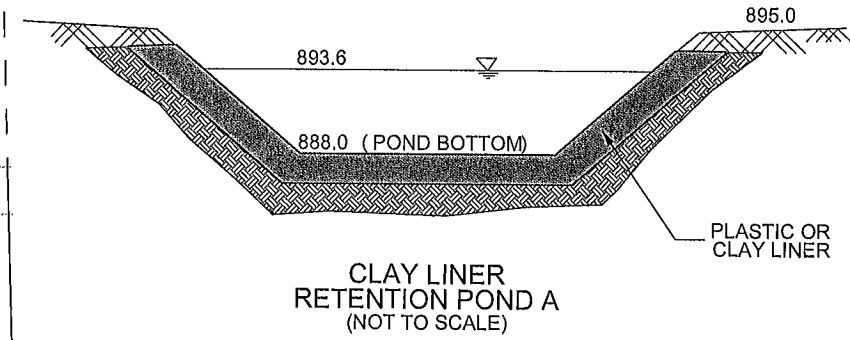
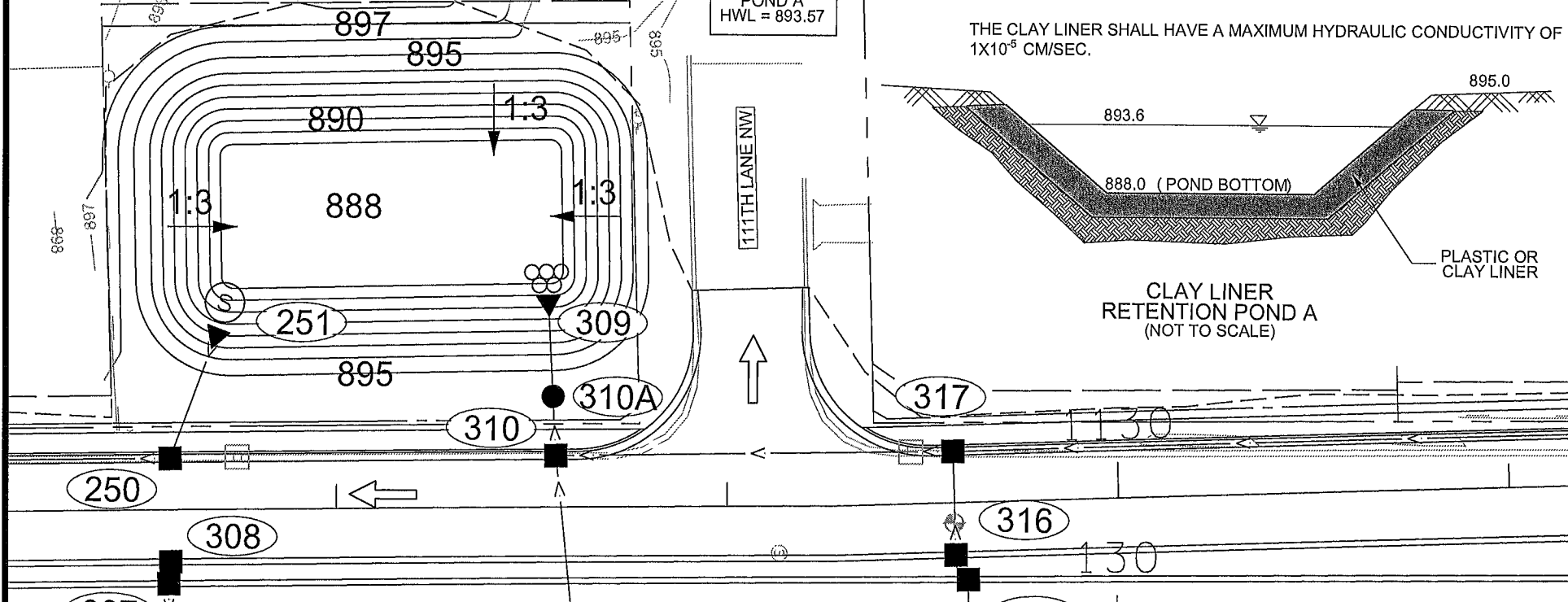
SP 002-611-034
 SP 114-020-050
 CP 13-10



RETENTION POND A

POND A NOTES:
 RETENTION POND A SHALL BE LINED WITH 18 INCH CLAY (597 CU YD) OR PLASTIC LINER TO PREVENT INFILTRATION. THIS WORK IS PAID FOR AS 'COMMON BORROW SPECIAL (CV) (POND A)'.
 POND OUTLETS ARE TO BE CLEANED AT THE CONCLUSION OF CONSTRUCTION.
 THE CLAY LINER SHALL HAVE A MAXIMUM HYDRAULIC CONDUCTIVITY OF 1×10^{-5} CM/SEC.

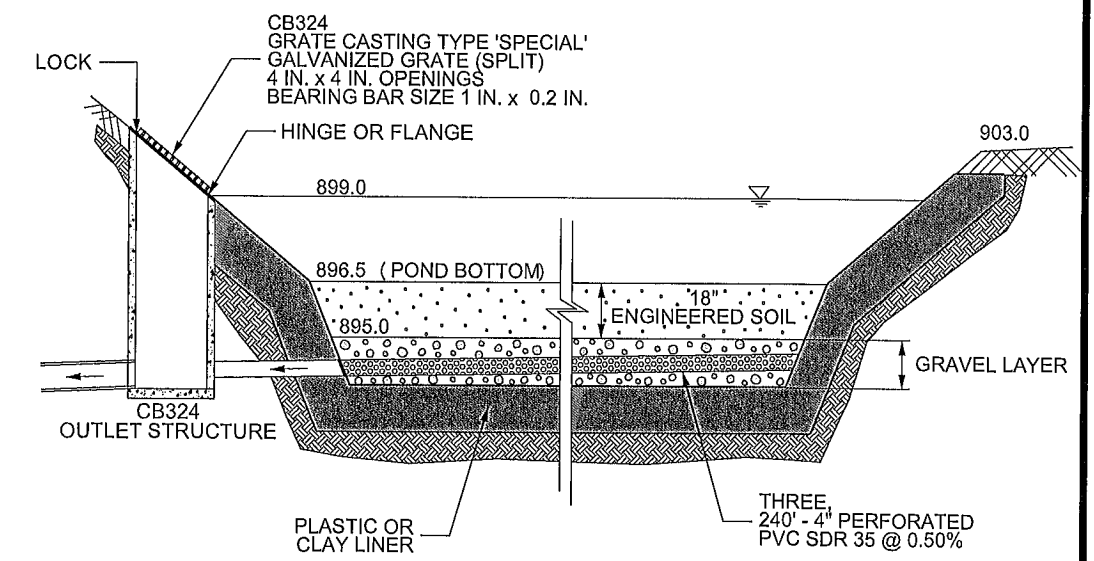
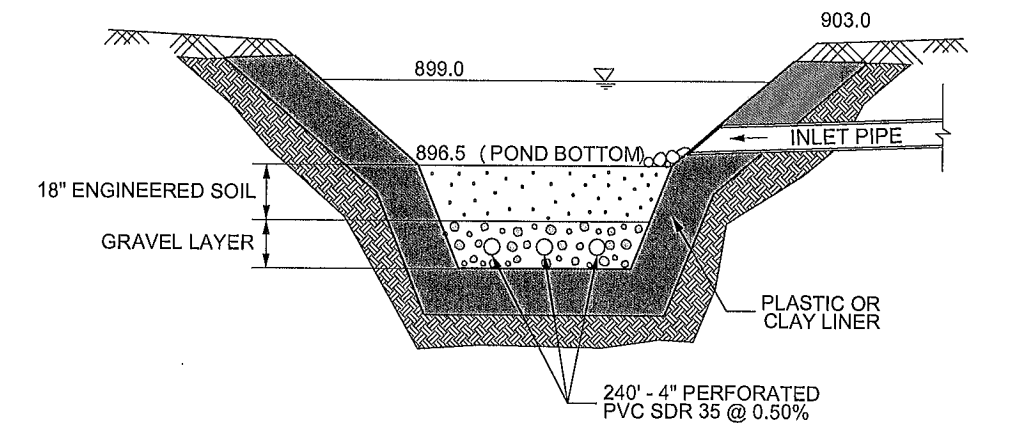
POND A
 HWL = 893.57



POND B NOTES:
 FILTRATION POND B SHALL BE LAYERED WITH ENGINEERED SOIL (86 CU YD) ABOVE GRAVEL (36 CU YD) ABOVE 18 INCH CLAY (749 CU YD) OR PLASTIC LINER AS SHOWN IN THE DETAILS BELOW. THIS WORK IS PAID FOR AS 'COMMON BORROW SPECIAL (CV) (POND B)'.
 THE GRAVEL LAYER SHALL HAVE A MINIMUM OF 3 IN. OF WASHED #57 STONE ABOVE AND ON EACH SIDE OF THE UNDERDRAIN.
 INCLUDE A UTILITY TRACE WIRE FOR ALL BURIED PIPING. THIS WORK IS INCIDENTAL TO UNDERDRAINS.
 POND OUTLETS ARE TO BE CLEANED AT THE CONCLUSION OF CONSTRUCTION.
 THE CLAY LINER SHALL HAVE A MAXIMUM HYDRAULIC CONDUCTIVITY OF 1×10^{-5} CM/SEC.

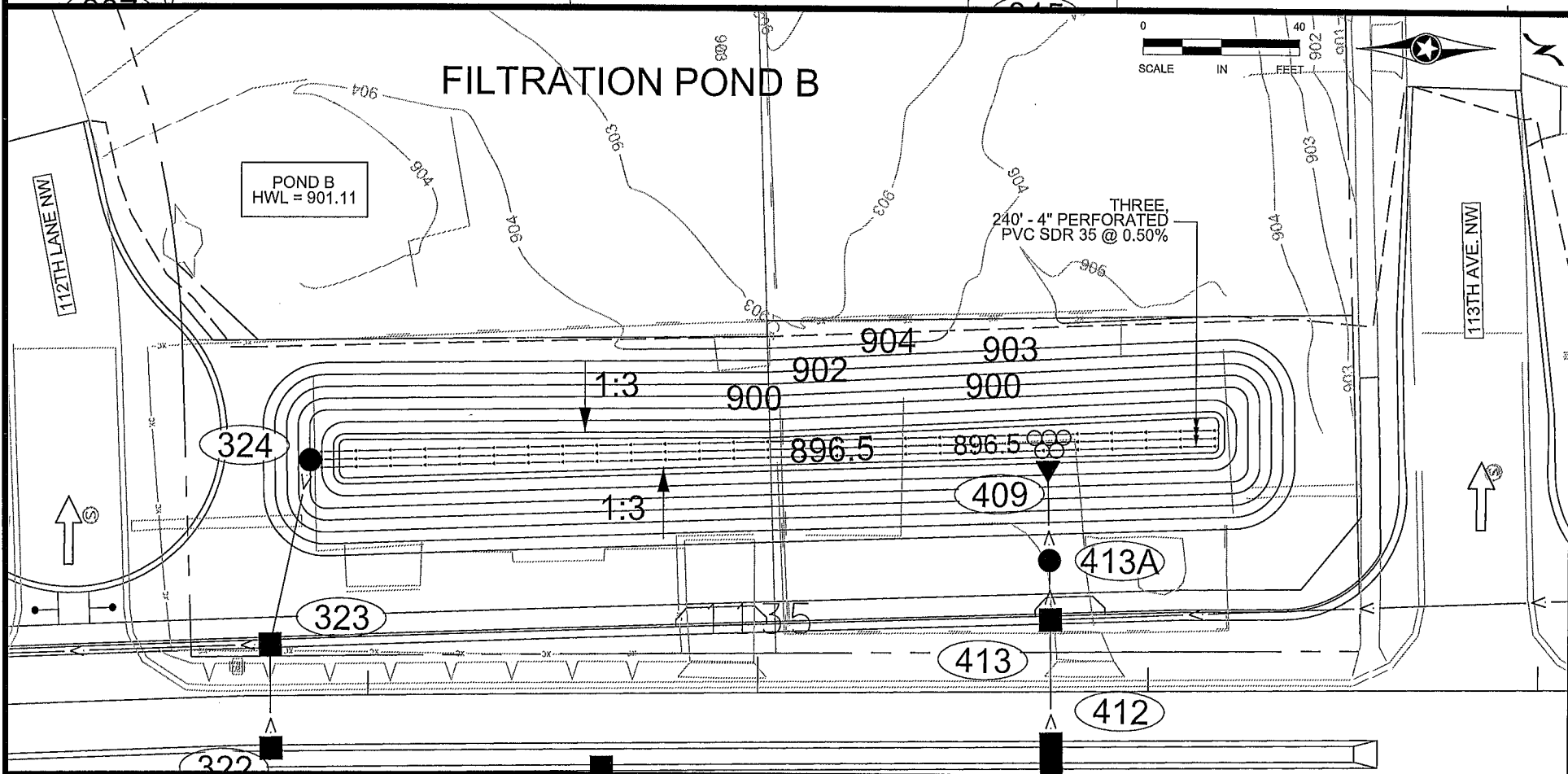
LEGEND

- PROPOSED CATCH BASIN
- PROPOSED MANHOLE
- ▲ PROPOSED APRON
- RIPRAP AT APRON OUTLET
- (S) SOD AT APRON INLET
- - - PROPOSED STORM SEWER
- SURFACE FLOW ARROW



NOT TO SCALE

1 OF 1



POND B
 HWL = 901.11

THREE,
 240' - 4" PERFORATED
 PVC SDR 35 @ 0.50%

1	07/08/2016	EM	GP	GP	ADDED A NOTE ON CLAY LINER HYDRAULIC CONDUCTIVITY.
NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-341Plan\002-611-034_DR_PA-B.dgn 07/06/2016 2:20:25 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 7-7-16 LICENSE NO. 22713

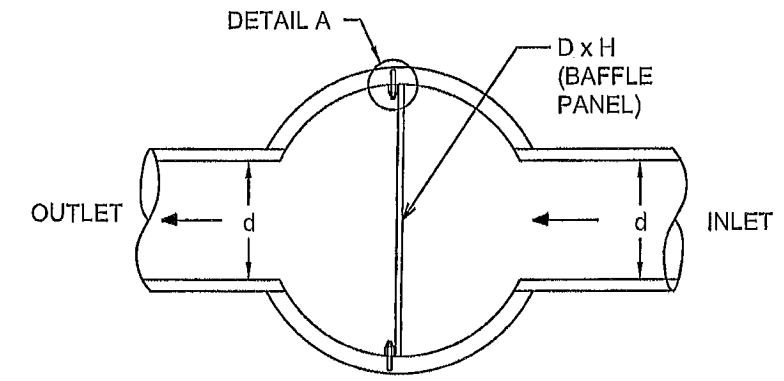
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 DESIGN BY: EM DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16

**ANOKA COUNTY
 HIGHWAY DEPT.**

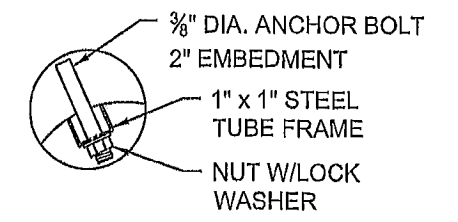
SP 002-611-034
 SP 114-020-050
 CP 13-10

POND GRADING PLAN
 PONDS A & B
 Sheet 127 of 298 Sheets

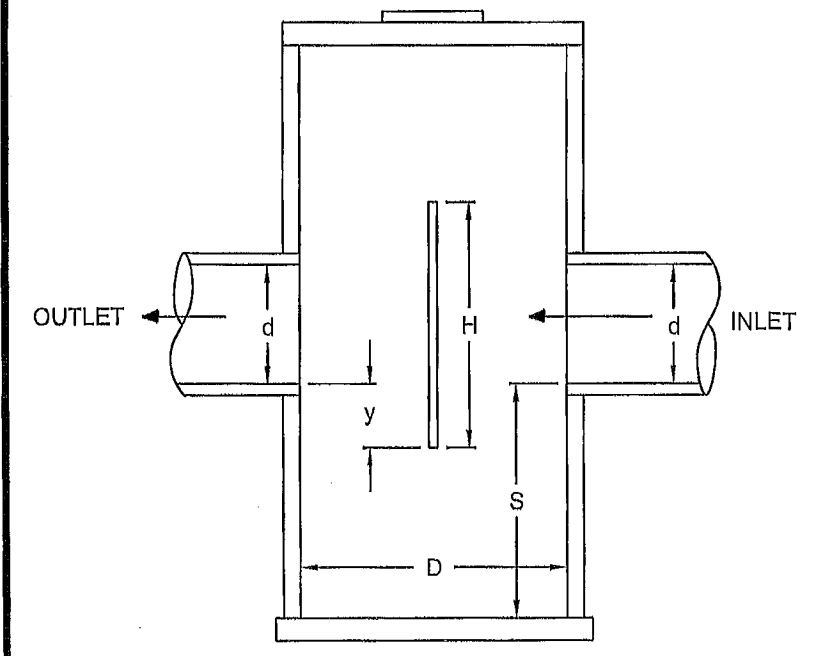
SUMP STRUCTURE & BAFFLE PANEL					
STRUCTURE	D (FT.)	S (FT.)	d (IN.)	H (IN.)	y (IN.)
109A	5	5	21	44	12
237A	10	6	27	44	12
242	5	5	24	44	12
310A	10	6	30	54	12
413A	6	6	27	44	12



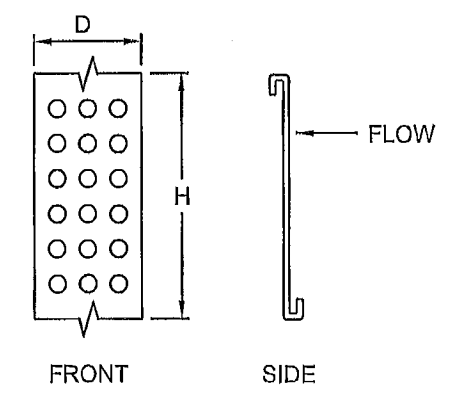
TOP VIEW OF SUMP CB OR MH



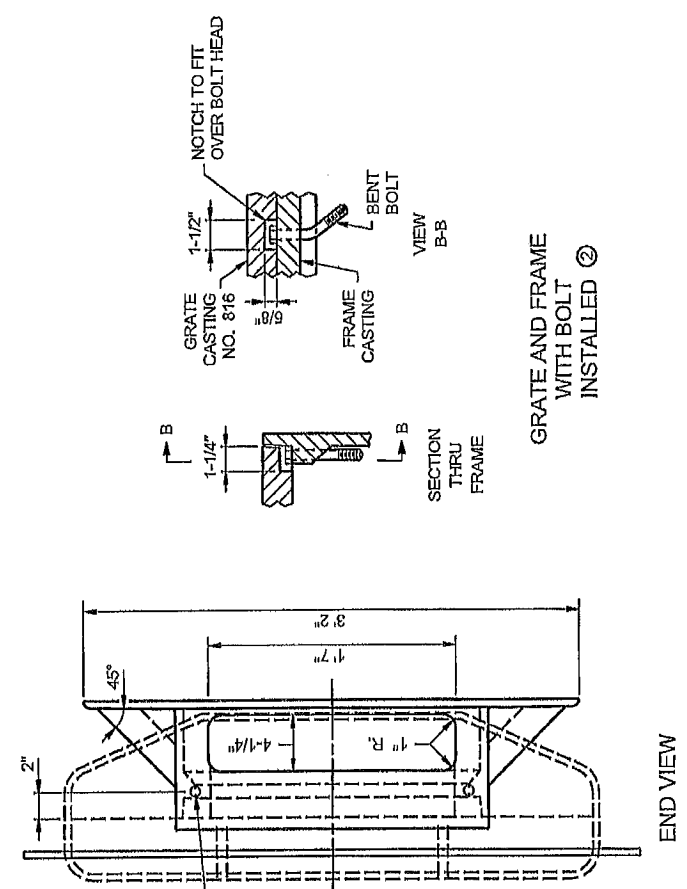
DETAIL A



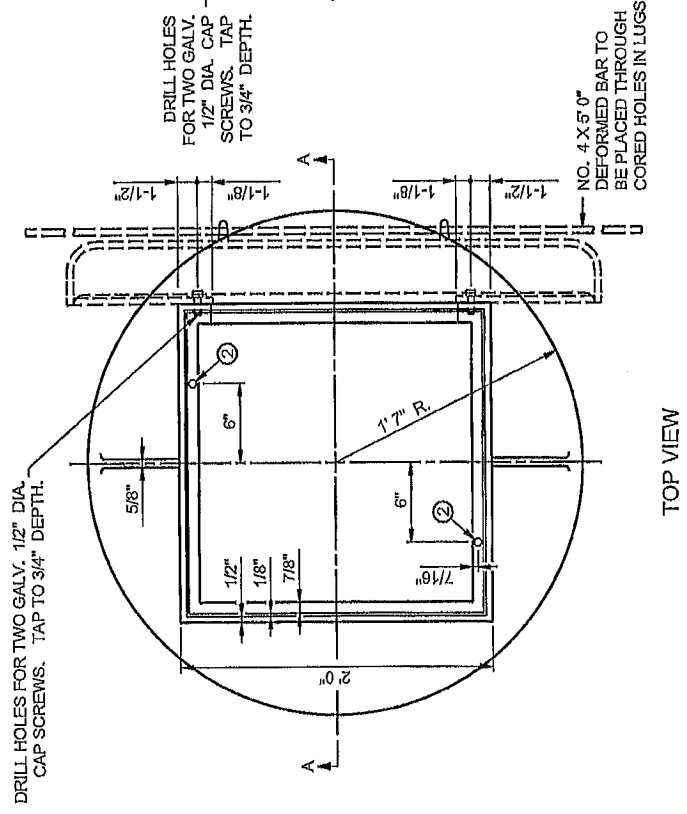
SIDE VIEW OF SUMP CB OR MH



SAFL BAFFLE PANEL



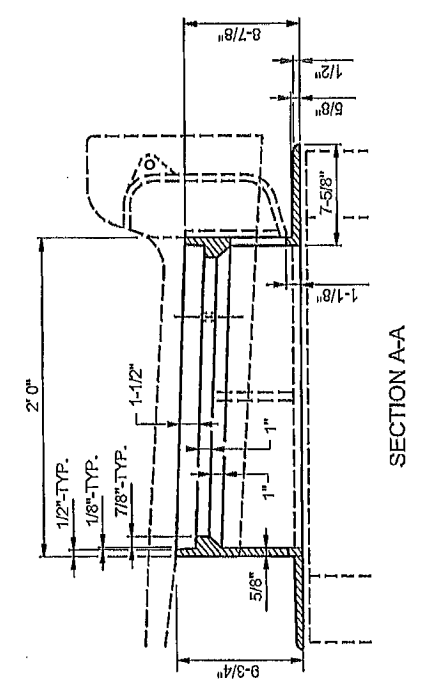
GRATE AND FRAME WITH BOLT INSTALLED



GRATE AND FRAME CASTING DETAIL FOR TYPE 'MODIFIED'

CASTINGS USED FOR ASSEMBLY	
GRATE	816 (4154)
CURB BOX	NO. 823A (4160)

NOTES:
 USE 1/4" FILLETS IN ALL CORNERS.
 SEE STANDARD PLATE 7111 FOR INSTALLATION REQUIREMENTS.
 ① APPLIES TO DESIGN B OR V CURB AND CURB AND GUTTER.
 ② AT LOCATIONS INDICATED IN TOP VIEW, PROVIDE 9/16" DIA. HOLES WHEN GRATE NO. 816 (4154) IS USED WITH THIS FRAME. FIELD PLACE 1/2" DIA. X 4" LONG GALV. BOLT IN UP STREAM SIDE AND BEND UNDERSIDE TO PREVENT REMOVAL THIS WILL PREVENT GRATE NO. 816 (4154) FROM BEING PLACED IN WRONG DIRECTION AND NOT BEING BICYCLE SAFE.



SECTION A-A

NOT TO SCALE

NOT TO SCALE

SUMP STRUCTURE AND SAFL BAFFLE PANEL DETAILS
 CB109A, CB237A, CB242, MH310A, MH413A

NO	DATE	BY	CHKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-011-034_DR_TAB.dgn 09/16/2010 9:18:00 AM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 6/15/16 LICENSE NO. 22713

DRAWN BY: EM DATE: 02/27/16
 DESIGN BY: EM DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16



ANOKA COUNTY
 HIGHWAY DEPT.

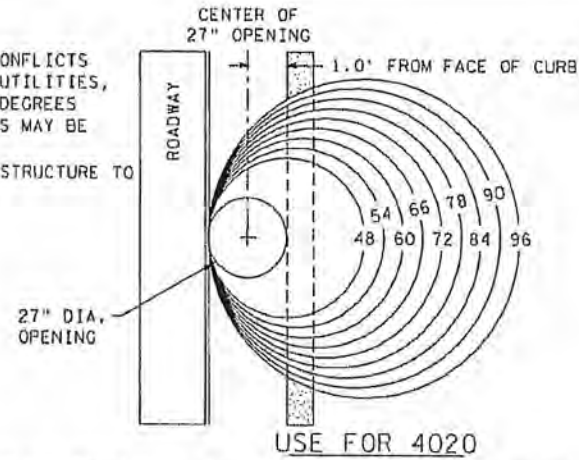
SP 002-611-034
 SP 114-020-050
 CP 13-10

MISCELLANEOUS DRAINAGE
 DETAILS
 Sheet 132 of 298 Sheets

TABLE A

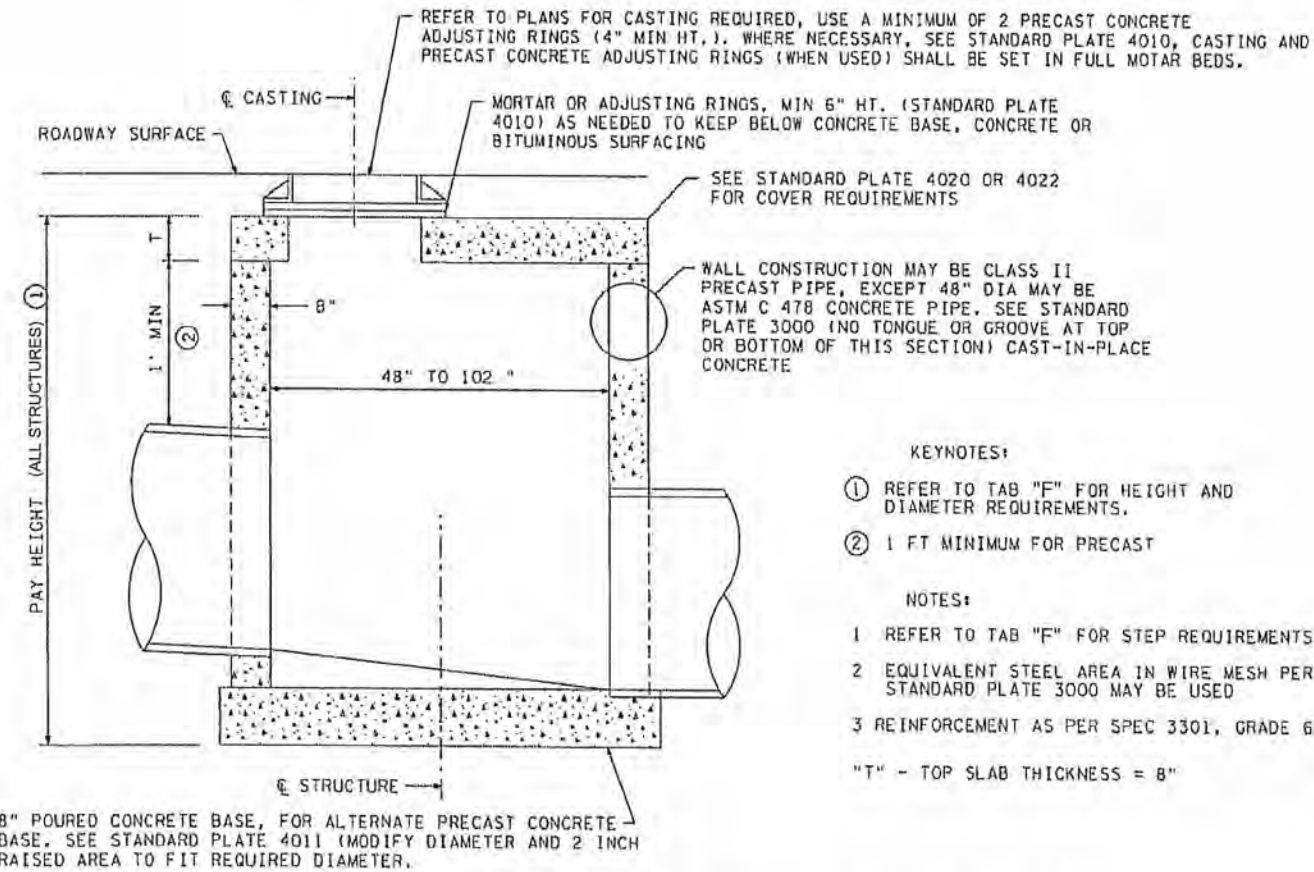
4020 DIAMETER	OFFSET FEET
48"	0.79
54"	1.08
60"	1.29
66"	1.58
72"	1.79
78"	2.08
84"	2.29
90"	2.58
96"	2.88
102"	3.17
108"	3.29
120"	3.79

WHERE THE 4020 DIAMETER CONFLICTS WITH OTHER STRUCTURES OR UTILITIES, ROTATE THE STRUCTURE 180 DEGREES TO PROVIDE CLEARANCE, THIS MAY BE ADJUSTED IN THE FIELD.
 * OFFSET IS FROM CENTER OF STRUCTURE TO CENTER OF OPENING.

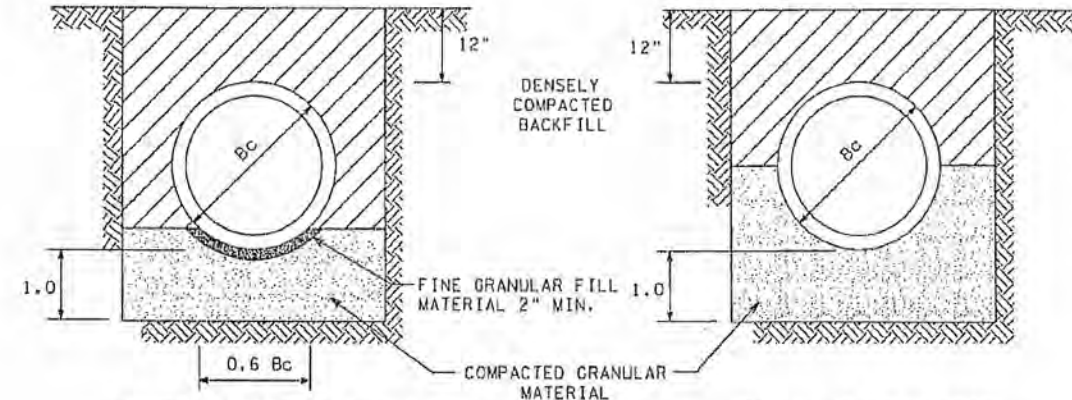


- THE FOLLOWING PLACEMENT LOCATIONS SHALL BE USED WITH CONCRETE CURB AND GUTTER.
1. THE CENTER OF GRATE STATION AND OFFSET LOCATION IS GIVEN IN THE DRAINAGE TABULATION
 2. THE OFFSET FROM THE CENTER OF STRUCTURE TO THE CENTER OF GRATE IS GIVEN IN TABLE "A" TO THE LEFT FOR 4020 STRUCTURES. OFFSET FOR 4005 STRUCTURES IS 0.9 FT.
 3. THE CENTER OF OPENING IS 1.0' TOWARD THE ROADWAY FROM THE FACE OF CURB.
 4. THE STRUCTURES THAT HAVE STEPS SHALL BE LOCATED ON THE ROADSIDE OF THE 27" OPENING AND MUST BE EASILY ACCESSIBLE, THE STEP LOCATION MAY NEED TO BE ADJUSTED IF THERE IS A LARGE PIPE DIRECTLY BELOW THE OPENING.

STRUCTURE LOCATION
NO SCALE



DRAINAGE STRUCTURE DESIGN 4020
NO SCALE



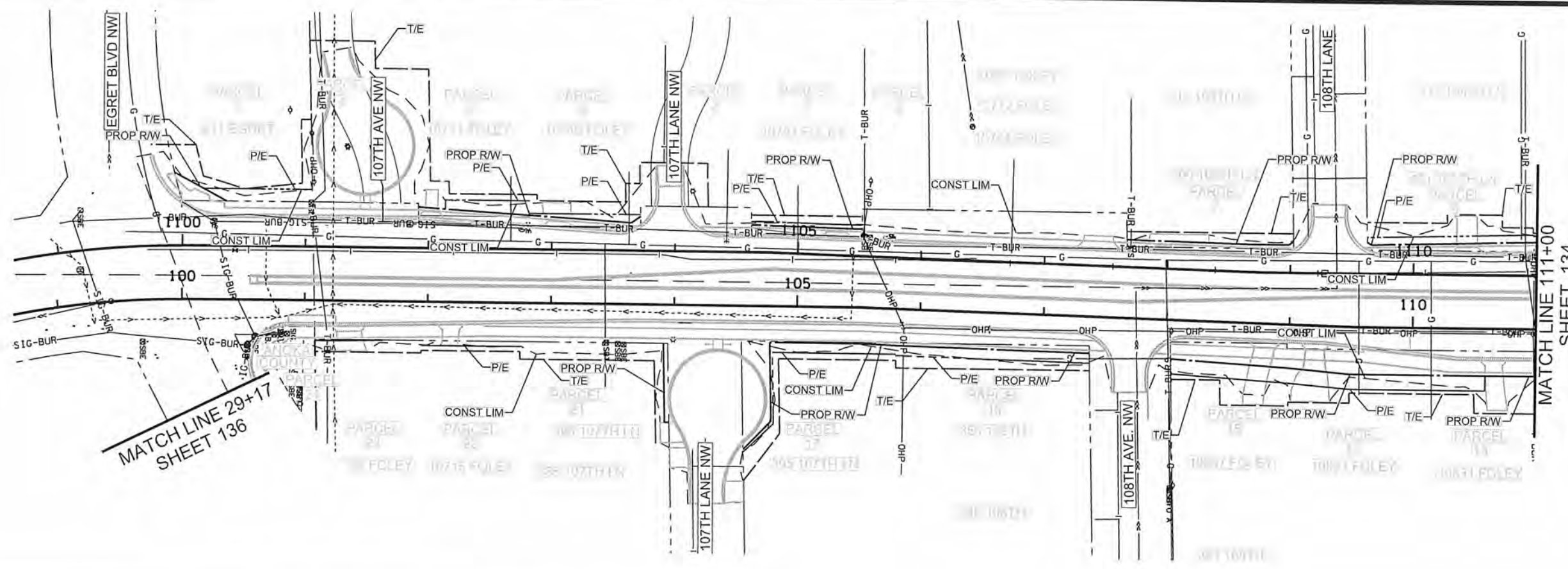
CLASS B
 $B_f = 1.9$

- LEGEND
- B_c = OUTSIDE DIAMETER
 - H = BACKFILL COVER ABOVE TOP OF PIPE
 - D = INSIDE DIAMETER
 - d = DEPTH OF BEDDING MATERIAL BELOW PIPE

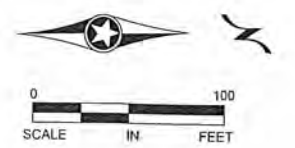
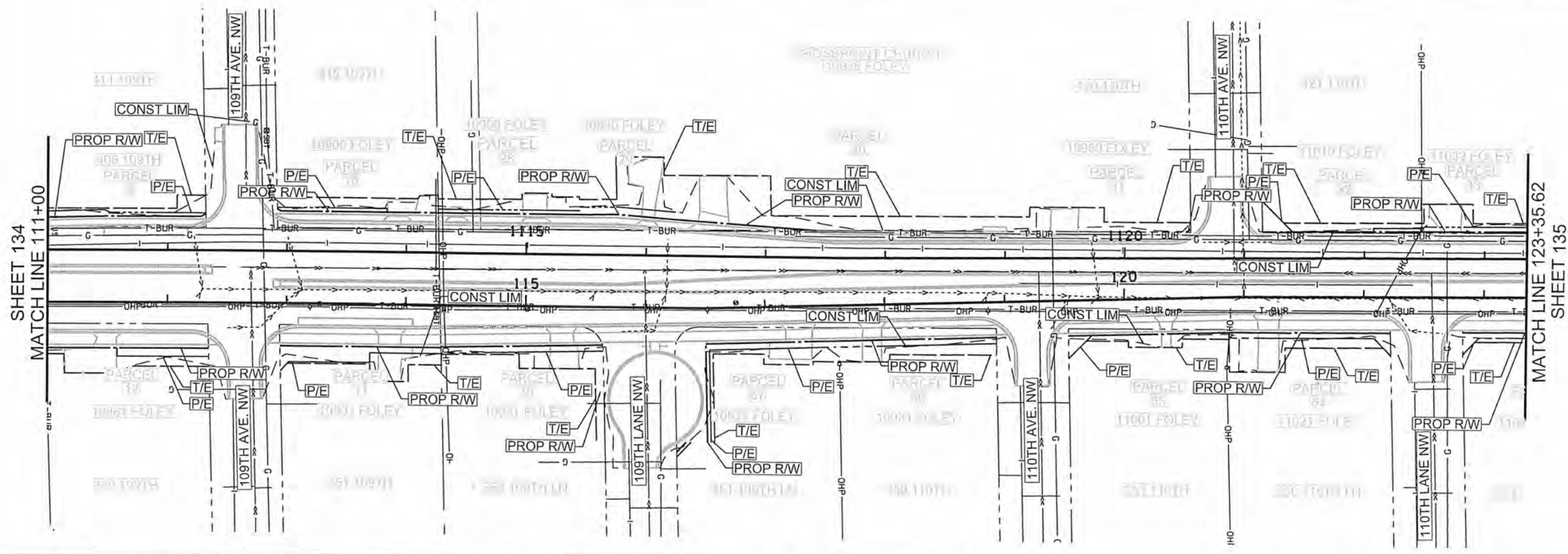
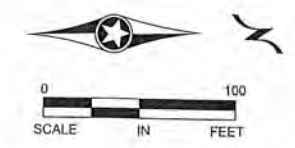
NOTES:
 FOR CLASS B BEDDINGS, SUBGRADES SHOULD BE EXCAVATED OR OVER EXCAVATED, IF NECESSARY, SO A UNIFORM FOUNDATION FREE OF PROTRUDING ROCKS MAY BE PROVIDED.
 PIPE BEDDING FOR PIPE LAYED IN TRENCHES WHERE UNSUITABLE SOILS ARE ENCOUNTERED IS INCIDENTAL.
 A MINIMUM OF ONE FOOT OF GRANULAR FOUNDATION SHALL BE PLACED BELOW BOTTOM OF PIPE, SEE SPECIFICATION.

TRENCH BEDDING CLASS B
NO SCALE

- KEYNOTES:
- ① REFER TO TAB "F" FOR HEIGHT AND DIAMETER REQUIREMENTS.
 - ② 1 FT MINIMUM FOR PRECAST
- NOTES:
- 1 REFER TO TAB "F" FOR STEP REQUIREMENTS.
 - 2 EQUIVALENT STEEL AREA IN WIRE MESH PER STANDARD PLATE 3000 MAY BE USED
 - 3 REINFORCEMENT AS PER SPEC 3301, GRADE 60
- "T" - TOP SLAB THICKNESS = 8"



LEGEND	
G	CENTERPOINT ENERGY
TV-BUR	COMCAST CABLE COMMUNICATIONS
OHU	CONNEXUS ENERGY
OHP	CONNEXUS ENERGY
T-BUR	CENTURYLINK
F/O-BUR	ZAYO
SIG-BUR	TRAFFIC SIGNAL
- - - - -	EXISTING STORM SEWER
- - - - -	EXISTING SAN SEWER
- - - - -	EXISTING WATER MAIN
- - - - -	PROPOSED STORM DRAIN
- - - - -	EXISTING R/W
- - - - -	PROPOSED R/W
- - - - -	EXISTING ROADWAY REMOVAL



1 OF 4

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:02-611-34\Plan\002-611-034_UT_P1.dgn 05/13/2016 12:59:14 PM

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 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-13-16 LICENSE NO. 22713

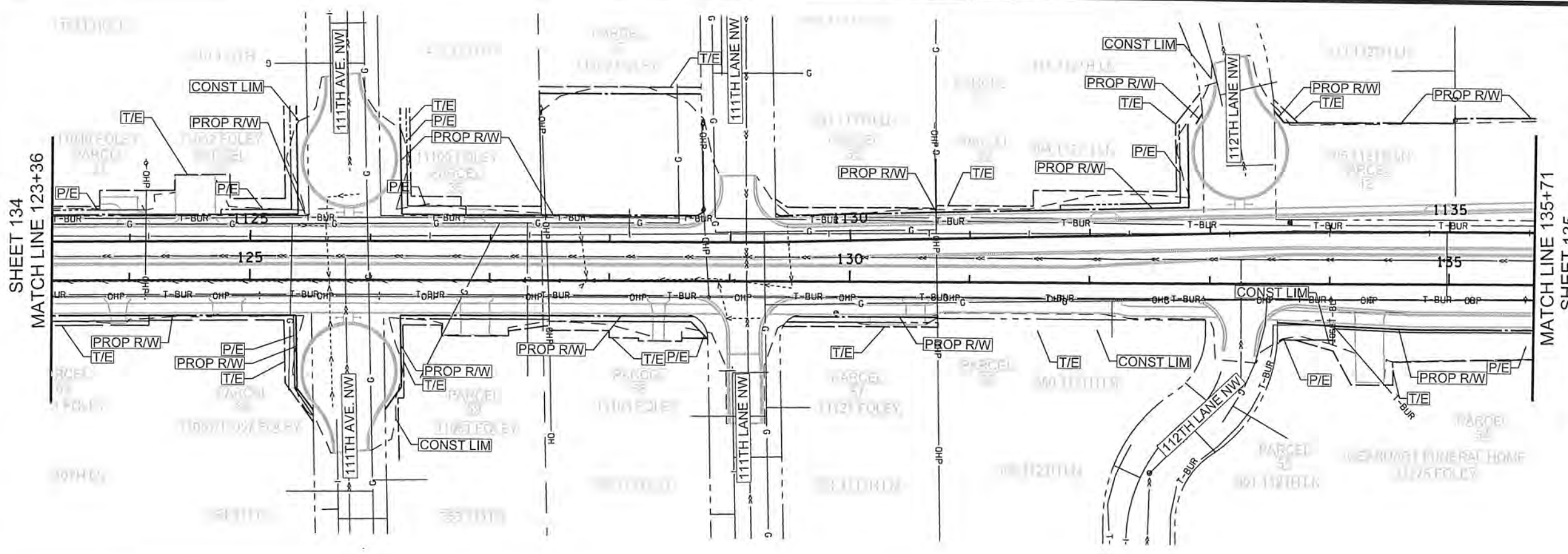
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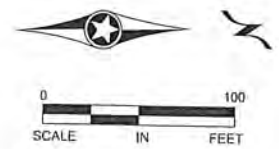
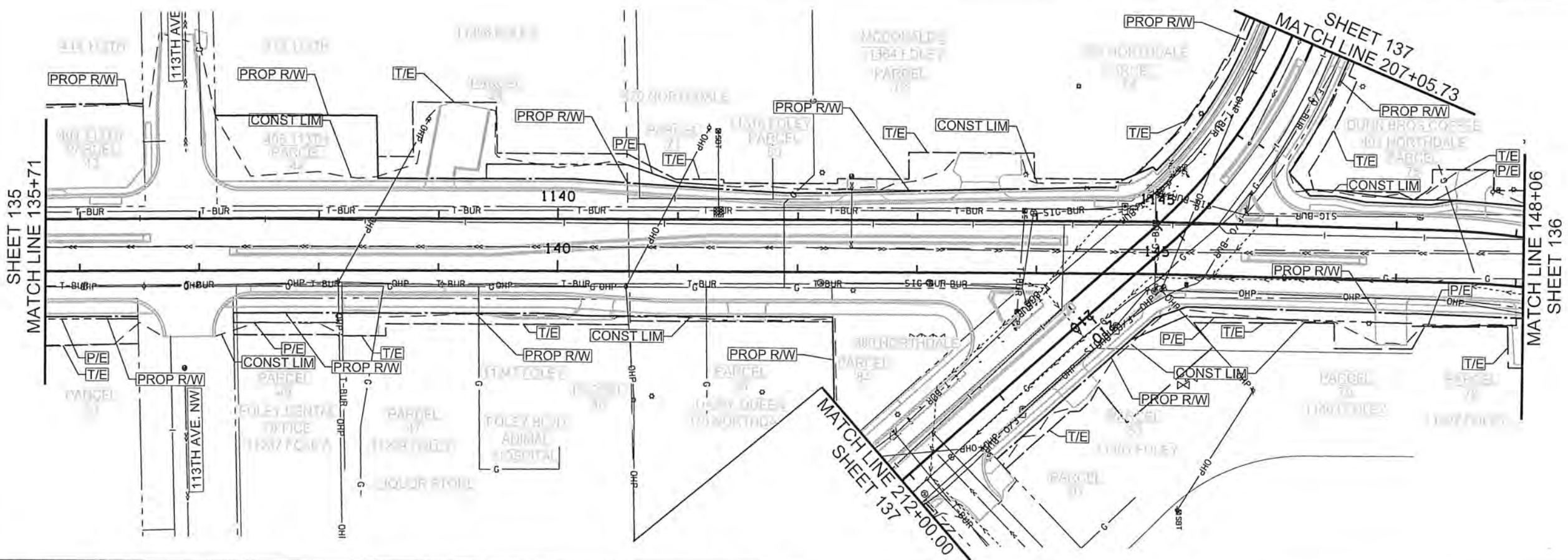
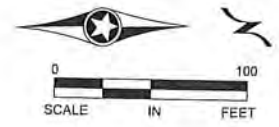
ANOKA COUNTY
 HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

UTILITY PLAN
 STA 99+76.95 TO 111+00.00
 Sheet 134 of 298 Sheets



LEGEND	
G	CENTERPOINT ENERGY
TV-BUR	COMCAST CABLE COMMUNICATIONS
OHP	CONNEXUS ENERGY
T-BUR	CENTURYLINK
F/O-BUR	ZAYO
SIG-BUR	TRAFFIC SIGNAL
- - - - -	EXISTING STORM SEWER
- - - - -	EXISTING SAN SEWER
- - - - -	EXISTING WATER MAIN
- - - - -	PROPOSED STORM DRAIN
- - - - -	EXISTING R/W
- - - - -	PROPOSED R/W
- - - - -	EXISTING ROADWAY REMOVAL



NO	DATE	BY	CKD	APPR	REVISION

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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO

SIGNATURE: *[Signature]*

DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16

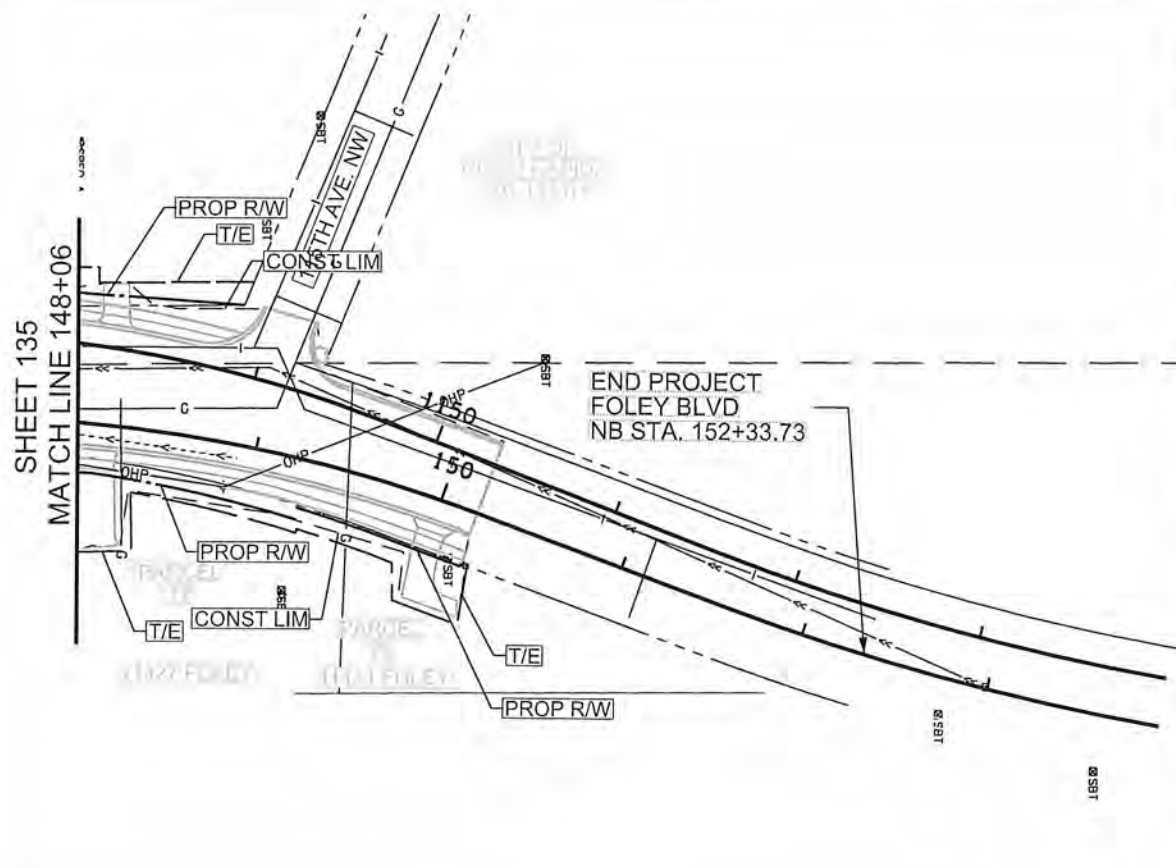
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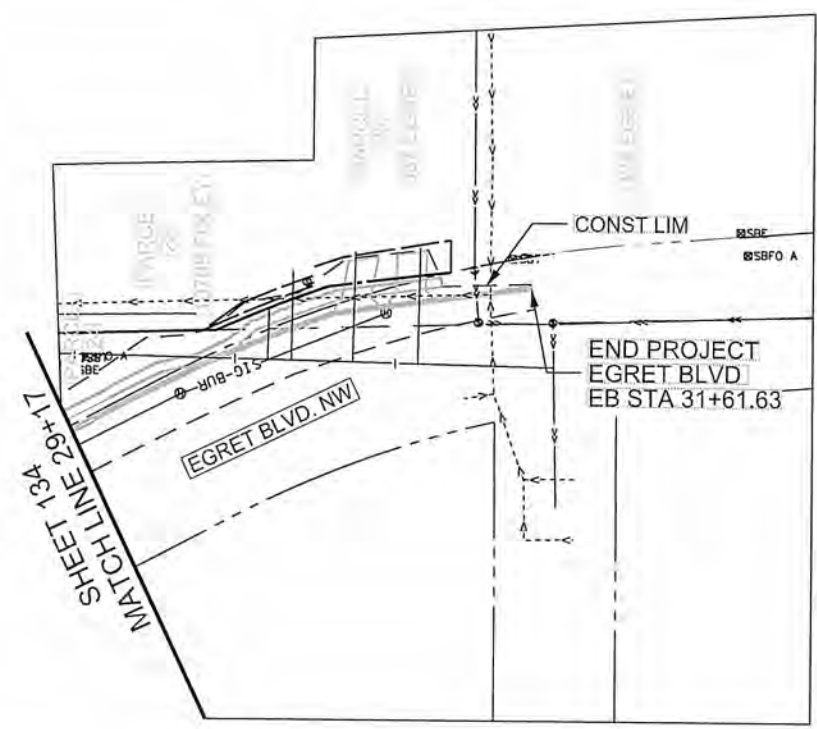
ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10



LEGEND	
G	CENTERPOINT ENERGY
TV-BUR	COMCAST CABLE COMMUNICATIONS
OHU	COMCAST CABLE COMMUNICATIONS
OHP	CONNEXUS ENERGY
T-BUR	CENTURYLINK
F/D-BUR	ZAYO
SIG-BUR	TRAFFIC SIGNAL
---	EXISTING STORM SEWER
---	EXISTING SAN SEWER
---	EXISTING WATER MAIN
-V-	PROPOSED STORM DRAIN
---	EXISTING R/W
---	PROPOSED R/W
---	EXISTING ROADWAY REMOVAL

EGRET BLVD



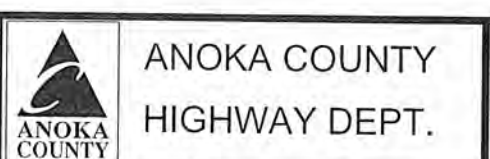
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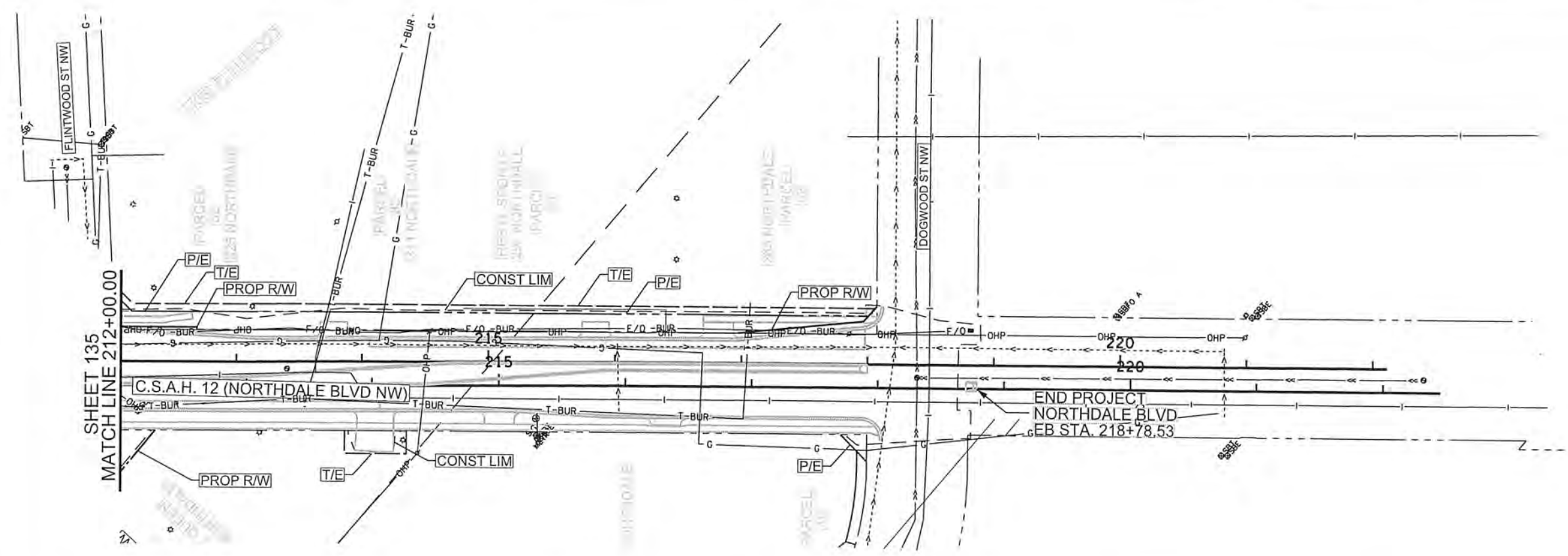
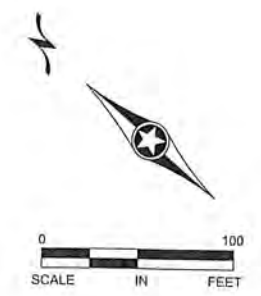
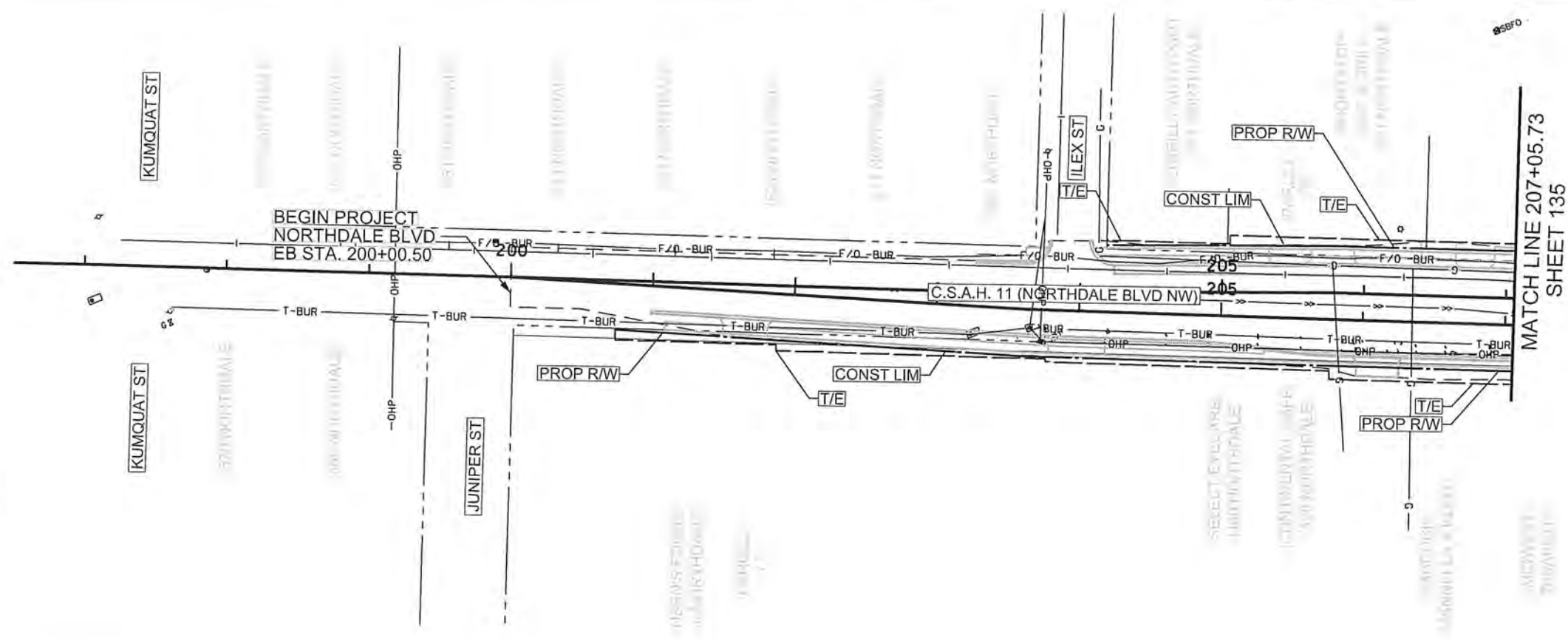
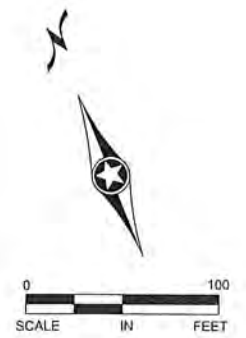
SP 002-611-034
 SP 114-020-050
 CP 13-10

3 OF 4

UTILITY PLAN	
STA 148+06.49	TO 151+56.41
STA 29+17	TO 31+61.63

Sheet 136 of 298 Sheets

LEGEND	
	CENTERPOINT ENERGY
	COMCAST CABLE COMMUNICATIONS
	CONNEXUS ENERGY
	CENTURYLINK
	ZAYO
	TRAFFIC SIGNAL
	EXISTING STORM SEWER
	EXISTING SAN SEWER
	EXISTING WATER MAIN
	PROPOSED STORM DRAIN
	EXISTING R/W
	PROPOSED R/W
	EXISTING ROADWAY REMOVAL

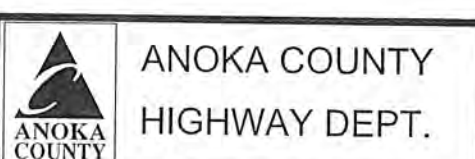


NO	DATE	BY	CKD	APPR	REVISION

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SP 002-611-034
 SP 114-020-050
 CP 13-10

4 OF 4
 UTILITY PLAN
 NORTHDALE BLVD
 STA 196+50 TO 218+04.77
 Sheet 137 of 298 Sheets

PROJECT LOCATION AND GENERAL INFORMATION

THIS ROAD CONSTRUCTION PROJECT CONSISTS OF RECONSTRUCTING CSAH 11 (FOLEY BLVD.) FROM EGRET BLVD NW THRU NORTHDAL BLVD NW IN THE CITY OF COON RAPIDS. CSAH 11 WILL BE A FOUR LANE DIVIDED ROADWAY WITH LEFT AND RIGHT TURN LANES, SIDEWALK AND TRAIL ALONG THE CORRIDOR.

THE PROJECT WILL PRIMARILY CONSIST OF GRADING, PLACING OF AGGREGATE BASE, BITUMINOUS PAVING, CURB AND GUTTER, STORM SEWER AND POND CONSTRUCTION.

THIS PROJECT WILL REQUIRE THE DISTURBANCE OF 19.2 ACRES OF SOILS AND DOES CREATE THE POTENTIAL FOR SEDIMENT DISCHARGE FROM THE SITE.

TRAINING REQUIREMENTS

THE CONTRACTOR WILL ENSURE THAT THE TRAINING REQUIRED IN PART 111.A.2 OF THE GENERAL STORMWATER PERMIT FOR CONSTRUCTION ACTIVITY IS COMPLIED WITH.

THE INDIVIDUALS TRAINED AND THE TRAINING RECEIVED WILL BE RECORDED IN THE SWPPP BEFORE THE START OF CONSTRUCTION OR AS SOON AS PERSONNEL FOR THE PROJECT HAVE BEEN DETERMINED.

LONG TERM OPERATION AND MAINTENANCE

THE CITY OF COON RAPIDS STREETS DIVISION WILL BE RESPONSIBLE FOR THE LONG TERM OPERATION AND MAINTENANCE OF THE PERMANENT STORMWATER MANAGEMENT.

TIM HIMMER
PUBLIC WORKS DIRECTOR
11155 ROBINSON DRIVE
COON RAPIDS, MN 55433
PHONE: (763) 767-6494

RECEIVING SURFACE WATERS, DISCHARGE TO IMPAIRED WATERS & SPECIAL WATERS

THE FOLLOWING TABLE IDENTIFIES ALL SURFACE WATERS WITHIN 1 MILE OF THE DISTURBED SOIL PROJECT BOUNDARIES, WHICH WILL RECEIVE STORMWATER RUNOFF FROM THE CONSTRUCTION SITE, DURING OR AFTER CONSTRUCTION.

STORMWATER FROM A DISCHARGE POINT ON THE PROJECT THAT FLOWS TO A SURFACE WATER IDENTIFIED AS IMPAIRED AND/OR SPECIAL MUST INCLUDE THE FOLLOWING ADDITIONAL BMP REQUIREMENTS:

- 1) ALL EXPOSED SOIL AREAS MUST BE STABILIZED AS SOON AS POSSIBLE TO LIMIT SOIL EROSION BUT IN NO CASE LATER THAN SEVEN (7) DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
- 2) TEMPORARY SEDIMENT BASINS MUST BE USED FOR COMMON DRAINAGE LOCATIONS THAT SERVE AN AREA WITH FIVE (5) OR MORE ACRES DISTURBED AT ONE TIME. THIS PROJECT AS DESIGNED DOES NOT HAVE FIVE (5) DISTURBED ACRES DRAINING TO A COMMON LOCATION AND TEMPORARY SEDIMENT BASINS WILL NOT BE REQUIRED.

RECEIVING SURFACE WATERS		
NAME OF WATER BODY	SPECIAL WATER	IMPAIRED WATER
ANOKA COUNTY		
UNNAMED DITCH	NO	NO

DISTURBED SOIL AREA

TOTAL DISTURBED SOILS AREA FOR THIS PROJECT IS 19.2 ACRES

IMPERVIOUS SOIL AREA

EXISTING AREA OF IMPERVIOUS SURFACE IS 11.5 ACRES.
POST CONSTRUCTION AREA OF IMPERVIOUS SURFACE 15.4 ACRES.

SOIL TYPES

THE PREDOMINANT SOIL TYPE FOUND ON THIS PROJECT IS SAND.

INFILTRATION PONDS

A POST CONSTRUCTION INFILTRATION TEST SHALL BE PERFORMED AT ALL POND LOCATIONS BY FILLING THE BASIN TO A MINIMUM DEPTH OF 6 INCHES WITH WATER AND MONITORING THE TIME TO DRAIN. THE COON CREEK WATERSHED DISTRICT SHALL BE NOTIFIED PRIOR TO THE TEST TO WITNESS THE RESULTS.

WITHHOLDING OF PAYMENT/NONCOMPLIANCE

IF THE CONTRACTOR FAILS TO INSTALL EROSION OR SEDIMENT CONTROL MEASURES ORDERED BY THE ENGINEER, THE ENGINEER MAY WITHHOLD PAYMENT FROM RELATED WORK UNTIL THE CONTROL MEASURES ARE UNDERTAKEN BY THE CONTRACTOR. WHEN THE CONTRACTOR FAILS TO CONDUCT THE QUALITY CONTROL PROGRAM, DOESN'T CONDUCT THE INSPECTION REQUIRED IN THE NPDES PERMIT, OR FAILS TO TAKE ACTION ORDERED BY THE ENGINEER TO REMEDY EROSION OR SEDIMENT CONTROL PROBLEMS, THE ENGINEER WILL ISSUE A WRITTEN ORDER TO THE CONTRACTOR. THE CONTRACTOR SHALL RESPOND WITHIN 24 HOURS WITH SUFFICIENT PERSONNEL, EQUIPMENT AND/OR MATERIALS AND CONDUCT THE REQUIRED WORK OR BE SUBJECT TO A \$500.00 PER CALENDAR DAY DEDUCTION FOR NONCOMPLIANCE.

CONSTRUCTION PHASING

SILT FENCE AND/OR OTHER SUITABLE PERIMETER BMP'S AS PROVIDED IN THE PLANS WILL BE INSTALLED PRIOR TO THE START OF ANY LAND DISTURBING ACTIVITY. CONSTRUCTION WILL BE PHASED SO THAT ALL DOWN GRADIENT SEDIMENT CONTROL MEASURES ARE INSTALLED PRIOR TO OR IN CONJUNCTION WITH ANY SOIL DISTURBING ACTIVITIES.

WHEN TOPSOIL IS DISTURBED, THE TOPSOIL WILL BE STRIPPED AND STOCKPILED IN SOIL BERMS AT THE TOE OF THE STRIPPED SLOPES ALONG THE PROJECT LIMITS. TEMPORARY VEGETATION WILL BE ESTABLISHED ON THE STOCKPILED TOPSOIL BERMS WITH HYDROMULCH AS NEEDED. STOCKPILED TOPSOIL BERMS WILL NOT BE PLACED IN ANY STORMWATER CONVEYANCES.

AFTER STRIPING THE TOPSOIL THE EXPOSED SOIL INSLOPES WILL BE STABILIZED WITH SEED AND EROSION CONTROL BLANKET WITHIN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS BEEN TEMPORARILY OR PERMANENTLY CEASED.

TEMPORARY SEDIMENT BASINS

THIS ROAD CONSTRUCTION PROJECT AS DESIGNED DOES NOT MEET ANY OF THE TEMPORARY SEDIMENT BASIN DISTURBED AREA THRESHOLD REQUIREMENTS AND TEMPORARY SEDIMENT BASINS WILL NOT BE REQUIRED.

PERMANENT STORMWATER MANAGEMENT SYSTEM

ALL STORMWATER MUST BE DISCHARGED IN A MANNER THAT DOES NOT CAUSE NUISANCE CONDITIONS, EROSION IN RECEIVING WATERS OR ON DOWNSLOPE PROPERTIES, OR INUNDATION IN WETLANDS CAUSING A SIGNIFICANT ADVERSE IMPACT TO THE WETLAND.

EROSION PREVENTION PRACTICES

ALL EXPOSED SOIL AREAS MUST BE STABILIZED AS SOON AS POSSIBLE TO LIMIT SOIL EROSION BUT IN NO CASE LATER THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED. FOR ALL AREAS WHERE DISTURBED SOILS DRAIN TO AN IMPAIRED OR SPECIAL WATER THE EXPOSED SOIL MUST BE STABILIZED NO LATER THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT AREA CEASED. SEE THE IMPAIRED & SPECIAL WATERS SECTION OF THIS SWPPP FOR ADDITIONAL BMP REQUIREMENTS FOR DISTURBED AREAS THAT DRAIN TO A SPECIAL OR IMPAIRED WATER

THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH OR SWALE THAT DRAIN WATER FROM ANY PORTION OF THE CONSTRUCTION SITE, OR DIVERTS WATER AROUND THE SITE, MUST BE STABILIZED WITHIN 200 LINEAL FEET FROM THE POINT OF DISCHARGE INTO ANY SURFACE WATER. STABILIZATION OF THE LAST 200 FEET MUST BE COMPLETED WITHIN 24 HOURS AFTER CONNECTING TO A SURFACE WATER.

PIPE CULVERT OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS AFTER CONNECTION TO A SURFACE WATER. THIS WILL INCLUDE DRAINAGE DITCHES THAT DRAIN WATER FROM ANY PORTION OF THE CONSTRUCTION SITE

AGENCY CONTACTS

AGENCY	CONTACT NAME	PHONE
MPCA	NPDES	LAURAL MEZNER 218-316-3889
MPCA	EMERGENCY	STATE DUTY OFFICER 800-422-0798
DNR	NOT REQUIRED	
COE	NOT REQUIRED	
ANOKA COUNTY DESIGN SWPPP PREPARATION	U OF MN DESIGN OF SWPPP EXPIRES 5/18	JEFF FOSTER 763-862-4268
ANOKA COUNTY PROJECT REPRESENTATIVE	U OF MN SITE MANAGEMENT EXPIRES 5/18	CHRIS OSTERHUS 763-274-8127
EROSION CONTROL SUPERVISOR (CONTRACTOR)		

SEDIMENT CONTROL PRACTICES

TEMPORARY STOCKPILED TOPSOIL BERMS MUST INCLUDE PERIMETER BMP'S (SILT FENCE AND BERM) AT LOCATIONS WHERE CONSTRUCTION STORMWATER DRAINS FROM THE PROJECT

IN ORDER TO MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES, THERE SHALL BE NO UNBROKEN SLOPE LENGTH OF GRATER THEN 75 FEET FOR SLOPES WITH A GRADE OF 1:3 OR STEEPER

ALL STORM DRAIN INLETS MUST BE PROTECTED BY APPROPRIATE BMP'S DURING CONSTRUCTION UNTIL ALL SOURCES WITH POTENTIAL DISCHARGE TO THE INLET HAVE BEEN STABILIZED

VEHICLE TRACKING SEDIMENT FROM THE CONSTRUCTION SITE MUST BE MINIMIZED. STREET SWEEPING MUST BE USED IF SEDIMENT IS BEING TRACKED OFF THE CONSTRUCTION SITE

DUST SHALL BE CONTROLLED PER MNDOT SPEC. 2130.

POLLUTION PREVENTION MEASURES

THE CONTRACTOR WILL IMPLEMENT THE POLLUTION PREVENTION MANAGEMENT MEASURES AS DIRECTED IN THE NPDES PERMIT PART IV.F AS PERTAINING TO SOLID WASTE, HAZARDOUS MATERIALS EXTERNAL TRUCK WASHING, AND CONCRETE WASHOUT ONSITE.

THESE MANAGEMENT MEASURES FOR POLLUTION PREVENTION WILL BE STRICTLY ENFORCED.

LOCATION OF SWPPP REQUIREMENTS

REQUIREMENT	PLAN		MN/DOT SPECIFICATION	SPECIAL PROVISION
	TITLE	LOCATION		
NPDES PERMIT COMPLIANCE			1701, 1702, & 1717	1717 (AIR, LAND & WATER) 1717 (NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT)
CERTIFIED PERSONNEL IN EROSION AND SEDIMENT CONTROL SITE MANAGEMENT			1506, 1717, & 2573	1717 (AIR, LAND & WATER) 1717 (NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT)
CHAIN OF RESPONSIBILITY	STORM WATER POLLUTION PREVENTION PLAN	SHEETS 138 - 139	1506, 1717, & 2573	1717 (AIR, LAND & WATER) 1717 (NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT)
PROJECT SCHEDULE / WEEKLY EROSION & SEDIMENT CONTROL SCHEDULE / COMPLETING INSPECTION / MAINTENANCE LOG			1717 & 2573	1717 (AIR, LAND & WATER) 1717 (NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT)
SWPPP PREPARATION				
SITE MAP / RECEIVING WATERS / DIRECTION OF FLOW	EROSION CONTROL PLAN	SHEETS 150 - 153	1717	
PROJECT SPECIFIC CONSTRUCTION STAGING	STAGING PLANS	SHEETS 37 - 53	1717	1717 (AIR, LAND & WATER) 1717 (NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT) 1806 (DETERMINATION AND EXTENSION OF CONTRACT TIME)
TEMPORARY EROSION AND SEDIMENT CONTROL BMP LOCATIONS, INSTALLATION, TIMING OF INSTALLATION AND TYPE OF BMP	EROSION CONTROL PLAN AND TABULATION CHARTS	SHEETS 17, 150 - 153	2573 & 2525	2575 (RAPID STABILIZATION SPECIFICATION)
ADDITIONAL TEMPORARY AND OR PERMANENT EROSION AND SEDIMENT CONTROL BMP'S NOT PROVIDED OR SHOWN IN THE PLAN	STORM WATER POLLUTION PREVENTION PLAN	SHEETS 138 - 139	1717, 2573, & 2575	1717 (AIR, LAND & WATER) 1717 (NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT) 2575 (RAPID STABILIZATION SPECIFICATION)
MAINTENANCE OF EROSION AND SEDIMENT CONTROL DEVICES, REMOVAL OF TRACKED SEDIMENT, REMOVAL OF DEVICES			1717 & 2573	1514 (MAINTENANCE DURING CONSTRUCTION) 1717 (LAND AIR & WATER) 1717 (NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT)
DEWATERING			2105.3B, & 2451.3C	DEWATERING MAY ALSO REQUIRE DNR PERMIT. NO DEWATERING IS ANTICIPATED FOR THIS PROJECT
FINAL STABILIZATION	QUANTITY TABULATIONS TURF ESTABLISHMENT PLAN	SHEETS 17, 150 - 153	1717, 2573, & 2575	1717 (AIR, LAND & WATER) 1717 (NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT)
TEMPORARY EROSION AND SEDIMENT CONTROL DETAILS	QUANTITY TABULATIONS EROSION CONTROL PLAN	SHEETS 17, 150 - 153	2575	2575 (RAPID STABILIZATION SPECIFICATION)
PERMANENT EROSION CONTROL DETAILS	TURF ESTABLISHMENT PLAN	SHEETS 104 - 105	2575	2575 (CONTROLLING EROSION AND ESTABLISHING VEGETATION)

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_SWPPP.dgn 05/13/2016 12:59:19 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
PRINT NAME: GINA M. PIZZO
SIGNATURE: *Gina M. Pizzo*
DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: NJD DATE: 11/25/13
DESIGN BY: NJD DATE: 11/25/13
CHECKED BY: GMP DATE: 12/13/13



ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

SWPPP NARRATIVE

PERMANENT PAVEMENT MARKING PLAN
NOTES AND GUIDELINES

GENERAL INFORMATION:

THE ENGINEER'S INVOLVEMENT IN THE APPLICATION OF THE MATERIAL SHALL BE LIMITED TO FIELD CONSULTATION AND INSPECTION. ANOKA COUNTY HIGHWAY DEPARTMENT WILL PLACE NECESSARY "SPOTTING" AT APPROPRIATE POINTS TO PROVIDE HORIZONTAL CONTROL FOR STRIPING AND TO DETERMINE NECESSARY STARTING AND CUTOFF POINTS, LONGITUDINAL JOINTS, PAVEMENT EDGES AND EXISTING MARKINGS MAY SERVE AS HORIZONTAL CONTROL WHEN SO DIRECTED.

EDGE LINES AND LANE LINES ARE TO BE BROKEN ONLY AT INTERSECTIONS WITH PUBLIC ROADS AND AT PRIVATE ENTRANCES IF THEY ARE CONTROLLED BY A YIELD SIGN, STOP SIGN OR TRAFFIC SIGNAL. THE BREAK POINT IS TO BE AT THE START OF THE RADIUS FOR THE INTERSECTION OR AT MARKED STOP LINES OR CROSSWALKS.

A TOLERANCE OF 1/4 INCH UNDER OR 1/4 INCH OVER THE SPECIFIED WIDTH WILL BE ALLOWED FOR STRIPING PROVIDED THE VARIATION IS GRADUAL AND DOES NOT DETRACT FROM THE GENERAL APPEARANCE. BROKEN LINE SEGMENTS MAY VARY UP TO ONE-HALF FOOT FROM THE SPECIFIED LENGTHS PROVIDED THE OVER AND UNDER VARIATIONS ARE REASONABLY COMPENSATORY. ALIGNMENT DEVIATIONS FROM THE CONTROL GUIDE SHALL NOT EXCEED 1 INCH. MATERIAL SHALL NOT BE APPLIED OVER LONGITUDINAL JOINTS, ESTABLISHMENT OF APPLICATION TOLERANCES SHALL NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO COMPLY AS CLOSELY AS PRACTICABLE WITH THE PLANNED DIMENSIONS.

EPOXY:

THE ROAD SURFACE SHALL BE CLEANED AT THE DIRECTION OF THE ENGINEER JUST PRIOR TO APPLICATION. PAVEMENT CLEANING SHALL CONSIST OF AT LEAST BRUSHING WITH A ROTARY BROOM (NON-METALLIC) OR AS RECOMMENDED BY THE MATERIAL MANUFACTURER AND ACCEPTABLE TO THE ENGINEER. NEW PORTLAND CEMENT CONCRETE SURFACES SHALL BE SANDBLAST CLEANED TO REMOVE ANY SURFACE TREATMENT AND/OR LAITANCE ON LOW SPEED (SPEED LIMIT 35 MPH OR LESS) URBAN PORTLAND CEMENT CONCRETE ROADWAYS. SANDBLAST CLEANING SHALL BE USED FOR ALL EPOXY PAVEMENT MARKINGS.

THE EPOXY MARKING APPLICATION SHALL IMMEDIATELY FOLLOW THE PAVEMENT CLEANING. GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE EPOXY RESIN LINE TO PROVIDE AN IMMEDIATE NO-TRACK SYSTEM.

AN EPOXY RESIN LINE 4" WIDE AND 15 MILL THICKNESS (WET), REQUIRES AN APPLICATION RATE OF ONE (1) GALLON OF COMPONENTS FOR 320 FEET OF LINE. GLASS BEADS SHALL BE APPLIED AT A POUND PER GALLON RATE SUFFICIENT TO ACHIEVE AN ACCEPTABLE NO-TRACK SYSTEM.

OPERATIONS SHALL BE CONDUCTED ONLY WHEN THE ROAD PAVEMENT SURFACE TEMPERATURES ARE 50 DEGREES FAHRENHEIT OR GREATER.

PERMANENT PAVEMENT MARKINGS SHALL NOT BE PLACED OVER TEMPORARY TAPE MARKINGS.

PERFORMED THERMOPLASTIC:

THE PERFORMED THERMOPLASTIC MARKINGS SHALL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS ON CLEAN AND DRY SURFACES. SEE SPECIAL PROVISIONS FOR PERFORMED THERMOPLASTIC MARKING SPECIFICATIONS.

PAINT:

AT THE TIME OF APPLYING THE MARKING MATERIAL, THE APPLICATION AREA SHALL BE FREE OF CONTAMINATION. THE CONTRACTOR SHALL CLEAN THE ROADWAY SURFACE PRIOR TO THE LINE APPLICATION IN A MANNER AND TO THE EXTENT REQUIRED BY THE ENGINEER.

GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE PAINT LINE.

EXCEPT WHEN USED AS A TEMPORARY MARKING, PAVEMENT MARKINGS SHALL ONLY BE APPLIED IN SEASONABLE WEATHER WHEN AIR TEMPERATURE IS 50 DEGREES FARHENHEIT OR HIGHER AND SHALL NOT BE APPLIED WHEN THE WIND OR OTHER CONDITIONS CAUSE A FILD OR DUST TO BE DEPOSITED ON THE PAVEMENT SURFACE AFTER CLEANING AND BEFORE THE MARKING MATERIAL CAN BE APPLIED.

THE FILLING OF TANKS, POURING OF MATERIALS OR CLEANING OF EQUIPMENT SHALL NOT BE PERFORMED ON UNPROTECTED PAVEMENT SURFACES UNLESS ADEQUATE PROVISIONS ARE MADE TO PREVENT SPILLAGE OF MATERIAL.

PERMANENT PAVEMENT MARKING TABULATION		
ITEM	UNIT	TOTAL QUANTITY
4" SOLID LINE WHITE - EPOXY	LIN FT	18737
4" BROKEN LINE WHITE - EPOXY (10' STRIPE, 40' SKIP)	LIN FT	1756
4" SOLID LINE YELLOW - EPOXY	LIN FT	10040
4" DOUBLE SOLID LINE YELLOW - EPOXY	LIN FT	2220
PAVEMENT MESSAGE (LT ARROW) PREFORMED THERMOPLASTIC	EACH	10
PAVEMENT MESSAGE (RT ARROW) PREFORMED THERMOPLASTIC	EACH	9
24" SOLID LINE YELLOW - PREFORMED THERMOPLASTIC	LIN FT	395
24" SOLID LINE WHITE - PREFORMED THERMOPLASTIC	LIN FT	254
3' x 6' CROSSWALK MARKING - PREFORMED THERMOPLASTIC	SQ FT	1782

SYMBOLS & MATERIALS LEGEND

■ CROSSWALK BLOCK WHITE-POLY PREFORM

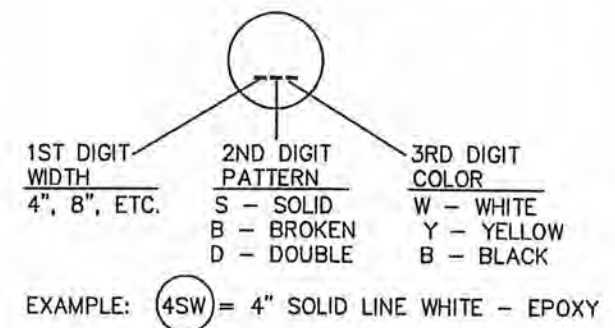
↩ PAVEMENT MESSAGE (LEFT ARROW) POLY PREFORM

STRIPING KEY

○ CIRCLE - EPOXY □ SQUARE - THERMOPLASTIC

△ TRIANGLE - PAINT

⬠ PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING



NO	DATE	BY	CKD	APPR	REVISION

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 5/27/16 LICENSE NO. 20235

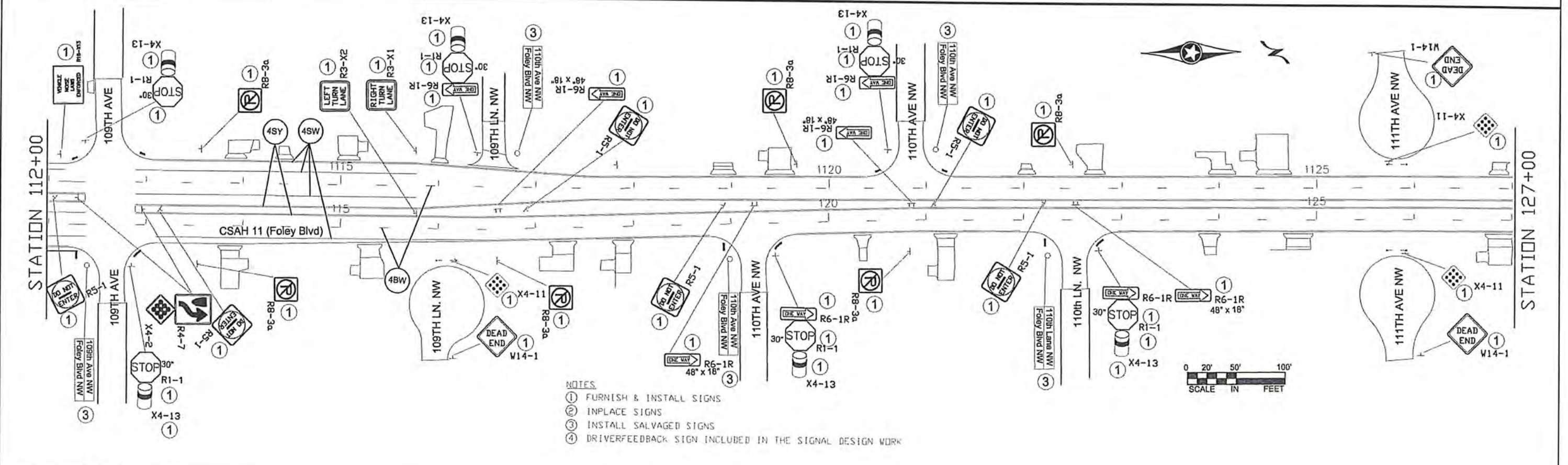
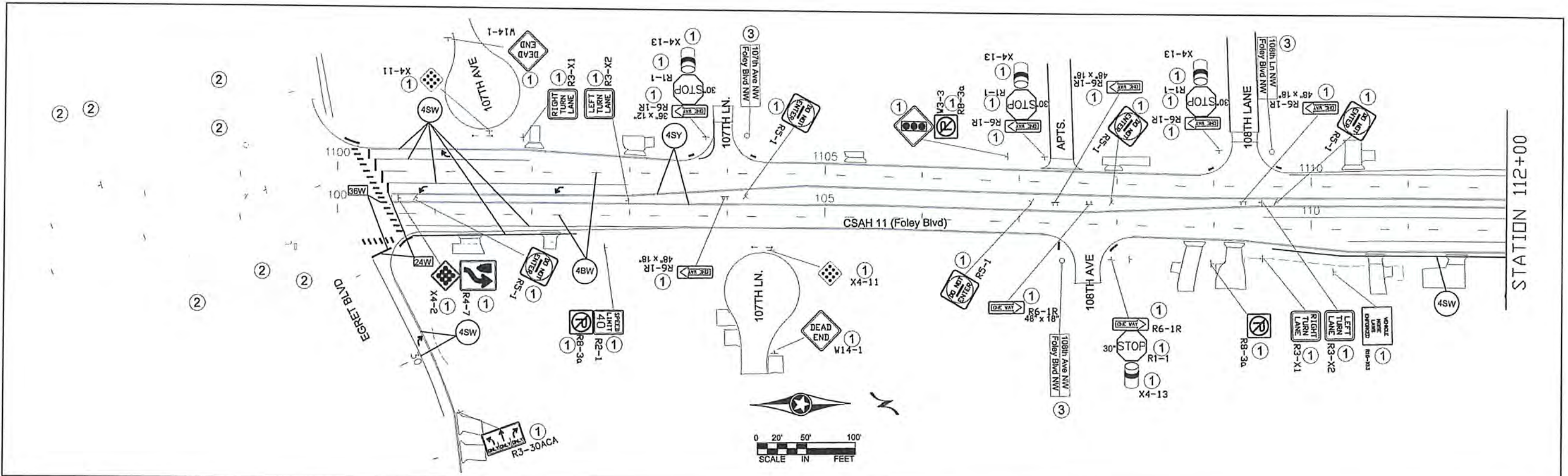
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 DESIGN BY TV DATE 03/16/16
 CHECKED BY RB DATE 03/16/16



**ANOKA COUNTY
HIGHWAY DEPT.**

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

PERMANENT MARKING
TABULATION



- NOTES
- ① FURNISH & INSTALL SIGNS
 - ② INPLACE SIGNS
 - ③ INSTALL SALVAGED SIGNS
 - ④ DRIVERFEEDBACK SIGN INCLUDED IN THE SIGNAL DESIGN WORK

NO	DATE	BY	CKD	APPR	REVISION

NAME: T:\Traffic\dw\CSAH 11 (Foley_Northdate)\from CSAH 7B to CSAH 12.dwg

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: DOUGLAS W. FISCHER

SIGNATURE: *[Signature]*

DATE: 5/31/16 LICENSE NO. 20235

DRAWN BY: TV DATE: 03/15/16

DESIGN BY: TV DATE: 03/15/16

CHECKED BY: RB DATE: 03/15/16

ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034

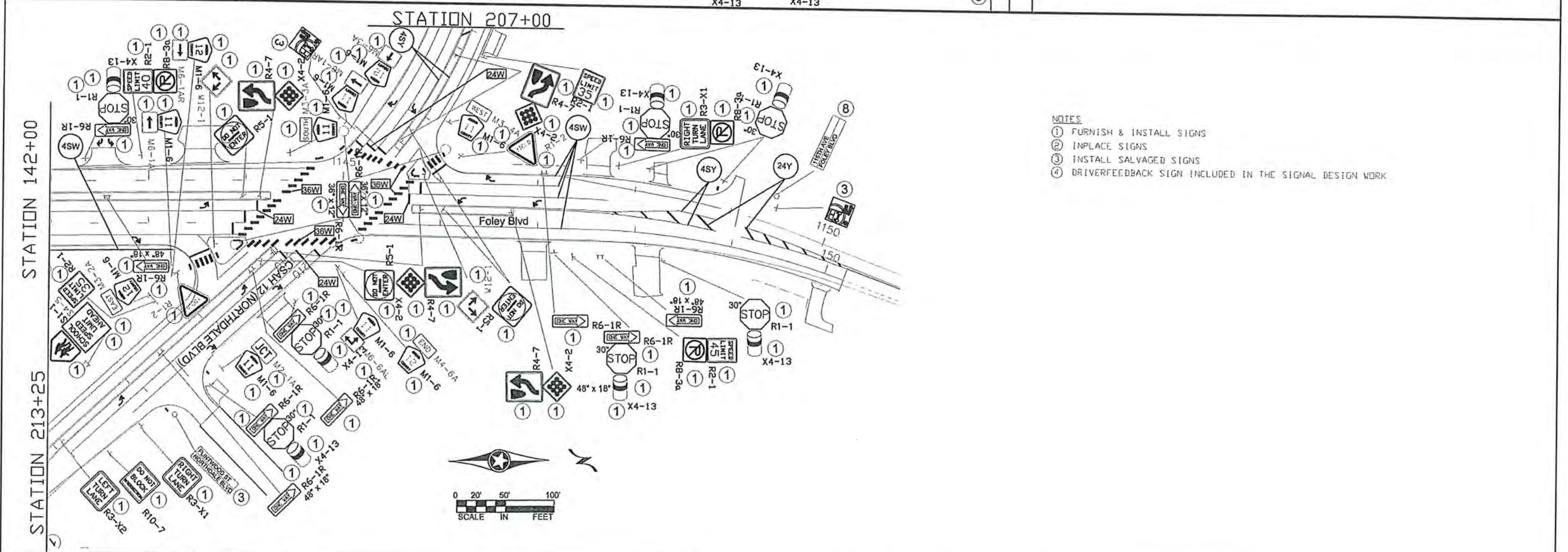
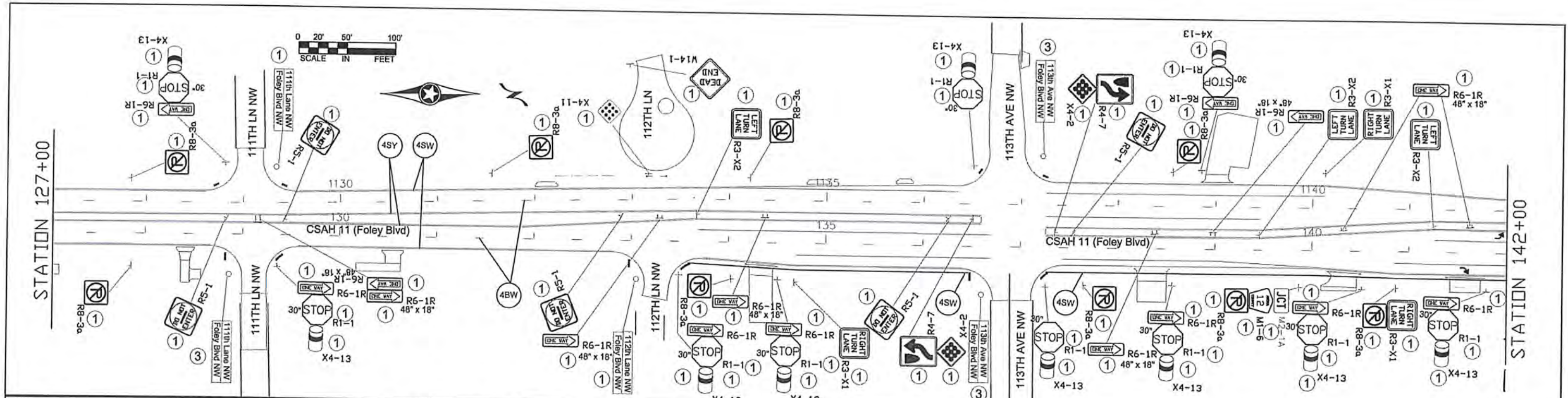
STATE PROJECT NO. 114-020-050

CITY PROJECT NO. 13-10

COUNTY PROJECT NO.

PERMANENT STRIPING & SIGNING PLAN

Sheet 141 of 298 Sheets



- NOTES
- ① FURNISH & INSTALL SIGNS
 - ② INPLACE SIGNS
 - ③ INSTALL SALVAGED SIGNS
 - ④ DRIVERFEEDBACK SIGN INCLUDED IN THE SIGNAL DESIGN WORK

NO	DATE	BY	CKD	APPR	REVISION
NAME: T:\Traffic\dwg\CSAH 11 (Foley_Northdale)\From CSAH 78 to CSAH 12.dwg					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: DOUGLAS W. FISCHER

SIGNATURE: *[Signature]*

DATE: 3/31/16 LICENSE NO. 20235

DRAWN BY: TV DATE: 03/15/16

DESIGN BY: TV DATE: 03/15/16

CHECKED BY: RB DATE: 03/15/16

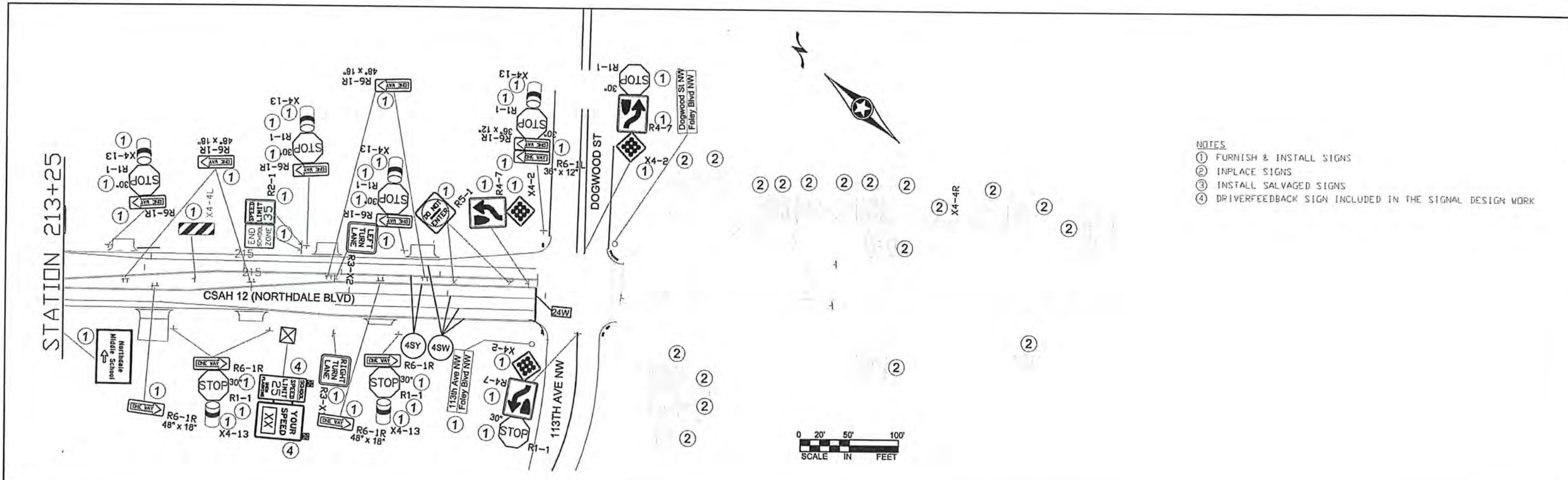


ANOKA COUNTY
HIGHWAY DEPT.

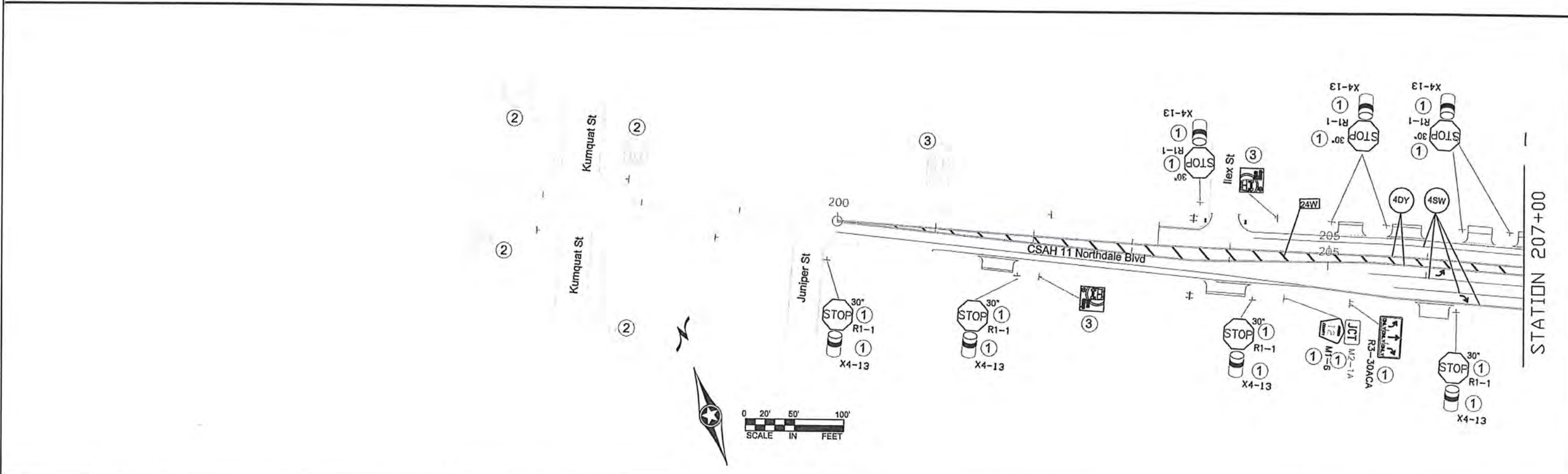
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 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

PERMANENT STRIPING & SIGNING PLAN

Sheet 142 of 298 Sheets



- NOTES
- ① FURNISH & INSTALL SIGNS
 - ② INPLACE SIGNS
 - ③ INSTALL SALVAGED SIGNS
 - ④ DRIVERFEEDBACK SIGN INCLUDED IN THE SIGNAL DESIGN WORK



NO	DATE	BY	CKD	APPR	REVISION

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DATE: 5/27/16 LICENSE NO. 20235

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CHECKED BY RB DATE 03/15/16



ANOKA COUNTY
HIGHWAY DEPT.

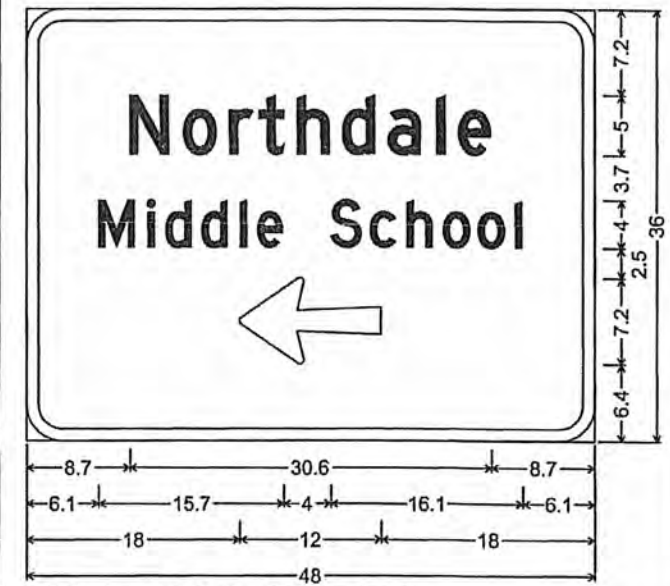
STATE PROJECT NO. 002-611-034
STATE PROJECT NO. 114-020-050
CITY PROJECT NO. 13-10
COUNTY PROJECT NO.

PERMANENT STRIPING & SIGNING PLAN

Sheet 143 of 298 Sheets

SIGN PANELS TYPE C							G
M.U.T.C.D. CODE	SIZE	INSERT	QUANTITY	SQ FT PANEL AREA	SQ FT TOTAL AREA	MOUNTING POST PER INSTALLATION	MOUNTING HEIGHT
R6-1L	36" X 12"		1	3.00	3.00		
R6-1R	36" X 12"		27	3.00	81.00		
R1-1	30" X 30"		43	6.25	268.75	1	7.0'
X4-13			43	1.31	56.23		
R1-2	36" x 36"		2	9.00	18.00	1	7.0'
R2-1	24" X 30"		3	5.00	15.00	1	7.0'
R2-1	24" X 30"		2	5.00	10.00	1	7.0'
R8-3a	24" X 24"		2	4.00	8.00		
R2-1	24" X 30"		1	5.00	5.00	1	7.0'
R2-1	24" X 24"		1	4.00	4.00		
R3-30ACA	54" X 30"		2	11.25	22.50	1	7.0'
R3-X1	30" X 30"		9	6.25	56.25	1	7.0'
R2-1	24" X 24"		2	4.00	8.00		
R3-X2	30" X 30"		9	6.25	56.25	1	7.0'
R1-1	30" X 30"		2	6.25	12.50		
R4-7	24" X 30"		15	5.00	75.00	1	7.0'
X4-2	18" X 18"		15	2.25	33.75		
R5-1	30" X 30"		22	6.25	137.50	1	7.0'
R6-1L	36" X 12" 48" X 18"		8 27	3.00 6.00	24.00 162.00	PLACE ON SIGNAL POLES 1	7.0'
R8-3a	24" X 24"		14	4.00	56.00	1	7.0'
R10-7	30" X 30"		1	6.25	6.25	1	7.0'
R16-X13	24" X 24"		2	4.00	8.00	1	7.0'
W3-3	36" X 36"		1	9.00	9.00		
R8-3a	24" x 24"		1	4.00	4.00	2	7.0'
W12-1	36" X 36"		2	6.25	12.50	1	4.0'

SIGN PANELS TYPE C							G
M.U.T.C.D. CODE	SIZE	INSERT	QUANTITY	SQ FT PANEL AREA	SQ FT TOTAL AREA	MOUNTING POST PER INSTALLATION	MOUNTING HEIGHT
S1-1	36" X 36"		1	9.00	9.00	2	7.0'
S4-5	36" X 36"		1	9.00	9.00		
S5-2	24" X 30"		1	5.00	5.00	1	7.0'
	48" X 36"		1	12.00	12.00	2	7.0'
X4-4L	12" X 36"		1	3.00	3.00	1	7.0'
M2-1A	21" X 15"		1	2.18	2.18		
M3-3A	24" X 12"		1	2.00	2.00		
M3-4A	24" X 12"		1	2.00	2.00		
M1-6	24" X 24"		6	4.00	24.00	1	7.0'
MG-1AL	21" X 15"		1	2.18	2.18		
MG-1AR	21" X 15"		1	2.18	2.18		
MG-6AL	21" X 15"		1	2.18	2.18		
M4-6A	24" X 12"		1	2.00	2.00		
M2-1A	21" X 15"		1	2.18	2.18		
M3-2A	24" X 12"		1	2.00	2.00		
M1-6	24" X 24"		6	4.00	24.00	1	7.0'
MG-1AR	21" X 15"		1	2.18	2.18		
MG-3A	21" X 15"		1	2.18	2.18		
R8-3a	24" X 24"		1	4.00	4.00		
W14-1	36" X 36"		6	9.00	54.00	1	7.0'
X4-11	18" X 18"		6	2.25	13.5	1	7.0'
TOTAL			294		1312.24		

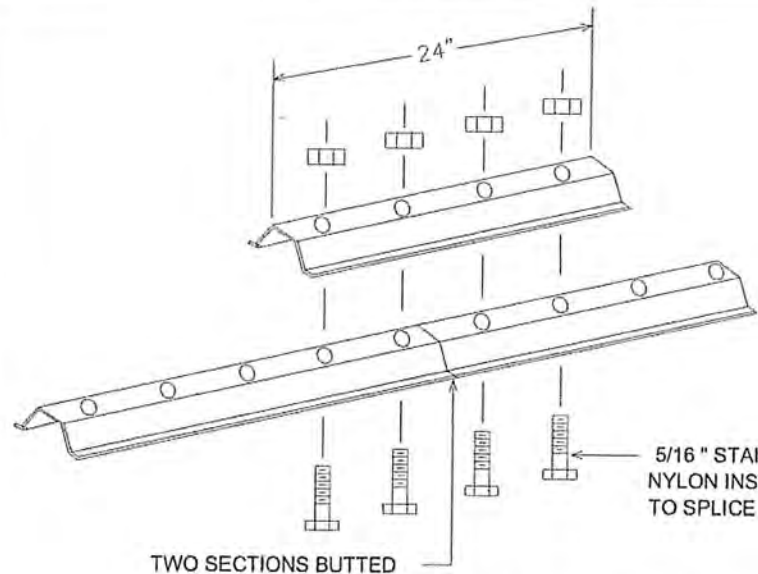


3.0" Radius, 1.0" Border, White on Green;
 [Northdale] D; [Middle School] D; Arrow 12 - 12.0" 180°;

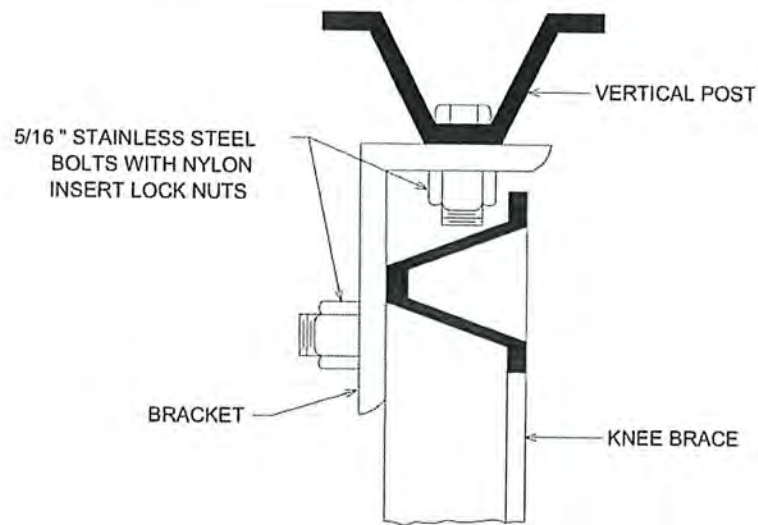
- CONSTRUCTION NOTES:
 1. FOR RELOCATING SIGNS DURING CONSTRUCTION, AS DIRECTED BY THE ENGINEER, RELOCATION INCIDENTAL TO TRAFFIC CONTROL.
 2. *ONE WAY* ON SIGNAL POLES AND DRIVER FEEDBACK SIGN REMOVAL INCIDENTAL TO TRAFFIC SIGNAL REMOVAL.

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: DOUGLAS W. FISCHER SIGNATURE: DATE: 5/27/16 LICENSE NO. 20235					DRAWN BY: TV DATE: 03/15/16 DESIGN BY: TV DATE: 03/15/16 CHECKED BY: RB DATE: 03/15/16	ANOKA COUNTY HIGHWAY DEPT.	STATE PROJECT NO. 002-611-034 STATE PROJECT NO. 114-020-050 CITY PROJECT NO. 13-10 COUNTY PROJECT NO.	PERMANENT STRIPING & SIGNING PLAN Sheet 144 of 298 Sheets	
NO	DATE	BY	CKD	APPR	REVISION				

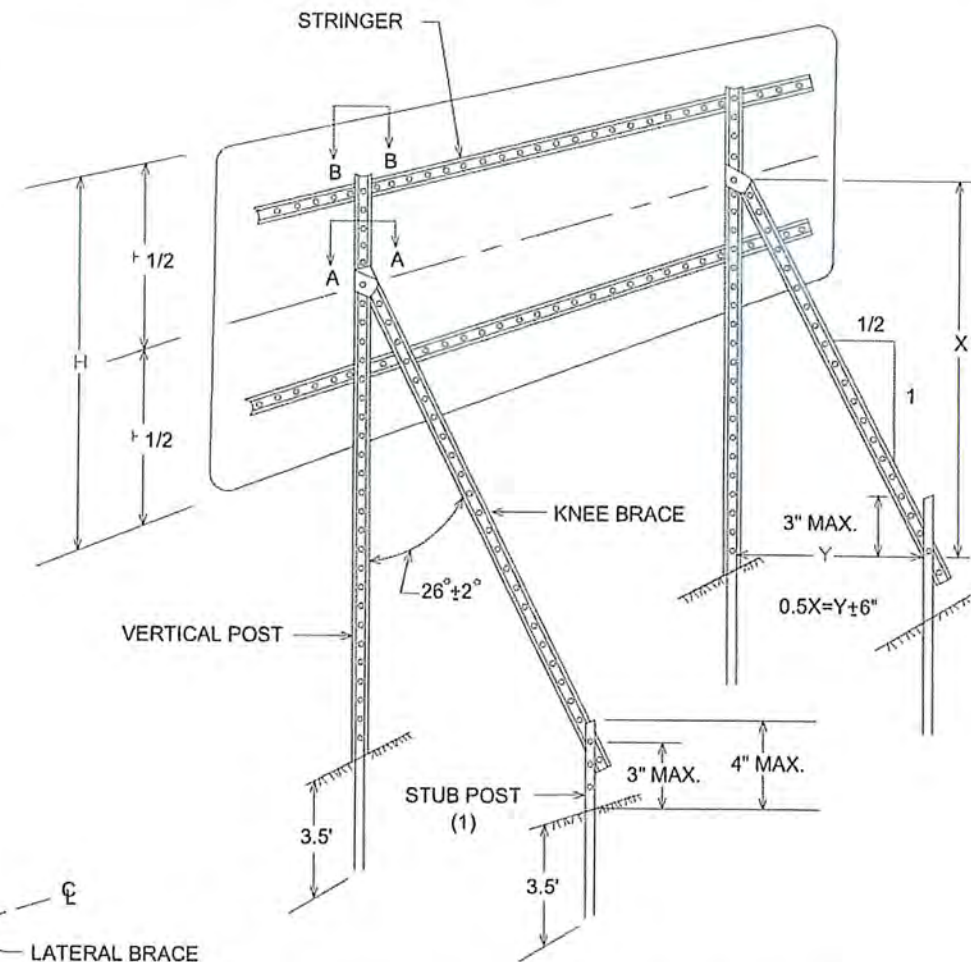
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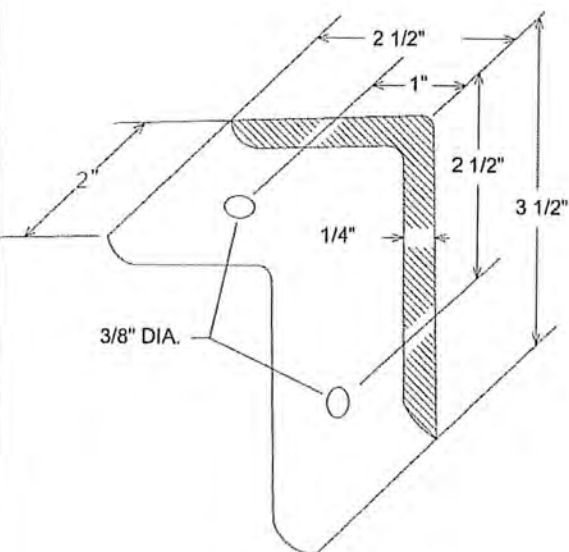
LATERAL BRACE OR STRINGER
SPLICE DETAIL (EXPLODED VIEW)



SECTION A-A

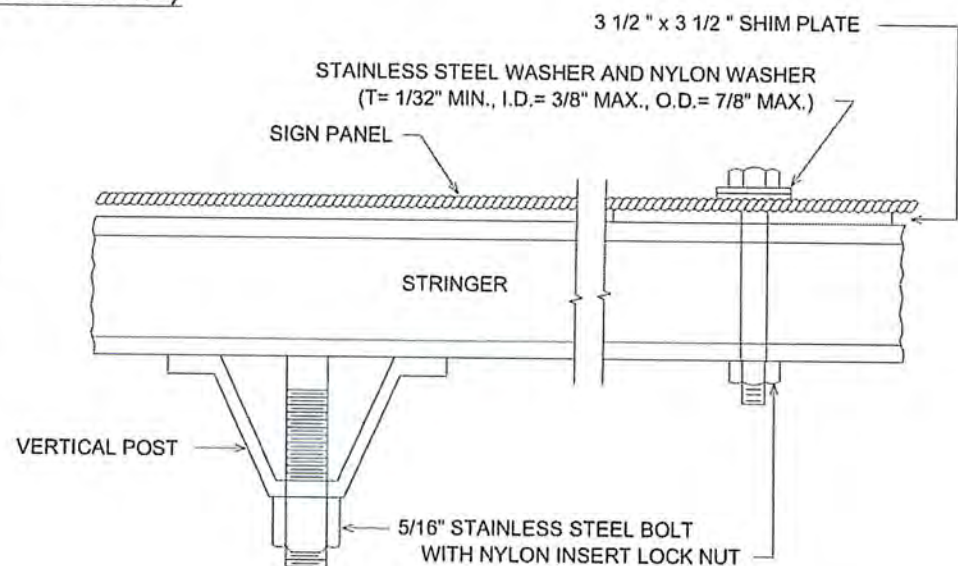


TYPICAL "A-FRAME" INSTALLATION
TYPE "D" SIGNS

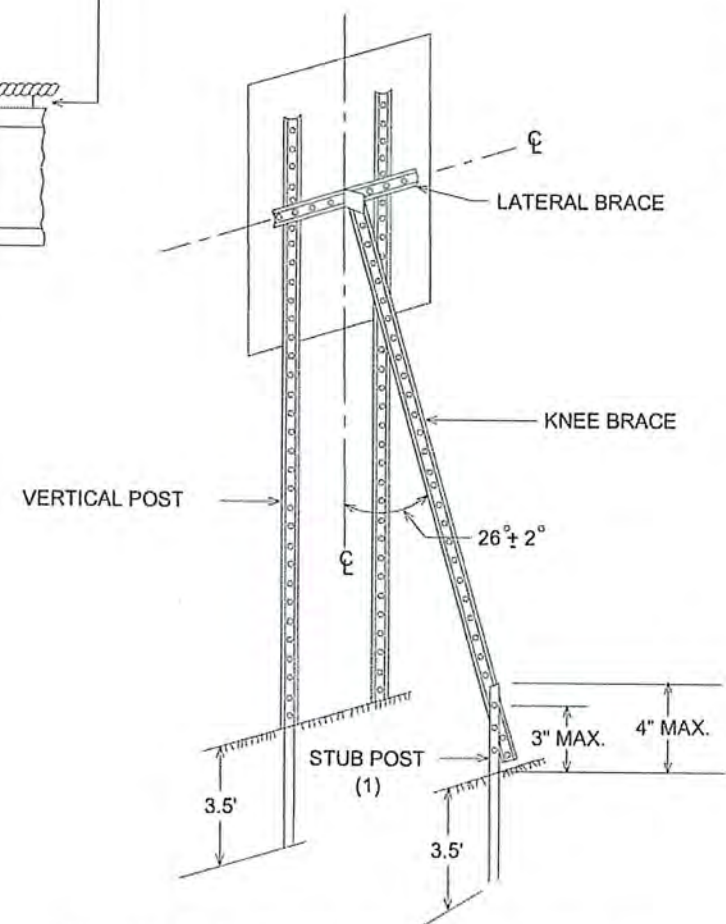


A-FRAME BRACKET

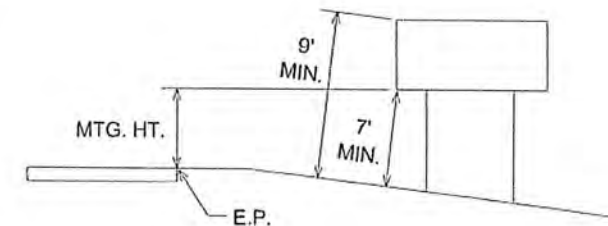
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SECTION B-B



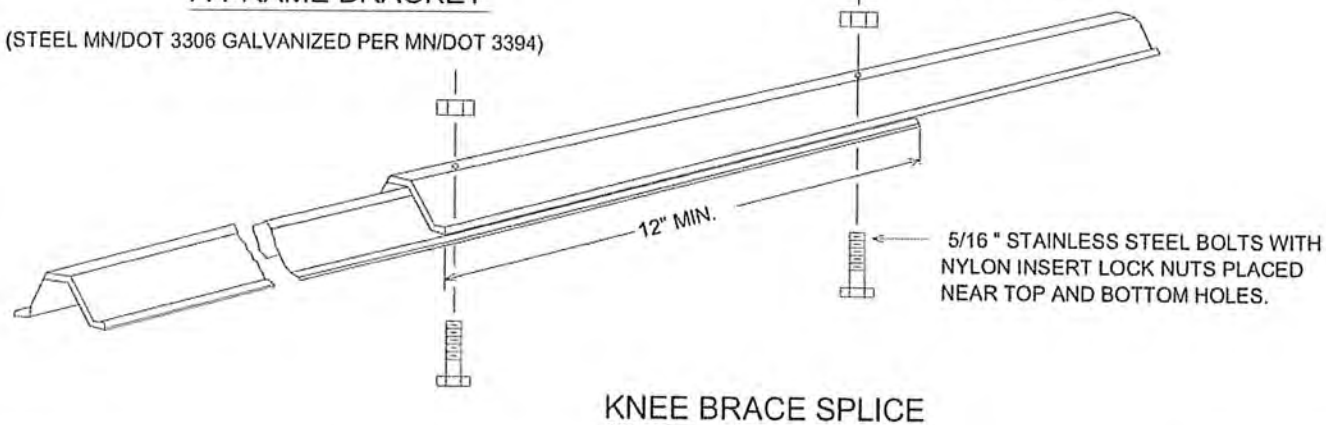
TYPICAL "A-FRAME" INSTALLATION
TYPE "C" SIGNS



TYPICAL MOUNTING

(1) OFFSET STUB POST 1' TOWARD ROADWAY
RELATIVE TO VERTICAL POST.

TYPE C & D SIGN
STRUCTURAL DETAILS



KNEE BRACE SPLICE

NO	DATE	BY	CKD	APPR	REVISION

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 5/21/16 LICENSE NO. 20235

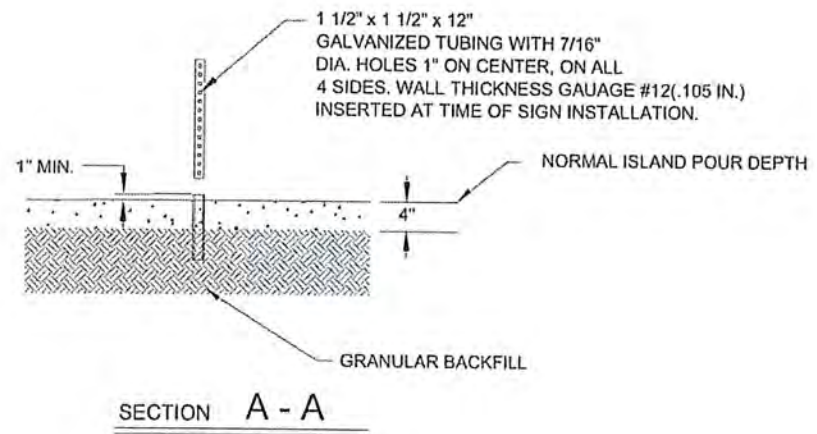
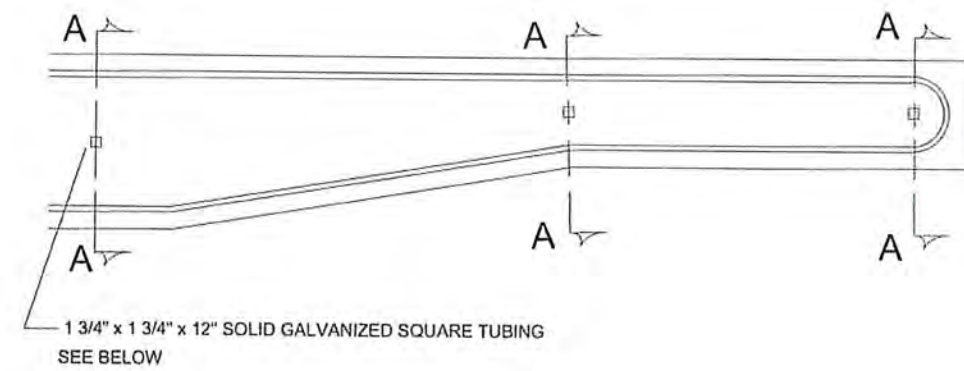
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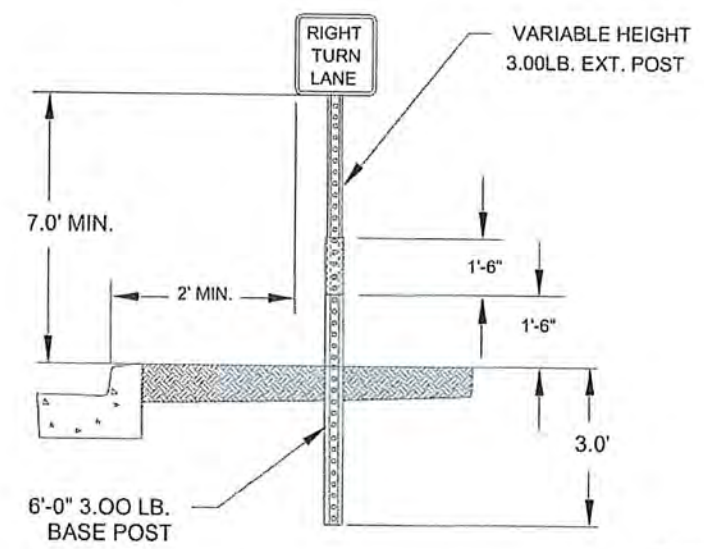
ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034
 STATE PROJECT NO. 114-020-050
 CITY PROJECT NO. 13-10
 COUNTY PROJECT NO. _____

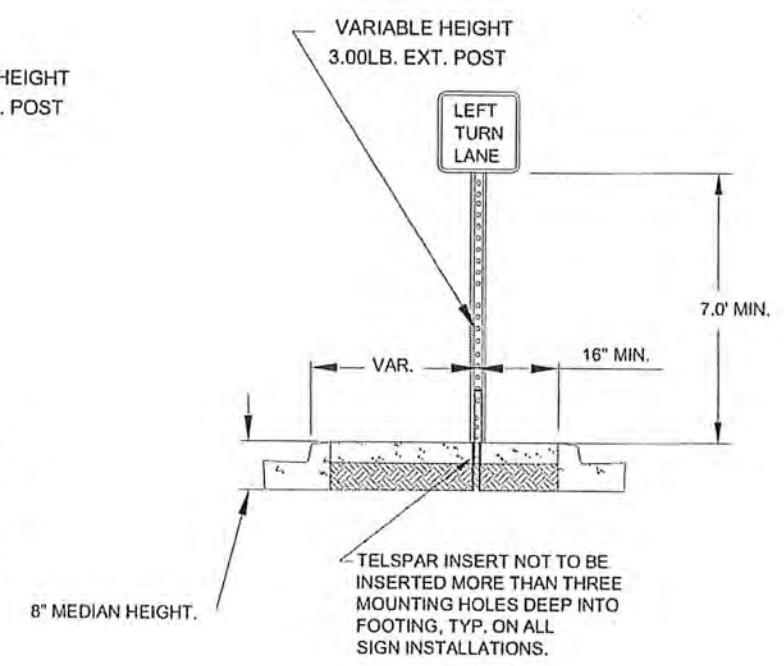
SIGNING & STRIPING DETAILS
 Sheet 145 of 298 Sheets



GROUND POST MOUNT SIGN
INSTALLATION TYPICAL



ISLAND MOUNT BREAK-AWAY SIGN
INSTALLATION TYPICAL



NO	DATE	BY	CHKD	APPR	REVISION

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PRINT NAME: DOUGLAS W. FISCHER
 SIGNATURE: *[Signature]*
 DATE: 3/16/16 LICENSE NO. 20235

DRAWN BY TV DATE 3/16/16
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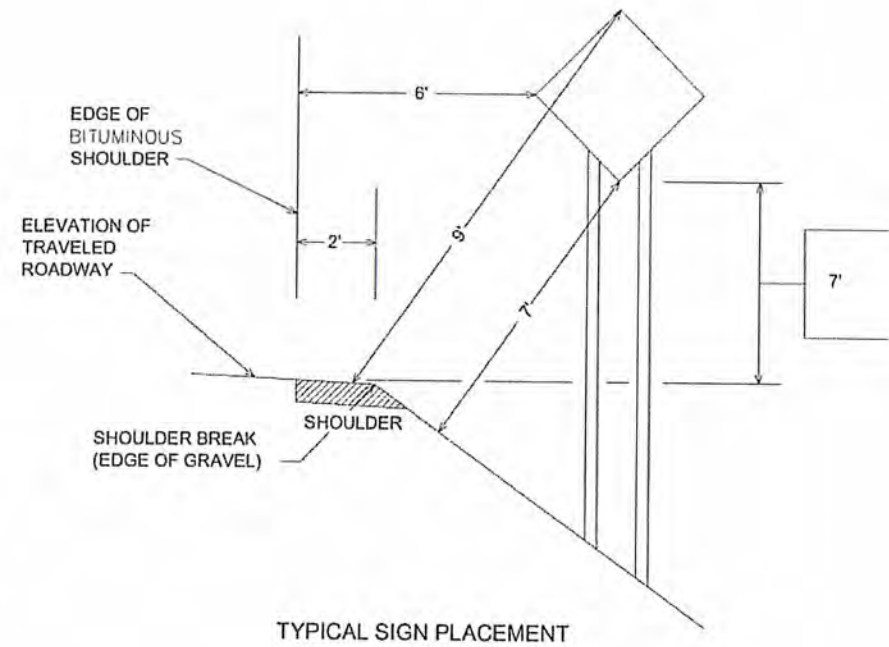


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STATE PROJECT NO. 002-611-034
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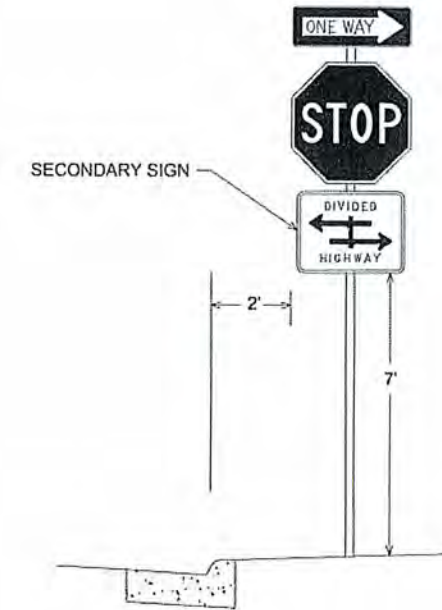
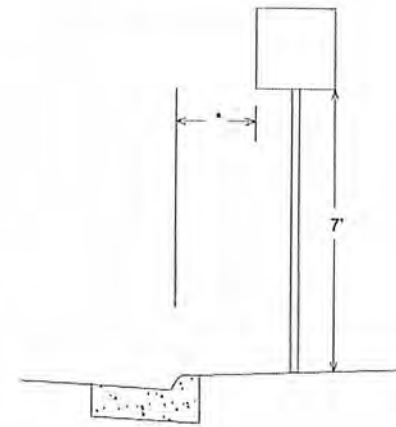
SIGNING & STRIPING DETAILS

RURAL



URBAN

2' - NARROW BOULEVARD (< 8' WIDE)
6' - WIDE BOULEVARD



NOTE:

- ALL DIMENSIONS ARE MINIMUMS
- MAINTAIN 2' CLEAR FROM SIGNS TO BITUMINOUS TRAIL

NO	DATE	BY	CKD	APPR	REVISION

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PRINT NAME: DOUGLAS W. FISCHER

SIGNATURE: *[Signature]*

DATE: 5/27/16 LICENSE NO. 20235

DRAWN BY TV DATE 3/16/16

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CHECKED BY RB DATE 3/16/16

ANOKA COUNTY
HIGHWAY DEPT.

STATE PROJECT NO. 002-611-034

STATE PROJECT NO. 114-020-050

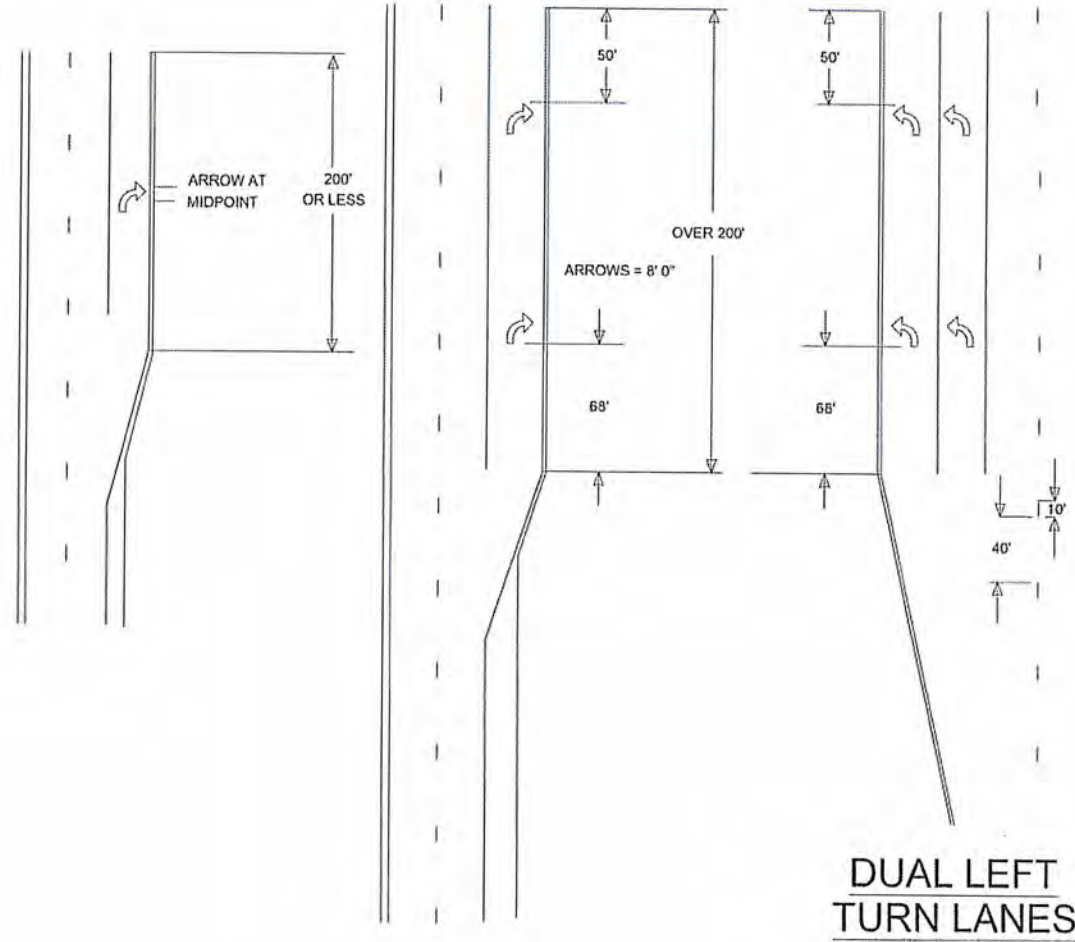
CITY PROJECT NO. 13-10

COUNTY PROJECT NO. _____

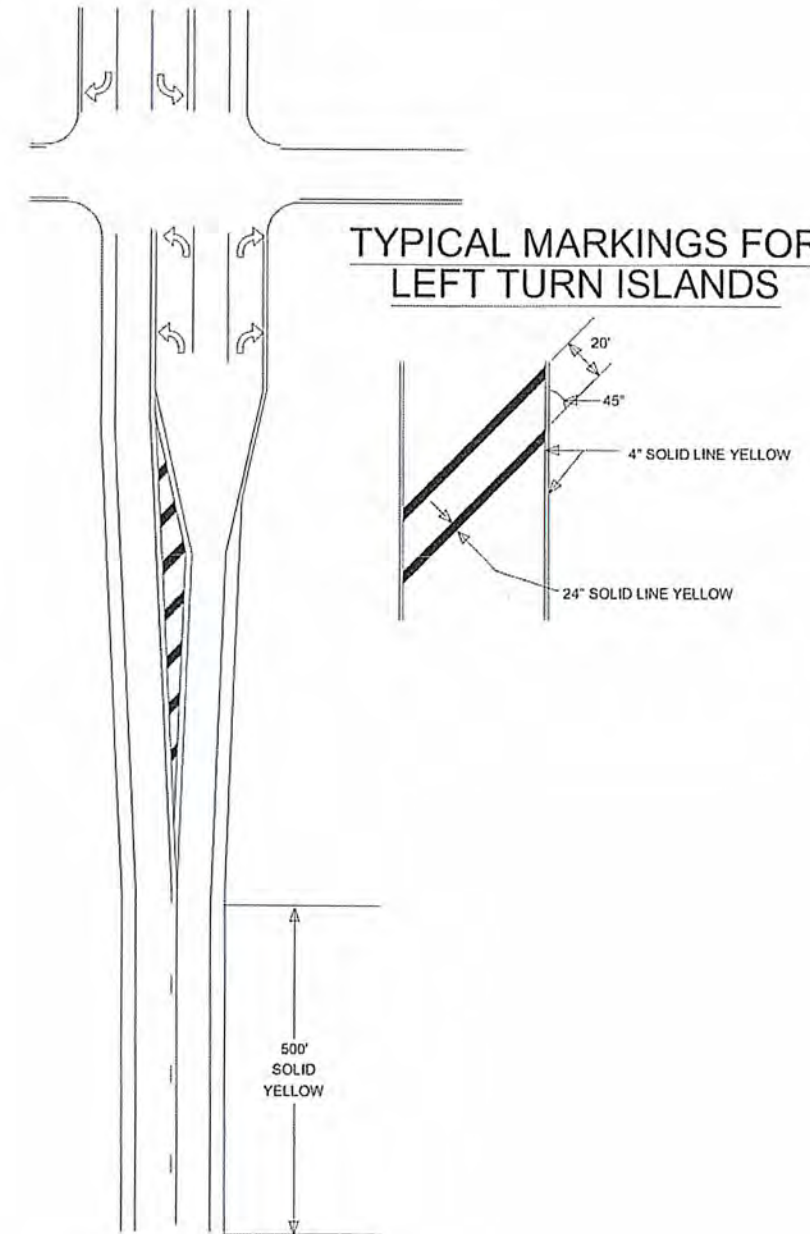
SIGNING & STRIPING DETAILS

Sheet 147 of 298 Sheets

**TYPICAL MESSAGE PLACEMENT
FOR TURN LANES**



**TYPICAL MARKINGS FOR
LEFT TURN ISLANDS**



NO	DATE	BY	CKD	APPR	REVISION

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: **DOUGLAS W. FISCHER**

SIGNATURE: *[Signature]*

DATE: **5/27/16** LICENSE NO. **20235**

DRAWN BY **TV** DATE **3/16/16**

DESIGN BY **TV** DATE **3/16/16**

CHECKED BY **RG** DATE **3/16/16**



**ANOKA COUNTY
HIGHWAY DEPT.**

STATE PROJECT NO. **002-611-034**

STATE PROJECT NO. **114-020-050**

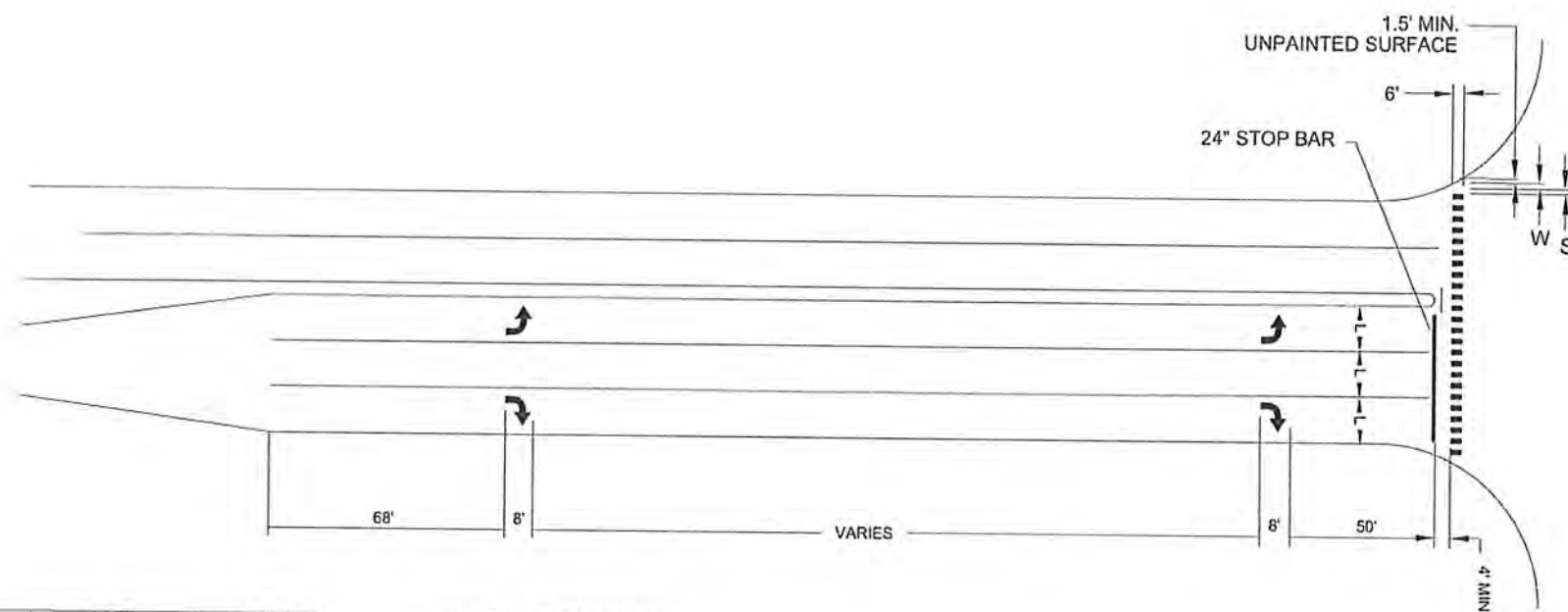
CITY PROJECT NO. **13-10**

COUNTY PROJECT NO. _____

SIGNING & STRIPING DETAILS

Sheet **148** of **298** Sheets

MARKINGS FOR PEDESTRIAN CROSSWALKS



(L)	(W)	(S)
WIDTH OF INSIDE LANE	WIDTH OF PAINTED AREAS	WIDTH OF SPACE
9'	2.0'	2.5'
10'	2.5'	2.5'
11'	2.5'	3.0'
12'	3.0'	3.0'
13'	3.0'	3.5'

NOTES: CROSSWALKS:

- 1.) PAINTED AREAS ARE TO BE CENTERED ON CENTER AND LANE LINES, EVEN IF INTERSECTION IS NOT ALIGNED.
- 2.) LOCATION OF ZEBRA CROSSWALKS AND STOP BARS, SIGNAL LOOPS AND PED RAMPS ARE APPROXIMATE. FINAL LOCATIONS ARE TO BE DETERMINED AND FIELD VERIFIED DURING CONSTRUCTION BY THE FIELD ENGR.
- 3.) ZEBRA CROSSWALKS ARE TO BE PARALLEL TO THE DRIVING LANE OR LANES. EVEN IF THE STREET IS ON AN ANGLE TO THE INTERSECTION.
- 4.) A MIN. OF 1.5' CLEAR DISTANCE MUST BE LEFT ADJACENT TO THE CURB. IF LAST PAINTED AREA FALLS INTO THIS AREA, IT MUST BE OMITTED.
- 5.) ON TWO LANE STREETS, USE SPACING SHOWN FOR AN 11' INSIDE LANE.

NOTES & GUIDELINES

GENERAL INFORMATION:

THE ENGINEER'S INVOLVEMENT IN THE APPLICATION OF THE MATERIAL SHALL BE LIMITED TO FIELD CONSULTATION AND INSPECTION. THE CONTRACTOR WILL PLACE NECESSARY "SPOTTING" AT APPROPRIATE POINTS TO PROVIDE HORIZONTAL CONTROL FOR STRIPING AND TO DETERMINE NECESSARY STARTING AND CUTOFF POINTS. LONGITUDINAL JOINTS, PAVEMENT EDGES AND EXISTING MARKINGS MAY SERVE AS HORIZONTAL CONTROL WHEN SO DIRECTED.

EDGE LINES AND LANE LINES ARE TO BE BROKEN ONLY AT INTERSECTIONS WITH PUBLIC ROADS AND AT PRIVATE ENTRANCES IF THEY ARE CONTROLLED BY A YIELD SIGN, STOP SIGN OR TRAFFIC SIGNAL. THE BREAK POINT IS TO BE AT THE START OF THE RADIUS FOR THE INTERSECTION OR AT MARKED STOP LINES OR CROSSWALKS.

A TOLERANCE OF 1/4 INCH UNDER OR 1/4 INCH OVER THE SPECIFIED WIDTH WILL BE ALLOWED FOR STRIPING PROVIDED THE VARIATION IS GRADUAL AND DOES NOT DETRACT FROM THE GENERAL APPEARANCE. BROKEN LINE SEGMENTS MAY VARY UP TO ONE-HALF FOOT FROM THE SPECIFIED LENGTHS PROVIDED THE OVER AND UNDER VARIATIONS ARE REASONABLY COMPENSATORY. ALIGNMENT DEVIATIONS FROM THE CONTROL GUIDE SHALL NOT EXCEED 1 INCH. MATERIAL SHALL NOT BE APPLIED OVER LONGITUDINAL JOINTS. ESTABLISHMENT OF APPLICATION TOLERANCES SHALL NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO COMPLY AS CLOSELY AS PRACTICABLE WITH THE PLANNED DIMENSIONS.

EPOXY:

THE ROAD SURFACE SHALL BE CLEANED AT THE DIRECTION OF THE ENGINEER JUST PRIOR TO APPLICATION. PAVEMENT CLEANING SHALL CONSIST OF AT LEAST BRUSHING WITH A ROTARY BROOM (NON-METALLIC) OR AS RECOMMENDED BY THE MATERIAL MANUFACTURER AND ACCEPTABLE TO THE ENGINEER. NEW PORTLAND CEMENT CONCRETE SURFACES SHALL BE SANDBLAST CLEANED TO REMOVE ANY SURFACE TREATMENTS AND/OR LAITANCE. ON LOW SPEED (SPEED LIMIT 35 OR LESS) URBAN PORTLAND CEMENT CONCRETE ROADWAYS, SANDBLAST CLEANING SHALL BE USED FOR ALL EPOXY PAVEMENT MARKINGS.

THE EPOXY MARKING APPLICATION SHALL IMMEDIATELY FOLLOW THE PAVEMENT CLEANING. GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE EPOXY RESIN LINE TO PROVIDE AN IMMEDIATE NO-TRACK SYSTEM.

AN EPOXY RESIN LINE 4" WIDE AND 15 MILL THICKNESS (WET), REQUIRES AN APPLICATION RATE OF ONE (1) GALLON OF COMPONENTS FOR 320 FEET OF LINE. GLASS BEADS SHALL BE APPLIED AT A POUND PER GALLON RATE SUFFICIENT TO ACHIEVE AN ACCEPTABLE NO-TRACK SYSTEM.

OPERATIONS SHALL BE CONDUCTED ONLY WHEN THE ROAD PAVEMENT SURFACE TEMPERATURES ARE 50 DEGREES F° OR GREATER.

PERMANENT PAVEMENT MARKINGS SHALL NOT BE PLACED OVER TEMPORARY TAPE MARKINGS.

PAINT:

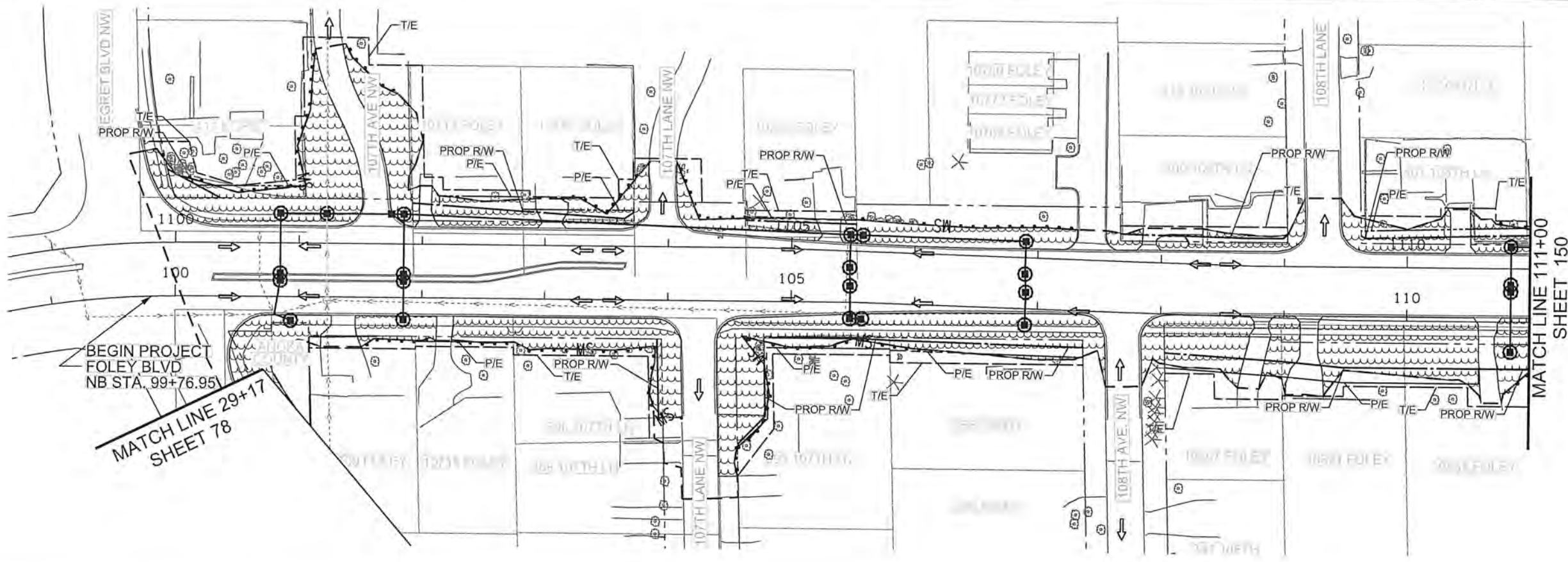
AT THE TIME OF APPLYING THE MARKING MATERIAL, THE APPLICATION AREA SHALL BE FREE OF CONTAMINATION. THE CONTRACTOR SHALL CLEAN THE ROADWAY SURFACE PRIOR TO THE LINE APPLICATION IN A MANNER AND TO THE EXTENT REQUIRED BY THE ENGINEER.

GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE PAINT LINE.

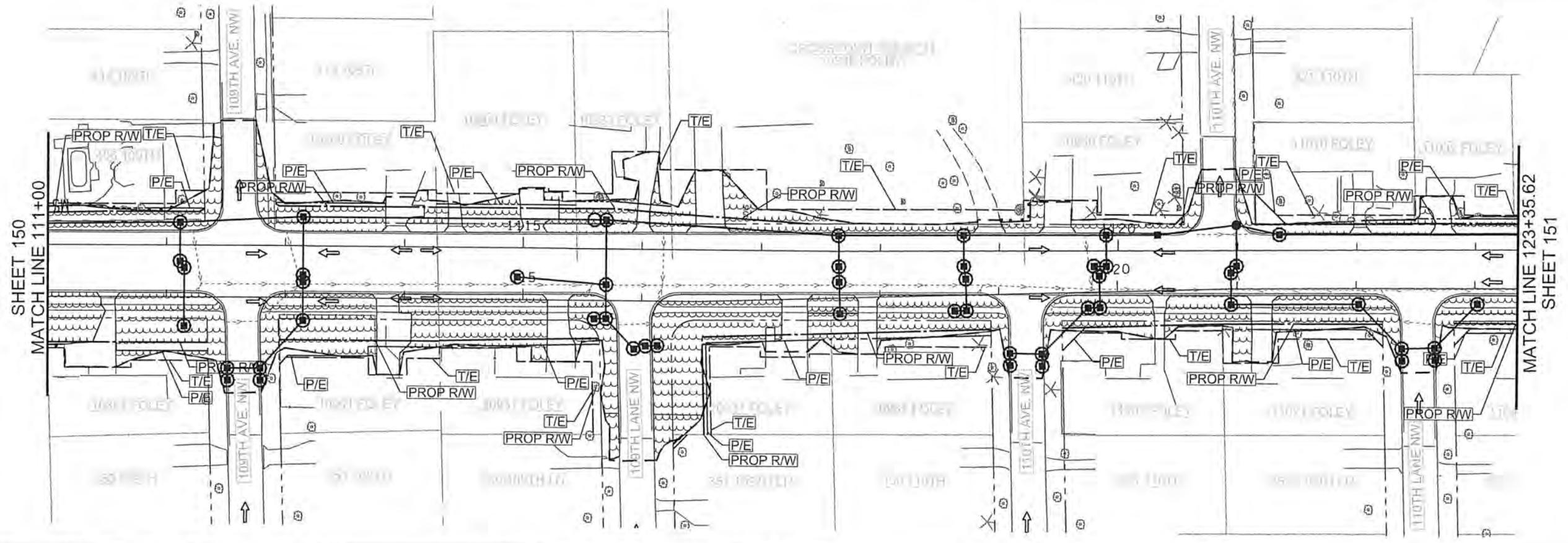
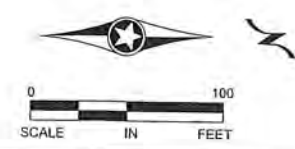
EXCEPT WHEN USED AS A TEMPORARY MARKING, PAVEMENT MARKINGS SHALL ONLY BE APPLIED IN SEASONABLE WEATHER WHEN AIR TEMPERATURE IS 50°F OR HIGHER AND SHALL NOT BE APPLIED WHEN THE WIND OR OTHER CONDITIONS CAUSE A FILM OF DUST TO BE DEPOSITED ON THE PAVEMENT SURFACE AFTER CLEANING AND BEFORE THE MARKING MATERIAL CAN BE APPLIED.

THE FILLING OF TANKS, POURING OF MATERIALS OR CLEANING OF EQUIPMENT SHALL NOT BE PERFORMED ON UNPROTECTED PAVEMENT SURFACES UNLESS ADEQUATE PROVISIONS ARE MADE TO PREVENT SPILLAGE OF MATERIAL.

	I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRINT NAME: DOUGLAS W. FISCHER SIGNATURE: <i>[Signature]</i> DATE: 5/27/16 LICENSE NO. 20235	DRAWN BY TV DATE 3/18/16 DESIGN BY TV DATE 3/18/16 CHECKED BY RB DATE 3/18/16	 ANOKA COUNTY HIGHWAY DEPT.	STATE PROJECT NO. 002-611-034 STATE PROJECT NO. 114-020-050 CITY PROJECT NO. 13-10 COUNTY PROJECT NO. _____	SIGNING & STRIPING DETAILS Sheet 149 of 298 Sheets
NAME: P:\02-605-18\Base\TRAFFIC\Sign&Stripe_Details.dwg NO. DATE BY CKD APPR REVISION					



- LEGEND**
- PROPOSED CATCH BASIN
 - INPLACE CATCH BASIN
 - PROPOSED MANHOLE
 - INPLACE MANHOLE
 - ▲ PROPOSED APRON
 - ▽ INPLACE APRON
 - PROPOSED STORM SEWER
 - - - INPLACE STORM SEWER
 - MS— SILT FENCE TYPE MACHINE SLICED
 - ⊗ RIPRAP (CLASS II UNLESS OTHERWISE NOTED)
 - INLET PROTECTION
 - SURFACE FLOW ARROW
 - 3333 EROSION CONTROL BLANKET CATEGORY 00



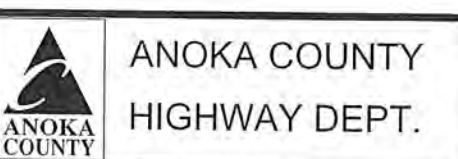
1 OF 4

NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_EC_P1.dgn					
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

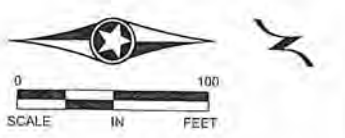
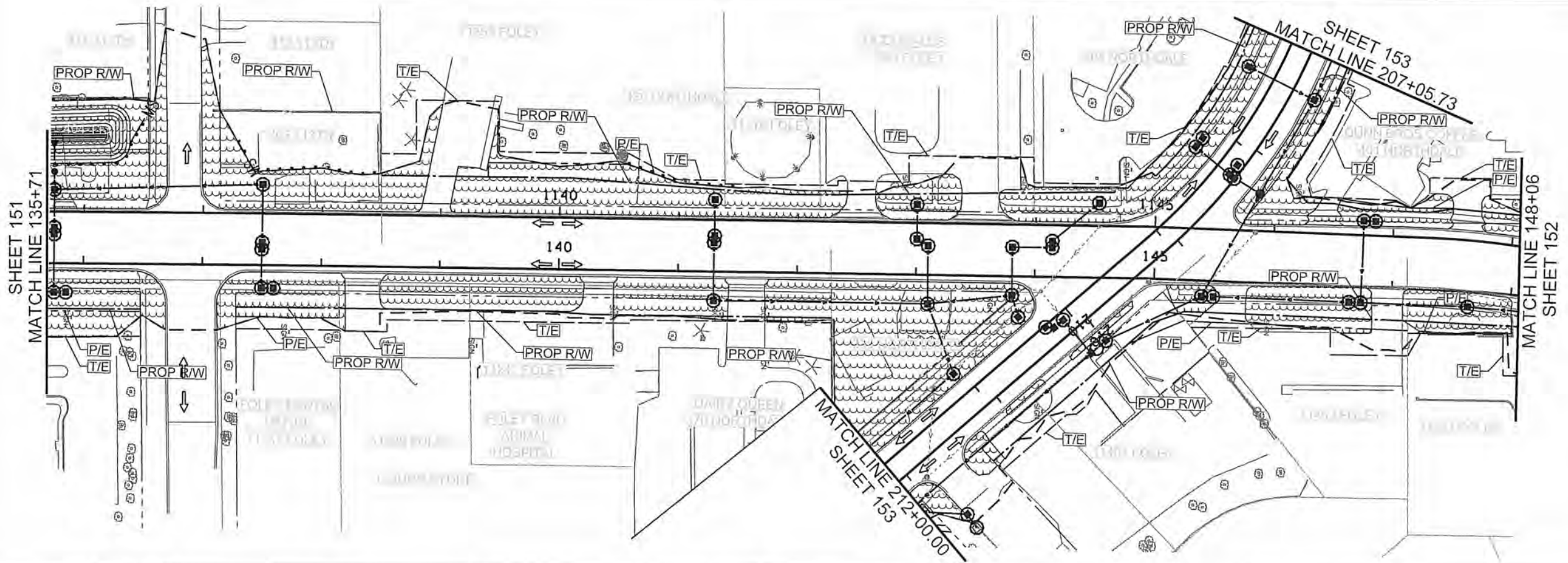
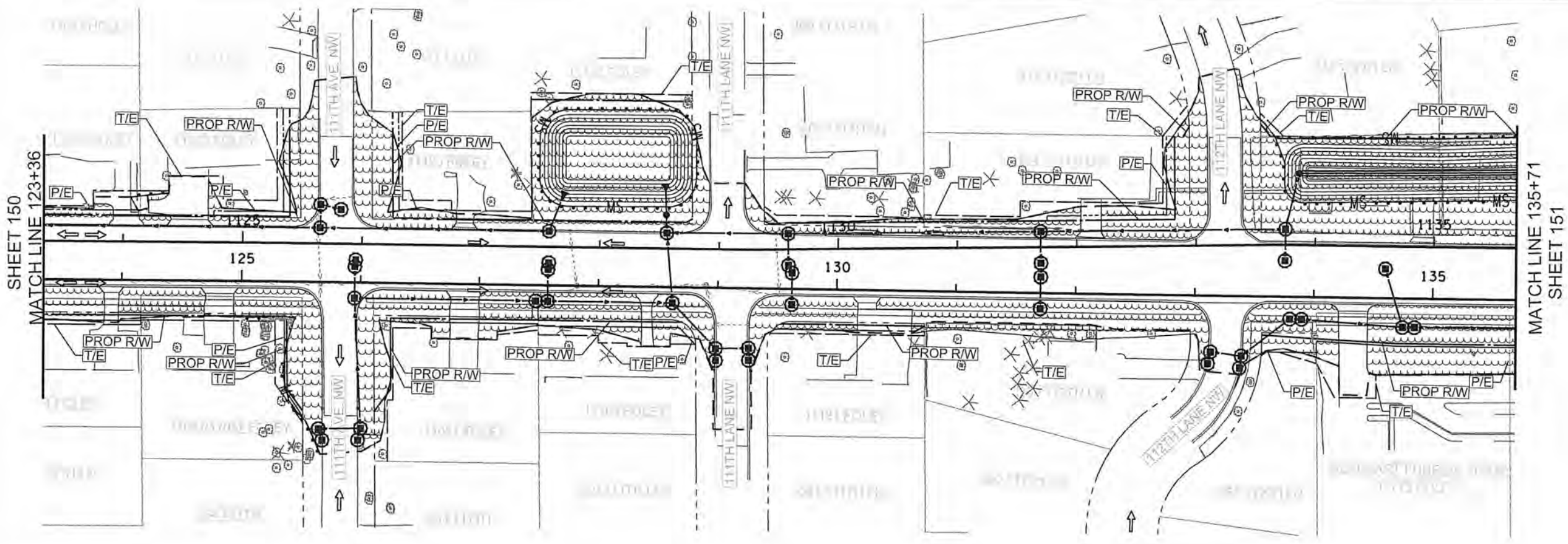
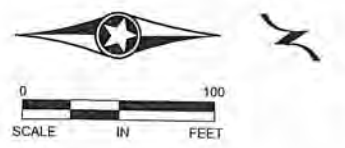
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 CHECKED BY: JEO DATE: 02/27/16



SP 002-611-034
 SP 114-020-050
 CP 13-10

EROSION CONTROL PLAN
 FOLEY BLVD.
 STA 99+76.95 TO 111+00.00
 Sheet 150 of 298 Sheets

- LEGEND**
- PROPOSED CATCH BASIN
 - INPLACE CATCH BASIN
 - PROPOSED MANHOLE
 - INPLACE MANHOLE
 - ▲ PROPOSED APRON
 - ▽ INPLACE APRON
 - PROPOSED STORM SEWER
 - - - INPLACE STORM SEWER
 - MS- SILT FENCE TYPE MACHINE SLICED
 - ⊗ RIPRAP (CLASS II UNLESS OTHERWISE NOTED)
 - INLET PROTECTION
 - SURFACE FLOW ARROW
 - ▨ EROSION CONTROL BLANKET CATEGORY 00



2 OF 4

NO	DATE	BY	CHKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_EC_P2.dgn					
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

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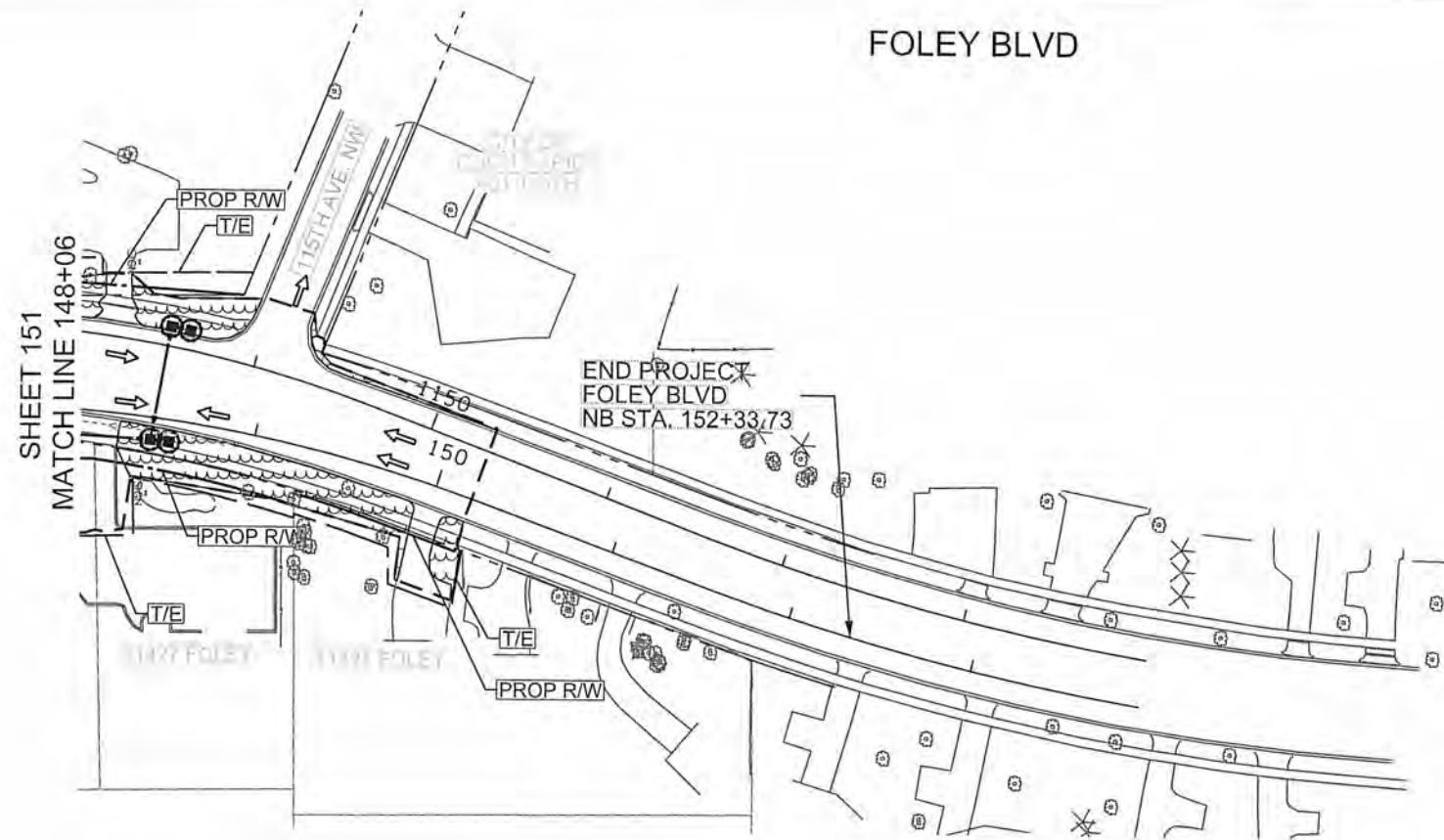
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 CHECKED BY: JEO DATE: 02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

EROSION CONTROL PLAN
 FOLEY BLVD
 STA 123+36 TO 148+06
 Sheet 151 of 298 Sheets

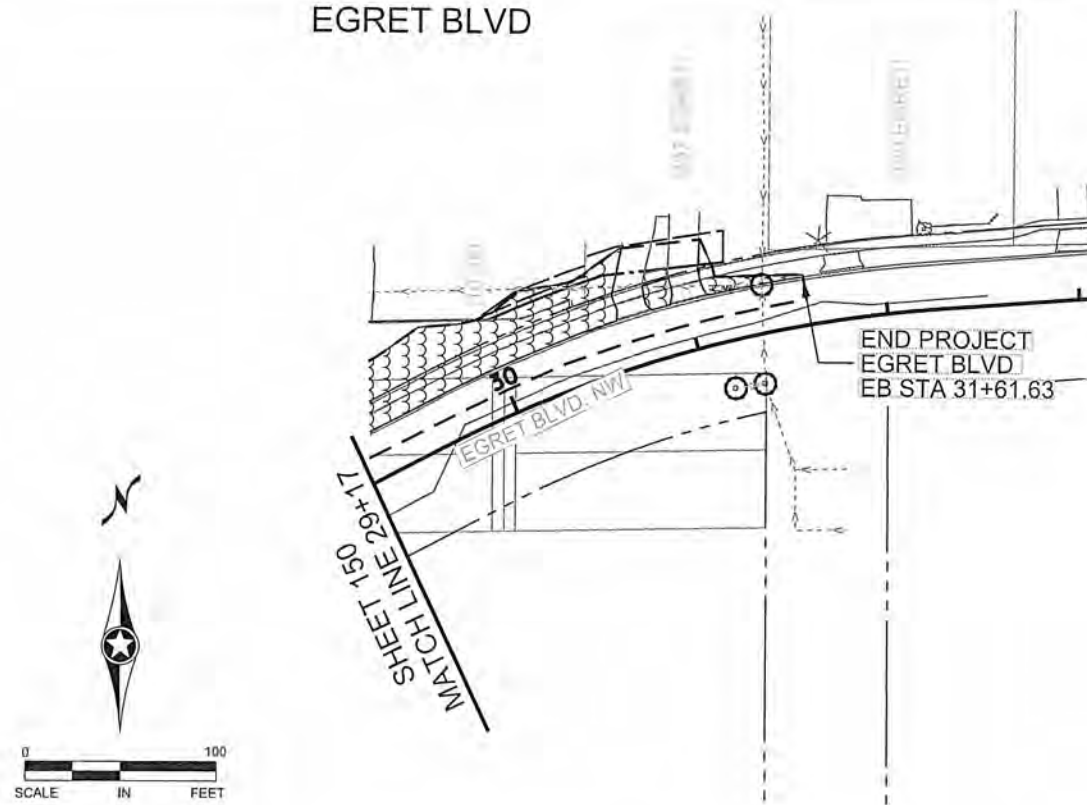
FOLEY BLVD



LEGEND

- PROPOSED CATCH BASIN
- INPLACE CATCH BASIN
- PROPOSED MANHOLE
- INPLACE MANHOLE
- ▼ PROPOSED APRON
- ▽ INPLACE APRON
- PROPOSED STORM SEWER
- INPLACE STORM SEWER
- MS— SILT FENCE TYPE MACHINE SLICED
- ⊗ RIPRAP (CLASS II UNLESS OTHERWISE NOTED)
- INLET PROTECTION
- SURFACE FLOW ARROW
- ▤ EROSION CONTROL BLANKET CATEGORY 00

EGRET BLVD



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-341Plan\002-611-034_EC_P3.dgn 05/13/2016 12:59:35 PM

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GINA M. PIZZO
 SIGNATURE: *[Signature]*
 DATE: 5-19-16 LICENSE NO. 22713

DRAWN BY: DFF DATE: 02/27/16
 DESIGN BY: DFF DATE: 02/27/16
 CHECKED BY: JEO DATE: 02/27/16







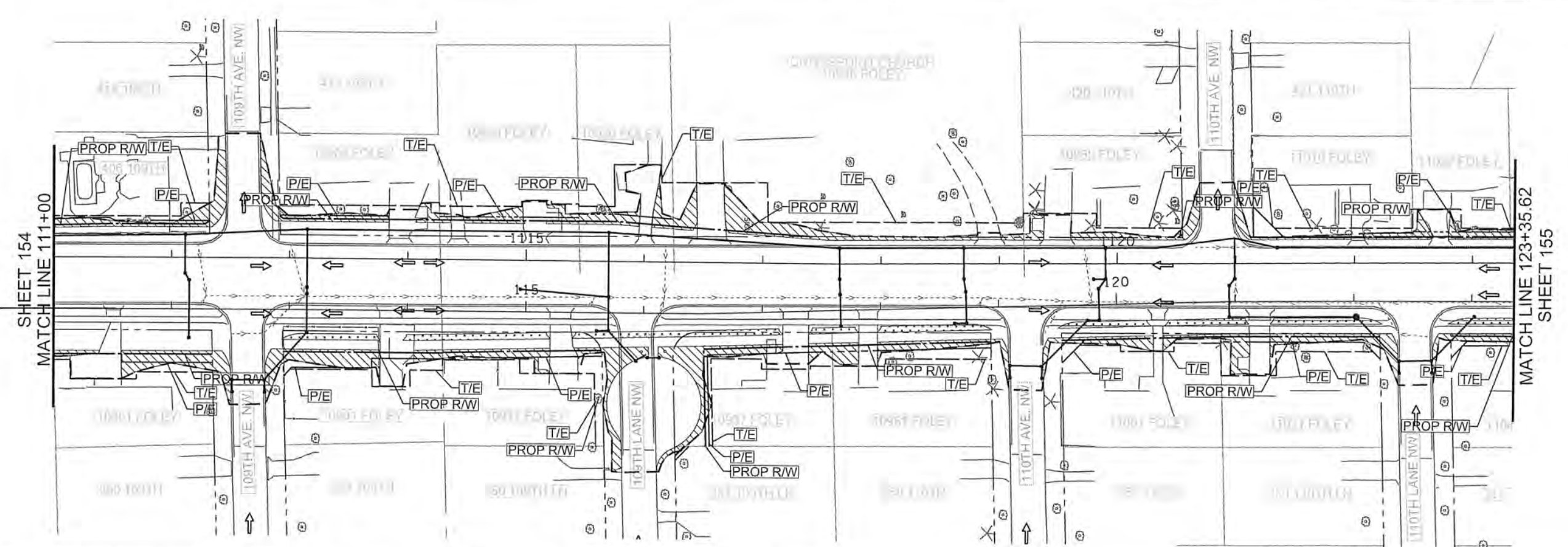
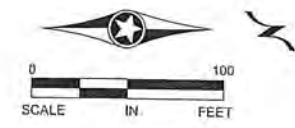
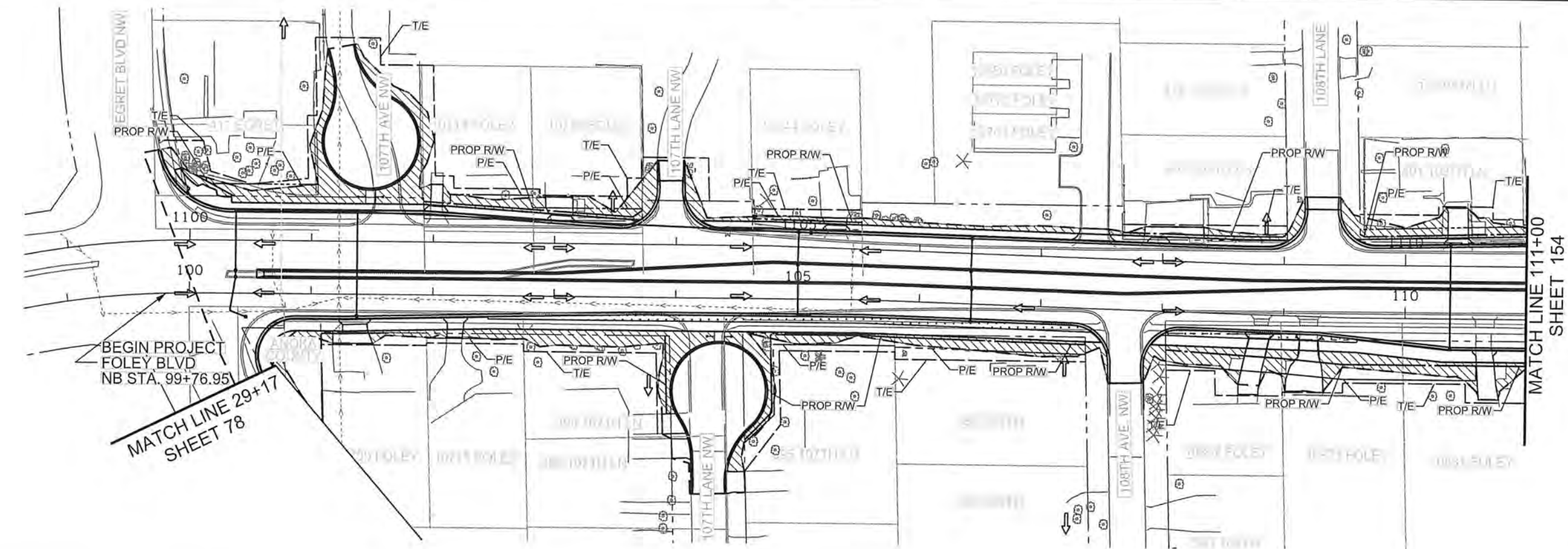
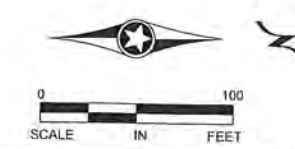
ANOKA COUNTY
 HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

EROSION CONTROL PLAN
 FOLEY BLVD, EGRET BLVD
 & 113TH AVE/DOGWOOD ST

LEGEND

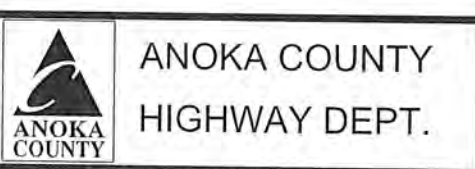
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-  SEEDING MIX 25-131
-  SEEDING MIX 25-151
-  SEEDING MIX 33-261



NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\02-611-34\Plan\002-611-034_TE_PL1.dgn					
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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GINA M. PIZZO
 SIGNATURE: *Gina M. Pizzo*
 DATE: 5-19-16 LICENSE NO. 22713

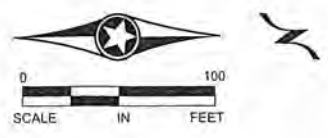
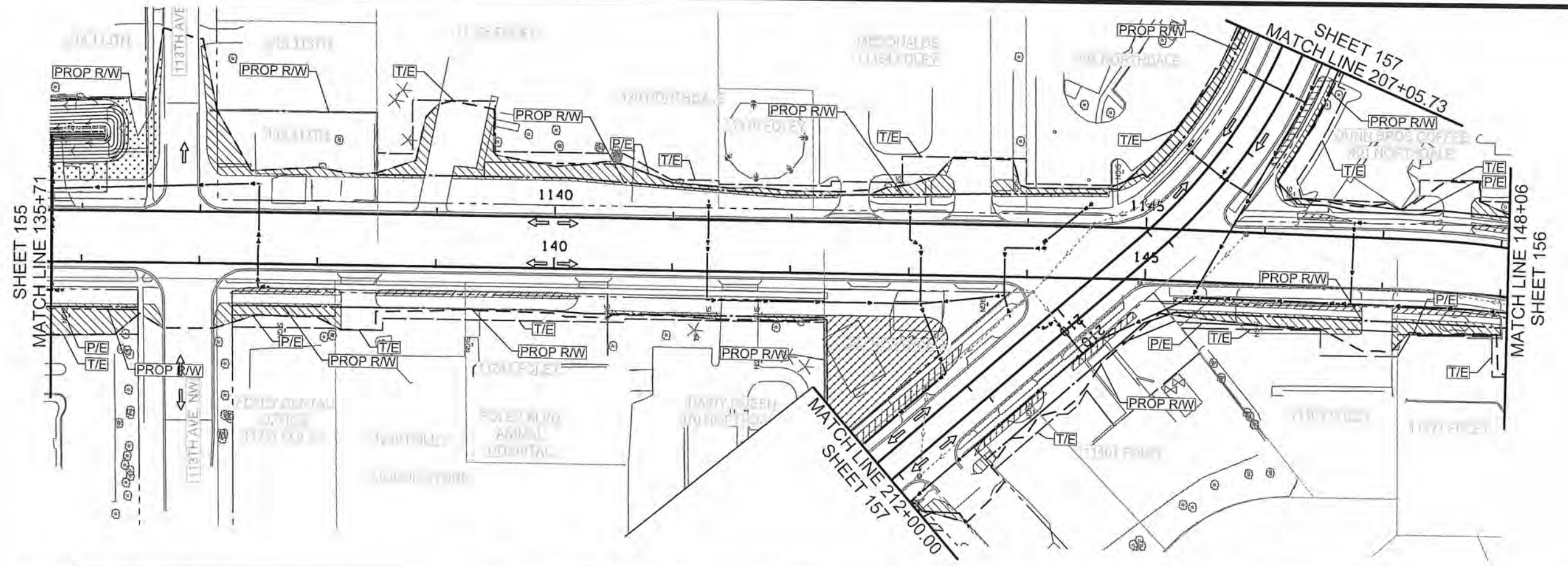
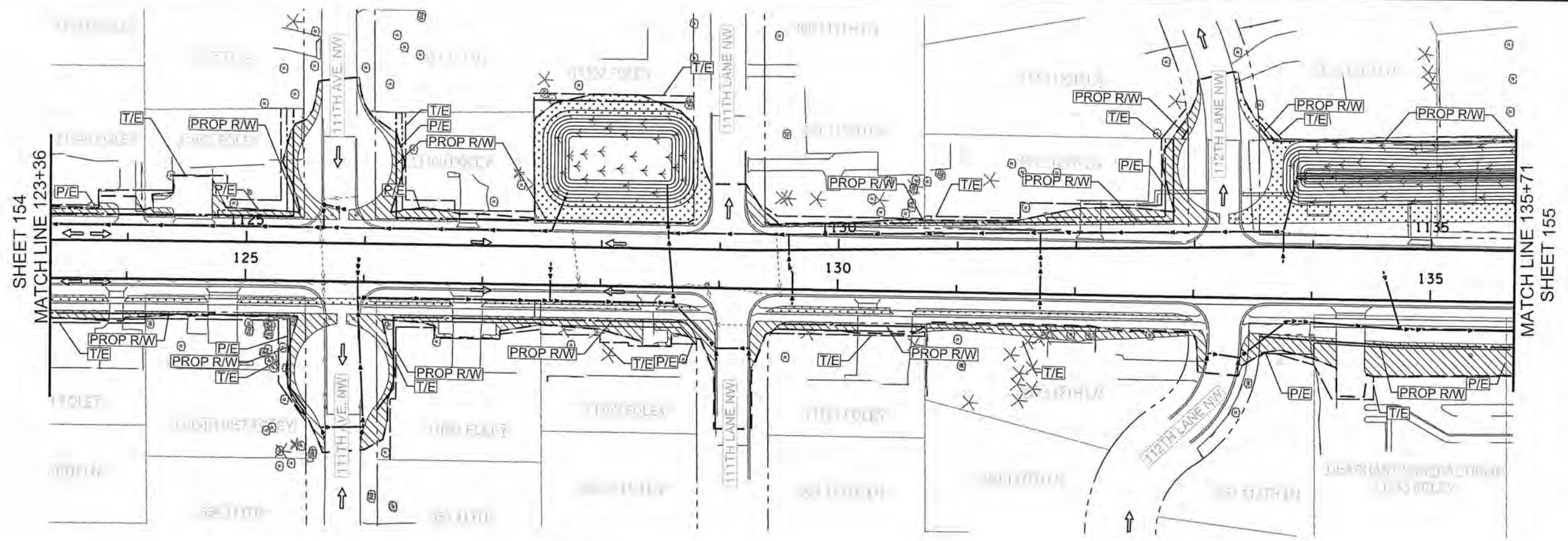
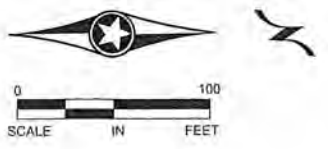
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SP 002-611-034
 SP 114-020-050
 CP 13-10

TURF ESTABLISHMENT PLAN
 FOLEY BLVD.
 STA 99+76.95 TO 111+00.00
 Sheet 154 of 298 Sheets

LEGEND	
	SODDING TYPE SALT TOLERANT
	SEEDING MIX 25-131
	SEEDING MIX 25-151
	SEEDING MIX 33-261



2 OF 4

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\02-611-034_TE_P2.dgn 05/13/2016 12:59:40 PM

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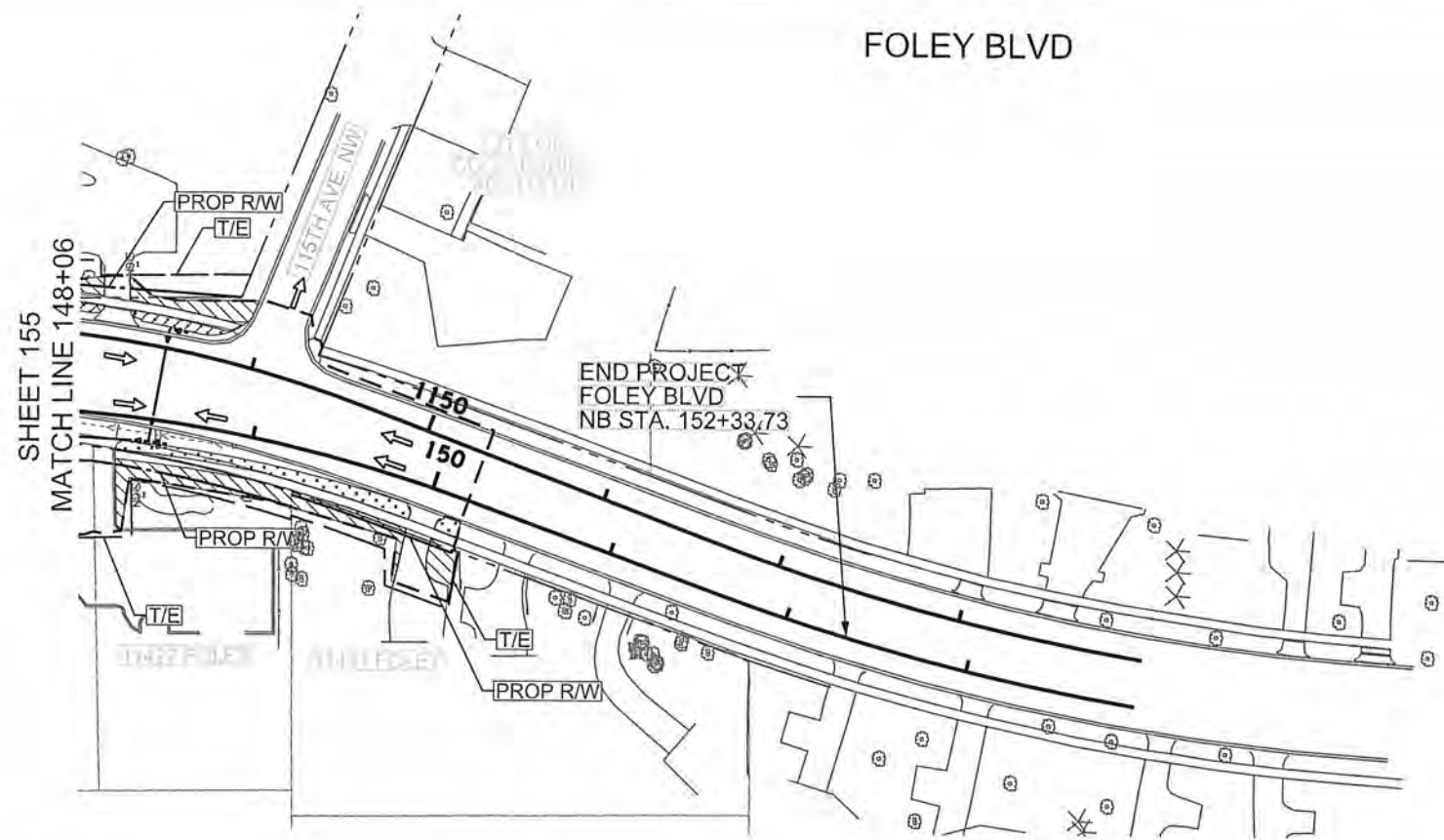
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ANOKA COUNTY
 HIGHWAY DEPT.

SP 002-611-034
 SP 114-020-050
 CP 13-10

TURF ESTABLISHMENT PLAN
 FOLEY BLVD
 STA 123+36 TO 148+06
 Sheet 155 of 298 Sheets

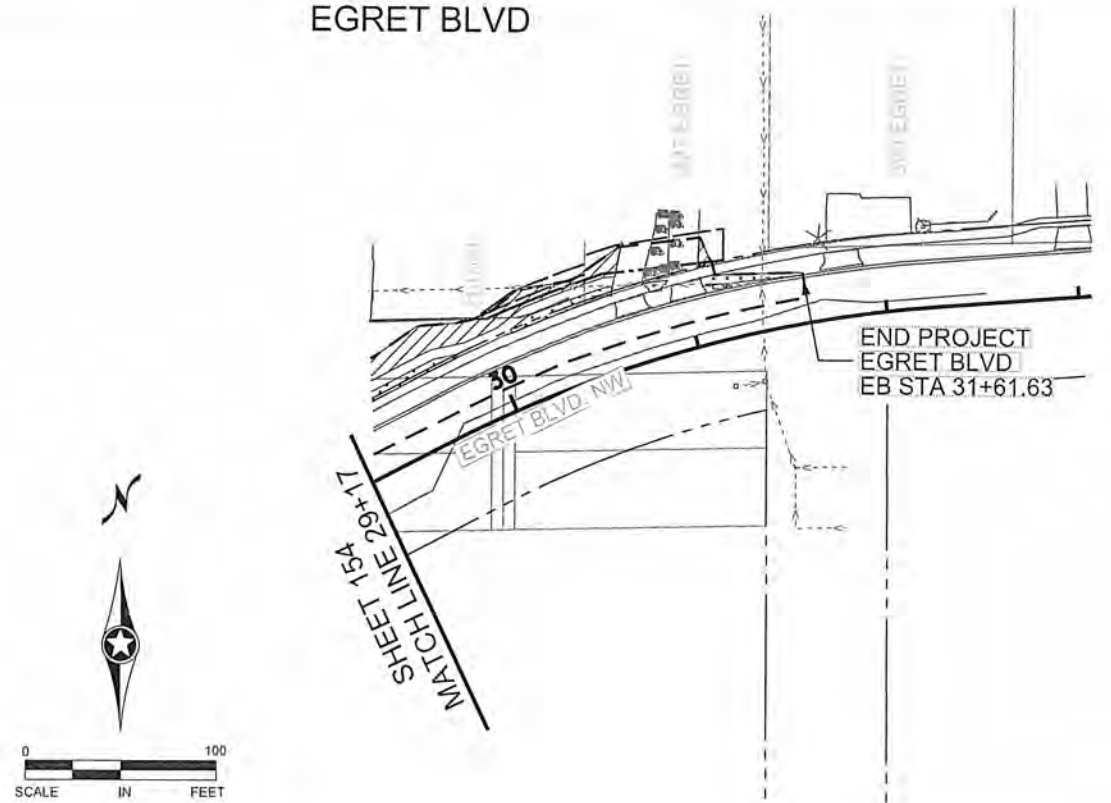
FOLEY BLVD



LEGEND	
	SODDING TYPE SALT TOLERANT
	SEEDING MIX 25-131
	SEEDING MIX 25-151
	SEEDING MIX 33-261



EGRET BLVD



NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-611-34\Plan\002-611-034_TE_P3.dgn 05/13/2016 12:59:41 PM

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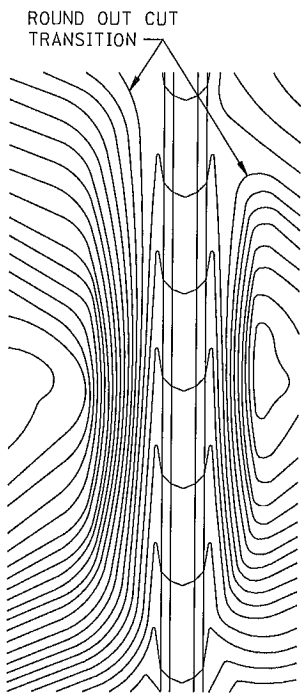


ANOKA COUNTY
 HIGHWAY DEPT.

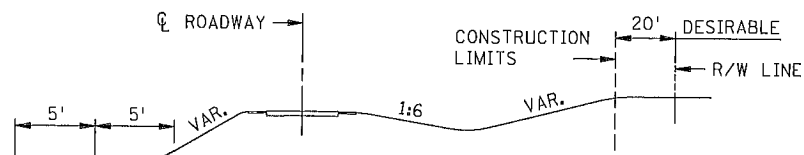
SP 002-611-034
 SP 114-020-050
 CP 13-10

TURF ESTABLISHMENT PLAN
 FOLEY BLVD, EGRET BLVD
 & 113TH AVE/DOGWOOD ST

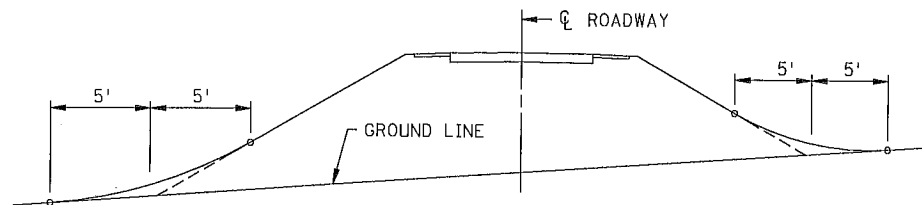
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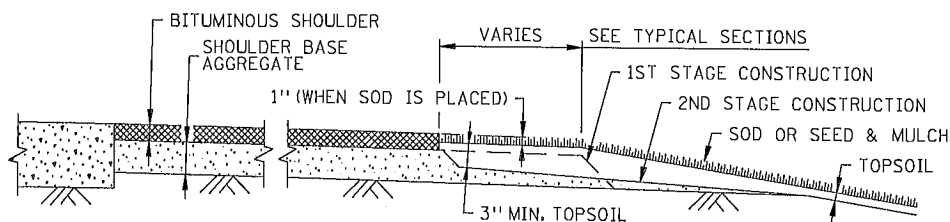
CONTOURING ROAD CUTS



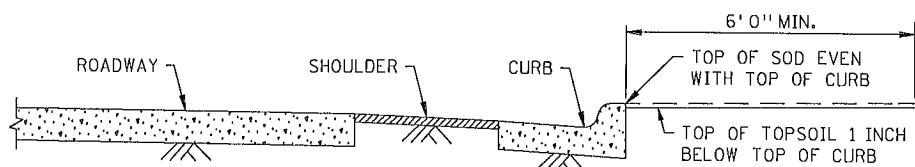
ROUNDING SHOULDERS AND BACKSLOPES



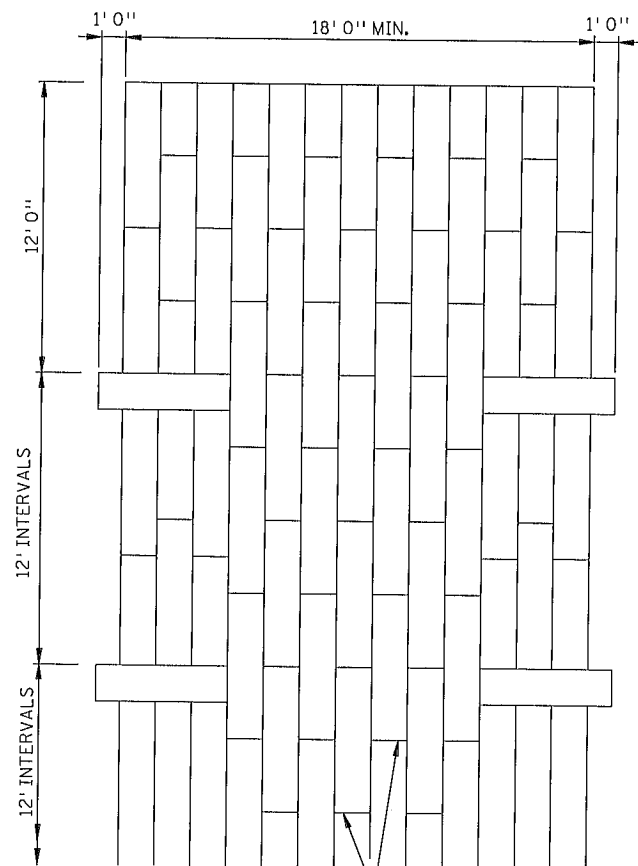
SHAPING FOR DRAINAGE ALONG THE TOE OF FILL SLOPES



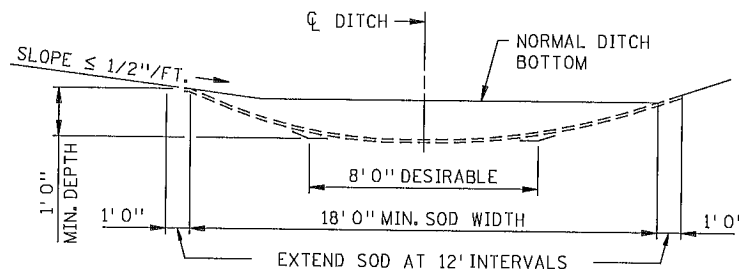
SHAPING AND TOPSOILING INSLOPES



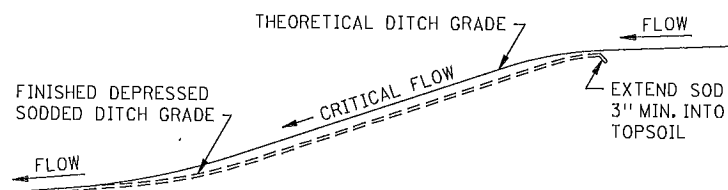
SHAPING ADJACENT TO CURBS WHEN SOD IS PLACED



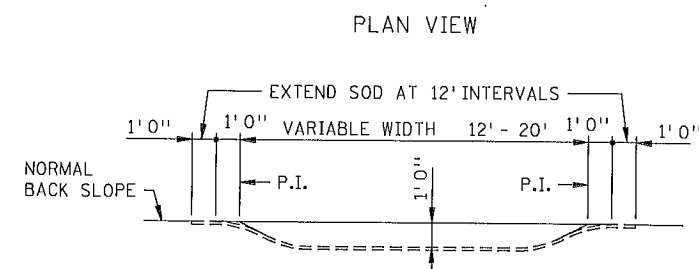
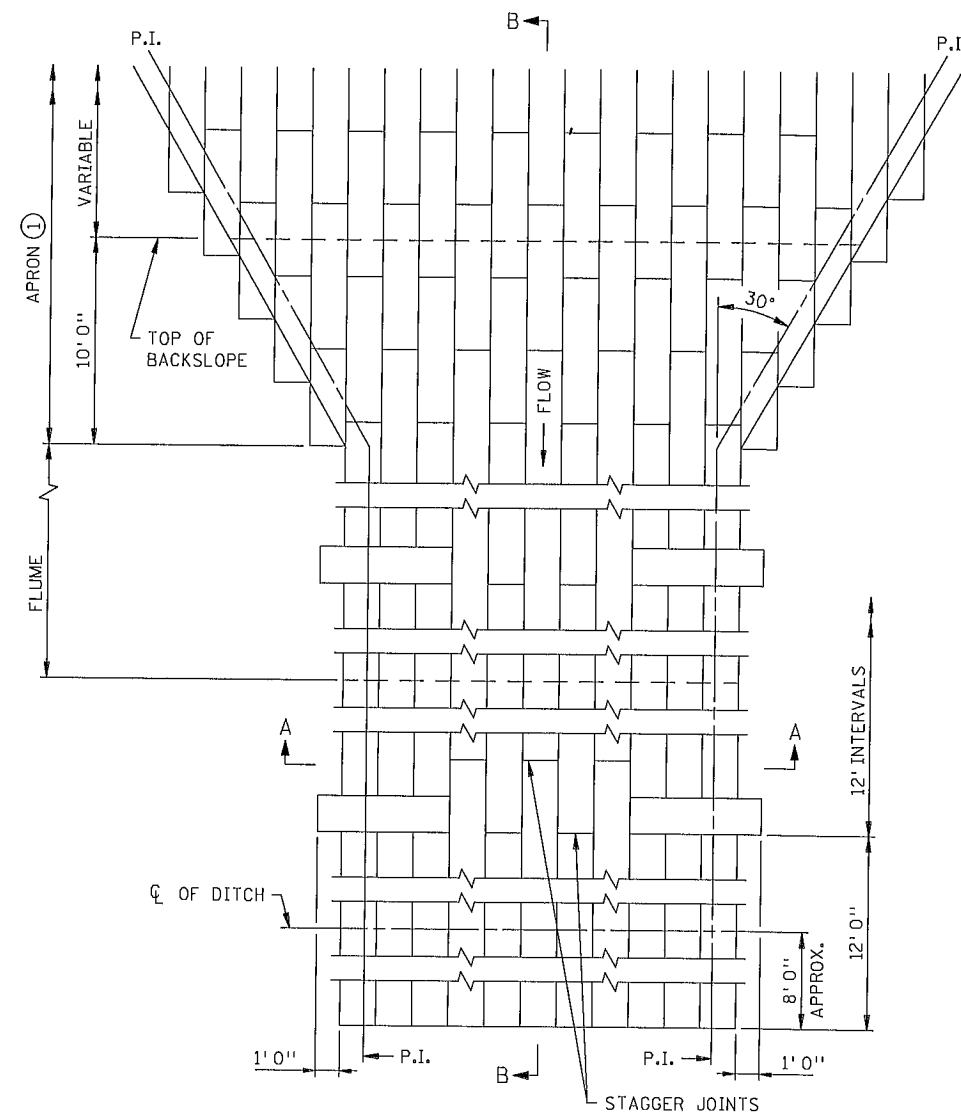
STAGGER JOINTS
PLAN VIEW



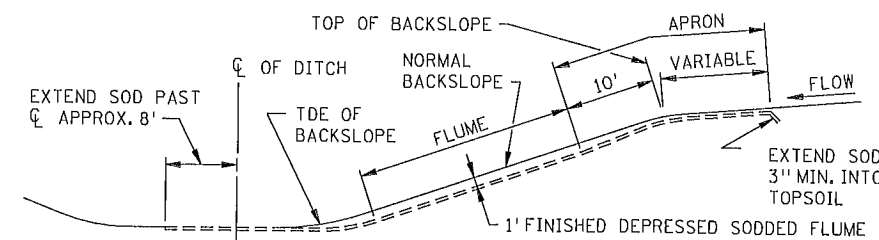
SODDED DITCH CROSS SECTION
WHERE FRONT OR BACK SLOPE IS FLAT (LESS THAN 1/2"/FT.),
FIRST NOTCH DITCH AND THEN PROVIDE ROUNDING.



DITCH PROFILE
SODDED DITCH DETAILS



SECTION A-A



SECTION B-B

SODDED FLUME DETAILS

NOTES:
SEE SPEC. 2575.3 FOR ADDITIONAL INFORMATION.
① CONSTRUCT TAPER AS DIRECTED BY THE ENGINEER.

DISTRICT #: 34
USER NAME: dffrey
PATH & FILENAME: P:\02-611-34\Plan\Standard_Plans\s404_J_spr.dgn

FILE NAME:
s404_J_spr.dgn

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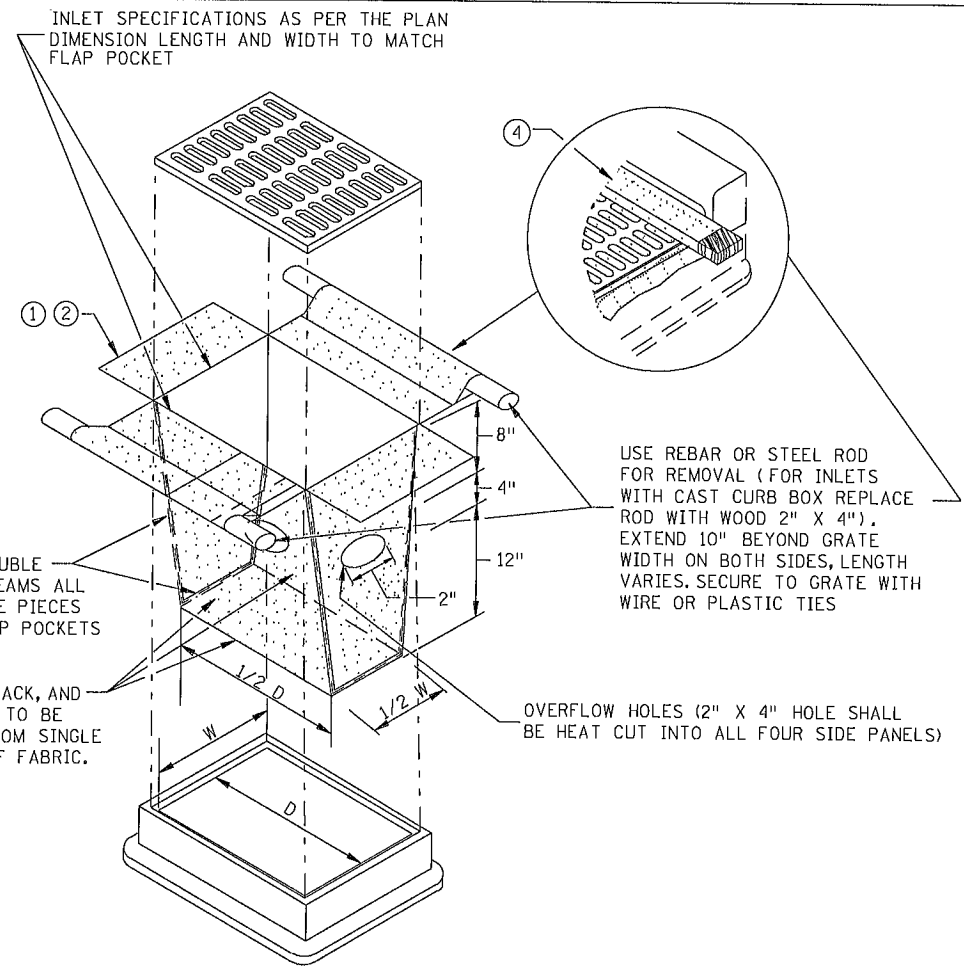
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[Signature]
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APPROVED: 8-6-2014

PERMANENT EROSION CONTROL
ALONG ROADWAYS, DITCHES AND FLUMES
STANDARD PLAN 5-297.404 158 OF 298

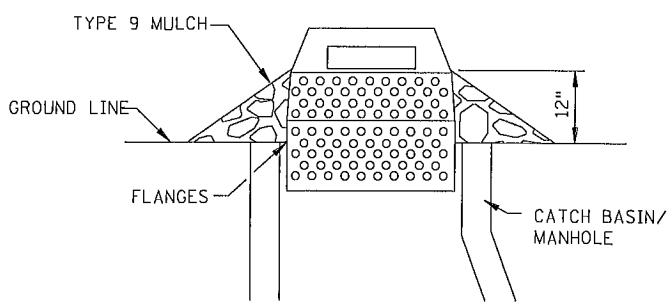
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05/13/2016

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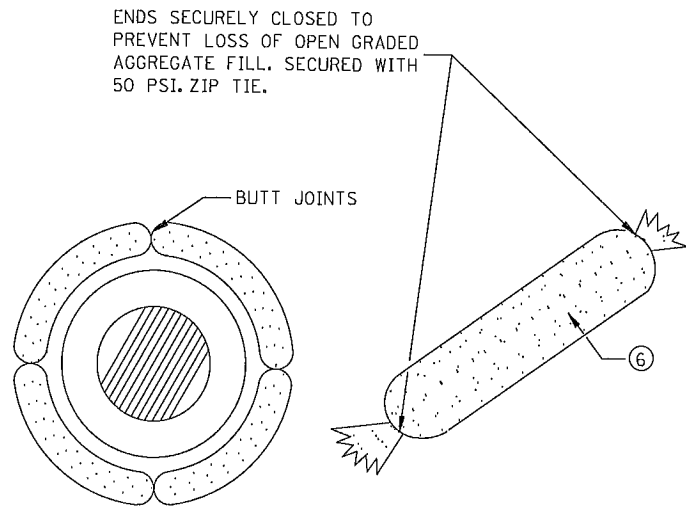


FILTER BAG INSERT ③
(CAN BE INSTALLED IN ANY INLET TYPE
WITH OR WITHOUT A CURB BOX)

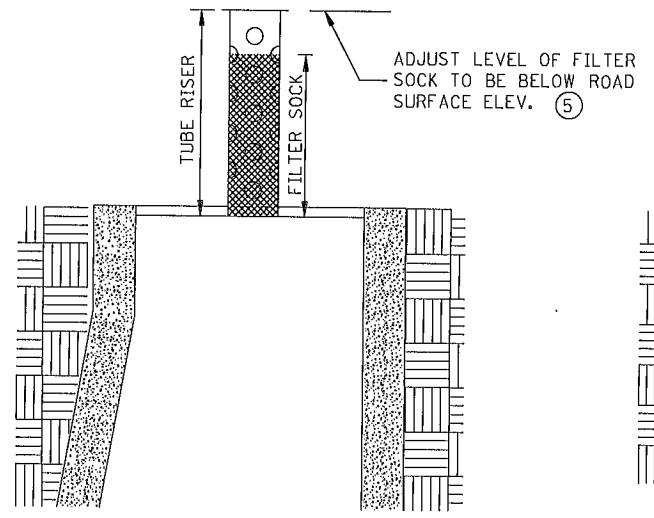
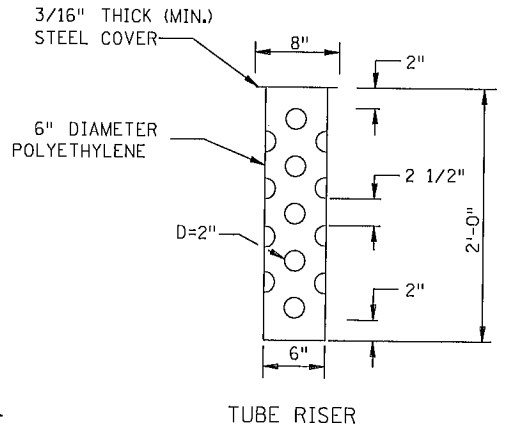


SEDIMENT CONTROL INLET HAT

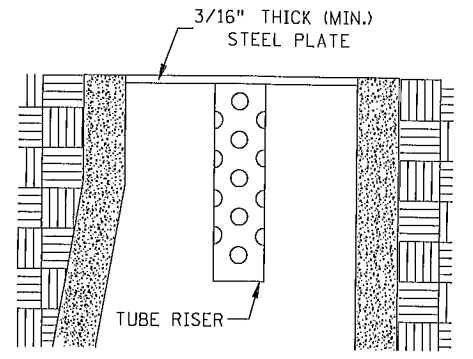
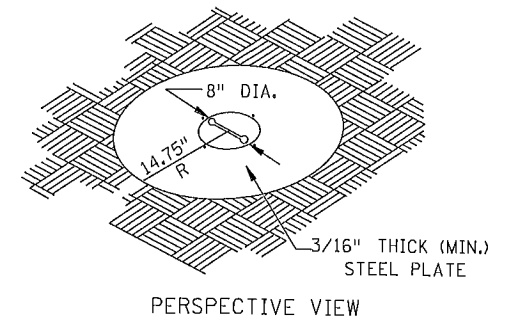
NOTE:
THE SEDIMENT CONTROL BARRIER SHALL BE A METAL
OR PLASTIC/POLYETHYLENE RISER SIZED TO FIT INSIDE
THE CATCH BASIN/MANHOLE; HAVE PERFORATIONS TO ALLOW
FOR WATER INFILTRATION; HAVE AN OVERFLOW OPENING,
FLANGES AND A LID/COVER.



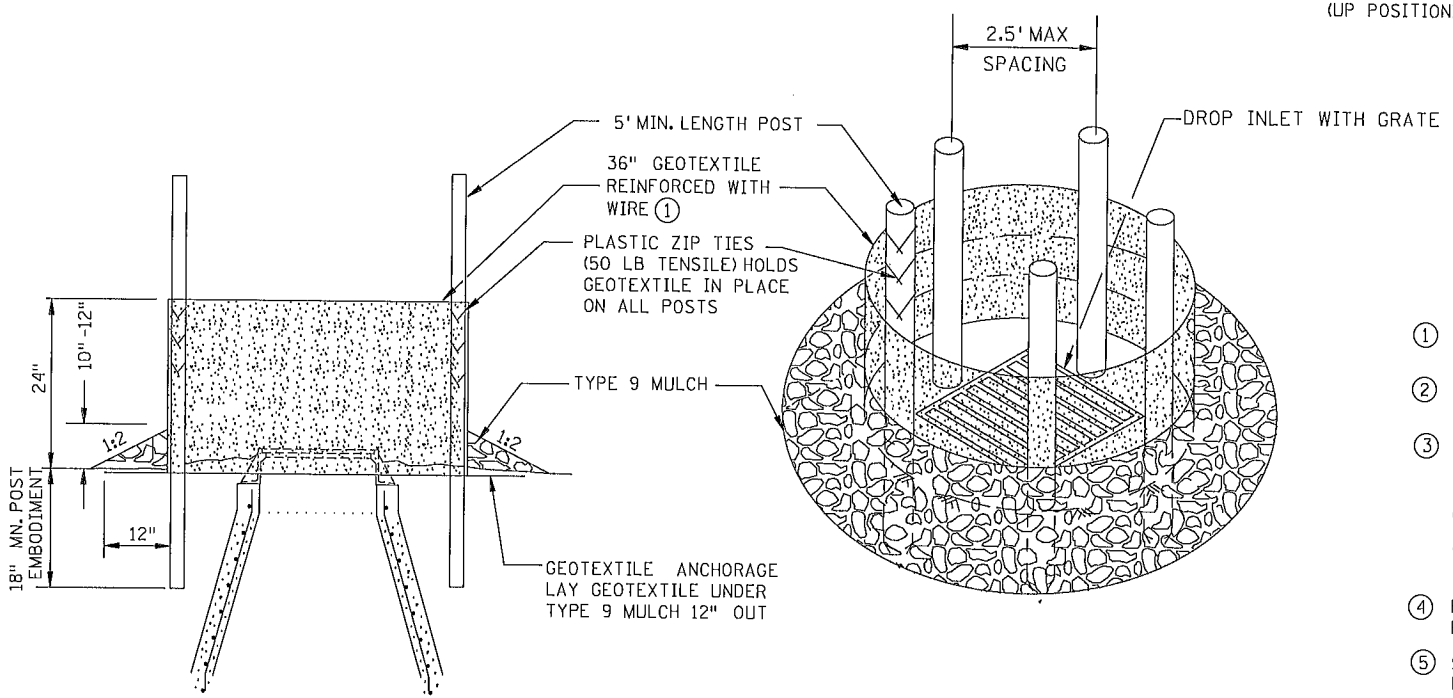
ROCK LOG/COMPOST LOG



**SECTION
(UP POSITION)**



**SECTION
(DOWN POSITION)**



SILT FENCE RING AND ROCK FILTER BERM
USE WHERE INLET DRAINS IN AN AREA WITH SLOPES AT 1:3 OR LESS

POP-UP HEAD

- NOTES:
SEE SPECS. 2573, 3137, & 3886.
- DEVICES MUST BE ADJUSTED ACCORDINGLY AS TO NOT CAUSE FLOODING ON ROADWAY
THAT WOULD IMPEED TRAFFIC FLOW.
- ① ALL GEOTEXTILE USED FOR INLET PROTECTION SHALL BE MONOFILAMENT IN BOTH
DIRECTIONS, MEETING SPEC. 3886.
 - ② FINISHED SIZE, INCLUDING POCKETS WHERE REQUIRED SHALL EXTEND A MINIMUM OF
10 INCHES AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
 - ③ INSTALLATION NOTES:
DO NOT PLACE FILTER BAG INSERT IN INLETS SHALLOWER THAN 30 INCHES,
MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE. THE
PLACED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE OF 3 INCHES BETWEEN
THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES.
WHERE NECESSARY THE CONTRACTOR SHALL CLINCH THE BAG, USING PLASTIC ZIP TIES,
TO ACHIEVE THE 3 INCH CLEARANCE.
 - ④ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2 INCH X 4 INCH OR USE A
ROCK SOCK OR SAND BAGS IN PLACE OF THE FLAP POCKETS.
 - ⑤ SOCK HEIGHT MUST NOT BE SO HIGH AS TO SLOW DOWN WATER FILTRATION TO CAUSE
FLOODING OF THE ROADWAY.
 - ⑥ GEOTEXTILE SOCK BETWEEN 4-10 FEET LONG AND 4-6 INCH DIAMETER. SEAM TO BE
JOINED BY TWO ROWS OF STITCHING WITH A PLASTIC MESH BACKING OR PROVIDE A
HEAT BONDED SEAM (OR APPROVED EQUIVALENT). FILL ROCK LOG WITH OPEN GRADED
AGGREGATE CONSISTING OF SOUND DURABLE PARTICLES OF COARSE AGGREGATE
CONFORMING TO SPEC. 3137 TABLE 3137-1; CA-3 GRADATION.

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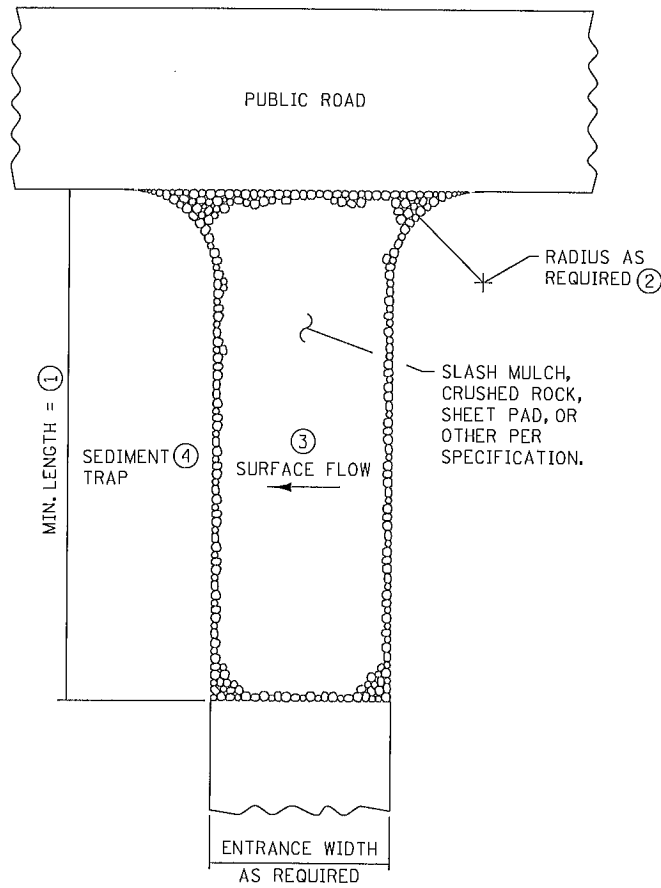
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APPROVED:
8-6-2014

TEMPORARY SEDIMENT CONTROL
STORM DRAIN INLET PROTECTION
STANDARD PLAN 5-297.405 159 OF 298

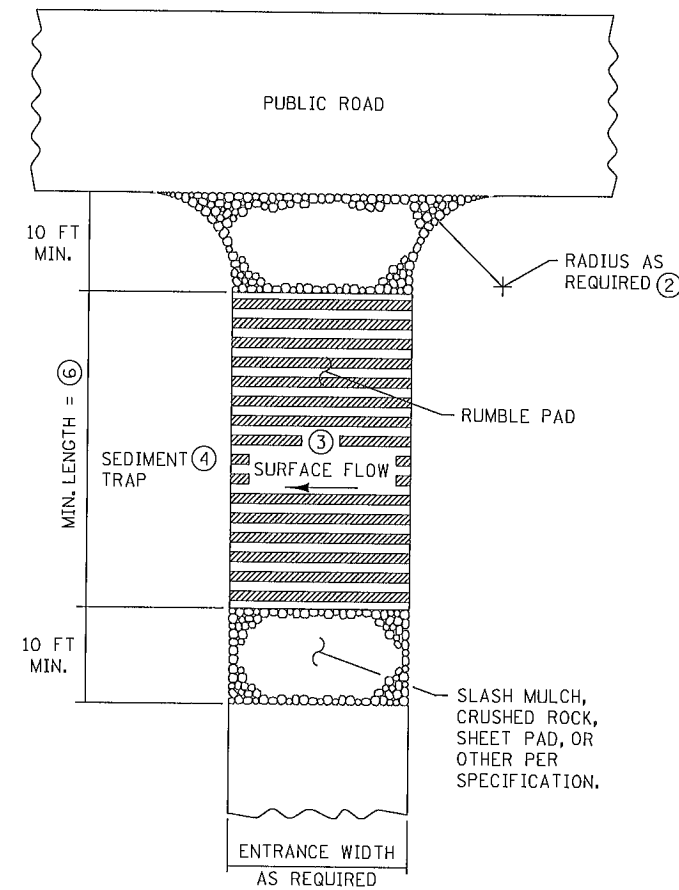
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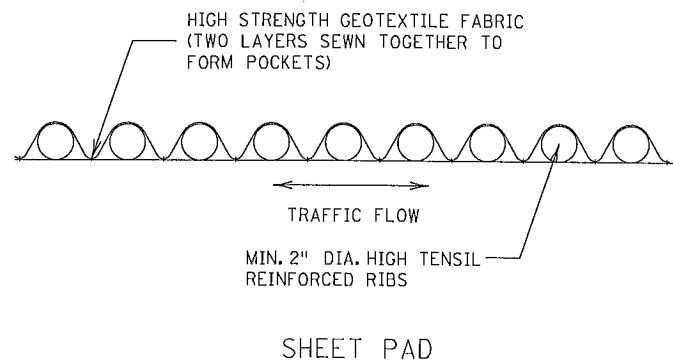
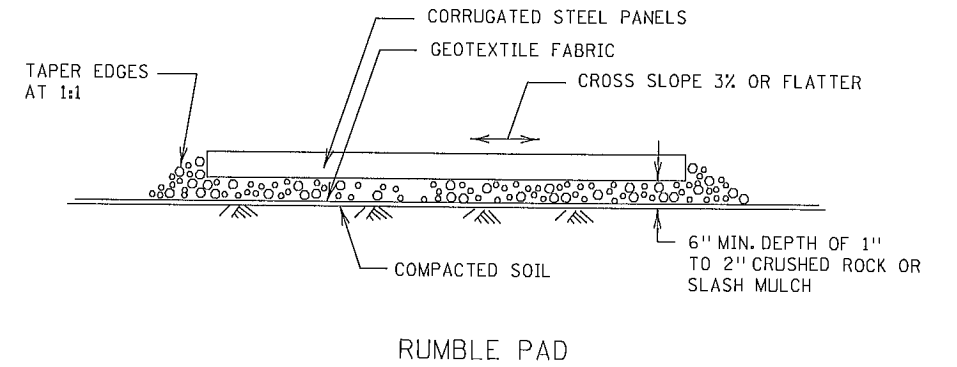
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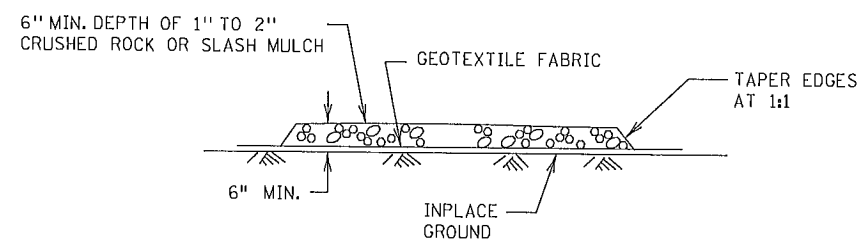
SLASH MULCH, CRUSHED ROCK, OR SHEET PAD CONSTRUCTION EXIT ⑤⑦



RUMBLE PAD CONSTRUCTION EXIT ⑤⑦



SHEET PAD



SLASH MULCH OR CRUSHED ROCK

NOTES:

- SEE SPECS. 2573 & 3882.
- ① MINIMUM LENGTH SHALL BE THE GREATER OF 50 FEET OR A LENGTH SUFFICIENT TO ALLOW A MINIMUM OF 5 TIRE ROTATIONS ON THE PROVIDED PAD. MINIMUM LENGTH SHALL BE CALCULATED USING THE LARGEST TIRE WHICH WILL BE USED IN TYPICAL OPERATIONS.
- ② PROVIDE RADIUS OR WIDEN PAD SUFFICIENTLY TO PREVENT VEHICLE TIRES FROM TRACKING OFF OF PAD WHEN LEAVING SITE.
- ③ IF RUNOFF FROM DISTURBED AREAS FLOWS TOWARD CONSTRUCTION EXITS, PREVENT RUNOFF FROM DRAINING DIRECTLY TO PUBLIC ROAD OVER CONSTRUCTION EXIT BY CROWNING THE EXIT OR SLOPING TO ONE SIDE. IF SURFACE GRADING IS INSUFFICIENT, PROVIDE OTHER MEANS OF INTERCEPTING RUNOFF.
- ④ IF RUNOFF FROM CONSTRUCTION EXITS WILL DRAIN OFF OF PROJECT SITE, PROVIDE SEDIMENT TRAP WITH STABILIZED OVERFLOW.
- ⑤ IF A TIRE WASH OFF IS REQUIRED THE CONSTRUCTION EXITS SHALL BE GRADED TO DRAIN THE WASH WATER TO A SEDIMENT TRAP.
- ⑥ MINIMUM LENGTH OF RUMBLE PAD SHALL BE 20 FEET, OR AS REQUIRED TO REMOVE SEDIMENT FROM TIRES. IF SIGNIFICANT SEDIMENT IS TRACKED FROM THE SITE, THE RUMBLE PAD SHALL BE LENGTHENED OR THE DESIGN MODIFIED TO PROVIDE ADDITIONAL VIBRATION. WASH-OFF LENGTH SHALL BE AS REQUIRED TO EFFECTIVELY REMOVE CONSTRUCTION SEDIMENT FROM VEHICLE TIRES.
- ⑦ MAINTENANCE OF CONSTRUCTION EXITS SHALL OCCUR WHEN THE EFFECTIVENESS OF SEDIMENT REMOVAL HAS BEEN REDUCED. MAINTENANCE SHALL CONSIST OF REMOVING SEDIMENT AND CLEANING THE MATERIALS OR PLACING ADDITIONAL MATERIAL (SLASH MULCH OR CRUSHED ROCK) OVER SEDIMENT FILLED MATERIAL TO RESTORE EFFECTIVENESS.

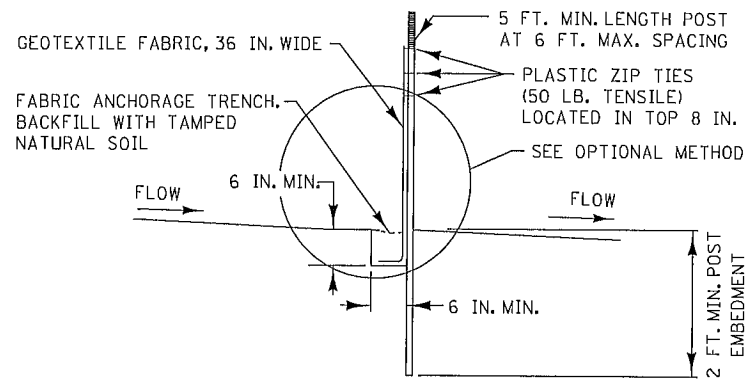
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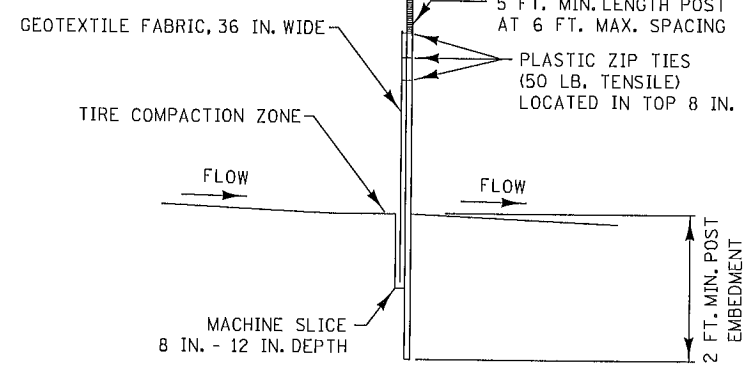
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TEMPORARY SEDIMENT CONTROL
CONSTRUCTION EXITS
STANDARD PLAN 5-297.405 160 OF 298

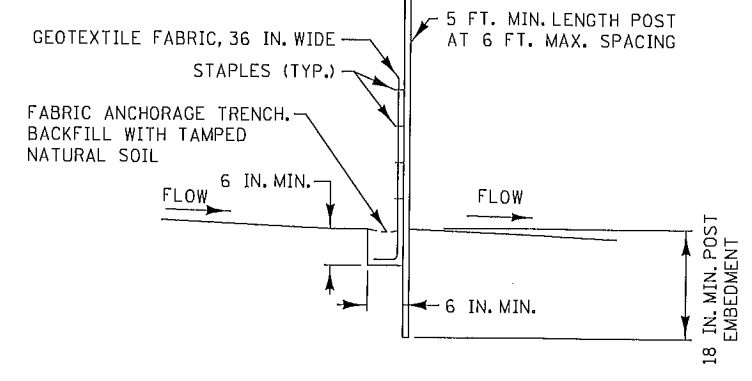
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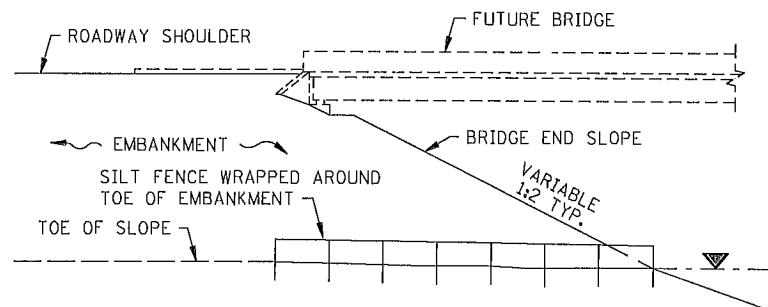
SILT FENCE TYPE HI ②
(HAND INSTALLED)



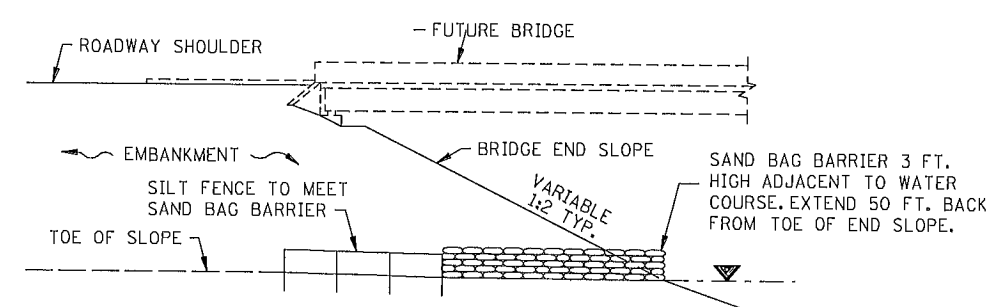
SILT FENCE TYPE MS ②
(MACHINE SLICED)



SILT FENCE TYPE PA ③
(PREASSEMBLED)

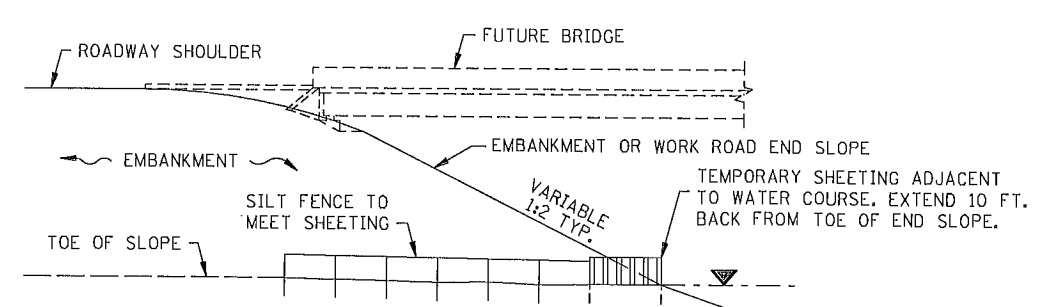


SILT FENCE ONLY ④

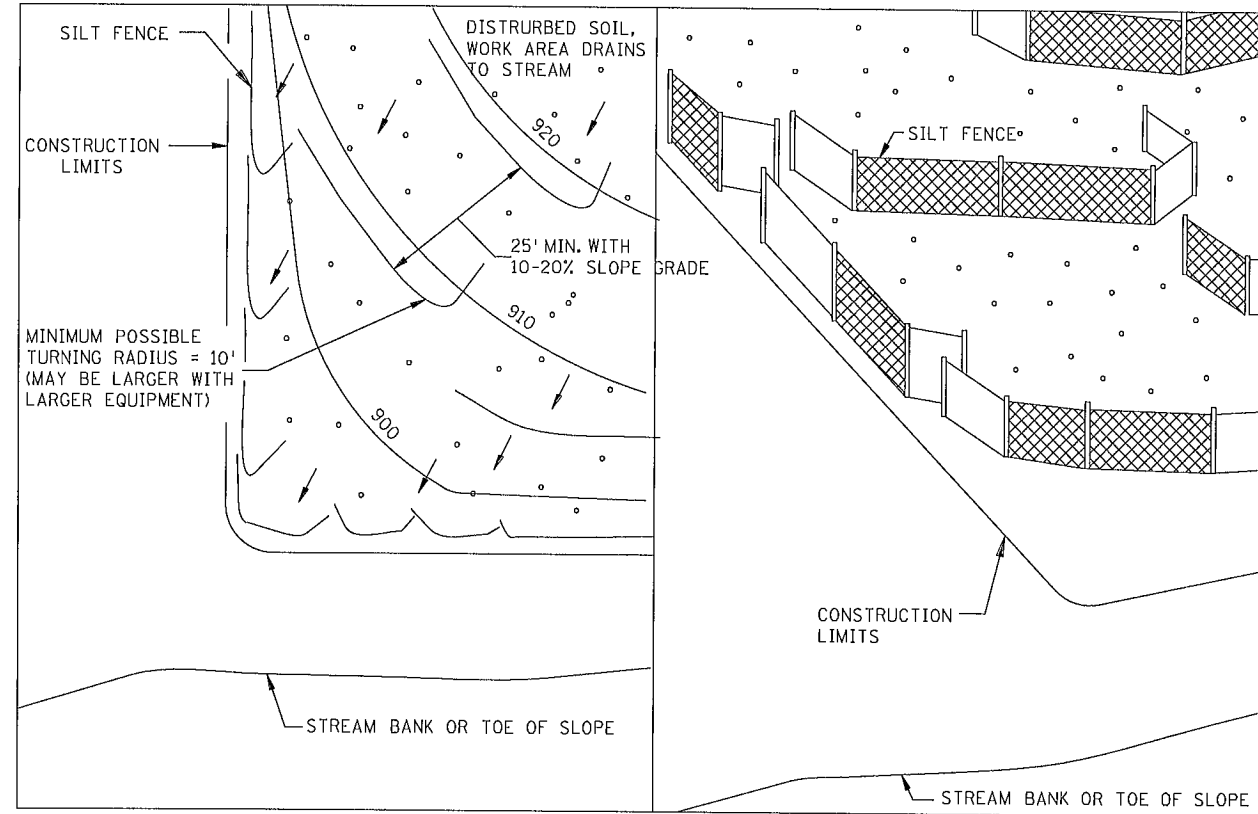


SILT FENCE WITH SAND BAGS ⑤

INSTALLATION AT BRIDGE EMBANKMENT ADJACENT TO WATER



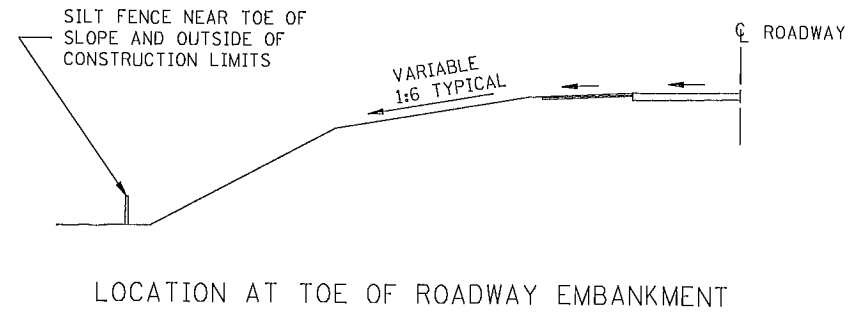
SILT FENCE WITH SHEETING ⑥



PLAN VIEW

PERSPECTIVE VIEW

J-HOOK INSTALLATION



LOCATION AT TOE OF ROADWAY EMBANKMENT

NOTES:

- SEE SPECS. 2573, 3149 & 3886.
- ① COARSE FILTER AGGREGATE (SPEC. 3149) SHALL BE INCIDENTAL.
- ② TO PROTECT AREAS FROM SHEET FLOW. MAXIMUM CONTRIBUTING AREA: 1 ACRE.
- ③ TO PROTECT AREAS FROM SHEET FLOW. MAXIMUM CONTRIBUTING AREA: 0.25 ACRE.
- ④ WATER COURSE FLOW VELOCITY: STANDING. CONTRIBUTING SLOPE AREA: 1/2 ACRE.
- ⑤ WATER COURSE FLOW VELOCITY: 1 TO 7 FT./SEC. CONTRIBUTING SLOPE AREA: 1 ACRE.
- ⑥ WATER COURSE FLOW VELOCITY: 8 TO 15 FT./SEC. CONTRIBUTING SLOPE AREA: 3 ACRES.

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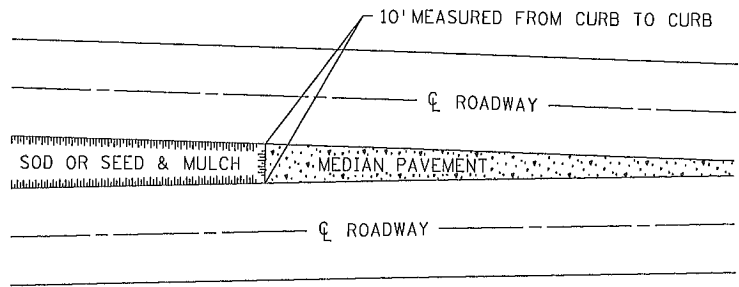
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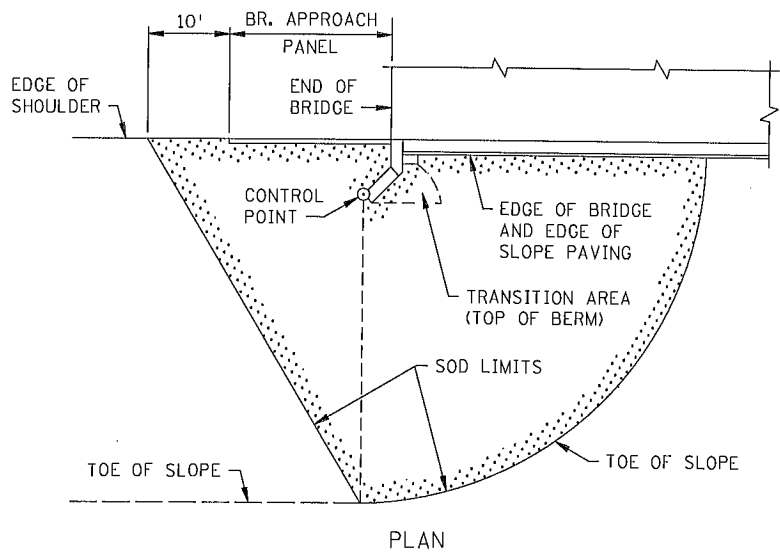
MINNESOTA DEPARTMENT OF TRANSPORTATION
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TEMPORARY SEDIMENT CONTROL
SILT FENCE
STANDARD PLAN 5-297.405 161 OF 298

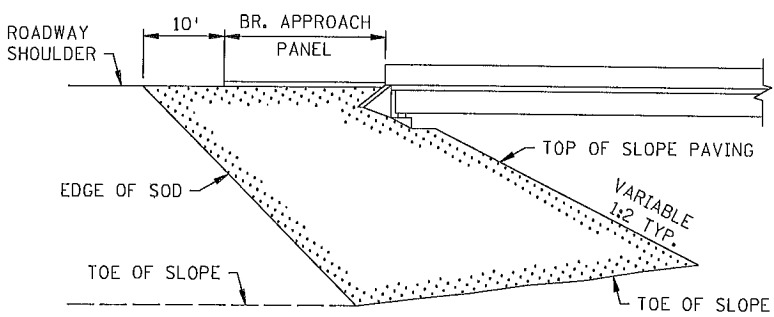
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SODDING LIMITS AT GORE AREA

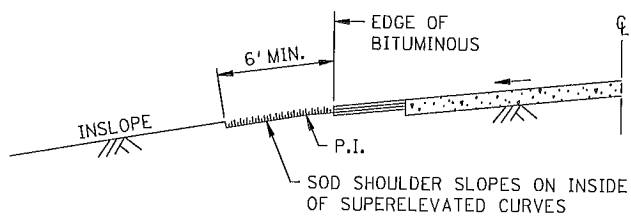


PLAN

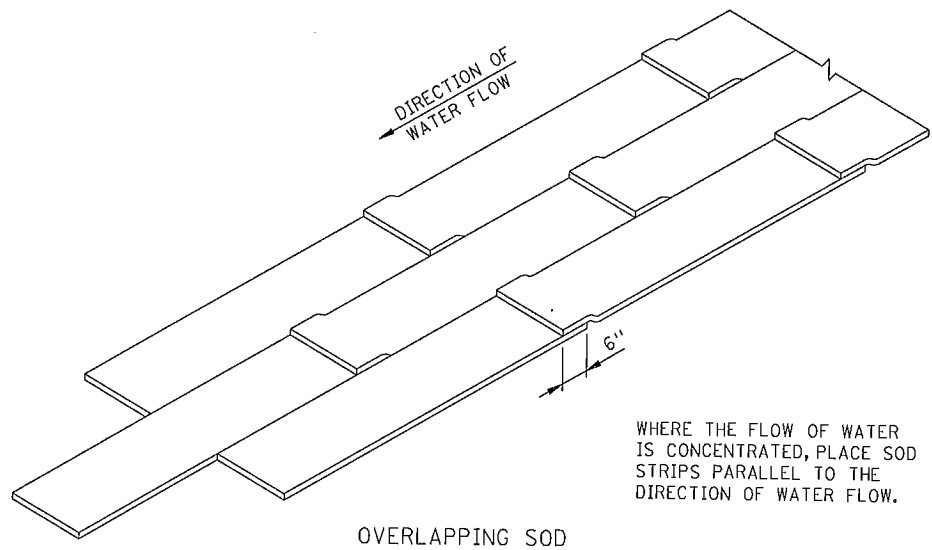


ELEVATION

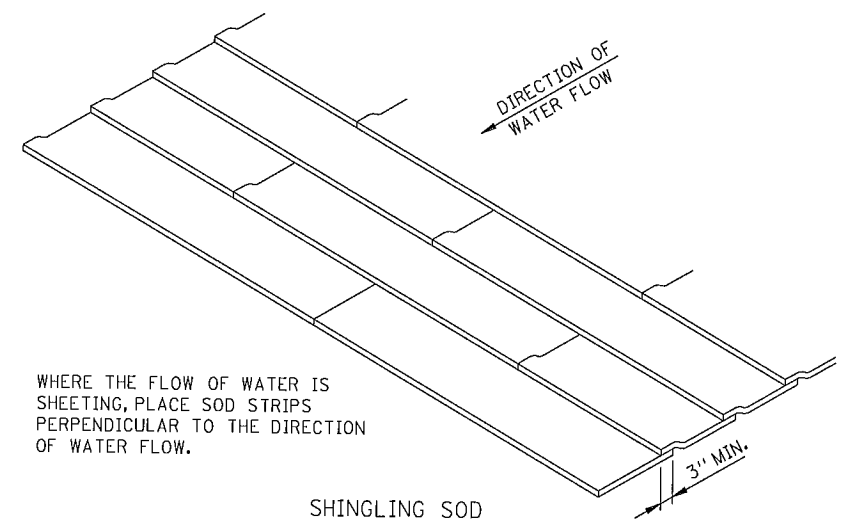
SODDING LIMITS AT BRIDGE APPROACH FILLS



SODDING INSLOPES OF SUPERELEVATED CURVES



OVERLAPPING SOD

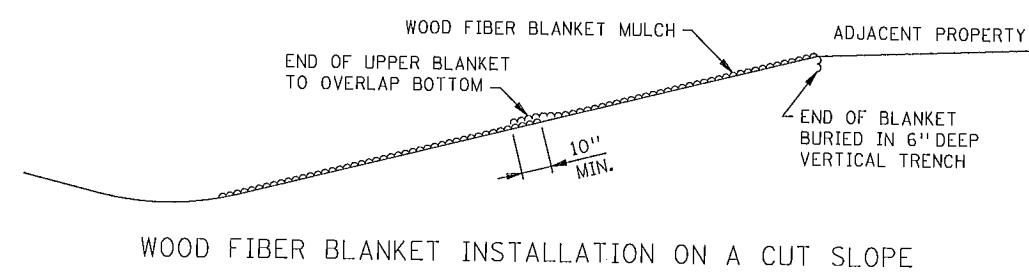


SHINGLING SOD

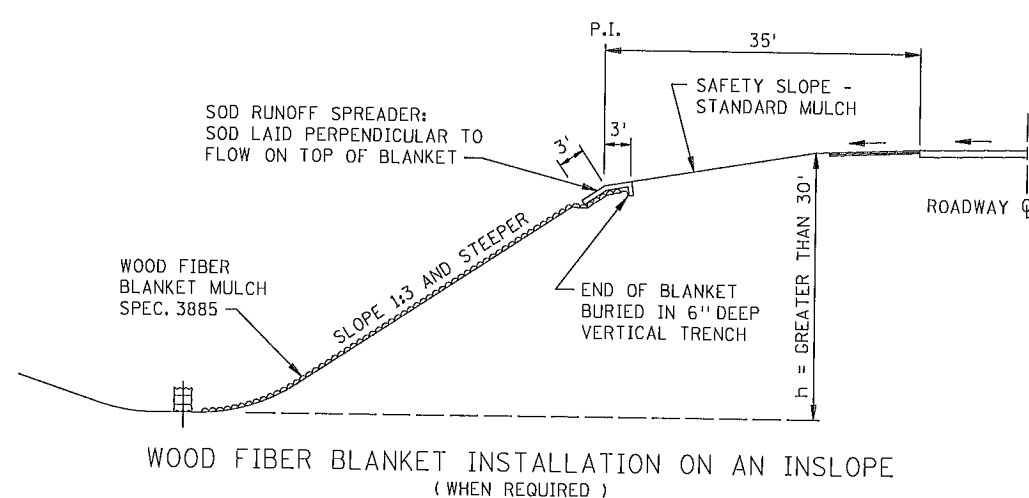
WHERE THE FLOW OF WATER IS CONCENTRATED, PLACE SOD STRIPS PARALLEL TO THE DIRECTION OF WATER FLOW.

WHERE THE FLOW OF WATER IS SHEETING, PLACE SOD STRIPS PERPENDICULAR TO THE DIRECTION OF WATER FLOW.

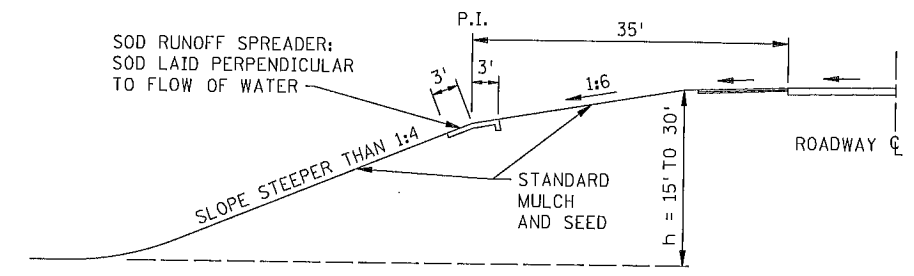
SPECIAL SOD PLACEMENT TECHNIQUES



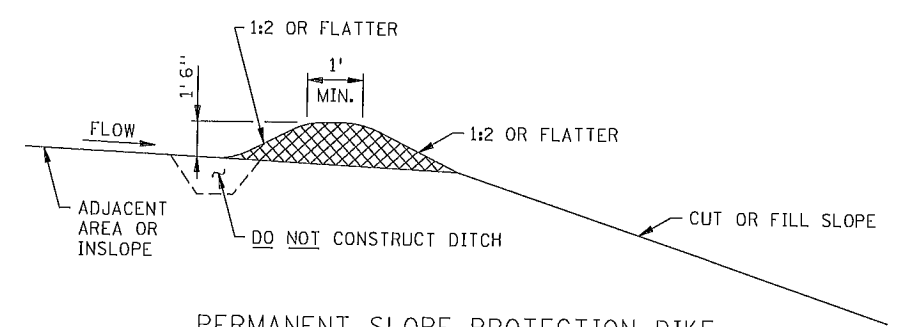
WOOD FIBER BLANKET INSTALLATION ON A CUT SLOPE



WOOD FIBER BLANKET INSTALLATION ON AN INSLOPE (WHEN REQUIRED)



BROKEN-BACK SAFETY FILL SLOPE



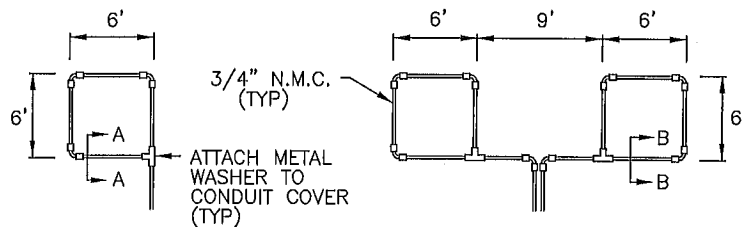
PERMANENT SLOPE PROTECTION DIKE

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Chief Environmental Officer
CHIEF ENVIRONMENTAL OFFICER

MINNESOTA DEPARTMENT OF TRANSPORTATION
STATE DESIGN ENGINEER
Christopher Ry
APPROVED: 8-6-2014

PERMANENT SEDIMENT CONTROL
ALONG ROADWAYS AND AT GORE AREAS & BRIDGE APPROACH FILLS
STANDARD PLAN 5-297.406 162 OF 298

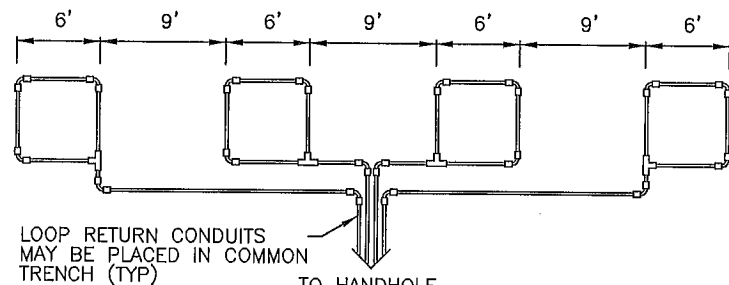


**LOOP DETECTOR
DETAIL 'A'**
(LOOP PHASING FOR
SINGLE CONNECTION)

LOOP CONNECTIONS SHALL BE
LABELED AND SPLICED IN THE
HANDHOLE AS FOLLOWS:

L1 TO 1A
1B TO 2A
2B TO L2

**LOOP DETECTOR
DETAIL 'B'**
(LOOP PHASING FOR
SERIES CONNECTION)

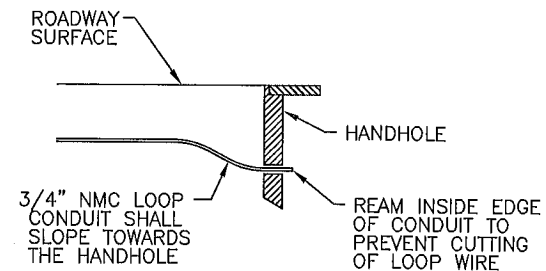
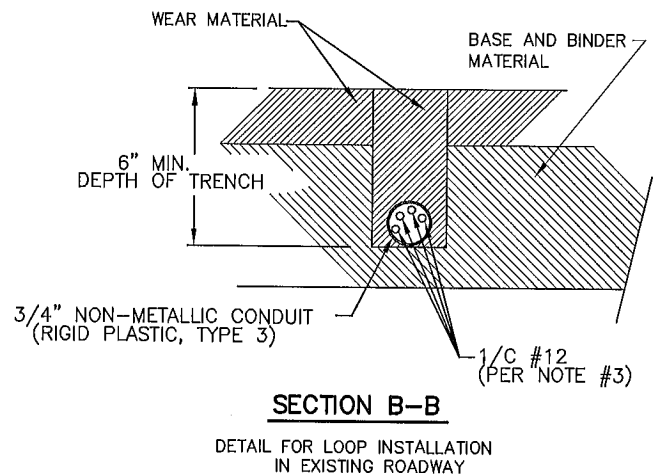
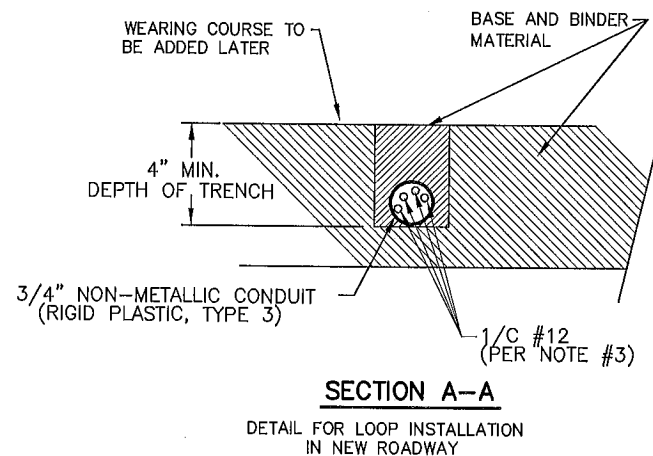


LOOP CONNECTIONS SHALL BE LABELED AND SPLICED
IN THE HANDHOLE AS FOLLOWS:

L1 TO 1A 3B TO 4A
1B TO 2A 4B TO L2
2B TO 3A

SPLICE CONTROL CABLE TO L1 & L2 IN HANDHOLE.
ALL CONDUCTORS SHALL BE TAGGED IN HANDHOLE
(1A, 1B, ECT)

**LOOP DETECTOR
DETAIL 'C'**
(LOOP PHASING FOR
SERIES CONNECTION)



LOOP DETECTOR WIRING

- 1) ALL CORNERS SHALL BE 90° CONDUIT BENDS.
- 2) CONNECT WIRES IN HANDHOLES USING SPLICE KIT METHOD DESCRIBED IN THE SPECIAL PROVISIONS.
- 3) LOOP DETECTOR WIRES SHALL BE #12 AWG CROSSED LINKED POLYETHYLENE (XLP). SEE SPECIAL PROVISIONS.
- 4) LOOP LEAD IN WIRES SHALL BE TWISTED A MIN. OF (5) TURNS PER FOOT THROUGH THE CONDUIT TO THE HANDHOLE.
- 5) NMC DESIGNATES NON-METALLIC CONDUIT (SPEC. 3803)
- 6) LOOPS 6' x 6' THRU 6' x 14' SHALL HAVE (4) TURNS.
- 7) LOOPS 6' x 15' AND LARGER SHALL HAVE (2) TURNS.

LEGEND OF SYMBOLS

CONTROLLER AND SERVICE EQUIP. NO's	(A)
SIGNAL BASE NO.	(B)
SIGNAL FACE NO.	(C)
LUMINAIRE NO.	(D)
CONTROLLER AND CABINET	(E)
CONTROLLER AND CABINET - IN PLACE	(F)
HANDHOLE	(G)
HANDHOLE - IN PLACE	(H)
RIGID STEEL CONDUIT (RSC)	(I)
RIGID STEEL CONDUIT (RSC) - IN PLACE	(J)
SIGNAL FACE WITH BACKGROUND SHIELD	(K)
SIGNAL FACE W/O BACKGROUND SHIELD	(L)
SIGNAL FACE - IN PLACE	(M)
PEDESTRIAN INDICATORS	(N)
PEDESTRIAN INDICATORS - IN PLACE	(O)
PEDESTRIAN PUSH BUTTONS ON PEDESTAL OR POLE	(P)
PEDESTRIAN PUSH BUTTON STATION	(Q)
TRAFFIC SIGNAL PEDESTAL	(R)
TRAFFIC SIGNAL PEDESTAL - INPLACE	(S)
TRAFFIC SIGNAL POLE AND MAST ARM	(T)
TRAFFIC SIGNAL POLE AND MAST ARM - IN PLACE	(U)
STREET LIGHT POLE AND LUMINAIRE	(V)
STREET LIGHT POLE AND LUMINAIRE - IN PLACE	(W)
MAST ARM AND LUMINAIRE	(X)
MAST ARM AND LUMINAIRE - INPLACE	(Y)
WOOD POLE	(Z)
WOOD POLE - IN PLACE	(AA)
SOURCE OF POWER	(AB)
RAILROAD SIGNAL - IN PLACE	(AC)
RIGHT OF WAY LINE	(AD)
CENTERLINE	(AE)
EDGE OF ROADWAY	(AF)
SHOULDERLINE	(AG)
CURB LINE	(AH)
STOP BAR	(AI)
EMERGENCY VEHICLE PREEMPTION DETECTOR	(AJ)

ABBREVIATIONS

3-1(EG)	SIGNAL HEAD PHASE "3" - NO "1"	P2-1(EG)	PED INDICATION PHASE "2" - NO. "1"
BR. GR.	BARE GROUND	PB	PUSH BUTTON
CH. SW.	CHECK SWITCH	PB2-1(EG)	PUSH BUTTON PHASE "2" - NO. "1"
CLR	CLEAR	PEC	PHOTOELECTRIC CELL
D2-1(EG)	DETECTOR PHASE "2" - NO. "1"	PED	PEDESTRIAN
DWK	DON'T WALK	R	RED
EQG	EQUIPMENT GROUND	R&S	REMOVE AND SALVAGE
EVP	EMERGENCY VEHICLE PRE-EMPTION	RLTA	RED LEFT TURN ARROW
F&I	FURNISH AND INSTALL	RRTA	RED RIGHT TURN ARROW
FL	FLASH/FLASHING	RSC	RIGID STEEL CONDUIT
G	GREEN	SOP	SOURCE OF POWER
GLTA	GREEN LEFT TURN ARROW	SPR	SPARE
GRN	GREEN	ST. LHT	STREET LIGHT
GR. R	GROUND ROD	STA	STATION
GRTA	GREEN RIGHT TURN ARROW	SW	SWITCH
GTHA	GREEN THRU ARROW	SWD	SWITCHED
HH	HANDHOLE	S&R	SALVAGE AND REINSTALL
HPS	HIGH PRESSURE SODIUM	TDW	TELEPHONE DROP WIRE
JB	JUNCTION BOX	WLK	WALK
LUM	LUMINAIRE	YEL	YELLOW
NEU	NEUTRAL	YLTA	YELLOW LEFT TURN ARROW
NMC	NONMETALLIC CONDUIT	YRTA	YELLOW RIGHT TURN ARROW
		YTHA	YELLOW THRU ARROW

CONDUCTOR COLOR CODE

R	RED
O	ORANGE
BL	BLUE
WH	WHITE
R/BLK	RED WITH BLACK TRACER
O/BLK	ORANGE WITH BLACK TRACER
BL/BLK	BLUE WITH BLACK TRACER
WH/BLK	WHITE WITH BLACK TRACER
BLK	BLACK
BLK/WH	BLACK WITH WHITE TRACER
G/BLK	GREEN WITH BLACK TRACER
G	GREEN

TRAFFIC SIGNAL TABULATION			
ITEM NO	ITEM	UNIT	TOTAL ESTIMATED QUANTITY
2104	REMOVE SIGNAL SYSTEM	EACH	1
2565	TRAFFIC CONTROL SIGNAL SYSTEM "B"	SIG. SYS.	1
2565	EMERGENCY VEHICLE PREEMPTION SYSTEM "B"	LUMP SUM	1
2565	TRAFFIC CONTROL INTERCONNECTION	LUMP SUM	1
2565	SIGNAL SERVICE CABINET	EACH	2
2565	REVISE SIGNAL SYSTEM "A"	SYSTEM	1
2565	TEMPORARY SIGNAL SYSTEM "A"	SYSTEM	1
2565	TEMPORARY SIGNAL SYSTEM "B"	SYSTEM	1

TRAFFIC SIGNAL STANDARD PLATES	
THE FOLLOWING STANDARD PLATES, APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION, SHALL APPLY ON THIS PROJECT	
PLATE NO.	DESCRIPTION
* 8000 J	CHANNELIZERS - TYPE A, B, C (3 SHEETS)
* 8110 E	TRAFFIC SIGNAL BRACKETING (POLE MOUNTED)
* 8111 E	TRAFFIC SIGNAL BRACKETING (PEDESTAL MOUNTED) (3 SHEETS)
* 8112 I	PEDESTAL FOUNDATION (TRAFFIC CONTROL SIGNALS)
* 8118 D	SERVICE EQUIPMENT & POLE-TRAFFIC CONTROL SIGNALS
* 8119 C	GROUND MOUNTED CABINET FOUNDATION
* 8120 Q	POLE FOUNDATION (PA 85)
* 8121 H	TRANSFORMER BASE & POLE BASE PLATE (2 SHEETS)
* 8122 F	PEDESTAL & PEDESTAL BASE (FOR TRAFFIC CONTROL SIGNALS SUPPORT) (2 SHEETS)
* 8123 G	POLE & MAST ARM-LUMINAIRES & TRAFFIC LIGHTS ASSEMBLY (2 SHEETS)
* 8126 L	POLE FOUNDATION (PA90 & PA100)
* 8129 A	SHIM AND WASHER (TRAFFIC CONTROL SIGNALS AND ROADWAY LIGHTING)

* - STANDARD PLATES APPLICABLE TO THIS PROJECT

S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10

DRAWN BY: JMG
DESIGNER: JMG
CHECKED BY: JMG

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
Date: April 4, 2016 Name: John M. Gray, PE Lic. No. 22457

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PHONE: (651) 490-2000
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ST. PAUL, MN 55110

ANOKA COUNTY
CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEMS 'A-B'
DETAILS AND STANDARD PLATES
CSAH 11 (FOLEY BLVD NW) SIGNAL SYSTEMS

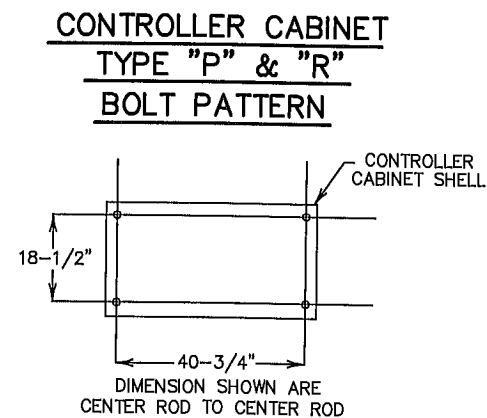
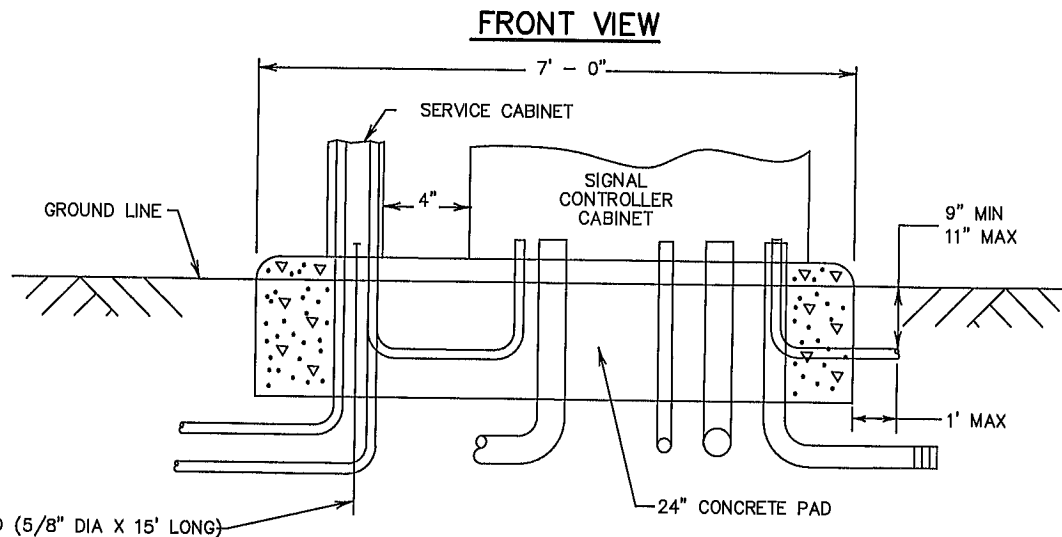
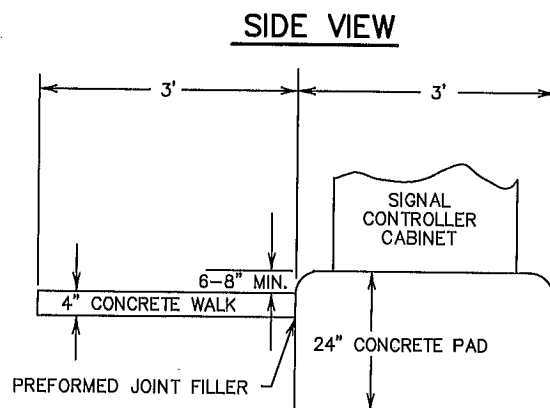
FILE NO. ANOKC 132239
SIGNAL SHEET 1 OF 29
298

TYPICAL PAD WITH CONTROLLER CABINET AND SERVICE CABINET

SEE INTERSECTION LAYOUT FOR CABLE INFORMATION (NOT TO SCALE)

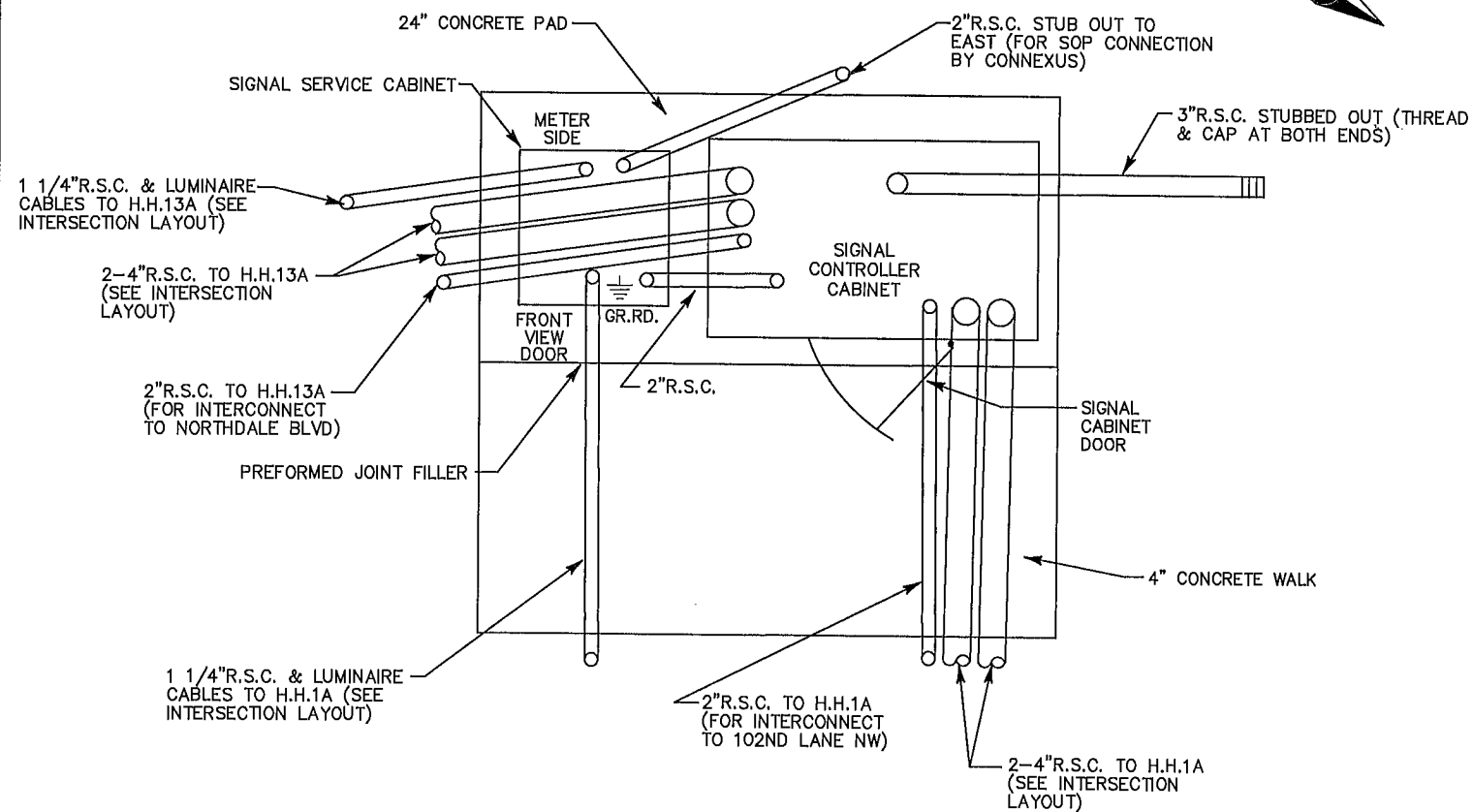
NOTES:

1. THE ANCHOR RODS, NUTS AND WASHERS FOR THE COUNTY FURNISHED CONTROLLERS AND CABINETS SHALL BE FURNISHED BY THE COUNTY AND INSTALLED BY THE CONTRACTOR.
2. THE UPPER PART OF EACH NEW EQUIPMENT PAD SHALL BE BEVELLED OR CHAMFERED IN A NEAT MANNER AS DIRECTED BY THE ENGINEER.
3. THE TOP OF THE CONDUITS SHALL BE THREADED AND CAPPED AFTER INSTALLATION (UNTIL CABLES ARE INSTALLED).
4. CONDUIT SHALL PROJECT A MINIMUM OF 2" ABOVE CONCRETE AND SHALL BE LOCATED INSIDE OF THE CABINET WHERE DIRECTED BY THE ENGINEER, BUT SHALL NOT INTERFERE WITH THE CABINET FUNCTIONS (SUPPORTING MEMBERS, ETC.).
5. CONCRETE MIX 3F52 OR EQUAL SHALL BE USED FOR THE EQUIPMENT PADS AND SIDEWALKS.
6. CONDUITS WITH BOTH ENDS TERMINATING WITHIN THE PADS SHALL NOT BE INSTALLED BELOW THE CONCRETE.
7. THE EXACT LOCATION OF CONDUITS WITHIN EACH PAD SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
8. ANCHOR RODS SHALL PROJECT A MINIMUM OF 3" ABOVE THE CONCRETE BUT SHALL NOT INTERFERE WITH THE CABINET FUNCTIONS (SUPPORTING MEMBERS, ETC.).
9. CONTRACTOR SHALL PROVIDE MINIMUM 4-INCH CLEARANCE BETWEEN CONTROLLER AND SERVICE CABINETS ON EACH EQUIPMENT PAD FOUNDATION AS SHOWN.



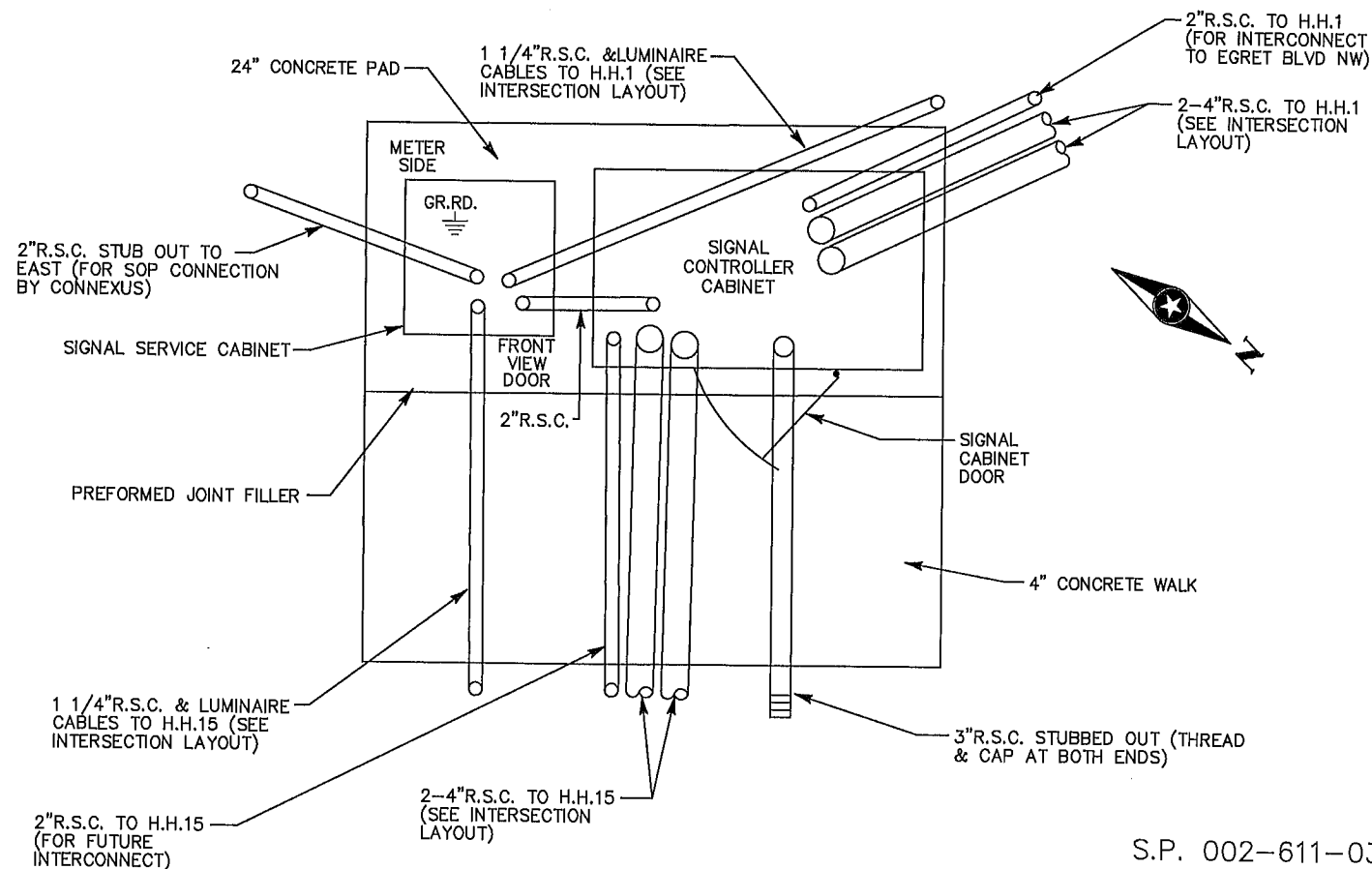
PLAN VIEW - SYSTEM "A"

CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW



PLAN VIEW - SYSTEM "B"

CSAH 11 (FOLEY BLVD NW) AT CSAH 11-12 (NORTHDALE BLVD NW)

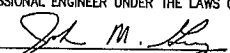



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S.P. 114-020-050
CITY PROJ. 13-10

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DESIGNER: JMG
CHECKED BY: JMG
DESIGN TEAM

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 Date: April 4, 2016

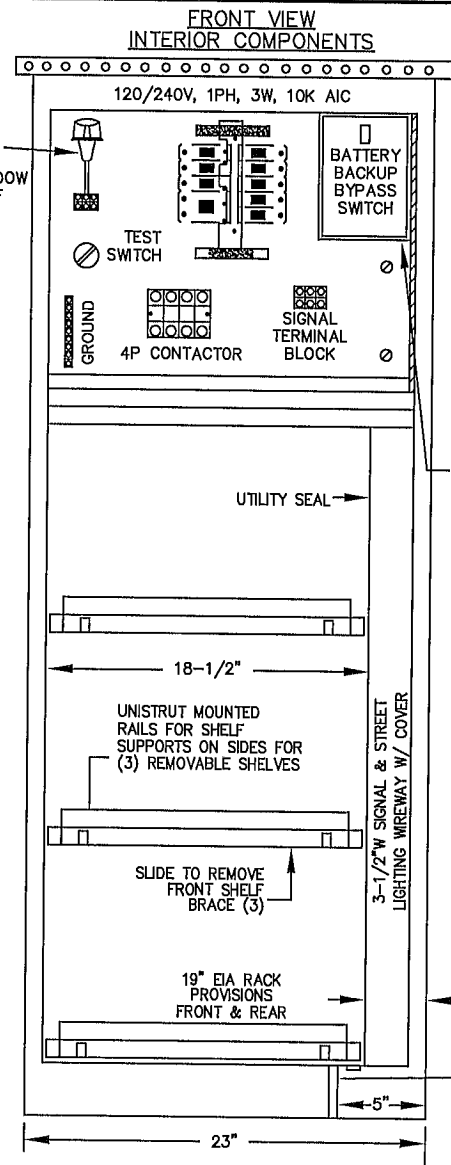
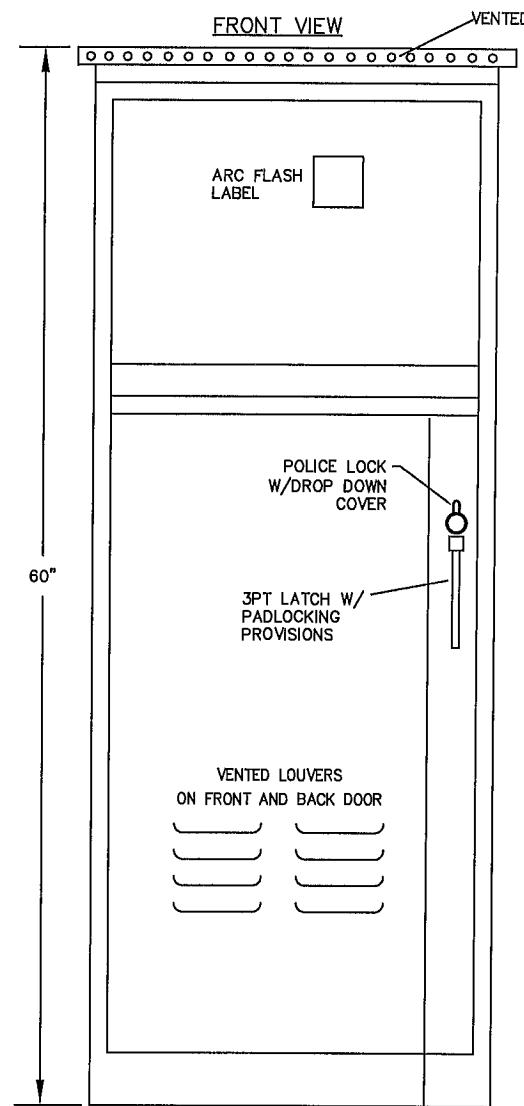

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ANOKA COUNTY
CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEMS "A-B"
EQUIPMENT PAD DETAILS
CSAH 11 (FOLEY BLVD NW) SIGNAL SYSTEMS

FILE NO. ANOKC 132239
SIGNAL SHEET 2 OF 29

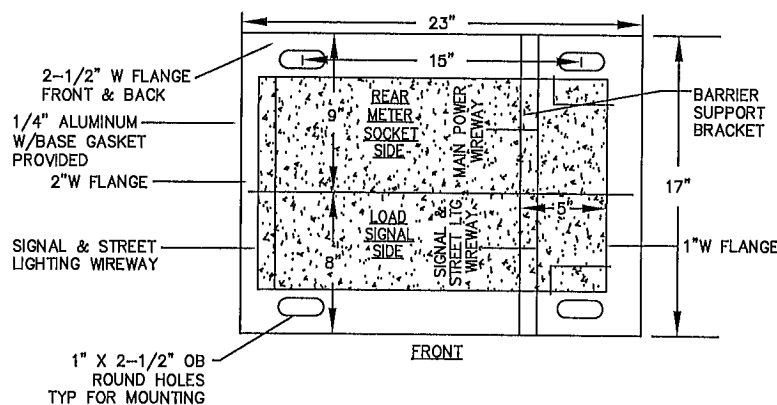
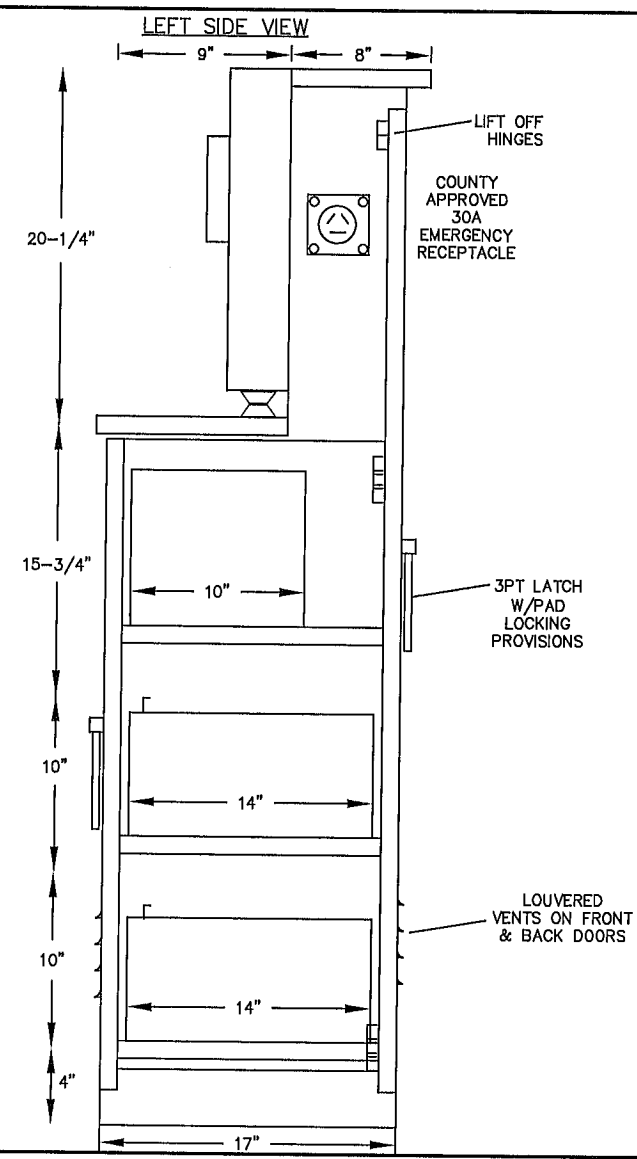
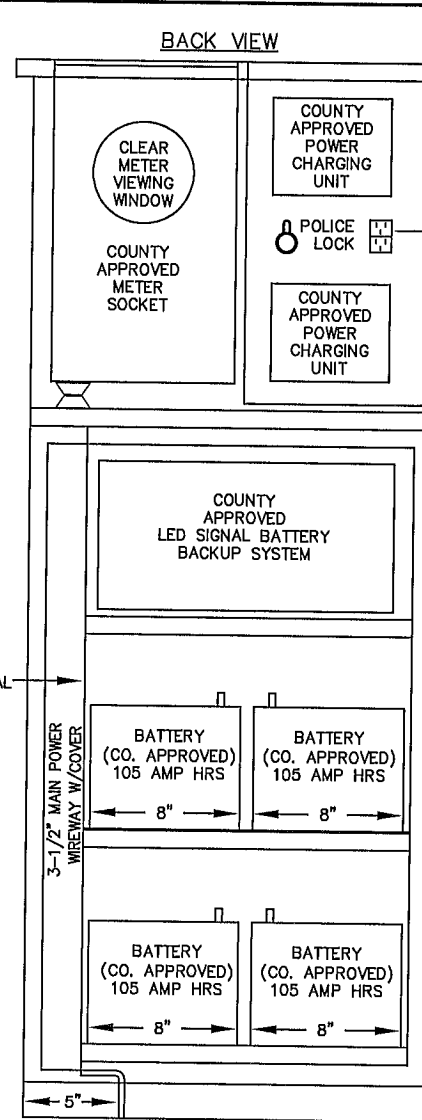
164
298



- LOAD CENTER
CIRCUIT
BREAKERS
ITE "Q" TYPE
1-100A/2P
SERVICE
DISCONNECT
1-20A/1P GFCI
RECEPTACLE
1-15/1P
PHOTOCELL
4-15A/1P
LUMINAIRES
1-30A/1P
SIGNAL SVC
1 SPARE

CUTOUT PROVISIONS IN
DEAD FRONT FOR BATTERY
BACKUP BYPASS SWITCH

INTERIOR
COMPONENTS
BEHIND HINGED
DEAD FRONT
W/ (2)-1/4
TURN LATCHES

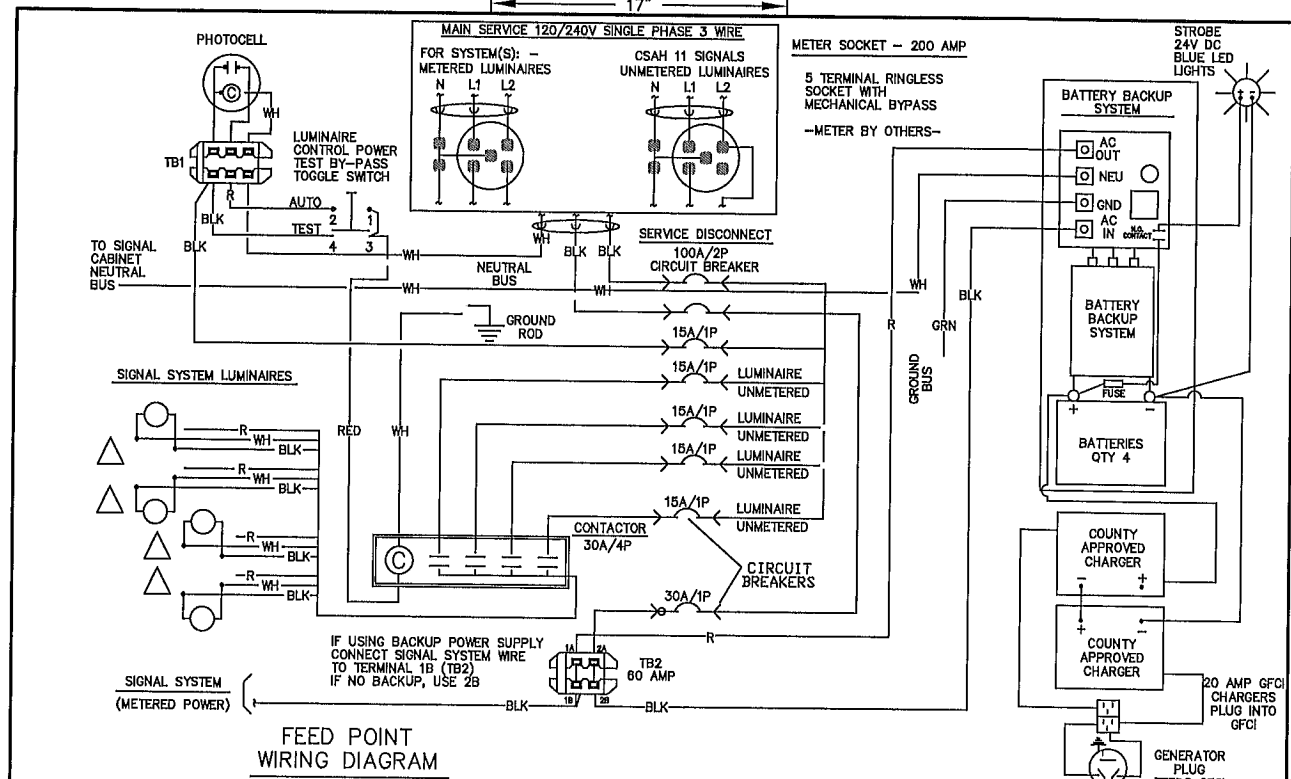


CABINET CONSTRUCTION

- NEMA 3R
- 1/8" ALUMINUM 5052-H32
- ANODIZED 30 MINUTE CLEAR
- NEOPRENE GASKETED DOORS
- NON-CORRODING HARDWARE
- ETL LISTED IN ACCORDANCE W/UL508A

SEE SPECIAL PROVISIONS AND STATEMENT OF
ESTIMATED QUANTITIES REGARDING SEPARATE PAY
ITEM FOR FURNISHING & INSTALLING NEW BATTERY
BACK-UP SIGNAL SERVICE CABINETS.

S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10



DRAWN BY: JMG
DESIGNER: JMG
CHECKED BY: JMG
DESIGN TEAM

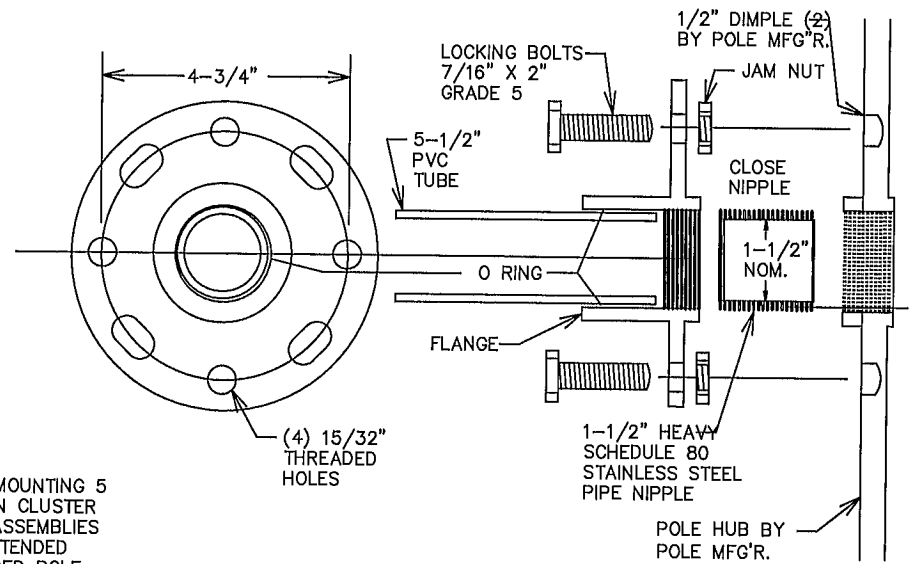
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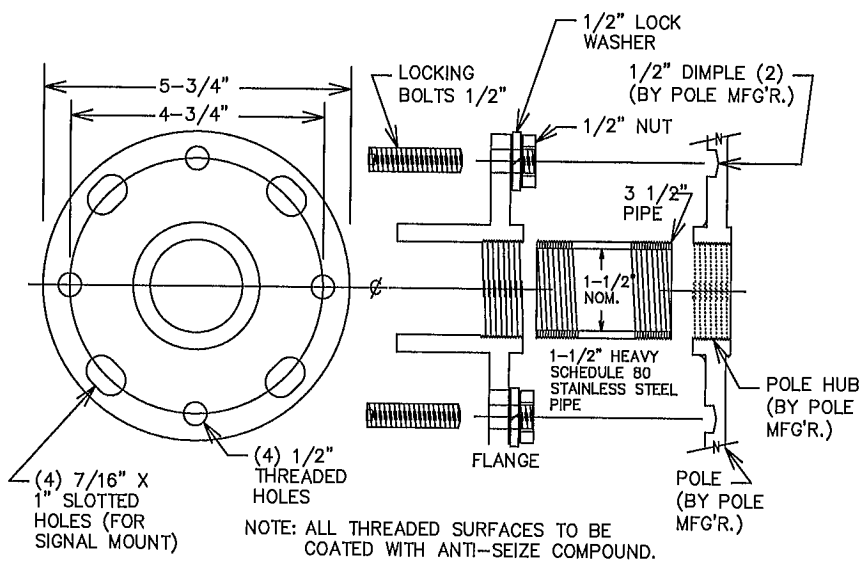
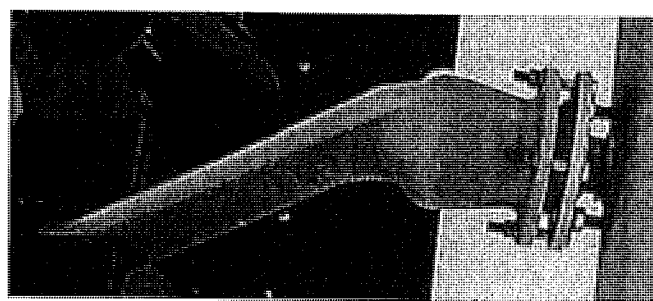
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ST. PAUL, MN 55110

ANOKA COUNTY
CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEMS "A-B"
SIGNAL SERVICE CABINET DETAILS
CSAH 11 (FOLEY BLVD NW) SIGNAL SYSTEMS
FILE NO. ANOKC 132239
SIGNAL SHEET 3 OF 29
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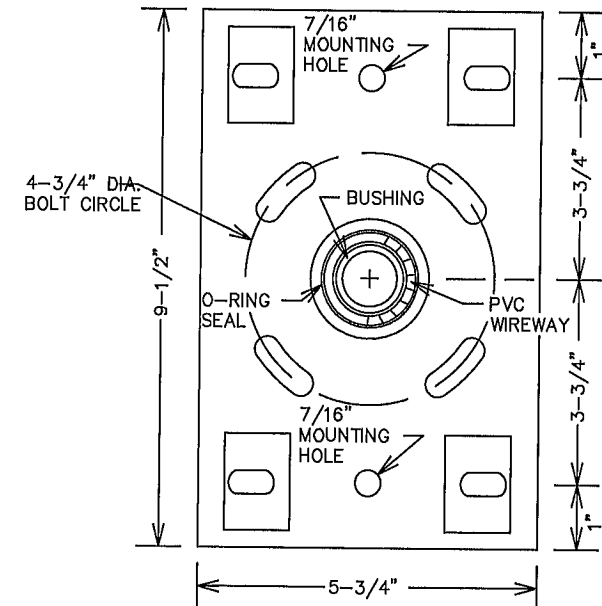


THREADED HUB AND FLANGE POLE ADAPTOR

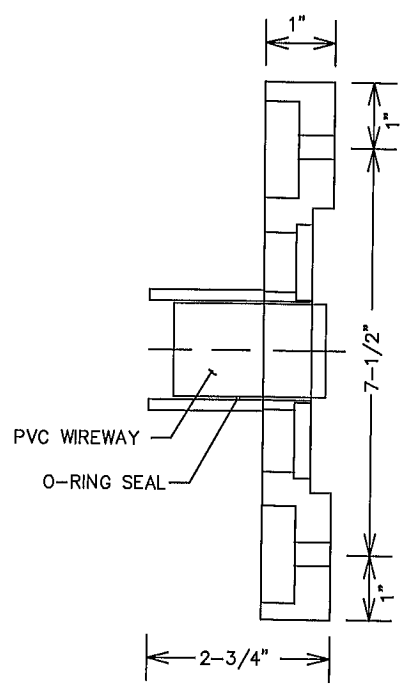


EXTENDED THREADED POLE ADAPTER

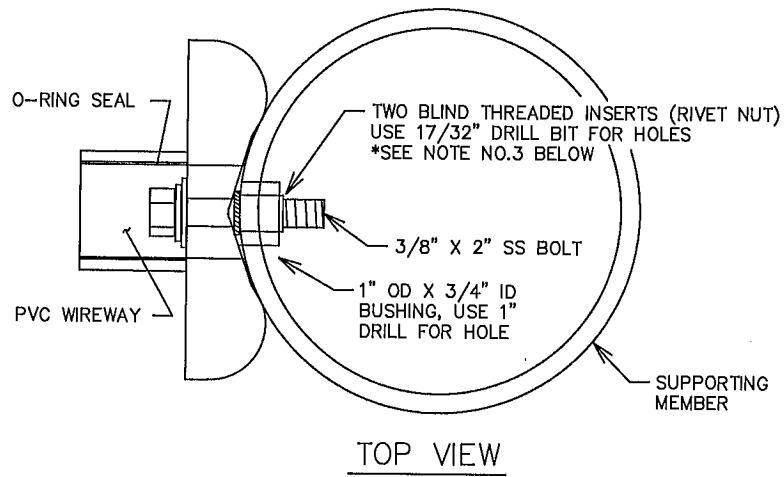
- NOTES:
1. ALL THREADED SURFACES TO BE COATED WITH ANTI-SEIZE COMPOUND.
 2. USE SIGNAL HEAD MOUNTED SPACERS FOR 4 SECTION POLY HEADS.
 3. SEE STANDARD PLATE NUMBER 8123 FOR ADDITIONAL SIGNAL POLE DETAILS.
 4. EXTENDED THREADED POLE ADAPTOR ONLY USED WITH 5 SECTION CLUSTER HEADS.



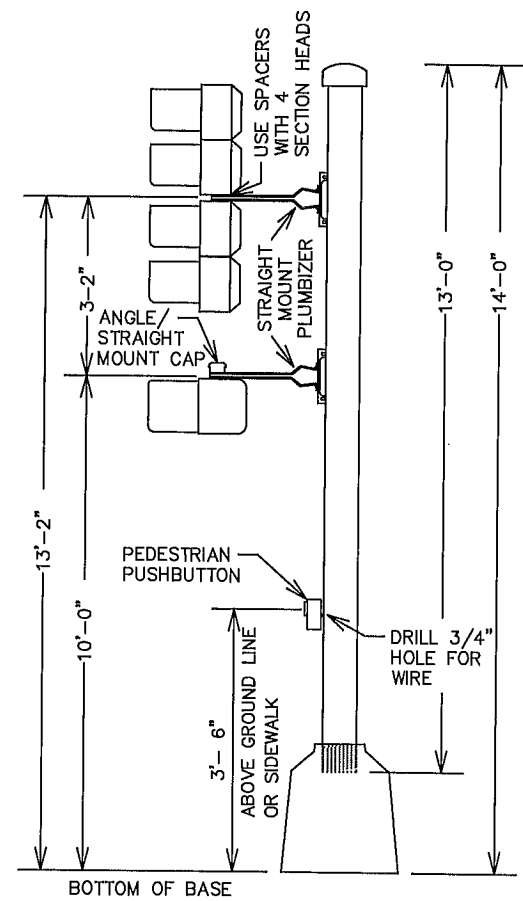
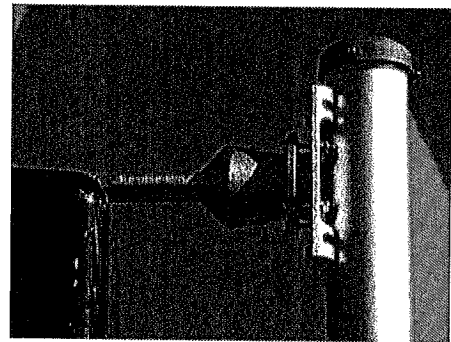
BOLT ON HUB & FLANGE



SIDE VIEW



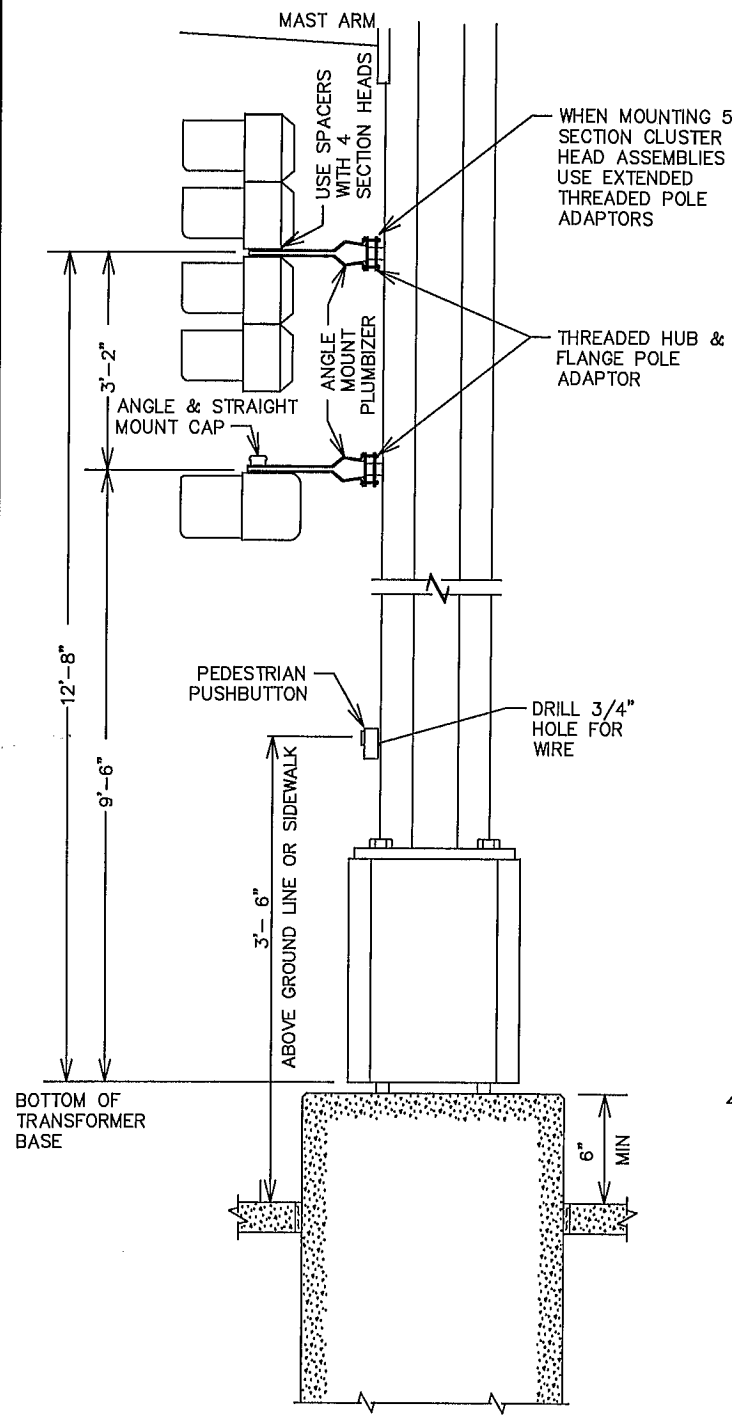
TOP VIEW



TYPICAL PEDESTAL MOUNTING

NOT TO SCALE

- NOTES:
1. ALL THREADED SURFACES TO BE COATED WITH ANTI-SEIZE COMPOUND.
 2. USE SIGNAL HEAD MOUNTED SPACERS FOR 4 SECTION POLY HEADS.
 3. BLIND THREADED INSERTS (RIVET NUT) MUST BE INSERTED USING MANUFACTURERS SPECIFIC INSERTION TOOL. NO OTHER METHOD IS ACCEPTABLE.
 4. SEE STANDARD PLATE NUMBER 8122 FOR ADDITIONAL PEDESTAL POLE DETAILS.



TYPICAL SIGNAL POLE MOUNTING

NOT TO SCALE

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DESIGNER:	JMG
CHECKED BY:	JMG
DESIGN TEAM	

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 Date: April 4, 2016

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ANOKA COUNTY
 CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEMS 'A-B'
 POLE MOUNT DETAILS
 C8AH 11 (FOLEY BLVD NW) SIGNAL SYSTEMS

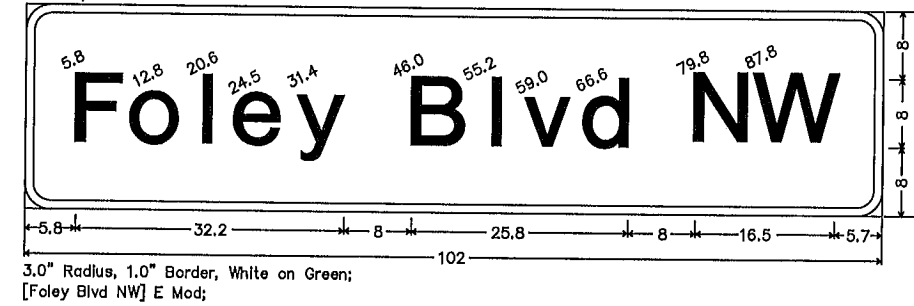
FILE NO. 166
 ANOKC 132239
 SIGNAL SHEET 4 OF 29
 298

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 S.P. 114-020-050
 CITY PROJ. 13-10

SIGNS FOR TRAFFIC SIGNAL SYSTEM										
INPLACE SIGN PANELS (SALVAGE AND INSTALL)										
SIGNAL SYSTEM	SIGN PANEL	POLE NO.	a (FT)	b (FT)	APPROX. SIZE (IN)	MOUNTING BRACKET		NO. REQ.	PANEL LEGEND	ACTION
						QUANTITY	SPACING (1)			
A	D-5	4	-	28'	102 x 24	4	----	1	Foley Blvd NW	RELOCATE FROM OLD TO NEW MAST ARM
A	R10-X12	4	1'	-	36 x 48	2	----	1	R10-X12	RELOCATE FROM OLD TO NEW MAST ARM

(1) = SPACING BETWEEN STIFFENERS SHALL NOT EXCEED 36 INCHES AND SHALL BE UNIFORMLY SPACED. SEE STANDARD SIGNS MANUAL, PAGE 105A (REVISION DATE 7/06/2007) FOR BRACKET SPACING REQUIREMENTS.

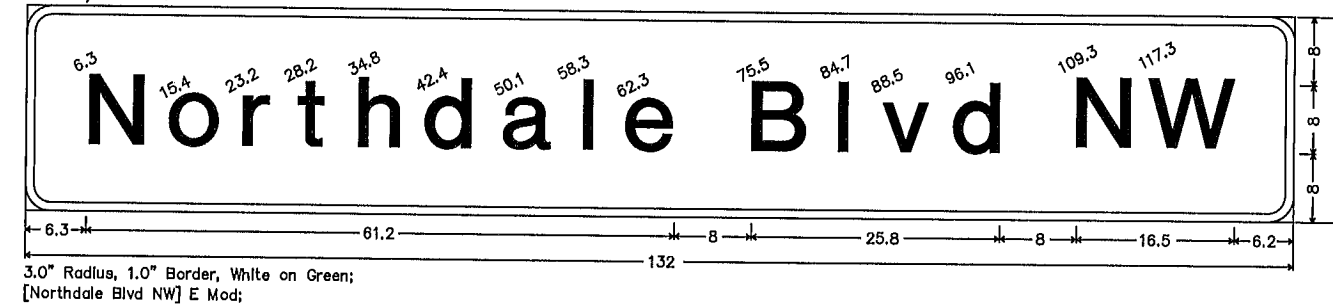
D-1, D-3



SIGNS FOR TRAFFIC SIGNAL SYSTEM										
SIGN PANELS - TYPE D (FURNISH AND INSTALL)										
SIGNAL SYSTEM	SIGN PANEL	POLE NO.	a (FT)	b (FT)	SIZE (IN)	MOUNTING BRACKET		AREA/SIGN (SQ. FT.)	NO. REQ.	PANEL LEGEND
						QUANTITY	SPACING (1)			
B	D-1	1	-	18'	102 x 24	4	----	17.00	1	Foley Blvd NW
B	D-2	2	-	28'	132 x 24	5	----	22.00	1	Northdale Blvd NW
B	D-3	4	-	18'	102 x 24	4	----	17.00	1	Foley Blvd NW
B	D-4	5	-	28'	132 x 24	5	----	22.00	1	Northdale Blvd NW
TOTAL QUANTITIES								78.00	4	

(1) = SPACING BETWEEN STIFFENERS SHALL NOT EXCEED 36 INCHES AND SHALL BE UNIFORMLY SPACED. SEE STANDARD SIGNS MANUAL, PAGE 105A (REVISION DATE 7/06/2007) FOR BRACKET SPACING REQUIREMENTS.

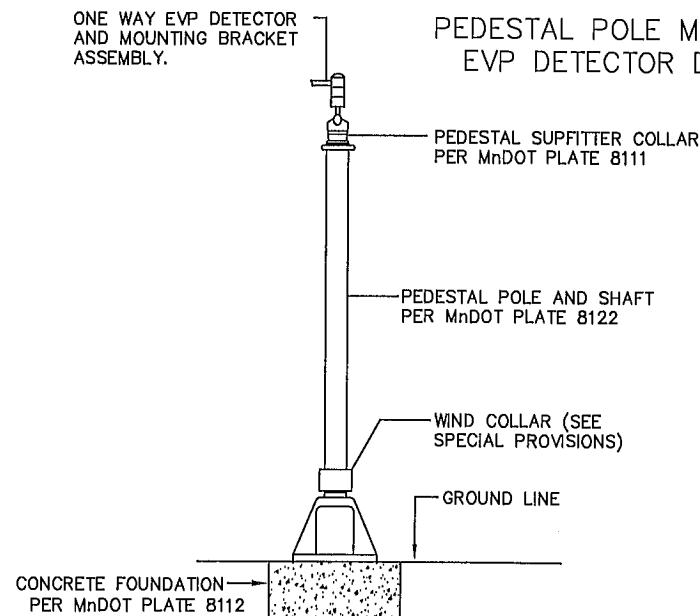
D-2, D-4



SIGNS FOR TRAFFIC SIGNAL SYSTEM										
SIGN PANELS - TYPE C (FURNISH AND INSTALL)										
SIGNAL SYSTEM	SIGN PANEL	POLE NO.	a (FT)	b (FT)	SIZE (IN)	MOUNTING BRACKET		AREA/SIGN (SQ. FT.)	NO. REQ.	PANEL LEGEND
						QUANTITY	SPACING (1)			
B	R10-X12	1,2,4,5	1'	-	36 x 48	2	----	12.00	4	LEFT TURN YIELD ON FLASHING YELLOW ARROW
TOTAL QUANTITIES								48.00	4	

ONE WAY EVP DETECTOR AND MOUNTING BRACKET ASSEMBLY.

PEDESTAL POLE MOUNTED EVP DETECTOR DETAIL

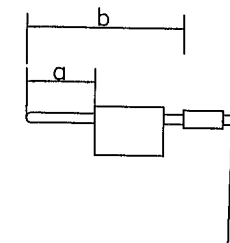


CONDUCTOR COLOR CODE (14 GAUGE)			
TO SIGNAL CABINET		TO DEVICE	
1/C#6 G	R	R	RED
6PR#19	O	O	YEL
R	BL	BL	GRN
3-1/C#2 WH	BLK/R	WH	NEU
BLK	BLK	BLK	YLA/FYLA
INPUT POWER	BLK	BLK	GLA
3-1/C#6 WH	R	R	RED/DWK
G	BLK/R	BLK/R	YEL/WLK
SIGNAL SERVICE	BLK	BLK	GRN/SPR
R	WH	WH	NEU
4/C#14	BLK	BLK	3 SECTION AND PED INDICATION
O	WH	WH	
BL	BLK	BLK	
WH	G	G	EVP LIGHT LUM/FLASHER
R/BLK	BLK	BLK	
O/BLK	WH	WH	
BL/BLK	BLK	BLK	
WH/BLK	WH or CL	WH or CL	PED PUSH BUTTON (if required)
BLK	R or O	R or O	
BLK/WH	WH or YEL	WH or YEL	
BLK/R	BLK or BL	BLK or BL	
WH/R			

NOTE: ALL TERMINAL BLOCK CONNECTIONS SHALL BE ARRANGED AS SPECIFIED ABOVE.

GENERAL SIGNING NOTES:

- COLOR FOR ALL TYPE D SIGNS SHALL BE WHITE LEGEND AND BORDER ON GREEN BACKGROUND, FULLY REFLECTORIZED.
- CORNERS EXTENDING BEYOND THE BORDER SHALL NOT BE TRIMMED.
- FOR STRUCTURAL DETAILS OF MAST ARM MOUNTED SIGNS, SEE STANDARD SIGNS MANUAL, PAGE 105A (REVISION DATE: 7/06/07), AND SPECIAL PROVISIONS.
- SEE STANDARD SIGNS MANUAL FOR DETAILED DRAWINGS OF TYPE C SIGN PANELS.
- FURNISHING AND INSTALLING NEW TYPE C & D SIGNS SHALL BE INCLUDED UNDER THE PAY ITEM FOR ITEM NO. 2565 (TRAFFIC CONTROL SIGNAL SYSTEM "B"). SEE SPECIAL PROVISIONS.
- SALVAGING AND REINSTALLING INPLACE TYPE C & D SIGNS SHALL BE INCLUDED UNDER THE PAY ITEM FOR ITEM NO. 2565 (REVISE SIGNAL SYSTEM "A"). SEE SPECIAL PROVISIONS.
- ALL NEW TYPE C AND D SIGN PANELS SHALL BE FABRICATED USING DG3 SHEETING. SEE SPECIAL PROVISIONS.



DRAWN BY: JMG
 DESIGNER: JMG
 CHECKED BY: JMG
 DESIGN TEAM

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ANOKA COUNTY
 CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEMS "A-B"
 SIGNING AND MISCELLANEOUS DETAILS
 CSAH 11 (FOLEY BLVD NW) SIGNAL SYSTEMS

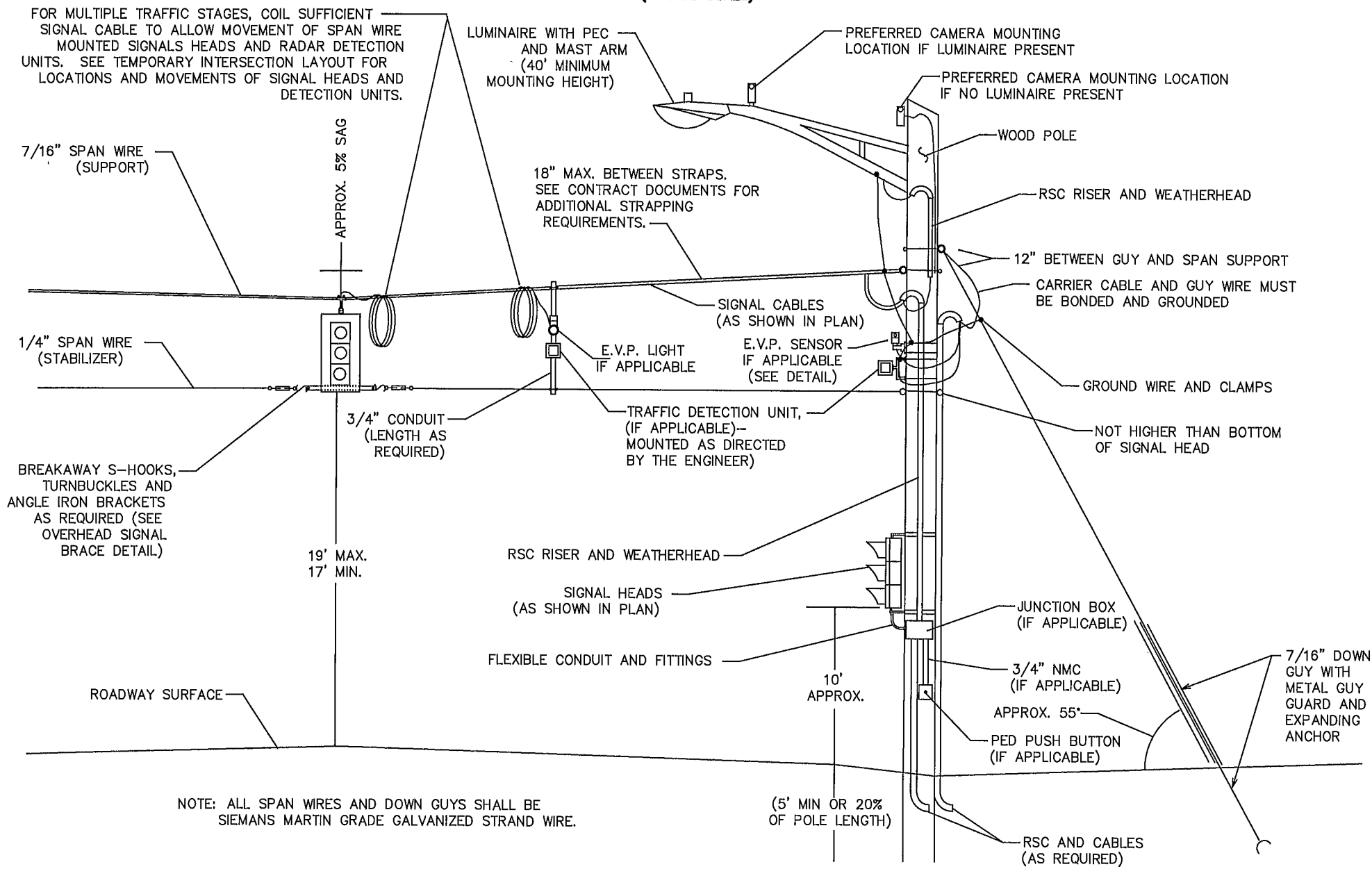
FILE NO. ANOKC 132239
 SIGNAL SHEET 5 OF 29

167
 298

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 S.P. 114-020-050
 CITY PROJ. 13-10

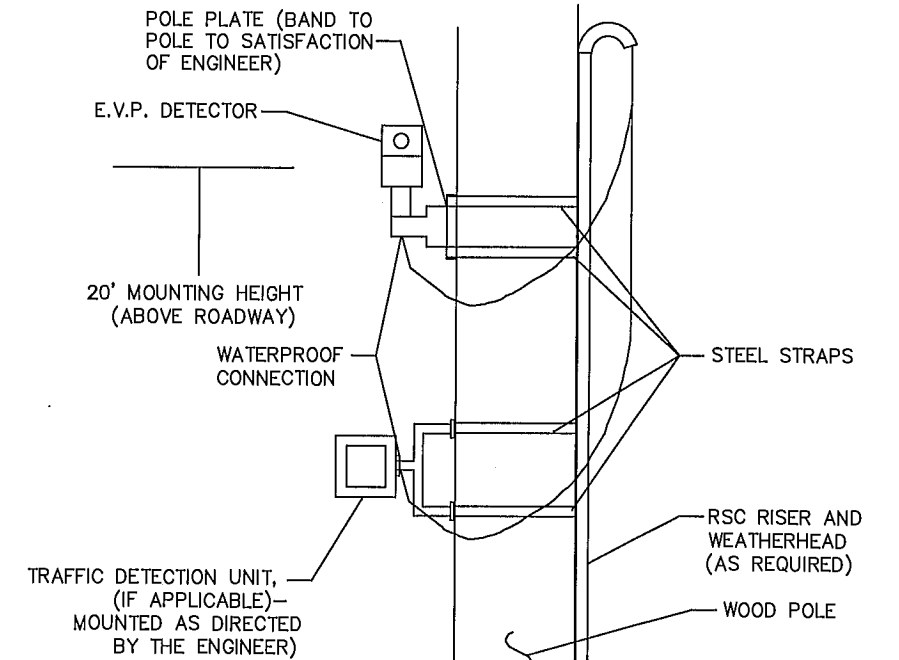
TYPICAL WOOD POLE AND SPAN WIRE MOUNTED TRAFFIC SIGNALS

(NOT TO SCALE)



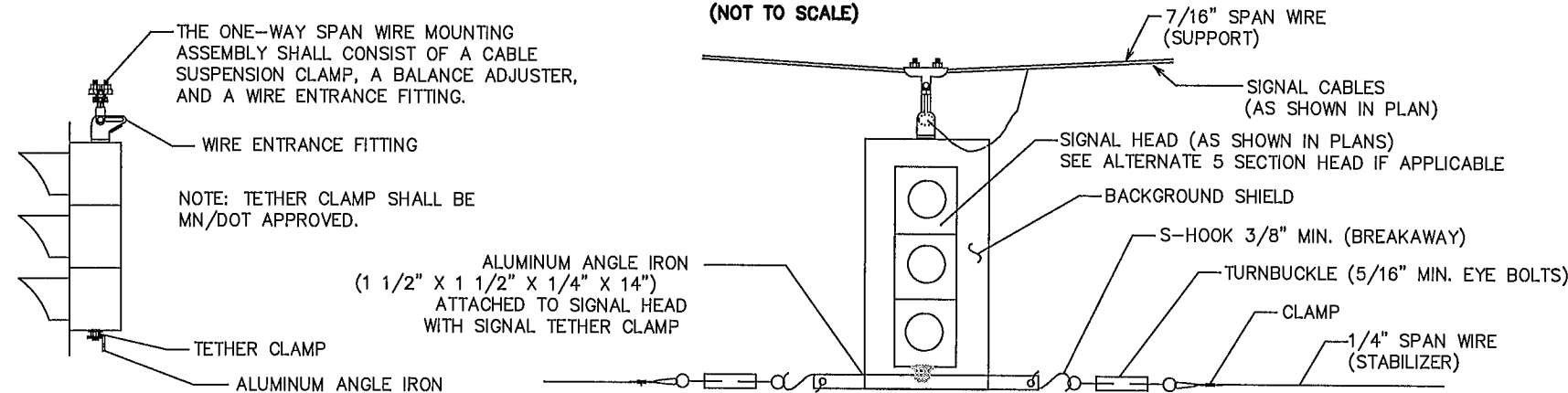
E.V.P. OR TRAFFIC DETECTOR WOOD POLE MOUNT

(NOT TO SCALE)



OVERHEAD SIGNAL BRACE DETAIL

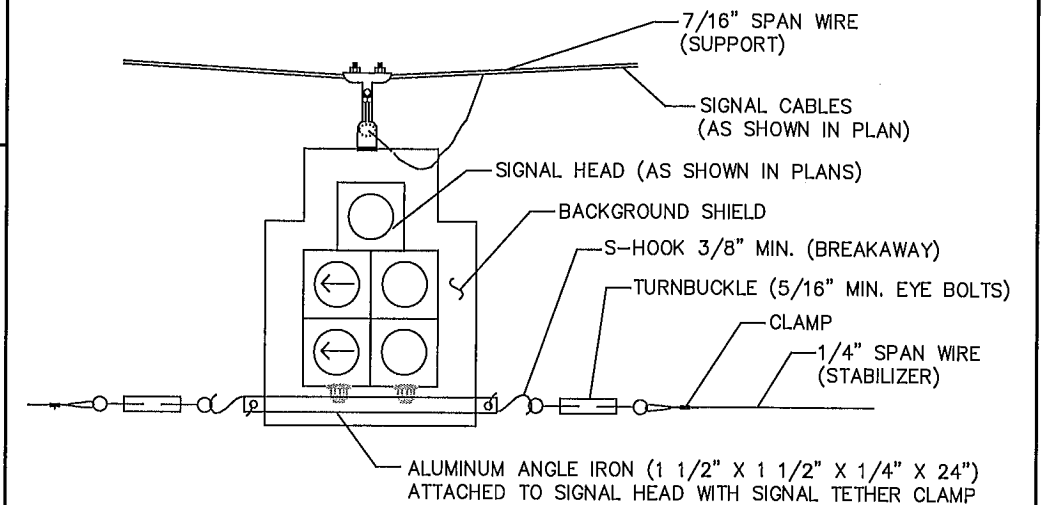
(NOT TO SCALE)



5 SECTION HEAD OVERHEAD

SIGNAL BRACE DETAIL

(NOT TO SCALE)



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S.P. 114-020-050
CITY PROJ. 13-10

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Date: April 4, 2016

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ANOKA COUNTY
CITY OF COON RAPIDS

TEMPORARY SIGNAL SYSTEMS "A-B"
WOOD POLE AND SPAN WIRE DETAILS
CSAH 11 (FOLEY BLVD NW) SIGNAL SYSTEMS

FILE NO.
ANOKC 132239
SIGNAL SHEET
6 OF 29

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NOTES:

- 1) LOCATION OF WOOD POLES, FOUNDATIONS, AND HANDHOLES SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) NEW HANDHOLE 100 SHALL BE A PVC HANDHOLE WITH METAL FRAME AND COVER (SEE SPECIAL PROVISIONS).
- 3) EACH NEW SIGNAL FACE SHALL HAVE BACKGROUND SHIELD.
- 4) SEE DETAILS FOR WOOD POLE AND SPAN WIRE MOUNTING DETAILS.
- 5) ALL NEW VEHICLE SIGNAL INDICATIONS SHALL BE LED AND SHALL BE FURNISHED AND INSTALLED BY CONTRACTOR AS SHOWN BELOW.
- 6) ALL TRAFFIC SIGNAL MATERIALS AND ELECTRICAL EQUIPMENT TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR THE TEMPORARY SIGNAL SYSTEM SHALL BE APPROVED BY ENGINEER PRIOR TO INSTALLATION AT THE INTERSECTION. SEE SPECIAL PROVISIONS.
- 7) MOVEMENT/RELOCATION OF HEADS, EVP DETECTORS AND INDICATOR LIGHTS, AND VIDEO DETECTOR CAMERAS SHALL BE INCLUDED IN THE PAY ITEM FOR "TEMPORARY SIGNAL SYSTEM A".
- 8) PROPOSED TEMPORARY SIGNAL SYSTEM PLAN SHOWN IS PROPOSED LAYOUT FOR STAGES 1B, 2A AND 2B OF THE TRAFFIC CONTROL PLAN. PLACEMENT OF ALL SIGNAL INDICATIONS AND OTHER ITEMS FOR STAGES 1B, 2A AND 2B STAGING PLANS SHALL BE IN ACCORDANCE WITH STAGING PLANS INCLUDED ELSEWHERE IN THE PLANS.
- 9) SEE SPECIAL PROVISIONS REGARDING VIDEO DETECTION SYSTEM TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR, AND FOR ALL VIDEO DETECTION SYSTEM COMPONENTS TO BE TURNED OVER TO THE COUNTY (FOR THE COUNTY TO OWN) AFTER TEMPORARY SIGNAL SYSTEM "A" IS REMOVED AND REVISE SIGNAL SYSTEM "A" IS MADE OPERATIONAL (INCLUDED AS PART OF PAY ITEM FOR "TEMPORARY SIGNAL SYSTEM "A").
- 10) (F & I) = ITEMS TO BE FURNISHED & INSTALLED BY CONTRACTOR.
(S & I) = ITEMS TO BE SALVAGED & INSTALLED BY CONTRACTOR.
- 11) CONTRACTOR SHALL BAG (AND MAKE IN-OPERATIONAL) ALL VEHICLE SIGNAL HEADS NOT IN USE DURING CONSTRUCTION.
- 12) CONTRACTOR SHALL MAINTAIN A SIGNAL SYSTEM IN OPERATION AT THIS INTERSECTION AT ALL TIMES, UNLESS OTHERWISE APPROVED BY THE ENGINEER FOR THE SIGNAL SYSTEM TO BE TURNED OFF DURING NON-PEAK TRAFFIC PERIODS (FOR SWITCHOVERS FROM EXISTING SIGNAL SYSTEM TO TEMPORARY SIGNAL SYSTEM, AND FROM TEMPORARY SIGNAL SYSTEM TO REVISED PERMANENT SIGNAL SYSTEM).
- 13) CONTRACTOR SHALL PROTECT AND MAINTAIN ALL ITEMS OF THE EXISTING PERMANENT SIGNAL SYSTEM THAT WILL BE REUSED AS PART OF BOTH THE TEMPORARY AND REVISE SIGNAL SYSTEMS, AND SHALL REPLACE ITEMS DAMAGED DURING CONSTRUCTION WITH NEW ITEMS (AT NO EXPENSE TO THE COUNTY).
- 14) CONTRACTOR SHALL FURNISH AND INSTALL ONE (1) EMERGENCY VEHICLE PREEMPTION (EVP) DETECTOR AND TWO (2) EVP CONFIRMATION LIGHTS WHERE SHOWN, ALONG WITH ALL WOOD POLE AND SPAN WIRE MOUNTING HARDWARE, ALL AS PART OF PAY ITEM FOR "TEMPORARY SIGNAL SYSTEM "A". NEW EVP DETECTORS SHALL BE COMPATIBLE WITH "OPTICOM" EVP SYSTEM ALREADY INPLACE ON SIGNAL SYSTEM.
- 15) CONTRACTOR SHALL TEMPORARILY MOUNT EXISTING STREET NAME SIGNS (CURRENTLY INPLACE ON NORTHEAST AND NORTHWEST MAST ARMS) AT INTERSECTION IN ORDER TO KEEP STREET NAME SIGNS VISIBLE TO MOTORISTS AND PEDESTRIANS AT ALL TIMES DURING CONSTRUCTION (INCIDENTAL).

① F & I 50' WOOD POLE--CLASS 2
 2-DOWN GUYS, GUY GUARDS, AND SCREW ANCHORS
 15' MAST ARM AND LUMINAIRE (250 W HPS) WITH PEC
 1-TYPE 10B-WOOD POLE MOUNTED 90 DEG (4-4, P3-1)
 1-TYPE 10B-WOOD POLE MOUNTED 180 DEG (2-1, P2-2)
 2-PEDESTRIAN PUSH BUTTONS, SIGNS (R10-3e), AND RISERS
 VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING NB TRAFFIC) (V2/5-1)
 MOUNTING HARDWARE FOR VIDEO CAMERA (FOR MOUNTING ON LUMINAIRE EXTENSION)
 METAL JUNCTION BOX WITH TERMINAL BLOCKS
 2" CONDUIT RISER & WEATHERHEAD ABOVE JUNCTION BOX WITH:
 1-6/c#14
 3-4/c#14
 2-2/c#14
 2" CONDUIT RISER AND WEATHERHEAD ABOVE SPAN WIRE WITH:
 1-3/c#14 (VIDEO)
 1-3/c#14 (LUM)
 EXTEND INTO H.H.100:
 2-4" CONDUIT RISERS AND WEATHERHEADS
 8-6/c#14
 17-4/c#14
 4-3/c#14 (EVP)
 4-3/c#20 (EVP)
 8-2/c#14
 4-3/c#14 (LUM)
 4-3/c#14 (VIDEO)

② INPLACE (MAINTAIN INPLACE) PA90 POLE FOUNDATION
 TYPE PA90-A-35-D40-9 (DAVIT AT 350 DEG)
 LUMINAIRE-LED "SHOEBOX"
 LUMINAIRE CHECK SWITCH AT 0 DEG
 1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0'
 1-STRAIGHT MOUNT SIGNAL-OVERHEAD AT 12'
 2-TYPE 10B-POLE MOUNTED 90 DEG & 180 DEG
 2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e)
 TYPE D SIGN PANEL-OVERHEAD
 ONE WAY EVP DETECTOR AND LIGHT (Ø4)-3 FEET FROM RIGHT END OF MAST ARM

INPLACE R10-X12 SIGN PANEL-OVERHEAD (COVER DURING ALL STAGES OF CONSTRUCTION)
 F & I R-Y-G LED LENSES FOR SIGNAL HEAD 4-3 (REMOVE AND PROTECT RLA-YLA-FYLA LENSES, AND REINSTALL FOR REVISE SIGNAL SYSTEM)
 VIDEO CAMERA-MAST ARM MOUNTED (FACING EB TRAFFIC) (V4-1)
 5-FOOT EXTENSION & MOUNTING HARDWARE FOR VIDEO CAMERA (FOR MOUNTING ON TRAFFIC SIGNAL MAST ARM)
 1/4" SPAN WIRE FROM POLE 2 TO WOOD POLE 5:
 2-6/c#14
 4-4/c#14
 1-3/c#14 (EVP)
 1-3/c#20 (EVP)
 2-2/c#14
 1-3/c#14 (LUM)
 1-3/c#14 (VIDEO)

③ INPLACE (MAINTAIN INPLACE) PA100 POLE FOUNDATION
 TYPE PA100-A-50-D40-9 (DAVIT AT 350 DEG)
 LUMINAIRE-LED "SHOEBOX"
 LUMINAIRE CHECK SWITCH AT 0 DEG
 1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0'
 2-STRAIGHT MOUNT SIGNALS-OVERHEAD AT 12' & 24'
 2-TYPE 10B-POLE MOUNTED 90 DEG & 180 DEG
 2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e)
 R10-X12 SIGN PANEL-ADJACENT TO 1-1
 TYPE D SIGN PANEL-OVERHEAD
 TWO WAY EVP DETECTOR AND ONE WAY LIGHT (Ø6,1 DETECTOR/LIGHT, Ø2,5 DETECTOR)

INPLACE FYA LED LENS FOR SIGNAL HEAD 3-3 (MAKE INOPERATIONAL FOR ALL STAGES OF CONSTRUCTION)
 F & I VIDEO CAMERA-MAST ARM MOUNTED (FACING SB TRAFFIC) (V6/1-1)
 5-FOOT EXTENSION & MOUNTING HARDWARE FOR VIDEO CAMERA (FOR MOUNTING ON TRAFFIC SIGNAL MAST ARM)
 1/4" SPAN WIRE FROM POLE 3 TO WOOD POLE 6:
 2-6/c#14
 5-4/c#14
 1-3/c#14 (EVP)
 2-3/c#20 (EVP)
 2-2/c#14
 1-3/c#14 (LUM)
 1-3/c#14 (VIDEO)

④ F & I 50' WOOD POLE--CLASS 2
 2-DOWN GUYS, GUY GUARDS, AND SCREW ANCHORS
 15' MAST ARM AND LUMINAIRE (250 W HPS) WITH PEC
 1-TYPE 10B-WOOD POLE MOUNTED 90 DEG (5-2, P6-1)
 1-TYPE 10B-WOOD POLE MOUNTED 180 DEG (3-1, P3-2)
 2-PEDESTRIAN PUSH BUTTONS, SIGNS (R10-3e), AND RISERS
 VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING WB TRAFFIC) (V3-1)
 MOUNTING HARDWARE FOR VIDEO CAMERA (FOR MOUNTING ON LUMINAIRE EXTENSION)
 METAL JUNCTION BOX WITH TERMINAL BLOCKS
 2" CONDUIT RISER AND WEATHERHEAD ABOVE JUNCTION BOX WITH:
 1-6/c#14
 3-4/c#14
 2-2/c#14
 2" CONDUIT RISER AND WEATHERHEAD ABOVE SPAN WIRE WITH:
 1-3/c#14 (VIDEO)
 1-3/c#14 (LUM)

⑤ F & I 50' WOOD POLE--CLASS 2
 1-DOWN GUY, GUY GUARD, AND SCREW ANCHOR
 1/4" SPAN WIRE FROM WOOD POLE 5 TO POLE 2 (SEE POLE 2 NOTES FOR CABLES AND CONDUCTORS)

⑥ F & I 50' WOOD POLE--CLASS 2
 1-DOWN GUY, GUY GUARD, AND SCREW ANCHOR
 1/4" SPAN WIRE FROM WOOD POLE 6 TO POLE 3 (SEE POLE 3 NOTES FOR CABLES AND CONDUCTORS)

DRAWN BY: JMG
 DESIGNER: JMG
 CHECKED BY: JMG
 DESIGN TEAM

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 Name: John M. Gray, PE
 Date: April 4, 2016
 Lic. No. 22457

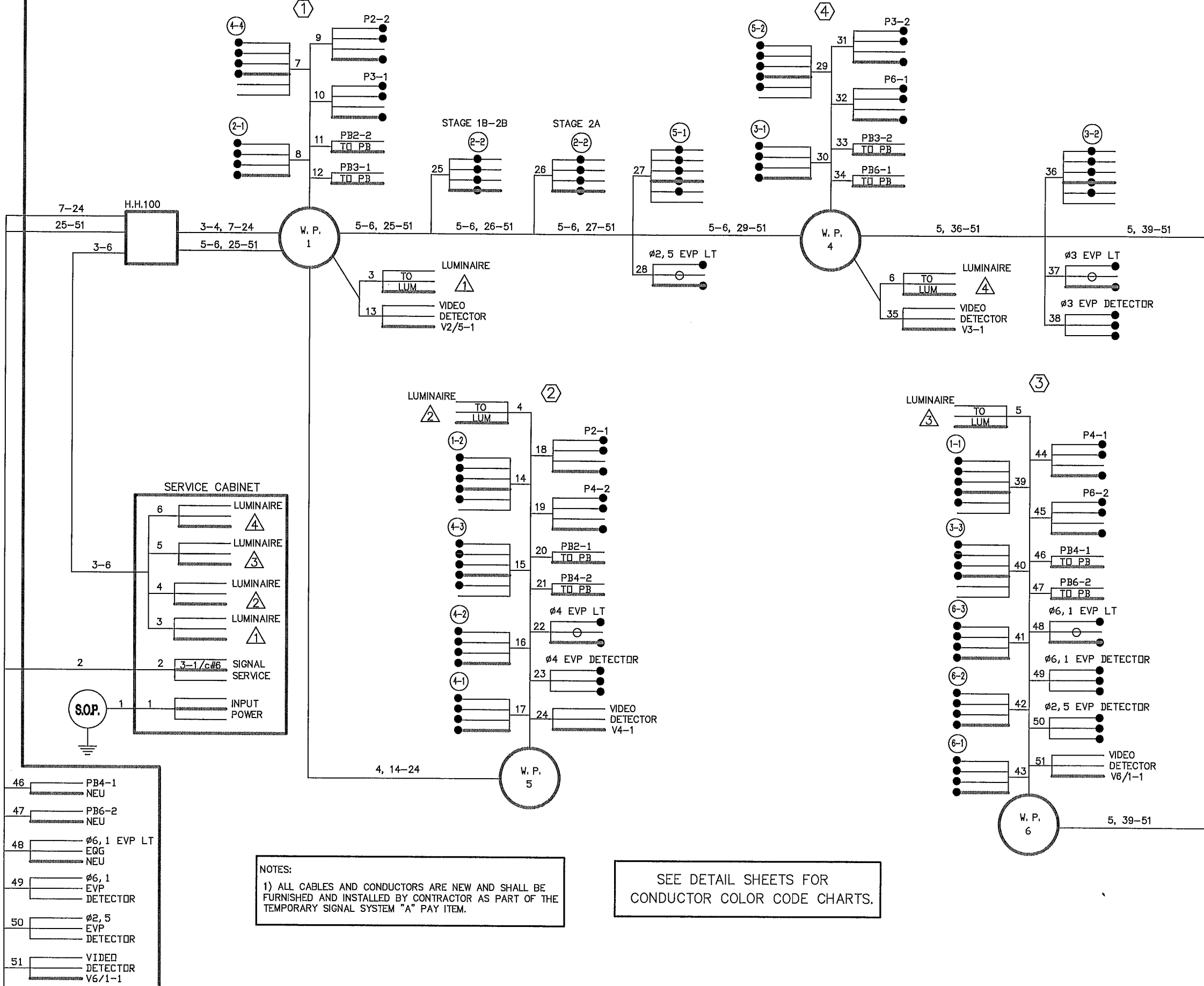
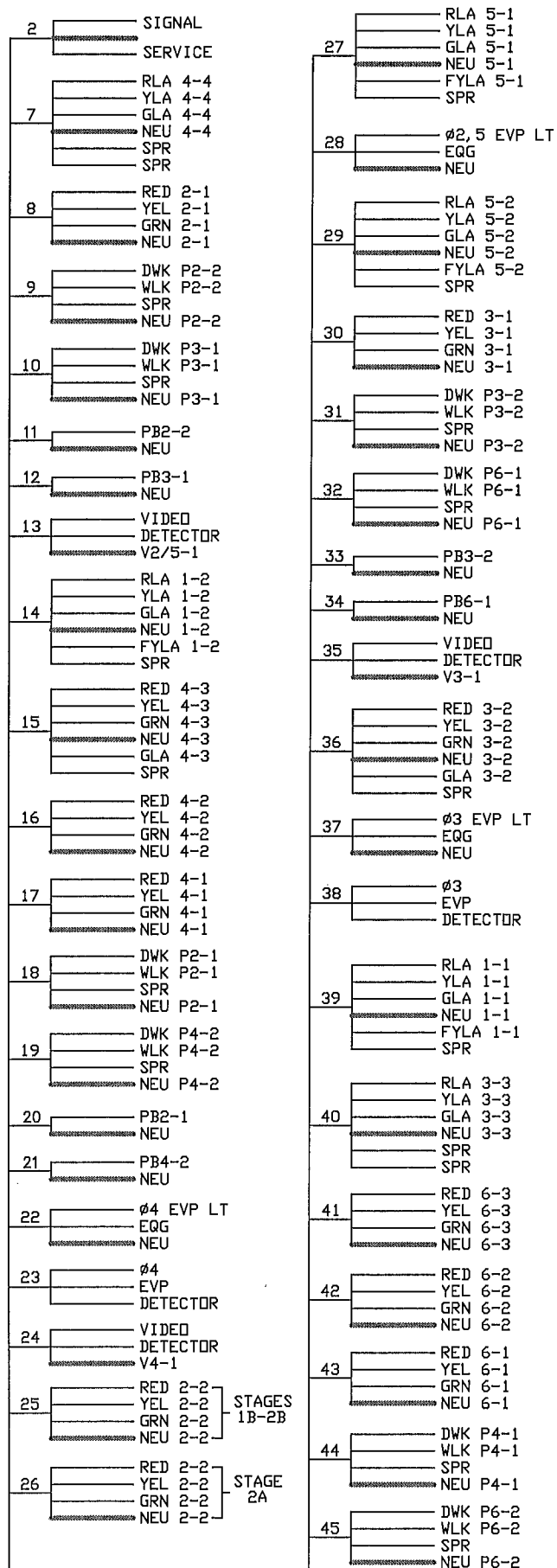
SEH
 PHONE: (651) 490-2000
 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
 CITY OF COON RAPIDS

TEMPORARY SIGNAL SYSTEM 'A'
 INTERSECTION LAYOUT
 CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW

FILE NO. 170
 SIGNAL SHEET 298
 8 OF 29

CONTROLLER CABINET



NOTES:
 1) ALL CABLES AND CONDUCTORS ARE NEW AND SHALL BE FURNISHED AND INSTALLED BY CONTRACTOR AS PART OF THE TEMPORARY SIGNAL SYSTEM "A" PAY ITEM.

SEE DETAIL SHEETS FOR CONDUCTOR COLOR CODE CHARTS.

DRAWN BY: JMG
 DESIGNER: JMG
 CHECKED BY: JMG

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 Date: April 4, 2018 Name: John M. Gray, PE Lic. No. 22457

SEH PHONE: (851) 490-2000 3835 VADNAIS CENTER DR. ST. PAUL, MN 55110

ANOKA COUNTY CITY OF COON RAPIDS

TEMPORARY SIGNAL SYSTEM 'A' FIELD WIRING DIAGRAM CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW

FILE NO. 171
 ANOKC 132239
 SIGNAL SHEET 298
 9 OF 29

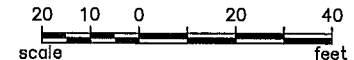
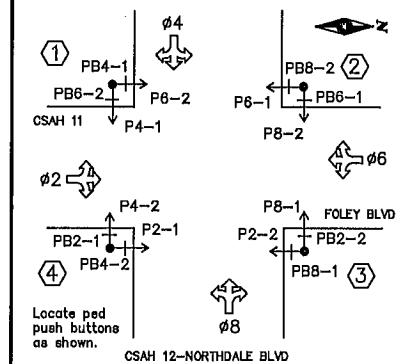
FURNISH & INSTALL VIDEO DETECTORS				
CAMERA NO.	INTERSECTION APPROACH FACING	SIGNAL POLE (CAMERA) LOCATION	CAMERA MOUNTED AT	MOUNTING HEIGHT
V2-1	NB CSAH 11	WP 2/WP 3	ON LUMINAIRE EXTENSION	40'
V4-1	EB CSAH 11	WP 3	ON LUMINAIRE EXTENSION	40'
V6-1	SB FOLEY BLVD	WP 1/WP 4	ON LUMINAIRE EXTENSION	40'
V8-1	WB CSAH 12	WP 1	ON LUMINAIRE EXTENSION	40'

NOTE: MOUNTING HEIGHT = APPROXIMATE HEIGHT ABOVE ADJACENT GROUND LINE.
 MOUNT CAMERA FOR V2-1 ON WOOD POLE 3 DURING STAGE 3, AND RELOCATE TO WOOD POLE 2 AND MAKE OPERATIONAL DURING STAGES 4 AND 5.
 MOUNT CAMERA FOR V6-1 ON WOOD POLE 4 DURING STAGES 3 AND 5, AND RELOCATE TO WOOD POLE 1 AND MAKE OPERATIONAL DURING STAGE 4.

NOTE: THIS PLAN IS INTENDED TO SHOW WOOD POLE LOCATION AND EQUIPMENT THAT IS TO BE USED AS PART OF THE TEMPORARY SIGNAL SYSTEM INSTALLATION. SPECIFIC ROADWAY ELEMENTS MAY OR MAY NOT BE IN PLACE AT THE TIME THE TEMPORARY SIGNAL IS IN OPERATION. THE LOCATION OF OVERHEAD SIGNALS AND DETECTORS FOR EACH STAGE OR PHASE WILL NEED TO BE ADJUSTED ACCORDINGLY.

NOTE: SEE NEXT SHEET FOR GENERAL NOTES AND DETAILED POLE NOTES.

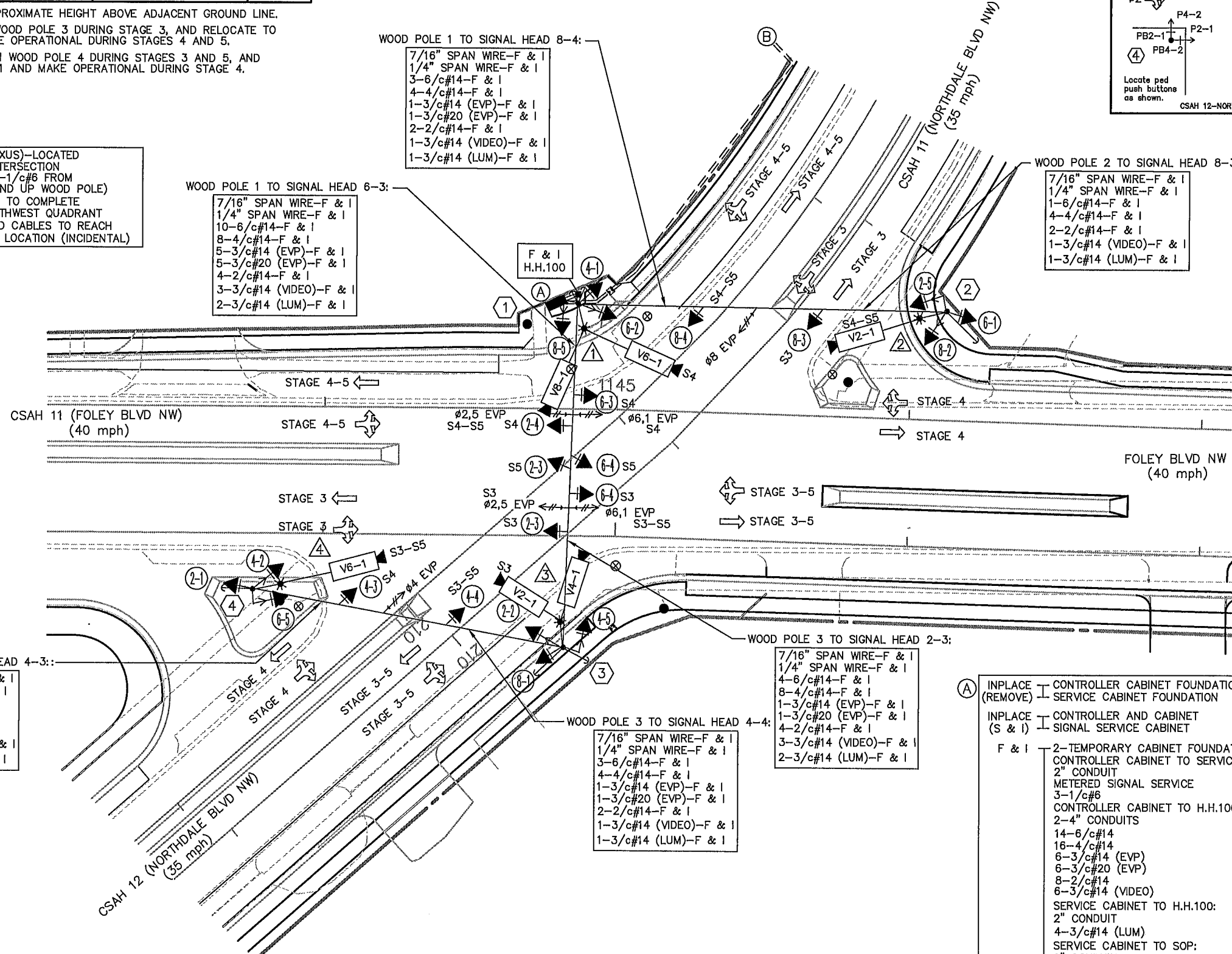
CONTROLLER PHASING, PEDESTRIAN INDICATIONS AND PUSH BUTTONS



SIGNAL SYSTEM OPERATIONS:

- SIGNAL SYSTEM FLASH MODE SHALL BE ALL RED.
- NORMAL OPERATION SHALL BE 4 PHASE DURING CONSTRUCTION, WITH PHASES 2, 4, 6, AND 8 INITIALLY OPERATING AS SPLIT PHASES DURING CONSTRUCTION.

(B) INPLACE WOOD POLE (CONNEXUS)-LOCATED APPROX. 300' WEST OF INTERSECTION (F & I 2" CONDUIT AND 3-1/c#6 FROM SERVICE CABINET UP TO AND UP WOOD POLE) COORDINATE WITH CONNEXUS TO COMPLETE POWER CONNECTION ON SOUTHWEST QUADRANT AND F & I ALL CONDUIT AND CABLES TO REACH EXISTING SOURCE OF POWER LOCATION (INCIDENTAL)



S3 = OPERATE DURING STAGE 3 ONLY.
 S4 = OPERATE DURING STAGE 4 ONLY.
 S5 = OPERATE DURING STAGE 5 ONLY.
 S3-S5 = OPERATE DURING BOTH STAGES 3 AND 5.
 S4-S5 = OPERATE DURING BOTH STAGES 4 AND 5.
 NO DENOTATION = OPERATE DURING ALL STAGES.

SIGNAL FACE STATUS:

- 1) FURNISH & INSTALL ON WOOD POLE.
- 2) FURNISH & INSTALL ON SPAN WIRE.
- 3) COVER DURING STAGE 4
- 4) RELOCATE ALONG SPAN WIRE AFTER STAGE 3 OR 4, OPERATE DURING STAGE 5.
- 5) COVER DURING STAGES 3 AND 5.
- 6) COVER DURING STAGE 3.
- 7) COVER DURING STAGES 4 AND 5.

LED SIGNAL HEADS ALL SIGNAL INDICATIONS SHALL BE 12".					
SIGNAL HEAD	R	Y	G	G	STATUS
2-1, 2-2	•	•	•		1
2-3 (3)	•	•	•	←	2, 3
2-3 (5)	•	•	•	←	4
2-4	•	•	•	←	2, 5
2-5	•	•	•	←	1
4-1, 4-2	•	•	•	←	1
4-3	•	•	•	←	2, 5
4-4	•	•	•	←	2, 3
4-5	•	•	•	←	1
6-1, 6-2	•	•	•	←	1
6-3	•	•	•	←	2, 5
6-4 (3)	•	•	•	←	2, 3
6-4 (5)	•	•	•	←	4
6-5	•	•	•	←	1
8-1, 8-2	•	•	•	←	1
8-3	•	•	•	←	2, 7
8-4	•	•	•	←	2, 6
8-5	•	•	•	←	1

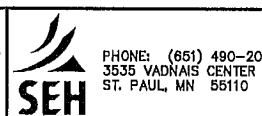
• ← = F & I NEW LED LENSES.

(A) INPLACE CONTROLLER CABINET FOUNDATION (REMOVE) SERVICE CABINET FOUNDATION
 INPLACE CONTROLLER AND CABINET (S & I) SIGNAL SERVICE CABINET
 F & I 2-TEMPORARY CABINET FOUNDATIONS
 CONTROLLER CABINET TO SERVICE CABINET:
 2" CONDUIT
 METERED SIGNAL SERVICE
 3-1/c#6
 CONTROLLER CABINET TO H.H.100:
 2-4" CONDUITS
 14-6/c#14
 6-3/c#14 (EVP)
 6-3/c#20 (EVP)
 8-2/c#14
 6-3/c#14 (VIDEO)
 SERVICE CABINET TO H.H.100:
 2" CONDUIT
 4-3/c#14 (LUM)
 SERVICE CABINET TO SOP:
 2" CONDUIT
 3-1/c#6

DRAWN BY: JMG			
DESIGNER: JMG			
CHECKED BY: JMG			
DESIGN TEAM	NO.	BY	DATE

REVISIONS		
NO.	BY	DATE

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 Date: April 4, 2016
 Name: John M. Gray, PE
 Lic. No.: 22457



ANOKA COUNTY
 CITY OF COON RAPIDS

TEMPORARY SIGNAL SYSTEM 'B'
 INTERSECTION LAYOUT
 CSAH 11 (FOLEY BLVD NW) AT
 CSAH 11/12 (NORTHDALE BLVD NW)
 FILE NO. ANOKC 132239
 SIGNAL SHEET 10 OF 29
 172
 298

S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10

NOTES:

- 1) LOCATION OF WOOD POLES, FOUNDATIONS, AND HANDHOLES SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) NEW HANDHOLE 100 SHALL BE A PVC HANDHOLE WITH METAL FRAME AND COVER (SEE SPECIAL PROVISIONS).
- 3) EACH NEW SIGNAL FACE SHALL HAVE BACKGROUND SHIELD.
- 4) SEE DETAILS FOR WOOD POLE AND SPAN WIRE MOUNTING DETAILS.
- 5) ALL NEW VEHICLE SIGNAL INDICATIONS SHALL BE LED AND SHALL BE FURNISHED AND INSTALLED BY CONTRACTOR AS SHOWN BELOW.
- 6) ALL TRAFFIC SIGNAL MATERIALS AND ELECTRICAL EQUIPMENT TO BE FURNISHED AND INSTALLED BY CONTRACTOR FOR THE TEMPORARY SIGNAL SYSTEM SHALL BE APPROVED BY ENGINEER PRIOR TO INSTALLATION AT THE INTERSECTION. SEE SPECIAL PROVISIONS.
- 7) MOVEMENT/RELOCATION OF HEADS, EVP DETECTORS AND INDICATOR LIGHTS, AND VIDEO DETECTOR CAMERAS SHALL BE INCLUDED IN THE PAY ITEM FOR "TEMPORARY SIGNAL SYSTEM B".
- 8) PROPOSED TEMPORARY SIGNAL SYSTEM PLAN SHOWN IS PROPOSED LAYOUT FOR STAGES 3, 4 AND 5 OF THE TRAFFIC CONTROL PLAN. PLACEMENT OF ALL SIGNAL INDICATIONS AND OTHER ITEMS FOR STAGES 3, 4 AND 5 STAGING PLANS SHALL BE IN ACCORDANCE WITH STAGING PLANS INCLUDED ELSEWHERE IN THE PLANS.
- 9) SEE SPECIAL PROVISIONS REGARDING VIDEO DETECTION SYSTEM TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR, AND FOR ALL VIDEO DETECTION SYSTEM COMPONENTS TO BE TURNED OVER TO THE COUNTY (FOR THE COUNTY TO OWN) AFTER TEMPORARY SIGNAL SYSTEM "B" IS REMOVED AND NEW SIGNAL SYSTEM "B" IS MADE OPERATIONAL (INCLUDED AS PART OF PAY ITEM FOR "TEMPORARY SIGNAL SYSTEM "B").
- 10) (F & I) = ITEMS TO BE FURNISHED & INSTALLED BY CONTRACTOR.
(S & I) = ITEMS TO BE SALVAGED & INSTALLED BY CONTRACTOR.
- 11) CONTRACTOR SHALL BAG (AND MAKE IN-OPERATIONAL) ALL VEHICLE SIGNAL HEADS NOT IN USE DURING CONSTRUCTION.
- 12) CONTRACTOR SHALL MAINTAIN A SIGNAL SYSTEM IN OPERATION AT THIS INTERSECTION AT ALL TIMES, UNLESS OTHERWISE APPROVED BY THE ENGINEER FOR THE SIGNAL SYSTEM TO BE TURNED OFF DURING NON-PEAK TRAFFIC PERIODS (FOR SWITCHOVERS FROM EXISTING SIGNAL SYSTEM TO TEMPORARY SIGNAL SYSTEM, AND FROM TEMPORARY SIGNAL SYSTEM TO NEW PERMANENT SIGNAL SYSTEM).
- 13) CONTRACTOR SHALL PROTECT AND MAINTAIN ALL ITEMS OF THE EXISTING PERMANENT SIGNAL SYSTEM THAT WILL BE REUSED AS PART OF THE TEMPORARY SIGNAL SYSTEM, AND SHALL REPLACE ITEMS DAMAGED DURING CONSTRUCTION WITH NEW ITEMS (AT NO EXPENSE TO THE COUNTY).
- 14) CONTRACTOR SHALL FURNISH AND INSTALL FOUR (4) EMERGENCY VEHICLE PREEMPTION (EVP) DETECTORS AND FOUR (4) EVP CONFIRMATION LIGHTS WHERE SHOWN, ALONG WITH ALL WOOD POLE AND SPAN WIRE MOUNTING HARDWARE, ALL AS PART OF PAY ITEM FOR "TEMPORARY SIGNAL SYSTEM "B". RELOCATION OF EVP DETECTORS AND LIGHTS ALONG SPAN WIRE DURING EACH STAGE OF CONSTRUCTION IS INCIDENTAL.
- 15) CONTRACTOR SHALL TEMPORARILY MOUNT EXISTING STREET NAME SIGNS AND POST (CURRENTLY INPLACE ON SOUTHWEST QUADRANT) IN ORDER TO KEEP STREET NAME SIGNS VISIBLE TO MOTORISTS AND PEDESTRIANS AT ALL TIMES DURING CONSTRUCTION (INCIDENTAL).

① F & I

50' WOOD POLE-CLASS 2
 2-DOWN GUYS, GUY GUARDS, AND SCREW ANCHORS
 15' MAST ARM AND LUMINAIRE (250 W HPS) WITH PEC
 1-TYPE 10B-WOOD POLE MOUNTED 90 DEG (8-5, P4-1)
 1-TYPE 10A-WOOD POLE MOUNTED 180 DEG (4-1)
 1-TYPE 10B-WOOD POLE MOUNTED 270 DEG (6-2, P6-2)
 2-PEDESTRIAN PUSH BUTTONS, SIGNS (R10-3e), AND RISERS
 VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING WB TRAFFIC) (V8-1)
 MOUNTING HARDWARE FOR 2-VIDEO CAMERAS (FOR MOUNTING BOTH ON LUMINAIRE EXTENSION)
 METAL JUNCTION BOX WITH TERMINAL BLOCKS
 2" CONDUIT RISER & WEATHERHEAD ABOVE JUNCTION BOX WITH:
 1-6/c#14
 4-4/c#14
 2-2/c#14
 2" CONDUIT RISER AND WEATHERHEAD ABOVE SPAN WIRE WITH:
 2-3/c#14 (VIDEO)
 1-3/c#14 (LUM)
 EXTEND INTO H.H.100:
 2-4" CONDUIT RISERS AND WEATHERHEADS
 14-6/c#14
 16-4/c#14
 6-3/c#14 (EVP)
 6-3/c#20 (EVP)
 8-2/c#14
 4-3/c#14 (LUM)
 6-3/c#14 (VIDEO)

INPLACE (S & I) VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING SB TRAFFIC) (V6-1) - RELOCATE FROM WOOD POLE 4 TO WOOD POLE 1 FOR OPERATION DURING STAGE 4
 RELOCATE VIDEO CAMERA V6-1 FROM WOOD POLE 1 BACK TO WOOD POLE 4 FOR OPERATION DURING STAGE 5

② F & I

50' WOOD POLE-CLASS 2
 1-DOWN GUY, GUY GUARD, AND SCREW ANCHOR
 15' MAST ARM AND LUMINAIRE (250 W HPS) WITH PEC
 1-TYPE 10B-WOOD POLE MOUNTED 90 DEG (2-5, P6-1)
 1-TYPE 10A-WOOD POLE MOUNTED 180 DEG (6-1)
 1-TYPE 10B-WOOD POLE MOUNTED 270 DEG (8-2, P8-2)
 2-PEDESTRIAN PUSH BUTTONS, SIGNS (R10-3e), AND RISERS
 MOUNTING HARDWARE FOR VIDEO CAMERA (FOR MOUNTING ON LUMINAIRE EXTENSION)
 METAL JUNCTION BOX WITH TERMINAL BLOCKS
 2" CONDUIT RISER & WEATHERHEAD ABOVE JUNCTION BOX WITH:
 1-6/c#14
 4-4/c#14
 2-2/c#14
 2" CONDUIT RISER AND WEATHERHEAD ABOVE SPAN WIRE WITH:
 1-3/c#14 (VIDEO)
 1-3/c#14 (LUM)

INPLACE (S & I) VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING NB TRAFFIC) (V2-1) - RELOCATE FROM WOOD POLE 3 TO WOOD POLE 2 FOR OPERATION DURING STAGES 4-5

③ F & I

50' WOOD POLE-CLASS 2
 2-DOWN GUYS, GUY GUARDS, AND SCREW ANCHORS
 15' MAST ARM AND LUMINAIRE (250 W HPS) WITH PEC
 1-TYPE 10B-WOOD POLE MOUNTED 90 DEG (4-5, P8-1)
 1-TYPE 10A-WOOD POLE MOUNTED 180 DEG (8-1)
 1-TYPE 10B-WOOD POLE MOUNTED 270 DEG (2-2, P2-2)
 2-PEDESTRIAN PUSH BUTTONS, SIGNS (R10-3e), AND RISERS
 VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING NB TRAFFIC) (V2-1)
 VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING EB TRAFFIC) (V4-1)
 MOUNTING HARDWARE FOR 2-VIDEO CAMERAS (FOR MOUNTING BOTH ON LUMINAIRE EXTENSION)
 METAL JUNCTION BOX WITH TERMINAL BLOCKS
 2" CONDUIT RISER & WEATHERHEAD ABOVE JUNCTION BOX WITH:
 1-6/c#14
 4-4/c#14
 2-2/c#14
 2" CONDUIT RISER AND WEATHERHEAD ABOVE SPAN WIRE WITH:
 2-3/c#14 (VIDEO)
 1-3/c#14 (LUM)

INPLACE (S & I) RELOCATE VIDEO CAMERA V2-1 FROM WOOD POLE 3 TO WOOD POLE 2 FOR OPERATION DURING STAGES 4-5

④ F & I

50' WOOD POLE-CLASS 2
 1-DOWN GUY, GUY GUARD, AND SCREW ANCHOR
 15' MAST ARM AND LUMINAIRE (250 W HPS) WITH PEC
 1-TYPE 10B-WOOD POLE MOUNTED 90 DEG (6-5, P2-1)
 1-TYPE 10A-WOOD POLE MOUNTED 180 DEG (2-1)
 1-TYPE 10B-WOOD POLE MOUNTED 270 DEG (4-2, P4-2)
 2-PEDESTRIAN PUSH BUTTONS, SIGNS (R10-3e), AND RISERS
 VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING SB TRAFFIC) (V6-1)
 MOUNTING HARDWARE FOR VIDEO CAMERA (FOR MOUNTING ON LUMINAIRE EXTENSION)
 METAL JUNCTION BOX WITH TERMINAL BLOCKS
 2" CONDUIT RISER & WEATHERHEAD ABOVE JUNCTION BOX WITH:
 1-6/c#14
 4-4/c#14
 2-2/c#14
 2" CONDUIT RISER AND WEATHERHEAD ABOVE SPAN WIRE WITH:
 1-3/c#14 (VIDEO)
 1-3/c#14 (LUM)

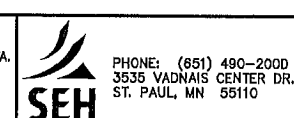
INPLACE (S & I) RELOCATE VIDEO CAMERA V6-1 FROM WOOD POLE 4 TO WOOD POLE 1 FOR OPERATION DURING STAGE 4
 VIDEO CAMERA-LUMINAIRE EXTENSION MOUNTED (FACING NB TRAFFIC) (V6-1) - RELOCATE FROM WOOD POLE 1 TO WOOD POLE 4 FOR OPERATION DURING STAGE 5

DRAWN BY: JMG			
DESIGNER: JMG			
CHECKED BY: JMG			
DESIGN TEAM	NO.	BY	DATE

DESIGN TEAM			REVISIONS		
NO.	BY	DATE	NO.	BY	DATE

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

John M. Gray
 Name: John M. Gray, PE
 Date: April 4, 2016
 Lic. No. 22457

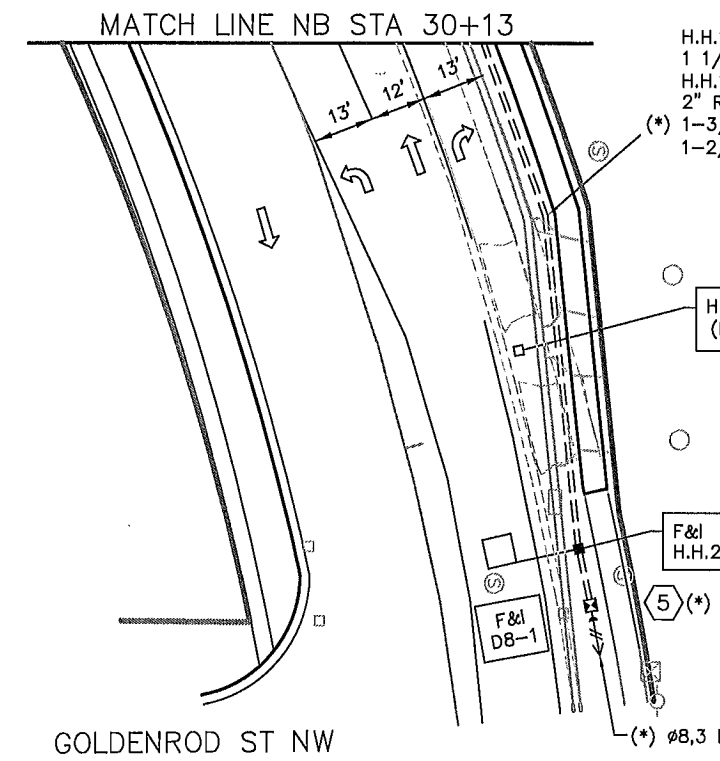
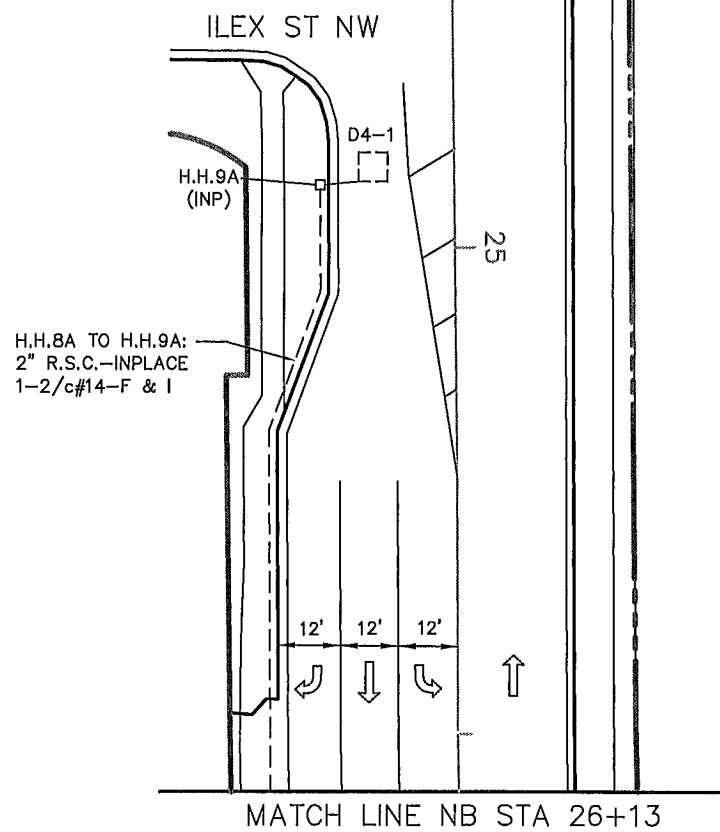


ANOKA COUNTY
CITY OF COON RAPIDS

TEMPORARY SIGNAL SYSTEM "B"
INTERSECTION LAYOUT
 CSAH 11 (FOLEY BLVD NW) AT
 CSAH 11/12 (NORTHDAL BLVD NW)

FILE NO.	173
SIGNAL SHEET	298
11 OF 29	

S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10



- (A)**
- INPLACE (S & I) CONTROLLER AND CABINET
 - INPLACE (REMOVE) CABINET FOUNDATION EXTENDED INTO H.H.16: 2" R.S.C. 3-1/c#6
 - EXTENDED INTO H.H.15: 4" R.S.C. 4-12/c#12 2-6/c#14 5-3/c#12 1-3/c#20 4-2/c#14
 - EXTENDED INTO H.H.1: 4" R.S.C. AND 3" R.S.C. 5-12/c#14 1-6/c#14 2-3/c#14 4-3/c#20 19-2/c#14 2-1/c#6 (INS. GR.)
 - F & I EQUIPMENT PAD FOUNDATION BATTERY BACK-UP SIGNAL SERVICE CABINET BETWEEN CONTROLLER CABINET AND SERVICE CABINET: METERED SIGNAL SERVICE 2" R.S.C. 3-1/c#6
 - CONTROLLER CABINET TO H.H.13A: 4" R.S.C. 4" R.S.C. 2-12/c#14 3-12/c#14 1-6/c#14 (*) 1-3/c#14 (*) 1-3/c#14 2-2/c#14 (*) 1-3/c#20 1-1/c#6 (INS. GR.) 6-2/c#14
 - CONTROLLER CABINET TO H.H.1A: 4" R.S.C. 4" R.S.C. 2-12/c#14 3-12/c#14 1-6/c#14 (*) 1-3/c#14 (*) 1-3/c#14 (*) 2-3/c#20 (*) 2-3/c#20 8-2/c#14 1-1/c#6 (INS. GR.)
 - INPLACE (S & I) (*) 1-3/c#20
 - F & I CONTROLLER CABINET TO H.H.13A: (**) 2" R.S.C. (**) 1-12 SM FIBER OPTIC CABLE (**) 1-1/c#10
 - CONTROLLER CABINET TO H.H.1A: (**) 2" R.S.C.
 - INPLACE (S & I) (**) 1-6 SM FO CABLE TO H.H.1A (S & I)
 - F & I SERVICE CABINET TO H.H.1A: 1 1/4" R.S.C. UNMETERED STREET LIGHT SERVICE 2-3/c#14 (LUM)
 - SERVICE CABINET TO H.H.13A: 1 1/4" R.S.C. UNMETERED STREET LIGHT SERVICE 2-3/c#14 (LUM)
 - STUB OUT 2" R.S.C. FROM SERVICE CABINET TO EAST (FOR POWER BY CONNEXUS)
 - STUB OUT 3" R.S.C. FROM CONTROLLER CABINET TO EAST (THREAD AND CAP-FOR FUTURE USE)

- (B)**
- INPLACE (REMOVE) CABINET FOUNDATION SIGNAL SERVICE CABINET EXTENDED INTO H.H.16: 2" R.S.C. 2-3/c#14 (LUM) 2-3/c#12 (LUM) 3-1/c#6
 - EXTENDED TO SOP: 2" R.S.C. 3-1/c#6
 - INPLACE GROUND MOUNTED TRANSFORMER (S.O.P.) (CONNEXUS)-TO BE REUSED AND MAINTAINED INPLACE BY OTHERS

- (5)**
- INPLACE (S & I) (*) ONE WAY EVP DETECTOR-MOUNT ON TOP OF NEW SLIPFITTER COLLAR (#8,3)
 - INPLACE (REMOVE) (*) PEDESTAL FOUNDATION (*) PEDESTAL POLE AND BASE EXTENDED INTO H.H.18: (*) 2" R.S.C. (*) 1-3/c#20
 - F & I (*) PEDESTAL FOUNDATION (*) 10' PEDESTAL POLE (INCLUDES BASE, WIND COLLAR) (*) SLIPFITTER COLLAR ATOP PEDESTAL POLE EXTEND INTO H.H.2A: (*) 2" R.S.C. (*) 1-3/c#20

- (1)**
- INPLACE (S & I) TYPE PA100-A-50-D40-9 (DAVIT AT 350 DEG) LUMINAIRE-LED "SHOEBOX" LUMINAIRE CHECK SWITCH AT 0 DEG 1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0' (5-1) 2-STRAIGHT MOUNT SIGNALS-OVERHEAD AT 12' AND 24' (2-3, 2-2) 2-TYPE 10B-POLE MOUNTED 90 DEG (7-2, P8-1) AND 180 DEG (2-1, P2-2) 2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e) R10-X12 SIGN PANEL-ADJACENT TO 5-1 TYPE D SIGN PANEL-OVERHEAD ONE WAY EVP CONFIRMATION LIGHT (#2,5)
 - INPLACE (REMOVE) PA100 POLE FOUNDATION EXTENDED INTO H.H.15: 3" R.S.C. 2-12/c#12 1-6/c#14 3-3/c#12 1-3/c#12 (LUM)
 - F & I PA100 POLE FOUNDATION EXTEND INTO H.H.13A: 3" CONDUIT 3-12/c#14 (*) 1-3/c#14 2-2/c#14 1-3/c#14 (LUM) 1-1/c#6 (INS. GR.)

- (3)**
- INPLACE (MAINTAIN INPLACE) PA100 POLE FOUNDATION TYPE PA100-A-50-D40-9 (DAVIT AT 350 DEG) LUMINAIRE-LED "SHOEBOX" LUMINAIRE CHECK SWITCH AT 0 DEG 1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0' 2-STRAIGHT MOUNT SIGNALS-OVERHEAD AT 12' & 24' 2-TYPE 10B-POLE MOUNTED 90 DEG & 180 DEG 2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e) R10-X12 SIGN PANEL-ADJACENT TO 1-1 TYPE D SIGN PANEL-OVERHEAD TWO WAY EVP DETECTOR AND ONE WAY LIGHT (#6,1 DETECTOR/LIGHT, #2,5 DETECTOR) EXTENDED INTO H.H.7A: 3" CONDUIT
 - INPLACE (REMOVE) 3-12/c#14 2-3/c#14 2-3/c#20 2-2/c#14 1-1/c#6 (INS. GR.)
 - F & I 3-12/c#14 (*) 1-3/c#14 (*) 2-3/c#20 2-2/c#14 1-3/c#14 (LUM) 1-1/c#6 (INS. GR.)

- (2)**
- INPLACE (MAINTAIN INPLACE) PA90 POLE FOUNDATION TYPE PA90-A-35-D40-9 (DAVIT AT 350 DEG) LUMINAIRE-LED "SHOEBOX" LUMINAIRE CHECK SWITCH AT 0 DEG 1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0' 1-STRAIGHT MOUNT SIGNAL-OVERHEAD AT 12' 2-TYPE 10B-POLE MOUNTED 90 DEG & 180 DEG 2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e) R10-X12 SIGN PANEL-ADJACENT TO 7-1 TYPE D SIGN PANEL-OVERHEAD ONE WAY EVP DETECTOR AND LIGHT (#4,7)-3 FEET FROM RIGHT END OF MAST ARM EXTENDED INTO H.H.3A: 3" CONDUIT
 - INPLACE (REMOVE) 2-12/c#14 1-6/c#14 2-3/c#14 1-3/c#20 2-2/c#14 1-1/c#6 (INS. GR.)
 - F & I 2-12/c#14 1-6/c#14 (*) 1-3/c#14 (*) 1-3/c#20 2-2/c#14 1-3/c#14 (LUM) 1-1/c#6 (INS. GR.)

- (4)**
- INPLACE (REMOVE) PA90 POLE FOUNDATION TYPE PA90-A-30-D40-9 (DAVIT AT 350 DEG) 2-TYPE 10B BRACKETING-POLE MOUNTED 90/180 DEG EXTENDED INTO H.H.10: 3" R.S.C. 2-12/c#12 1-6/c#14 2-3/c#12 1-3/c#20 1-3/c#12 (LUM)
 - INPLACE (S & I) LUMINAIRE-LED "SHOEBOX" 1-ONE WAY SIGNAL-OVERHEAD AT 0' (3-1) 1-ONE WAY SIGNAL-OVERHEAD AT 11' (8-2) 2-ONE WAY SIGNALS-POLE MOUNTED 90 DEG (5-2) AND 180 DEG (8-1) 2-SETS CD PED INDICATIONS (P6-1, P8-2) 2-PEDESTRIAN PUSH BUTTONS (PB6-1, PB8-2) R10-X12 SIGN PANEL-ADJACENT TO 3-1 TYPE D SIGN PANEL-OVERHEAD (D-5) (*) ONE WAY EVP DETECTOR AND LIGHT AT 6' (#8,3)
 - F & I PA100 POLE FOUNDATION TYPE PA100-A-45-D40-9 (DAVIT AT 350 DEG) MID-MAST ARM MOUNT AT 11' (FOR 8-2) 1-ANGLE MOUNT-OVERHEAD AT 0' (FOR 3-1) 1-STRAIGHT MOUNT-OVERHEAD AT 11' (FOR 8-2) 2-TYPE 10B BRACKETING-POLE MOUNTED 90 DEG (5-2, P6-1) AND 180 DEG (8-1, P8-2) 2-PEDESTRIAN INSTRUCTION SIGNS (R10-3e) EXTEND INTO H.H.10A: 3" CONDUIT 2-12/c#14 1-6/c#14 (*) 1-3/c#14 (*) 1-3/c#20 2-2/c#14 1-3/c#14 (LUM) 1-1/c#6 (INS. GR.)

DRAWN BY: LC
 DESIGNER: JMG
 CHECKED BY: JMG
 DESIGN TEAM

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 Date: April 4, 2016 Name: John M. Gray, PE Lic. No. 22457

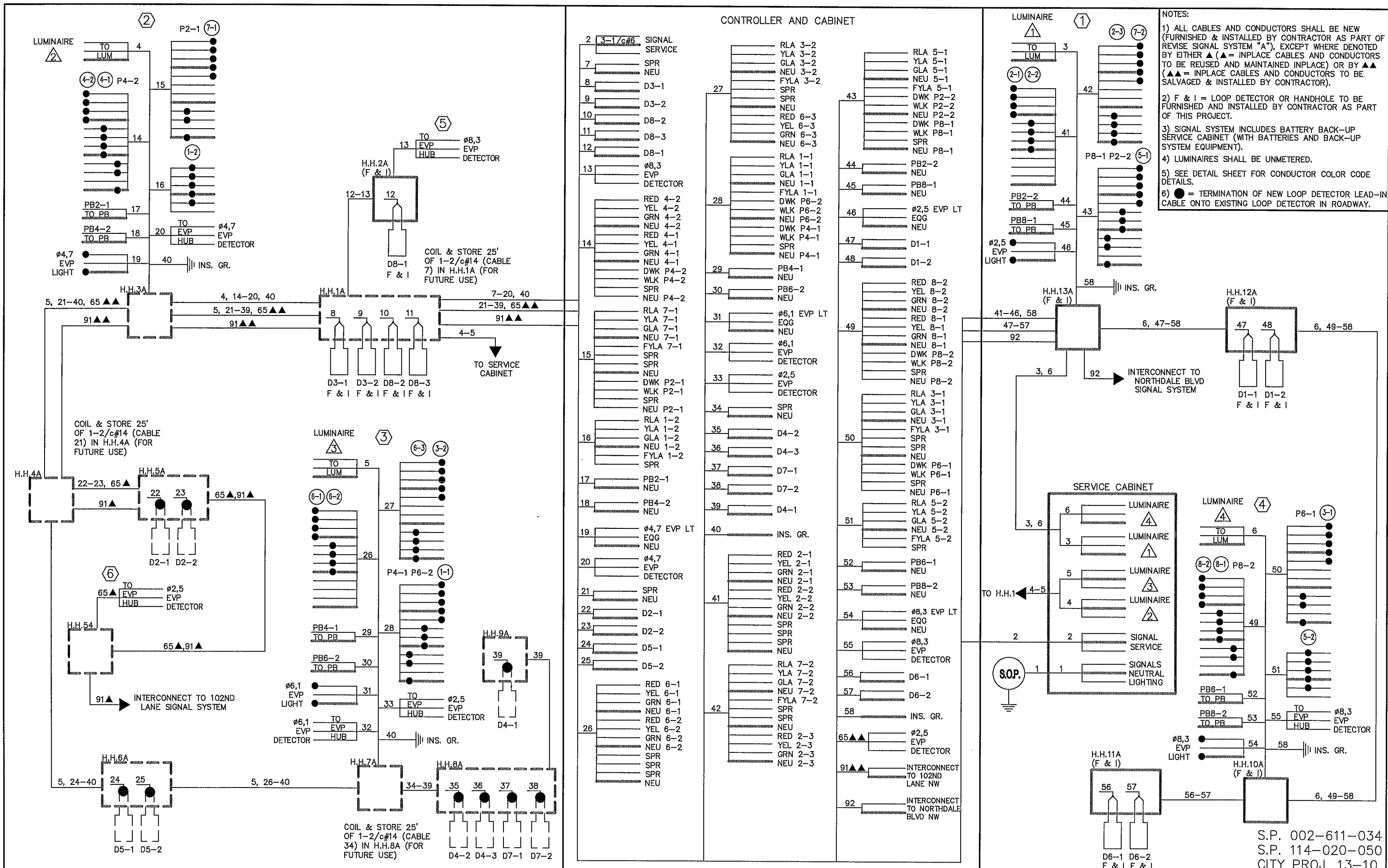
SEH
 PHONE: (651) 490-2000
 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
 CITY OF COON RAPIDS

REVISE SIGNAL SYSTEM 'A'
 INTERSECTION LAYOUT
 CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW

FILE NO. 176
 SIGNAL SHEET 298
 14 OF 29

S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10



- NOTES:
- 1) ALL CABLES AND CONDUCTORS SHALL BE NEW (FURNISHED & INSTALLED BY CONTRACTOR AS PART OF REVISE SIGNAL SYSTEM "A"), EXCEPT WHERE DENOTED BY EITHER ▲ (▲ = INPLACE CABLES AND CONDUCTORS TO BE REUSED AND MAINTAINED INPLACE) OR BY ▲▲ (▲▲ = INPLACE CABLES AND CONDUCTORS TO BE SALVAGED & INSTALLED BY CONTRACTOR).
 - 2) F & I = LOOP DETECTOR OR HANDHOLE TO BE FURNISHED AND INSTALLED BY CONTRACTOR AS PART OF THIS PROJECT.
 - 3) SIGNAL SYSTEM INCLUDES BATTERY BACK-UP SERVICE CABINET (WITH BATTERIES AND BACK-UP SYSTEM EQUIPMENT).
 - 4) LUMINAIRES SHALL BE UNMETERED.
 - 5) SEE DETAIL SHEET FOR CONDUCTOR COLOR CODE DETAILS.
 - 6) ● = TERMINATION OF NEW LOOP DETECTOR LEAD-IN CABLE ONTO EXISTING LOOP DETECTOR IN ROADWAY.

DRAWN BY: LC
 DESIGNER: JMG
 CHECKED BY: JMG
 DESIGN TEAM

NO.	BY	DATE	REVISIONS

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John M. Gray
 Name: John M. Gray, PE
 Date: April 4, 2016
 Lic. No. 22457

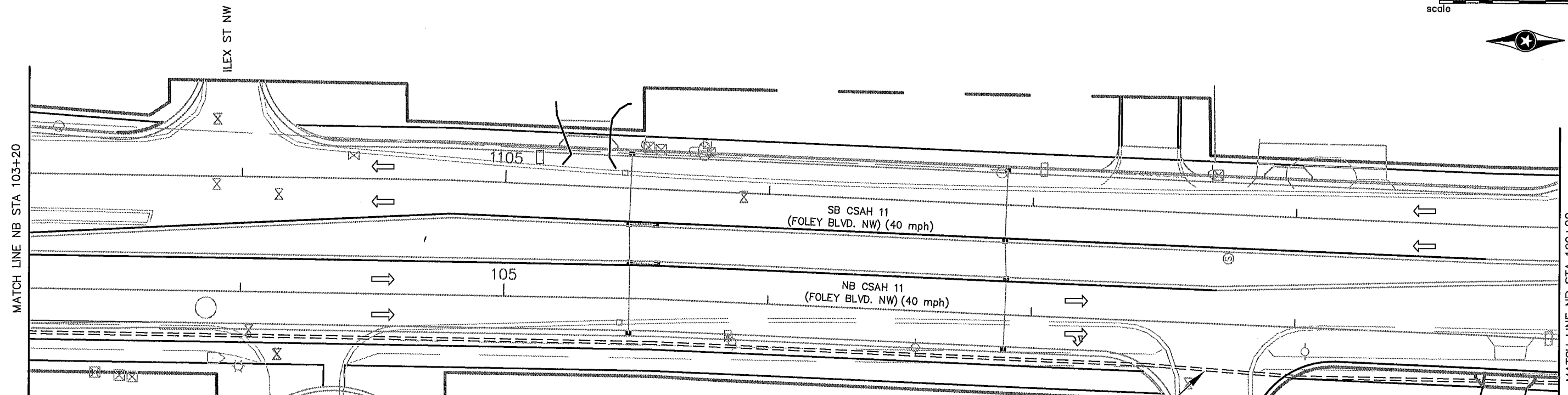
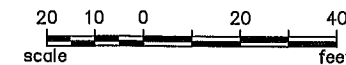
SEH
 PHONE: (651) 490-2000
 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
 CITY OF COON RAPIDS

REVISE SIGNAL SYSTEM "A"
 FIELD WIRING DIAGRAM
 CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW

FILE NO. ANOKC 132239
 SIGNAL SHEET 15 OF 29
 177
 298

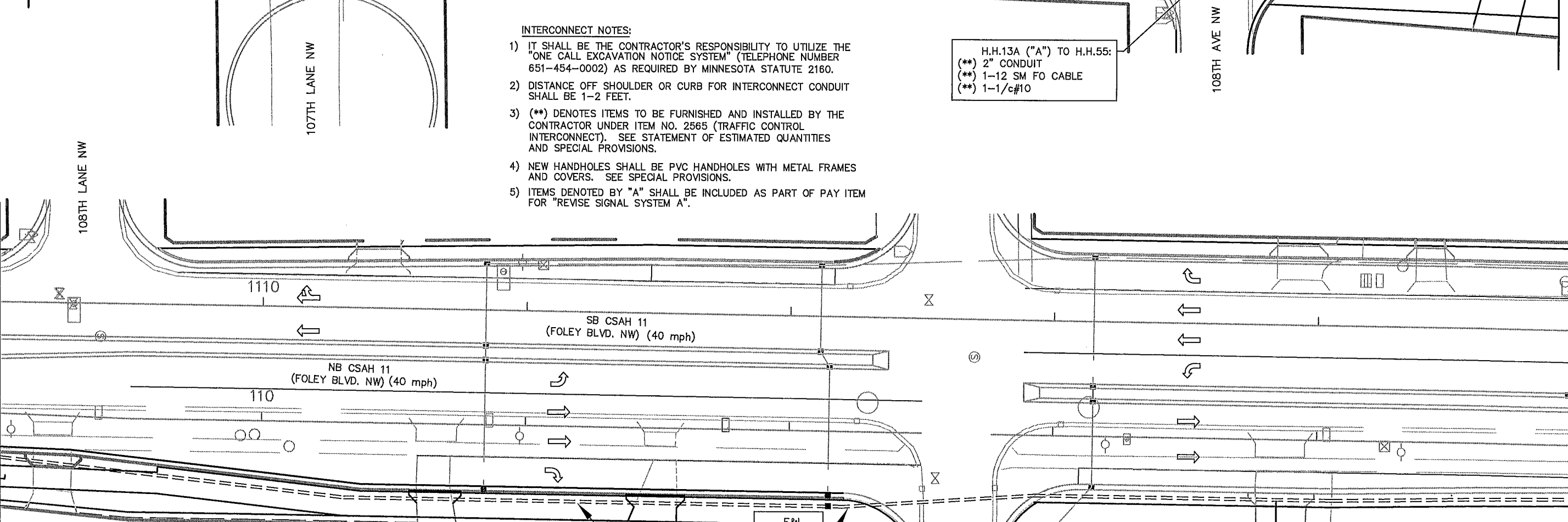
S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10



INTERCONNECT NOTES:

- 1) IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO UTILIZE THE "ONE CALL EXCAVATION NOTICE SYSTEM" (TELEPHONE NUMBER 651-454-0002) AS REQUIRED BY MINNESOTA STATUTE 2160.
- 2) DISTANCE OFF SHOULDER OR CURB FOR INTERCONNECT CONDUIT SHALL BE 1-2 FEET.
- 3) (**) DENOTES ITEMS TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNDER ITEM NO. 2565 (TRAFFIC CONTROL INTERCONNECT). SEE STATEMENT OF ESTIMATED QUANTITIES AND SPECIAL PROVISIONS.
- 4) NEW HANDHOLES SHALL BE PVC HANDHOLES WITH METAL FRAMES AND COVERS. SEE SPECIAL PROVISIONS.
- 5) ITEMS DENOTED BY "A" SHALL BE INCLUDED AS PART OF PAY ITEM FOR "REVISE SIGNAL SYSTEM A".

H.H.13A ("A") TO H.H.55:
 (**) 2" CONDUIT
 (**) 1-12 SM FO CABLE
 (**) 1-1/c#10



H.H.13A ("A") TO H.H.55:
 H.H.55 TO H.H.56:
 (**) 2" CONDUIT
 (**) 1-12 SM FO CABLE
 (**) 1-1/c#10

F&I
 (**) H.H.55

DRAWN BY: LC
 DESIGNER: JMG
 CHECKED BY: JMG
 DESIGN TEAM

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 Date: April 4, 2016
 Name: John M. Gray, PE
 Lic. No. 22457

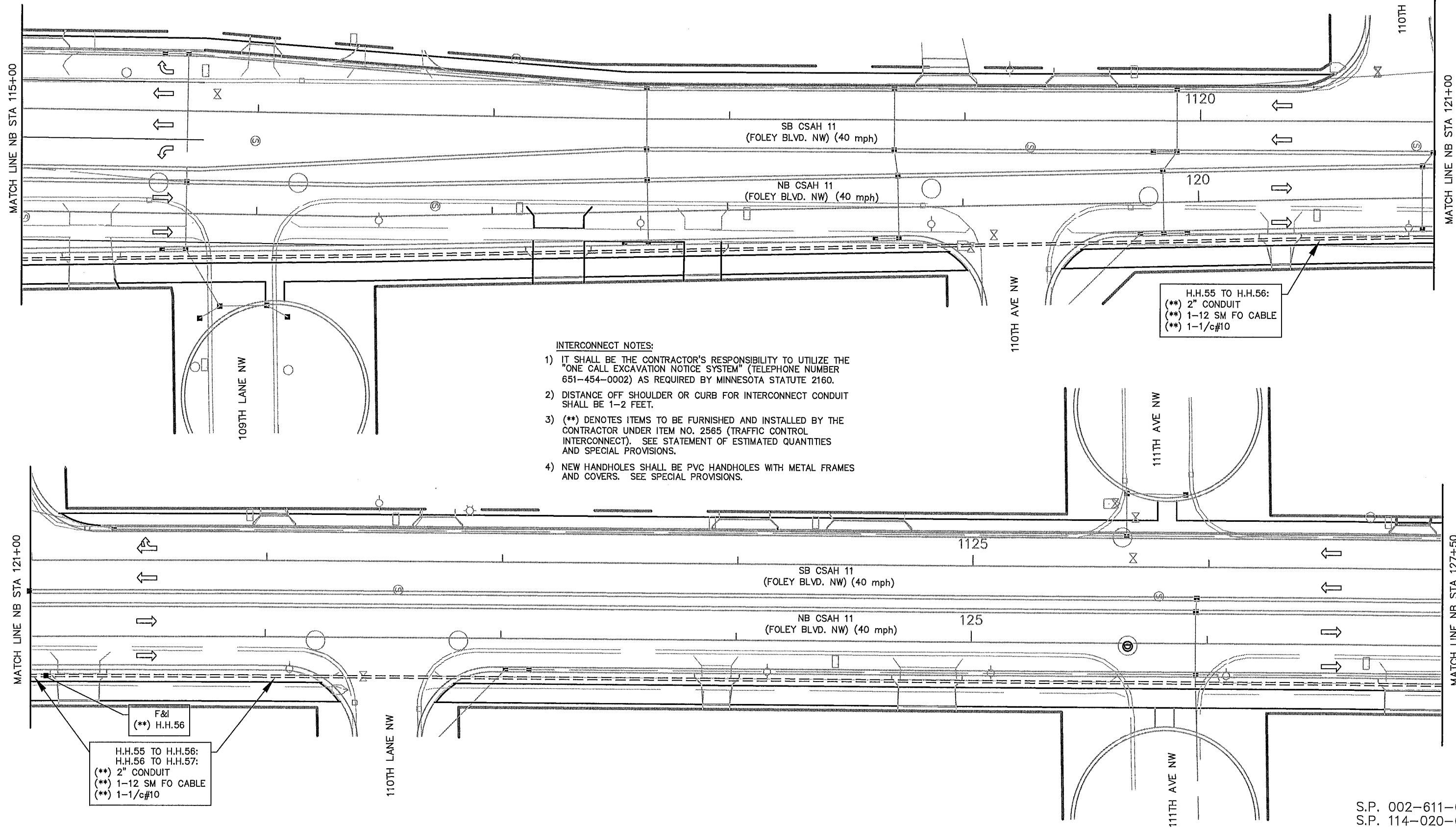
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 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
CITY OF COON RAPIDS

TRAFFIC CONTROL INTERCONNECTION
INTERSECTION LAYOUT
 CSAH 11 (EGRET BLVD TO NORTHDAL BLVD)

FILE NO. 178
 ANOKC 132239
 SIGNAL SHEET 16 OF 29
 CITY PROJ. 13-10

S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10



INTERCONNECT NOTES:

- 1) IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO UTILIZE THE "ONE CALL EXCAVATION NOTICE SYSTEM" (TELEPHONE NUMBER 651-454-0002) AS REQUIRED BY MINNESOTA STATUTE 2160.
- 2) DISTANCE OFF SHOULDER OR CURB FOR INTERCONNECT CONDUIT SHALL BE 1-2 FEET.
- 3) (**) DENOTES ITEMS TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNDER ITEM NO. 2565 (TRAFFIC CONTROL INTERCONNECT). SEE STATEMENT OF ESTIMATED QUANTITIES AND SPECIAL PROVISIONS.
- 4) NEW HANDHOLES SHALL BE PVC HANDHOLES WITH METAL FRAMES AND COVERS. SEE SPECIAL PROVISIONS.

F&I
(**) H.H.56

H.H.55 TO H.H.56:
H.H.56 TO H.H.57:
(**) 2" CONDUIT
(**) 1-12 SM FO CABLE
(**) 1-1/c#10

S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10

DRAWN BY: LC
DESIGNER: JMG
CHECKED BY: JMG

NO.	BY	DATE	REVISIONS

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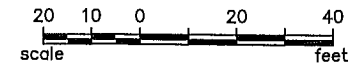
John M. Gray
Name: John M. Gray, PE
Lic. No. 22457
Date: April 4, 2016

SEH
PHONE: (851) 490-2000
3535 VADNAIS CENTER DR.
ST. PAUL, MN 55110

**ANOKA COUNTY
CITY OF COON RAPIDS**

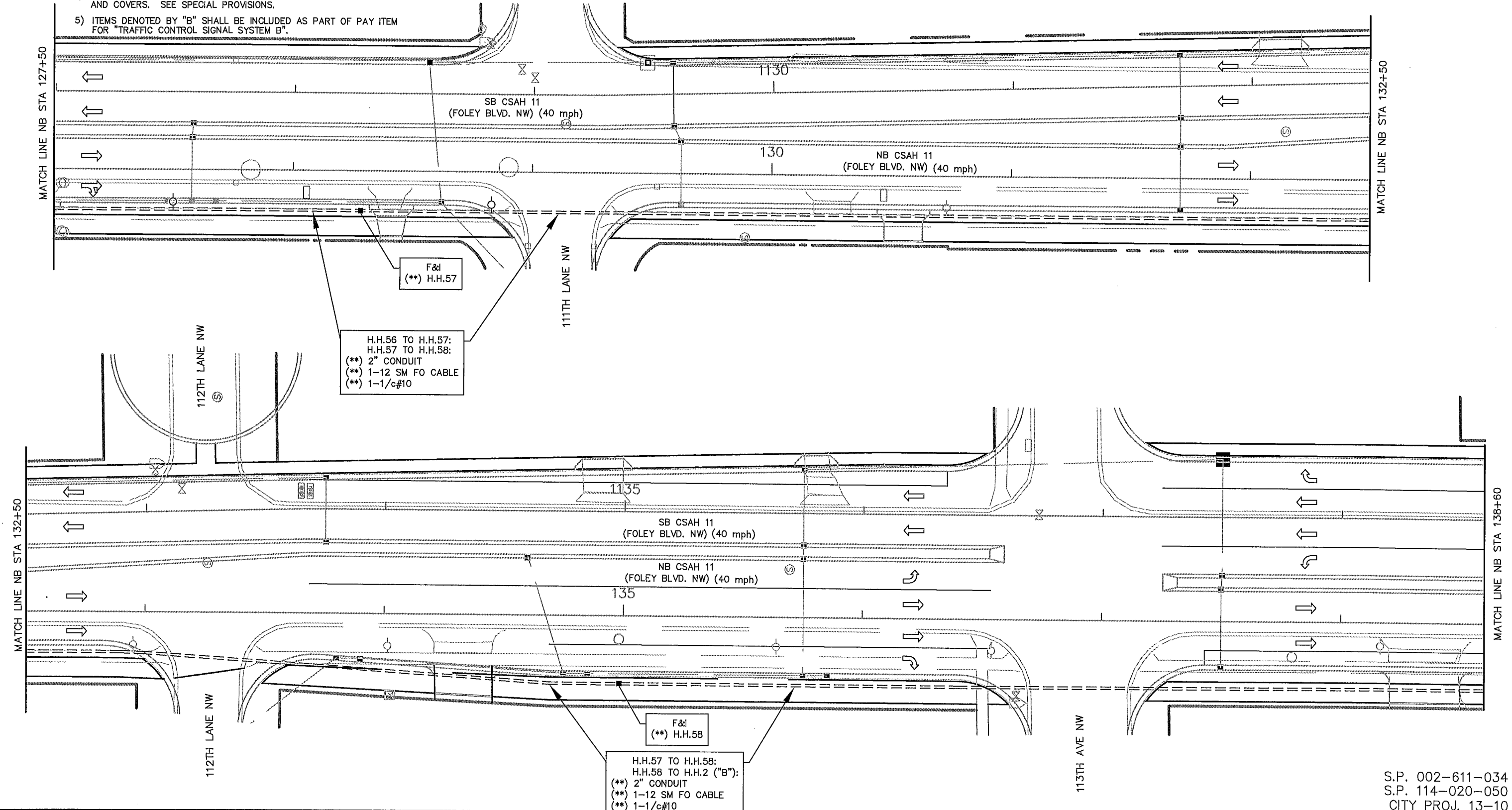
**TRAFFIC CONTROL INTERCONNECTION
INTERSECTION LAYOUT**
CSAH 11 (EGRET BLVD TO NORTHDAL BLVD)

FILE NO. 179
ANOKC 132239
SIGNAL SHEET 298
17 OF 29



INTERCONNECT NOTES:

- 1) IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO UTILIZE THE "ONE CALL EXCAVATION NOTICE SYSTEM" (TELEPHONE NUMBER 651-454-0002) AS REQUIRED BY MINNESOTA STATUTE 2160.
- 2) DISTANCE OFF SHOULDER OR CURB FOR INTERCONNECT CONDUIT SHALL BE 1-2 FEET.
- 3) (**) DENOTES ITEMS TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNDER ITEM NO. 2565 (TRAFFIC CONTROL INTERCONNECT). SEE STATEMENT OF ESTIMATED QUANTITIES AND SPECIAL PROVISIONS.
- 4) NEW HANDHOLES SHALL BE PVC HANDHOLES WITH METAL FRAMES AND COVERS. SEE SPECIAL PROVISIONS.
- 5) ITEMS DENOTED BY "B" SHALL BE INCLUDED AS PART OF PAY ITEM FOR "TRAFFIC CONTROL SIGNAL SYSTEM B".



H.H.56 TO H.H.57:
H.H.57 TO H.H.58:
(**) 2" CONDUIT
(**) 1-12 SM FO CABLE
(**) 1-1/c#10

F&I
(**) H.H.58
H.H.57 TO H.H.58:
H.H.58 TO H.H.2 ("B"):
(**) 2" CONDUIT
(**) 1-12 SM FO CABLE
(**) 1-1/c#10

DRAWN BY: LC
DESIGNER: JMG
CHECKED BY: JMG
DESIGN TEAM

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
Date: April 3, 2016 Name: John M. Gray, PE Lic. No. 22457

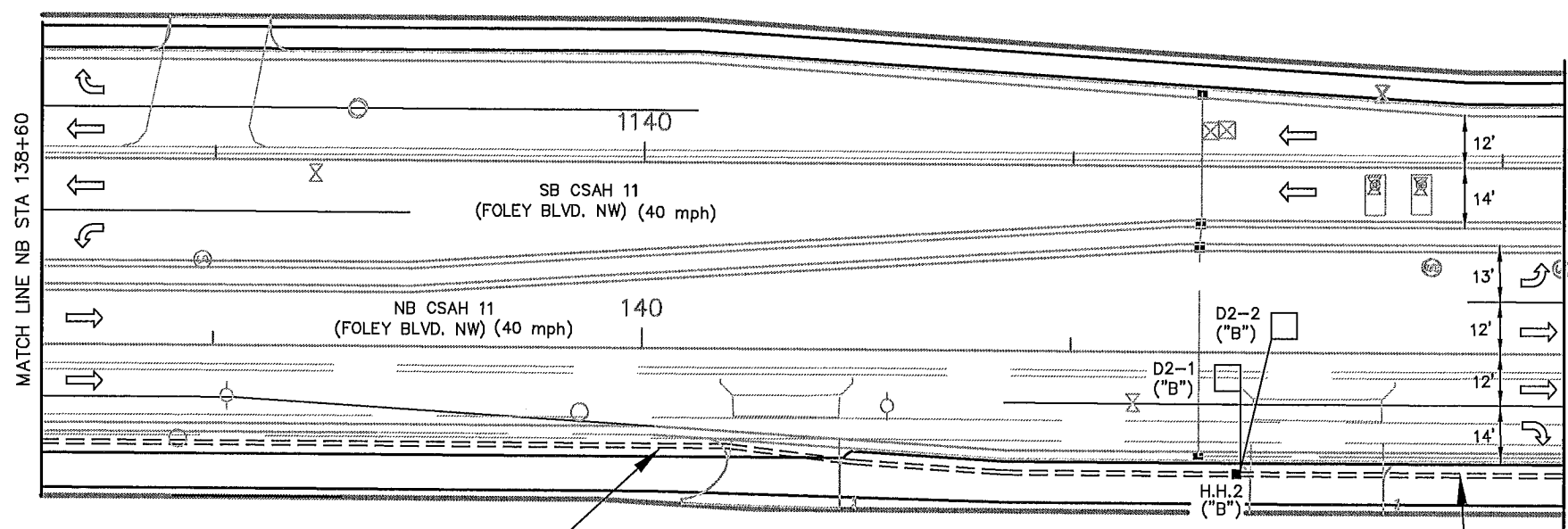
SEH
PHONE: (651) 490-2000
3535 VADNAIS CENTER DR.
ST. PAUL, MN 55110

**ANOKA COUNTY
CITY OF COON RAPIDS**

**TRAFFIC CONTROL INTERCONNECTION
INTERSECTION LAYOUT
CSAH 11 (EGRET BLVD TO NORTHDAL BLVD)**

S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10
FILE NO. ANOKC 132239
SIGNAL SHEET 18 OF 29

180
298



H.H.58 TO H.H.2 ("B"):
 (**) 2" CONDUIT
 (**) 1-12 SM FO CABLE
 (**) 1-1/c#10

H.H.1 TO H.H.2:
 2" R.S.C. ("B")
 2-2/c#14 ("B")
 (**) 2" CONDUIT
 (**) 1-12 SM FO CABLE
 (**) 1-1/c#10

INTERCONNECT NOTES:

- 1) IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO UTILIZE THE "ONE CALL EXCAVATION NOTICE SYSTEM" (TELEPHONE NUMBER 651-454-0002) AS REQUIRED BY MINNESOTA STATUTE 2160.
- 2) DISTANCE OFF SHOULDER OR CURB FOR INTERCONNECT CONDUIT SHALL BE 1-2 FEET.
- 3) (**) DENOTES ITEMS TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNDER ITEM NO. 2565 (TRAFFIC CONTROL INTERCONNECT). SEE STATEMENT OF ESTIMATED QUANTITIES AND SPECIAL PROVISIONS.
- 4) NEW HANDHOLES SHALL BE PVC HANDHOLES WITH METAL FRAMES AND COVERS. SEE SPECIAL PROVISIONS.
- 5) ITEMS DENOTED BY "B" SHALL BE INCLUDED AS PART OF PAY ITEM FOR "TRAFFIC CONTROL SIGNAL SYSTEM B".

S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10

DRAWN BY:	LC
DESIGNER:	JMG
CHECKED BY:	JMG
DESIGN TEAM	

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

John M. Gray
 Name: John M. Gray, PE
 Lic. No. 22457
 Date: April 4, 2016

SEH
 PHONE: (651) 490-2000
 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
CITY OF COON RAPIDS

SIGNAL SYSTEM 'B'/INTERCONNECT
INTERSECTION LAYOUT
 CSAH 11 (FOLEY BLVD NW) AT
 CSAH 11/12 (NORTHDALE BLVD NW)

FILE NO.	181
ANOKC 132239	
SIGNAL SHEET	298
19 OF 29	

NOTES:

- THE EXACT LOCATION OF HANDHOLES, FOUNDATIONS, AND LOOP DETECTORS SHALL BE DETERMINED IN FIELD BY ENGINEER.
- SEE SPECIAL PROVISIONS FOR COUNTY FURNISHED MATERIALS.
- NEW HANDHOLES SHALL BE PVC HANDHOLES WITH METAL FRAMES AND COVERS. SEE SPECIAL PROVISIONS.
- A 3/4" HALF COUPLING, 3/4" PIPE NIPPLE AND CONDUIT OUTLET BODY SHALL BE FURNISHED AND INSTALLED 6 FEET FROM THE LEFT END OF EACH MAST ARM (FOR EVP).
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE POWER COMPANY TO ARRANGE FOR THE POWER CONNECTION.
- THE CONTRACTOR SHALL LOCATE AND VERIFY INPLACE UTILITIES PRIOR TO COMMENCING WORK.
- SEE SPECIAL PROVISIONS AND DETAILS REGARDING SIGNS, CURB RAMPS, AND SIDEWALK TO BE F & I BY CONTRACTOR.
- EACH PEDESTRIAN INDICATION SHALL BE ONE SECTION "FILLED" COUNTDOWN TIMER HAND/WALKING PERSON INDICATION.
- ALL VEHICLE SIGNAL INDICATIONS, AND ALL PEDESTRIAN SIGNAL INDICATIONS SHALL BE LED.
- EACH SIGNAL FACE SHALL HAVE BACKGROUND SHIELD.
- SEE DETAILS, SPECIAL PROVISIONS, AND STATEMENT OF ESTIMATED QUANTITIES REGARDING BATTERY BACK-UP SIGNAL SERVICE CABINET TO BE FURNISHED AND INSTALLED BY CONTRACTOR (SEPARATE FROM ITEM NO. 2565 FOR THIS SIGNAL SYSTEM).
- LOOP DETECTOR WIRES SHALL BE CROSS-LINKED POLYETHYLENE (XLP) IN 3/4" N.M.C. SEE SPECIAL PROVISIONS.

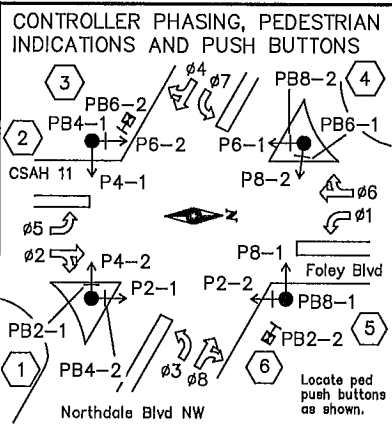
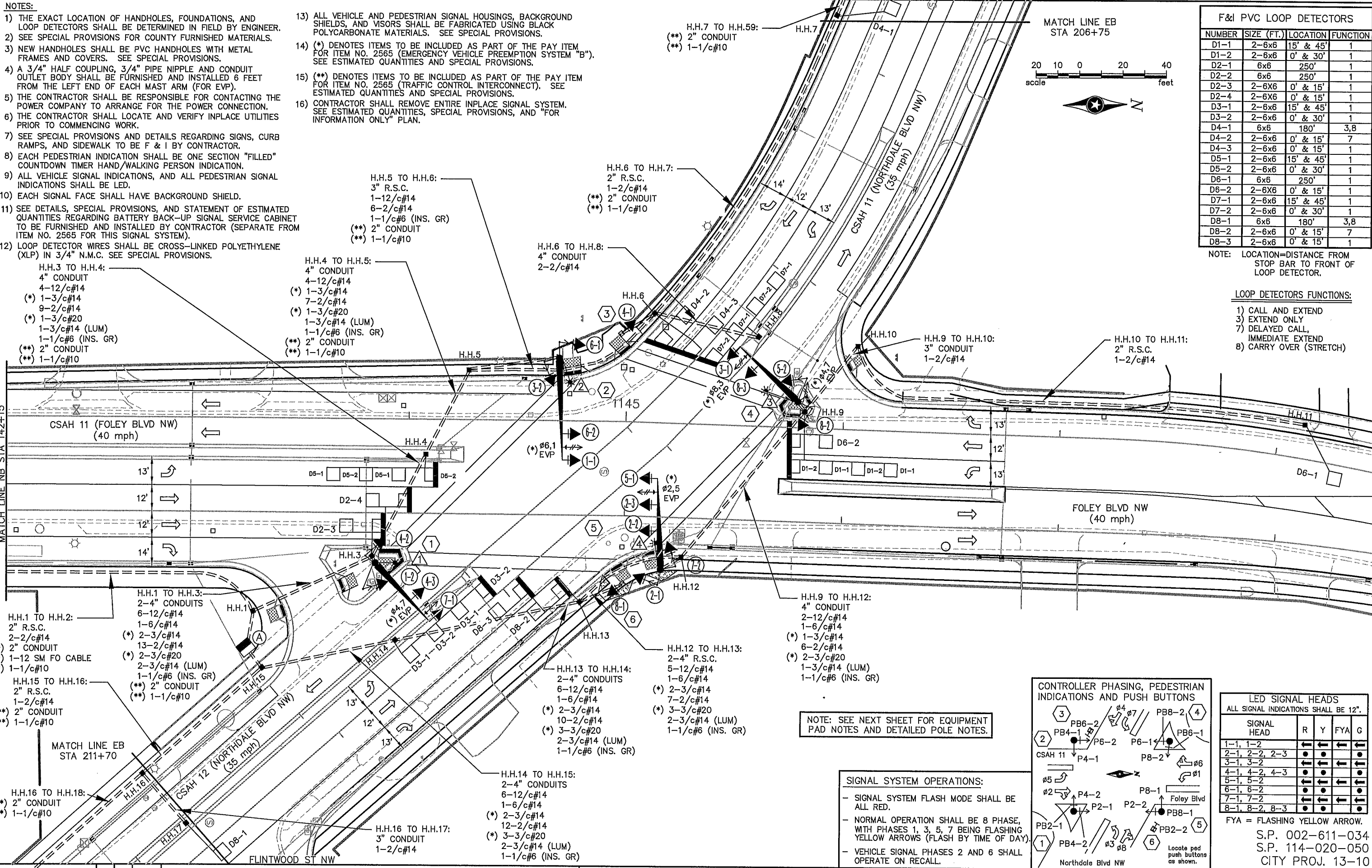
- ALL VEHICLE AND PEDESTRIAN SIGNAL HOUSINGS, BACKGROUND SHIELDS, AND VISORS SHALL BE FABRICATED USING BLACK POLYCARBONATE MATERIALS. SEE SPECIAL PROVISIONS.
- (*) DENOTES ITEMS TO BE INCLUDED AS PART OF THE PAY ITEM FOR ITEM NO. 2565 (EMERGENCY VEHICLE PREEMPTION SYSTEM "B"). SEE ESTIMATED QUANTITIES AND SPECIAL PROVISIONS.
- (**) DENOTES ITEMS TO BE INCLUDED AS PART OF THE PAY ITEM FOR ITEM NO. 2565 (TRAFFIC CONTROL INTERCONNECT). SEE ESTIMATED QUANTITIES AND SPECIAL PROVISIONS.
- CONTRACTOR SHALL REMOVE ENTIRE INPLACE SIGNAL SYSTEM. SEE ESTIMATED QUANTITIES, SPECIAL PROVISIONS, AND "FOR INFORMATION ONLY" PLAN.

F&I PVC LOOP DETECTORS			
NUMBER	SIZE (FT.)	LOCATION	FUNCTION
D1-1	2-6x6	15' & 45'	1
D1-2	2-6x6	0' & 30'	1
D2-1	6x6	250'	1
D2-2	6x6	250'	1
D2-3	2-6x6	0' & 15'	1
D2-4	2-6x6	0' & 15'	1
D3-1	2-6x6	15' & 45'	1
D3-2	2-6x6	0' & 30'	1
D4-1	6x6	180'	3,8
D4-2	2-6x6	0' & 15'	7
D4-3	2-6x6	0' & 15'	1
D5-1	2-6x6	15' & 45'	1
D5-2	2-6x6	0' & 30'	1
D6-1	6x6	250'	1
D6-2	2-6x6	0' & 15'	1
D7-1	2-6x6	15' & 45'	1
D7-2	2-6x6	0' & 30'	1
D8-1	6x6	180'	3,8
D8-2	2-6x6	0' & 15'	7
D8-3	2-6x6	0' & 15'	1

NOTE: LOCATION=DISTANCE FROM STOP BAR TO FRONT OF LOOP DETECTOR.

LOOP DETECTORS FUNCTIONS:

- CALL AND EXTEND
- EXTEND ONLY
- DELAYED CALL, IMMEDIATE EXTEND
- CARRY OVER (STRETCH)



LED SIGNAL HEADS				
ALL SIGNAL INDICATIONS SHALL BE 12".				
SIGNAL HEAD	R	Y	FYA	G
1-1, 1-2	←	←	←	←
2-1, 2-2, 2-3	←	←	←	←
3-1, 3-2	←	←	←	←
4-1, 4-2, 4-3	←	←	←	←
5-1, 5-2	←	←	←	←
6-1, 6-2	←	←	←	←
7-1, 7-2	←	←	←	←
8-1, 8-2, 8-3	←	←	←	←

FYA = FLASHING YELLOW ARROW.

SIGNAL SYSTEM OPERATIONS:

- SIGNAL SYSTEM FLASH MODE SHALL BE ALL RED.
- NORMAL OPERATION SHALL BE 8 PHASE, WITH PHASES 1, 3, 5, 7 BEING FLASHING YELLOW ARROWS (FLASH BY TIME OF DAY).
- VEHICLE SIGNAL PHASES 2 AND 6 SHALL OPERATE ON RECALL.

NOTE: SEE NEXT SHEET FOR EQUIPMENT PAD NOTES AND DETAILED POLE NOTES.

DRAWN BY: LC
 DESIGNER: JMG
 CHECKED BY: JMG

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Date: April 4, 2016 Name: John M. Gray, PE Lic. No. 22457

SEH
 PHONE: (651) 490-2000
 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
 CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEM "B"
 INTERSECTION LAYOUT
 CSAH 11 (FOLEY BLVD NW) AT
 CSAH 11/12 (NORTHDALE BLVD NW)

FILE NO. 182
 ANOKC 132239
 SIGNAL SHEET 20 OF 29
 CITY PROJ. 13-10



- (A) INSTALL CONTROLLER AND CABINET (FURNISHED BY COUNTY)
EQUIPMENT PAD FOUNDATION
BATTERY BACK-UP SIGNAL SERVICE CABINET
BETWEEN CONTROLLER CABINET AND SERVICE CABINET:
METERED SIGNAL SERVICE
2"R.S.C.
3-1/c#6
CONTROLLER CABINET TO H.H.1:
4"R.S.C. 4"R.S.C.
2-12/c#14 4-12/c#14
1-6/c#14 (*) 1-3/c#14
(*) 1-3/c#14 9-2/c#14
6-2/c#14 (*) 1-3/c#20
(*) 1-3/c#20 1-1/c#6 (GRD)
CONTROLLER CABINET TO H.H.15:
4"R.S.C. 4"R.S.C.
4-12/c#14 2-12/c#14
(*) 1-3/c#14 1-6/c#14
7-2/c#14 (*) 1-3/c#14
(*) 1-3/c#20 6-2/c#14
1-1/c#6 (GRD) (*) 2-3/c#20
CONTROLLER CABINET TO H.H.1:
(**) 2"R.S.C.
(**) 1-12 SM FIBER OPTIC CABLE
(**) 1-1/c#10
CONTROLLER CABINET TO H.H.15:
(**) 2"R.S.C.
(**) 1-1/c#10

- SERVICE CABINET TO H.H.1:
1 1/4"R.S.C.
UNMETERED STREET LIGHT SERVICE
2-3/c#14 (LUM)
SERVICE CABINET TO H.H.15:
1 1/4"R.S.C.
UNMETERED STREET LIGHT SERVICE
2-3/c#14 (LUM)
STUB OUT 2"R.S.C. FROM SERVICE CABINET TO EAST (FOR SERVICE BY CONNEXUS)
STUB OUT 3"R.S.C. FROM CONTROLLER CABINET TO EAST (THREAD AND CAP-FOR FUTURE USE)

- ① PA90 POLE FOUNDATION
TYPE PA90-A-35-D30-9 (DAVIT AT 270 DEG)
LUMINAIRE-LED "SHOEBOX"
1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0'
1-STRAIGHT MOUNT SIGNAL-OVERHEAD AT 12'
2-ANGLE MOUNT SIGNALS-POLE MOUNTED 90/180 DEG
2-ANGLE MOUNT C.D. PED INDICATIONS-POLE MOUNTED 90/180 DEG
2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e)
R10-X12 SIGN PANEL-ADJACENT TO 7-1
TYPE D SIGN PANEL-OVERHEAD (D-1)
(*) INSTALL ONE WAY EVP DETECTOR AND CONFIRMATION LIGHT (FURNISHED BY COUNTY) (Ø4,7)
(*) ONE WAY EVP MOUNTING HARDWARE (FOR COUNTY FURNISHED EVP DETECTOR AND LIGHT)
EXTEND INTO H.H.3:
3"R.S.C.
2-12/c#14
1-6/c#14
(*) 1-3/c#14
2-2/c#14
(*) 1-3/c#20
1-3/c#14 (LUM)
1-1/c#6 (GRD)

- ② PA100 POLE FOUNDATION
TYPE PA100-A-45-D30-9 (DAVIT AT 270 DEG)
LUMINAIRE-LED "SHOEBOX"
1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0'
1-STRAIGHT MOUNT SIGNAL-OVERHEAD AT 11'
2-ANGLE MOUNT SIGNALS-POLE MOUNTED 90/180 DEG
2-ANGLE MOUNT C.D. PED INDICATIONS-POLE MOUNTED 90/180 DEG
1-PEDESTRIAN PUSH BUTTON & SIGN (R10-3e)
R10-X12 SIGN PANEL-ADJACENT TO 1-1
TYPE D SIGN PANEL-OVERHEAD (D-2)
(*) INSTALL ONE WAY EVP DETECTOR AND CONFIRMATION LIGHT (FURNISHED BY COUNTY) (Ø6,1)
(*) ONE WAY EVP MOUNTING HARDWARE (FOR COUNTY FURNISHED EVP DETECTOR AND LIGHT)
EXTEND INTO H.H.5:
3"R.S.C.
3-12/c#14
(*) 1-3/c#14
1-2/c#14
(*) 1-3/c#20
1-3/c#14 (LUM)
1-1/c#6 (GRD)

- ③ PEDESTAL FOUNDATION
12' PEDESTAL POLE (INCLUDES BASE, WIND COLLAR)
1-STRAIGHT MOUNT SIGNAL-POLE MOUNTED
1-PEDESTRIAN PUSH BUTTON & SIGN (R10-3e)
EXTEND INTO H.H.6:
3"R.S.C.
1-12/c#14
1-2/c#14
1-1/c#6 (GRD)

- ⑤ PA100 POLE FOUNDATION
TYPE PA100-A-40-D30-9 (DAVIT AT 270 DEG)
LUMINAIRE-LED "SHOEBOX"
1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0'
2-STRAIGHT MOUNT SIGNALS-OVERHEAD AT 11' & 23'
2-ANGLE MOUNT SIGNALS-POLE MOUNTED 90/180 DEG
2-ANGLE MOUNT C.D. PED INDICATIONS-POLE MOUNTED 90/180 DEG
1-PEDESTRIAN PUSH BUTTON & SIGN (R10-3e)
R10-X12 SIGN PANEL-ADJACENT TO 5-1
TYPE D SIGN PANEL-OVERHEAD (D-4)
(*) INSTALL ONE WAY EVP DETECTOR AND CONFIRMATION LIGHT (FURNISHED BY COUNTY) (Ø2,5)
(*) ONE WAY EVP MOUNTING HARDWARE (FOR COUNTY FURNISHED EVP DETECTOR AND LIGHT)
EXTEND INTO H.H.12
3"R.S.C.
3-12/c#14
(*) 1-3/c#14
1-2/c#14
(*) 1-3/c#20
1-3/c#14 (LUM)
1-1/c#6 (GRD)

- ⑥ PEDESTAL FOUNDATION
12' PEDESTAL POLE (INCLUDES BASE, WIND COLLAR)
1-STRAIGHT MOUNT SIGNAL-POLE MOUNTED
1-PEDESTRIAN PUSH BUTTON & SIGN (R10-3e)
EXTEND INTO H.H.13:
3"R.S.C.
1-12/c#14
1-2/c#14
1-1/c#6 (GRD)

H.H.7 TO H.H.59:
H.H.59 TO H.H.60:
(**) 2" CONDUIT
(**) 1-1/c#10

F&I
(**) H.H.59

F&I
(**) H.H.60

H.H.59 TO H.H.60:
(**) 2" CONDUIT
(**) 1-1/c#10

H.H.6 TO H.H.7:
2" CONDUIT
1-2/c#14
(**) 2" CONDUIT
(**) 1-1/c#10

MATCH LINE EB STA 206+75

MATCH LINE STA 202+80

DRAWN BY: LC
DESIGNER: JMG
CHECKED BY: JMG
DESIGN TEAM

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
Name: John M. Gray, PE
Date: April 4, 2016 Lc. No. 22457

SEH
PHONE: (651) 490-2000
3535 VADNAIS CENTER DR.
ST. PAUL, MN 55110

ANOKA COUNTY
CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEM 'B'
INTERSECTION LAYOUT
CSAH 11 (FOLEY BLVD NW) AT
CSAH 11/12 (NORTHDALE BLVD NW)

S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10
FILE NO. 183
SIGNAL SHEET 298
21 OF 29



H.H.15 TO H.H.16:
2" CONDUIT
1-2/c#14
(**) 2" CONDUIT
(**) 1-1/c#10

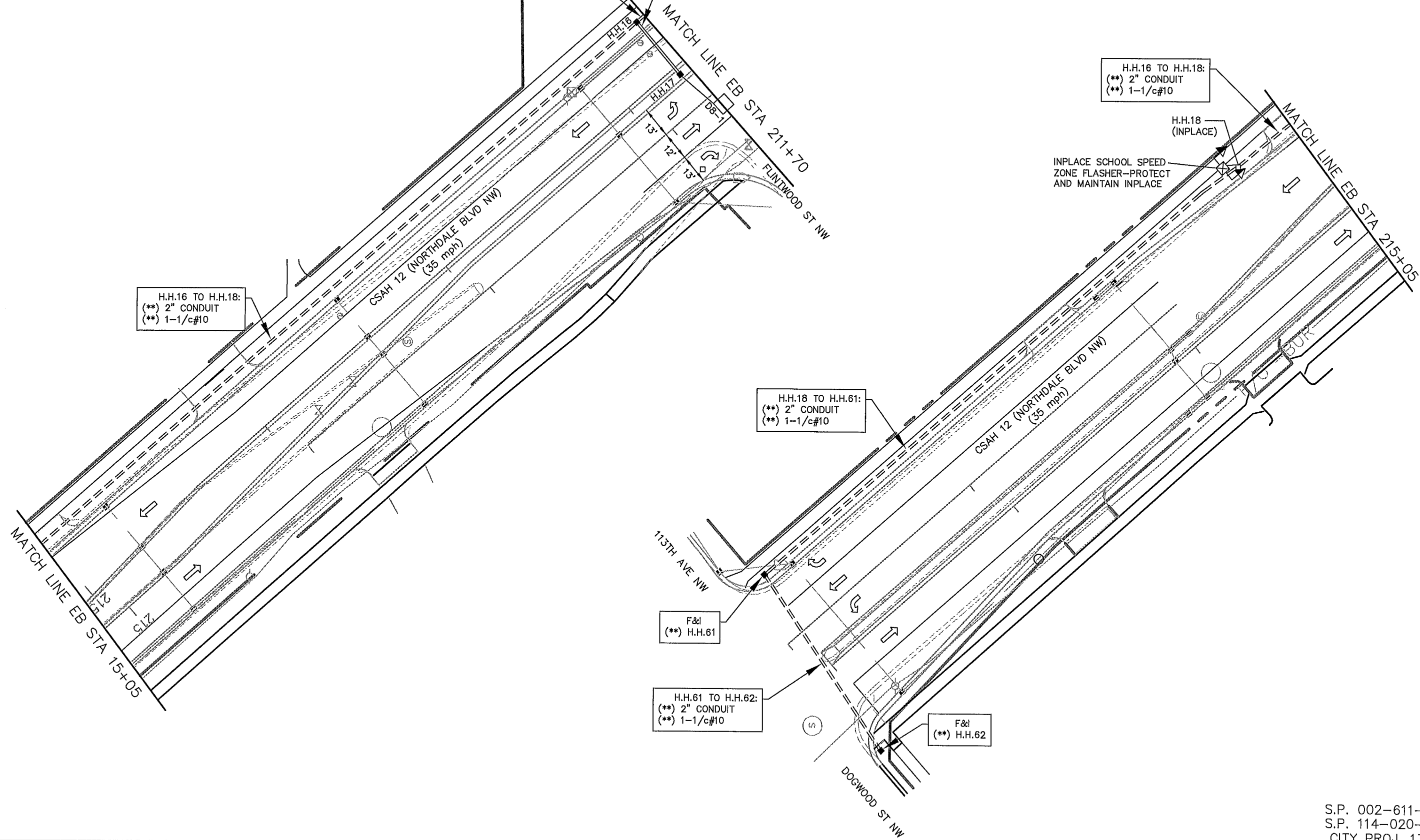
H.H.16 TO H.H.17:
3" CONDUIT
1-2/c#14

H.H.16 TO H.H.18:
(**) 2" CONDUIT
(**) 1-1/c#10

H.H.16 TO H.H.18:
(**) 2" CONDUIT
(**) 1-1/c#10

H.H.18 TO H.H.61:
(**) 2" CONDUIT
(**) 1-1/c#10

H.H.61 TO H.H.62:
(**) 2" CONDUIT
(**) 1-1/c#10



S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10

DRAWN BY: LC
DESIGNER: JMG
CHECKED BY: JMG
DESIGN TEAM

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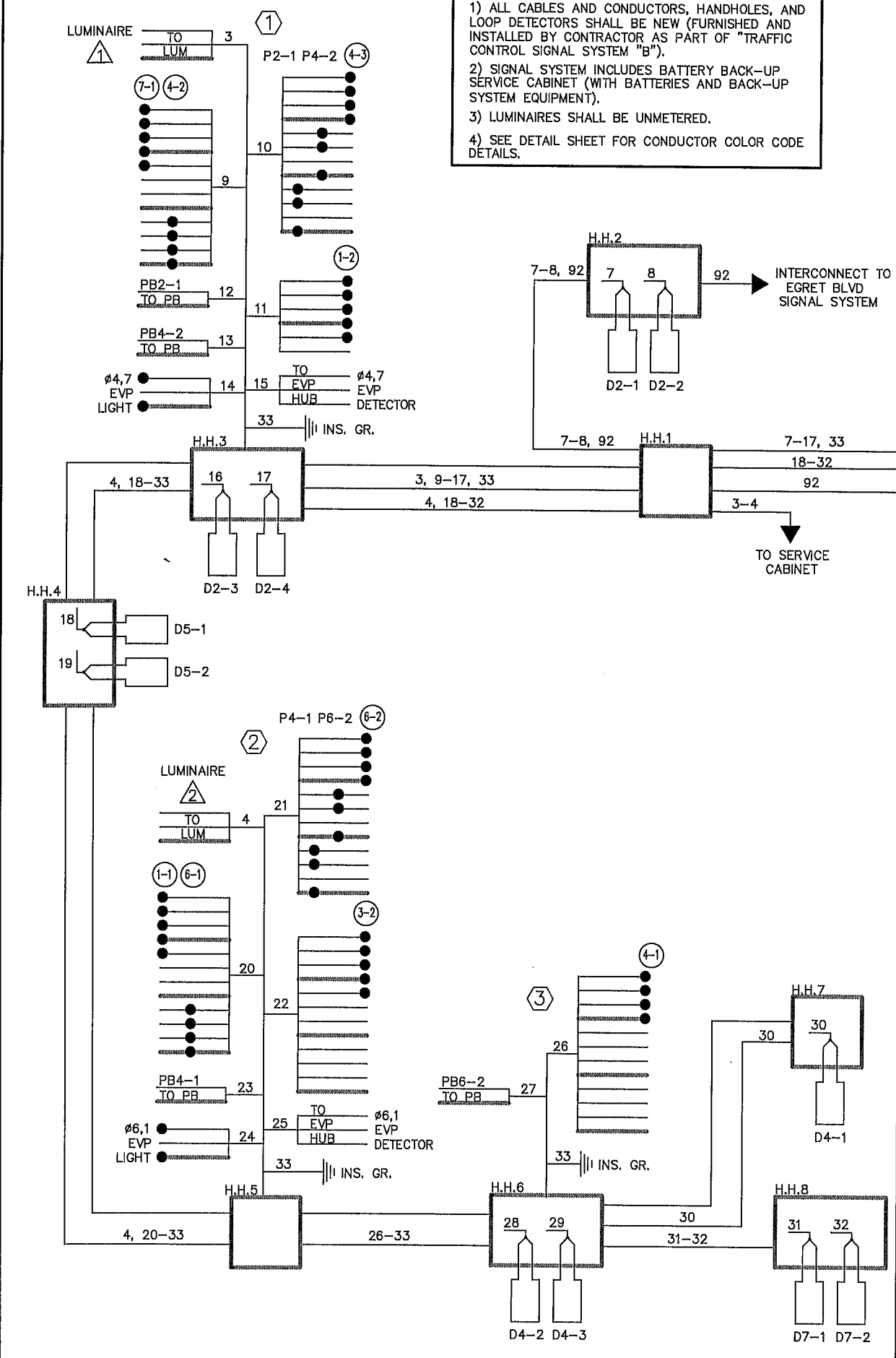
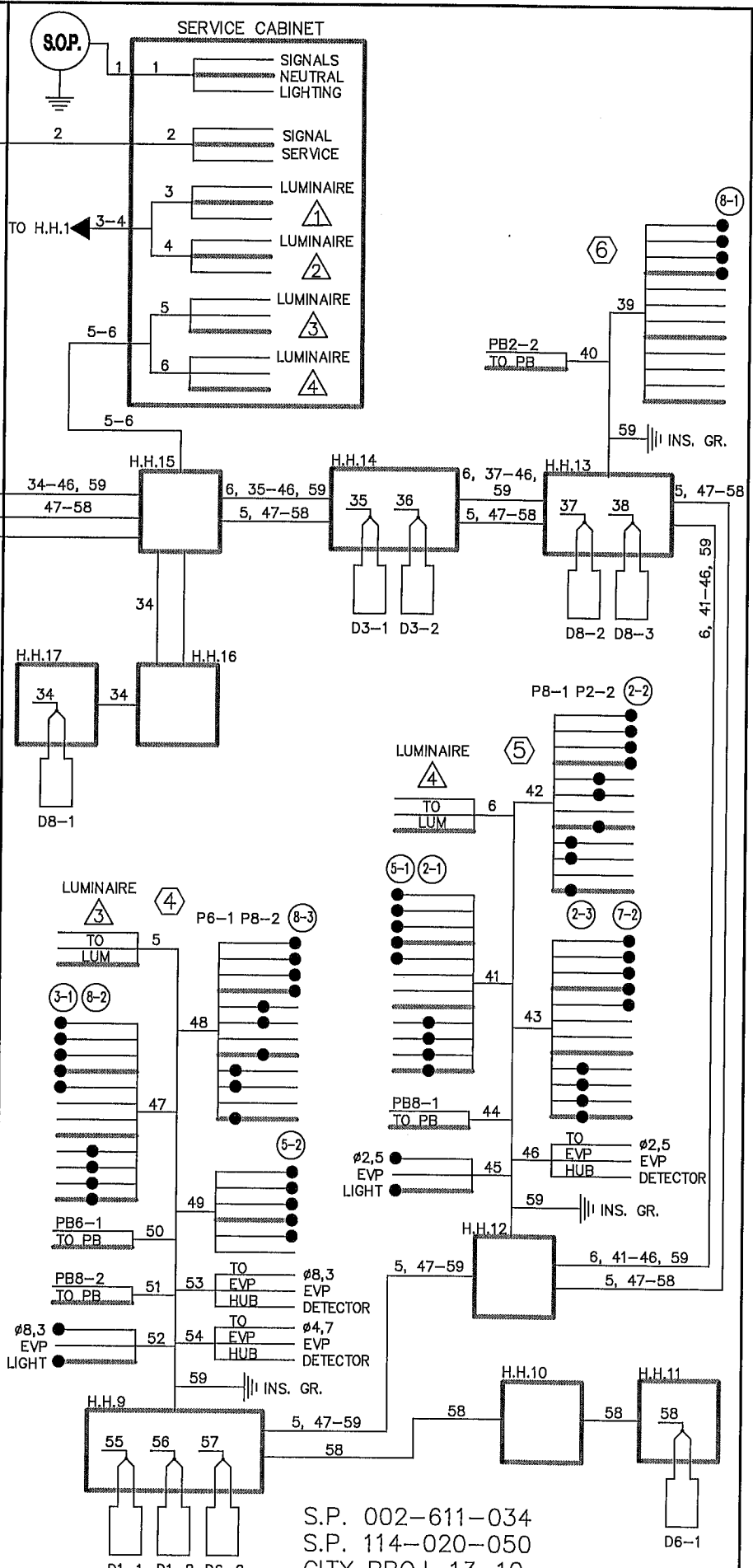
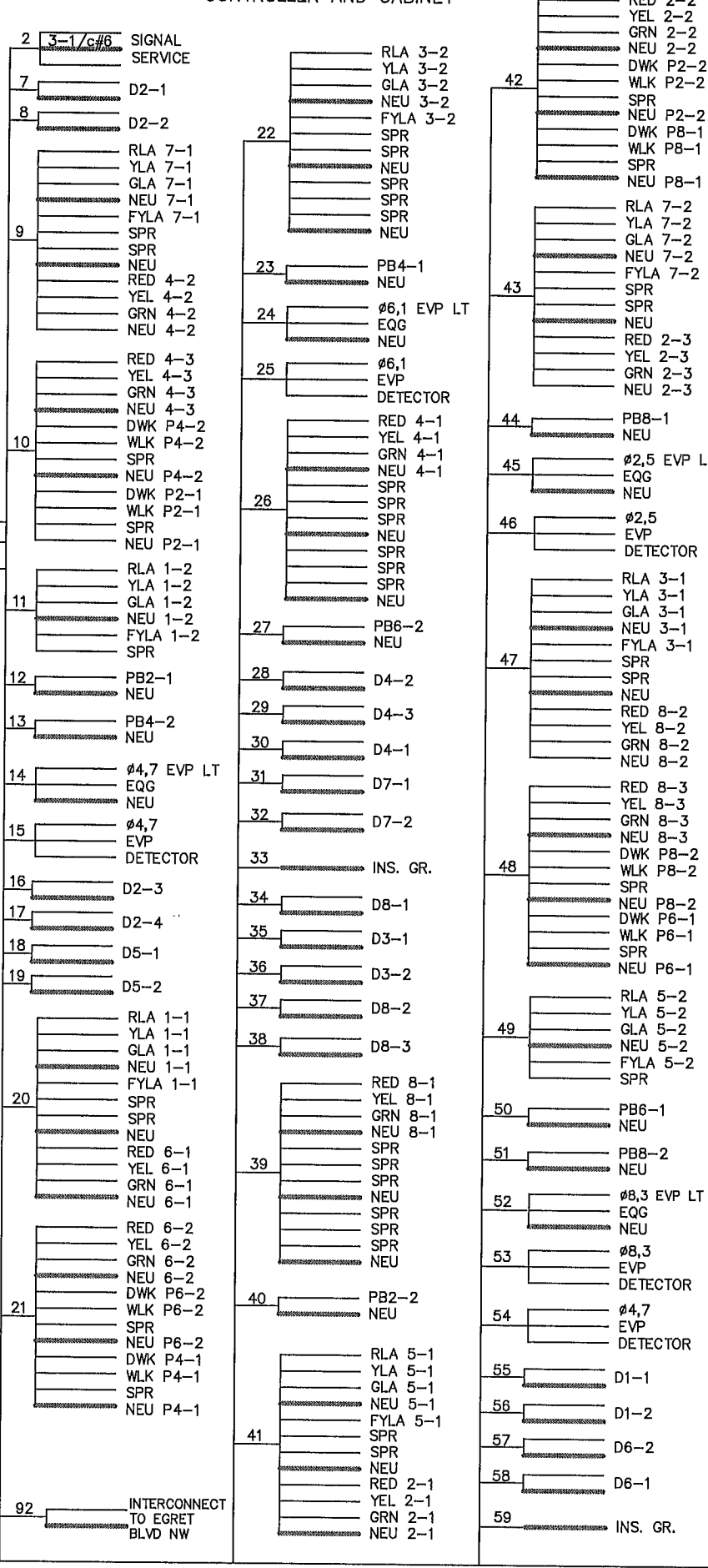
ANOKA COUNTY
CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEM 'B'
INTERSECTION LAYOUT
CSAH 11 (FOLEY BLVD NW) AT
CSAH 11/12 (NORTHDALE BLVD NW)

FILE NO. ANOKC 132239
SIGNAL SHEET 22 OF 29
184
298

NOTES:
 1) ALL CABLES AND CONDUCTORS, HANDHOLES, AND LOOP DETECTORS SHALL BE NEW (FURNISHED AND INSTALLED BY CONTRACTOR AS PART OF "TRAFFIC CONTROL SIGNAL SYSTEM 'B'").
 2) SIGNAL SYSTEM INCLUDES BATTERY BACK-UP SERVICE CABINET (WITH BATTERIES AND BACK-UP SYSTEM EQUIPMENT).
 3) LUMINAIRES SHALL BE UNMETERED.
 4) SEE DETAIL SHEET FOR CONDUCTOR COLOR CODE DETAILS.

CONTROLLER AND CABINET



DRAWN BY: LC
 DESIGNER: JMG
 CHECKED BY: JMG
 DESIGN TEAM

NO.	BY	DATE	REVISIONS

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 Name: John M. Gray, PE
 Ltc. No. 22457
 Date: April 4, 2016

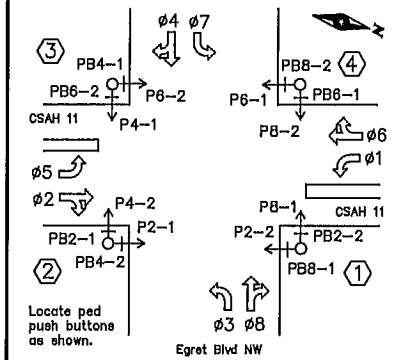
SEH
 PHONE: (651) 490-2000
 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
 CITY OF COON RAPIDS

TRAFFIC SIGNAL SYSTEM 'B'
 FIELD WIRING DIAGRAM
 CSAH 11 (FOLEY BLVD NW) AT
 CSAH 11/12 (NORTHDAL BLVD NW)

FILE NO. ANOKC 132239
 SIGNAL SHEET 23 OF 29
 185
 298

CONTROLLER PHASING, PEDESTRIAN INDICATIONS AND PUSH BUTTONS



SIGNAL SYSTEM OPERATIONS:

- SIGNAL SYSTEM FLASH MODE IS ALL RED.
- NORMAL OPERATION IS 8 PHASE, WITH PHASES 1, 3, 5, AND 7 BEING FLASHING YELLOW ARROW PHASES (FLASH BY TIME OF DAY).
- VEHICLE SIGNAL PHASES 2 AND 6 OPERATE ON RECALL.

PVC LOOP DETECTORS

NUMBER	SIZE (FT.)	LOCATION	FUNCTION	STATUS
D1-1	2-6x6	10' & 40'	1	INPLACE
D1-2	2-6x6	-5' & 25'	1	INPLACE
D2-1	6x6	250'	1	INPLACE
D2-2	6x6	250'	1	INPLACE
D3-1	2-6x6	10' & 40'	7	INPLACE
D3-2	2-6x6	-5' & 25'	7	INPLACE
D4-1	6x6	250'	3, 8	INPLACE
D4-2	2-6x6	0' & 15'	7	INPLACE
D4-3	2-6x6	0' & 15'	7	INPLACE
D5-1	2-6x6	20' & 50'	1	INPLACE
D5-2	2-6x6	5' & 35'	1	INPLACE
D6-1	6x6	250'	1	INPLACE
D6-2	6x6	250'	1	INPLACE
D7-1	2-6x6	15' & 45'	7	INPLACE
D7-2	2-6x6	0' & 30'	7	INPLACE
D8-1	6x6	220'	3, 8	INPLACE
D8-2	6x6	220'	3, 8	INPLACE
D8-3	6x10&6x6	AS SHOWN	7	INPLACE

NOTE: LOCATION=DISTANCE FROM STOP BAR TO FRONT OF DETECTOR.

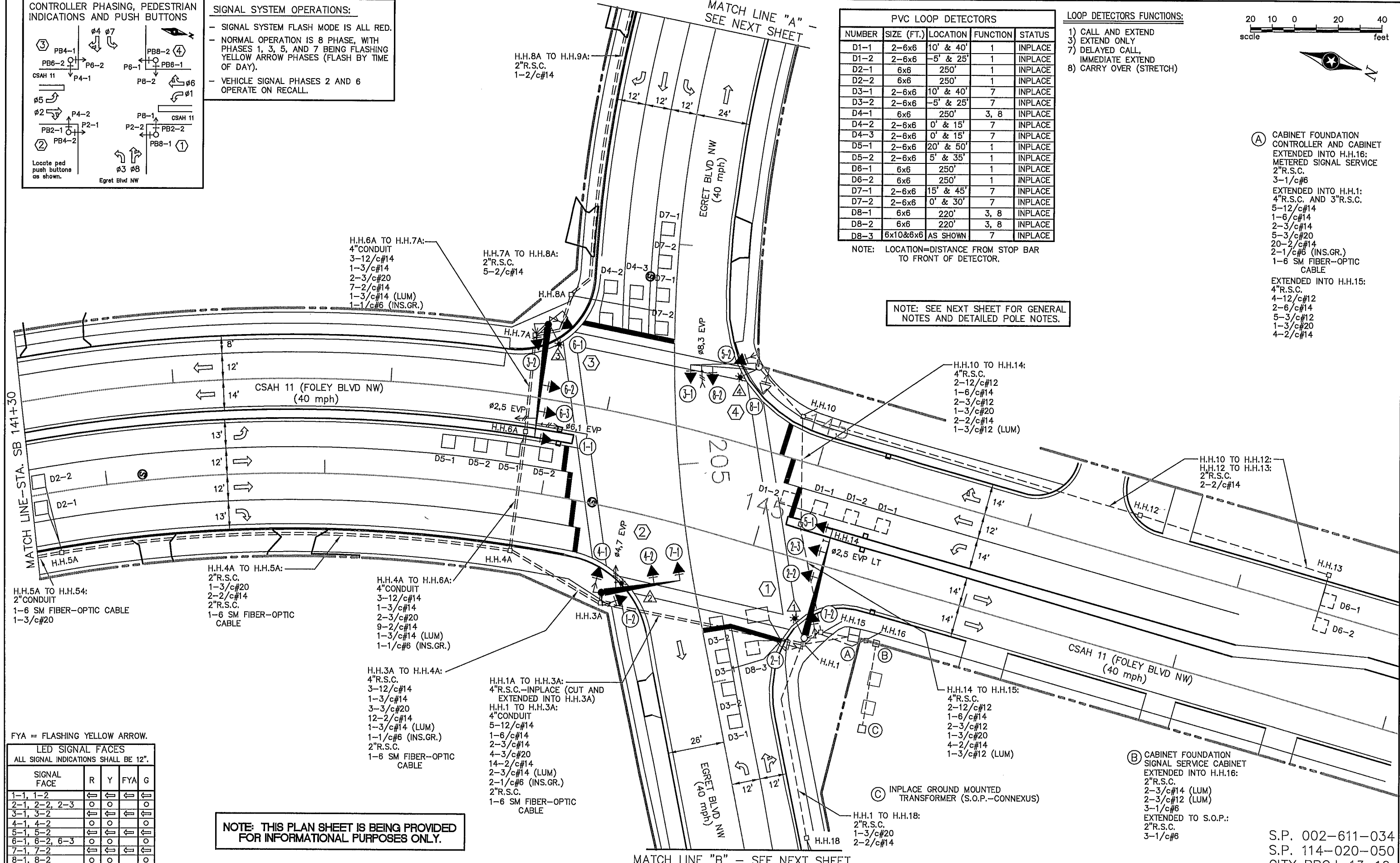
LOOP DETECTORS FUNCTIONS:

- 1) CALL AND EXTEND
- 3) EXTEND ONLY
- 7) DELAYED CALL, IMMEDIATE EXTEND
- 8) CARRY OVER (STRETCH)



- (A) CABINET FOUNDATION CONTROLLER AND CABINET EXTENDED INTO H.H.16: METERED SIGNAL SERVICE 2"R.S.C. 3-1/c#6 EXTENDED INTO H.H.1: 4"R.S.C. AND 3"R.S.C. 5-12/c#14 1-6/c#14 2-3/c#14 5-3/c#20 20-2/c#14 2-1/c#6 (INS.GR.) 1-6 SM FIBER-OPTIC CABLE EXTENDED INTO H.H.15: 4"R.S.C. 4-12/c#12 2-6/c#14 5-3/c#12 1-3/c#20 4-2/c#14

NOTE: SEE NEXT SHEET FOR GENERAL NOTES AND DETAILED POLE NOTES.



FYA = FLASHING YELLOW ARROW.

LED SIGNAL FACES

ALL SIGNAL INDICATIONS SHALL BE 12".

SIGNAL FACE	R	Y	FYA	G
1-1, 1-2	←	←	←	←
2-1, 2-2, 2-3	○	○	○	○
3-1, 3-2	○	○	←	←
4-1, 4-2	○	○	←	←
5-1, 5-2	○	○	←	←
6-1, 6-2, 6-3	○	○	←	←
7-1, 7-2	←	←	←	←
8-1, 8-2	○	○	←	←

NOTE: THIS PLAN SHEET IS BEING PROVIDED FOR INFORMATIONAL PURPOSES ONLY.

DRAWN BY: LC
 DESIGNER: JMG
 CHECKED BY: JMG
 DESIGN TEAM

NO.	BY	DATE	REVISIONS

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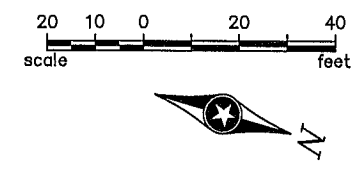
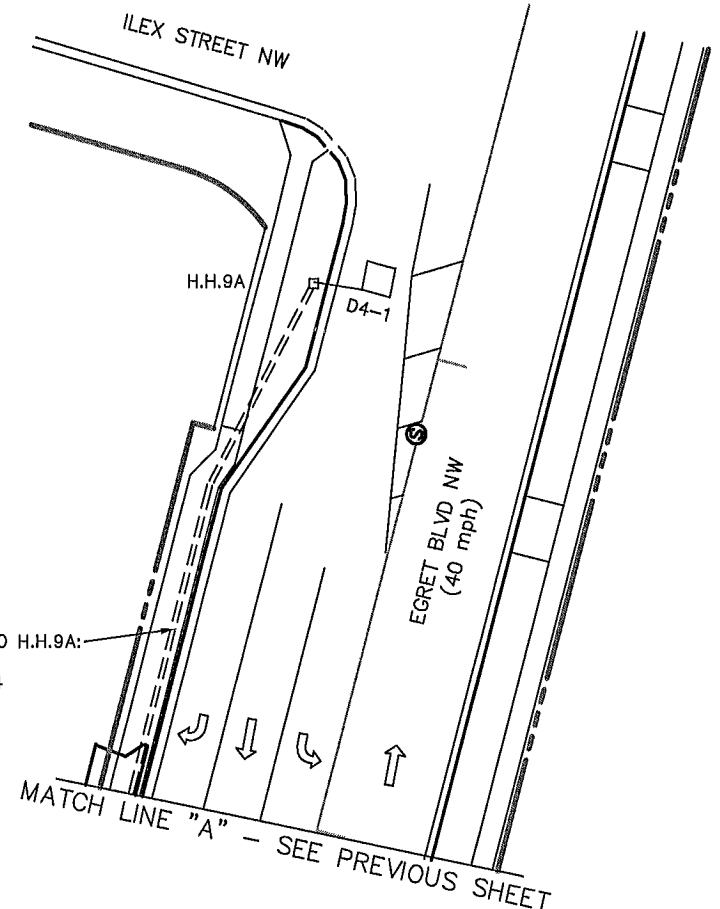
SEH
 PHONE: (651) 490-2000
 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
CITY OF COON RAPIDS

INPLACE SIGNAL SYSTEM 'A'
'FOR INFORMATION ONLY'
 CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW

FILE NO. 186
 ANOKC 132239
 SIGNAL SHEET 298
 24 OF 29

S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10

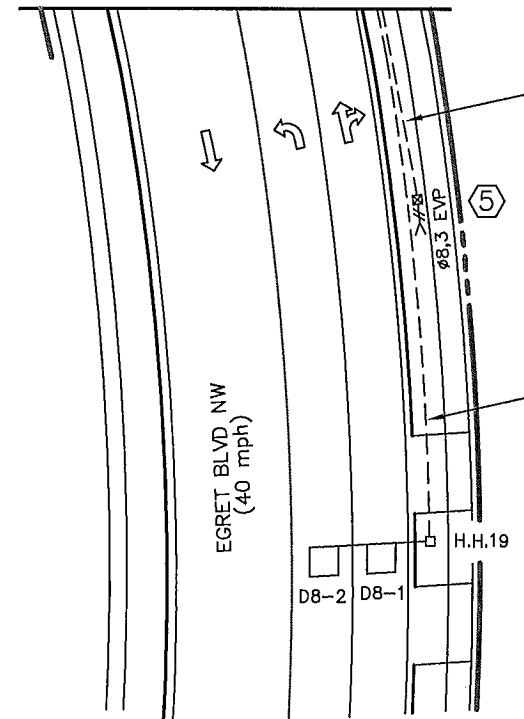


H.H.8A TO H.H.9A:
2"R.S.C.
1-2/c#14

① PA100 POLE FOUNDATION
TYPE PA100-A-50-D40-9 (DAVIT AT 350 DEG)
LUMINAIRE-LED SHOEBOX
LUMINAIRE CHECK SWITCH AT 0 DEG
1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0'
2-STRAIGHT MOUNT SIGNALS-OVERHEAD AT 12' & 24'
2-TYPE 10B-POLE MOUNTED 90 DEG & 180 DEG
2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e)
R10-X12 SIGN PANEL-ADJACENT TO 5-1
TYPE D SIGN PANEL-OVERHEAD
ONE WAY EVP CONFIRMATION LIGHT (#2,5)
EXTENDED INTO H.H.15:
3"R.S.C.
2-12/c#12
1-6/c#14
3-3/c#12
1-3/c#12 (LUM)

② PA90 POLE FOUNDATION
TYPE PA90-A-35-D40-9 (DAVIT AT 350 DEG)
LUMINAIRE-LED "SHOEBOX"
LUMINAIRE CHECK SWITCH AT 0 DEG
1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0'
1-STRAIGHT MOUNT SIGNAL-OVERHEAD AT 12'
2-TYPE 10B-POLE MOUNTED 90 DEG & 180 DEG
2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e)
R10-X12 SIGN PANEL-ADJACENT TO 7-1
TYPE D SIGN PANEL-OVERHEAD
ONE WAY EVP DETECTOR AND LIGHT (#4,7)-3 FEET
FROM RIGHT END OF MAST ARM
EXTENDED INTO H.H.3A:
3" CONDUIT
2-12/c#14
1-6/c#14
2-3/c#14
1-3/c#20
2-2/c#14
1-1/c#6 (INS. GR.)

MATCH LINE "B" - SEE PREVIOUS SHEET



BASE 5 TO H.H.18:
2"R.S.C.
1-3/c#20

⑤ PEDESTAL FOUNDATION
PEDESTAL POLE AND BASE
ONE WAY EVP DETECTOR-MOUNTED ON TOP OF
SLIPFITTER COLLAR (#8,3)
EXTENDED INTO H.H.18:
2"R.S.C.
1-3/c#20

H.H.18 TO H.H.19:
1/4"R.S.C.
2-2/c#14

③ PA100 POLE FOUNDATION
TYPE PA100-A-50-D40-9 (DAVIT AT 350 DEG)
LUMINAIRE-LED "SHOEBOX"
LUMINAIRE CHECK SWITCH AT 0 DEG
1-ANGLE MOUNT SIGNAL-OVERHEAD AT 0'
2-STRAIGHT MOUNT SIGNALS-OVERHEAD AT 12' & 24'
2-TYPE 10B-POLE MOUNTED 90 DEG & 180 DEG
2-PEDESTRIAN PUSH BUTTONS & SIGNS (R10-3e)
R10-X12 SIGN PANEL-ADJACENT TO 1-1
TYPE D SIGN PANEL-OVERHEAD
TWO WAY EVP DETECTOR AND ONE WAY LIGHT
(#6,1 DETECTOR/LIGHT, #2,5 DETECTOR)
EXTENDED INTO H.H.7A:
3" CONDUIT
3-12/c#14
2-3/c#14
2-3/c#20
2-2/c#14
1-1/c#6 (INS. GR.)

④ PA90 POLE FOUNDATION
TYPE PA90-A-30-D40-9 (DAVIT AT 350 DEG)
LUMINAIRE-LED SHOEBOX
2-ONE WAY SIGNALS-OVERHEAD AT 0' & 11'
2-TYPE 10B-POLE MOUNTED 90/180 DEG
2-PEDESTRIAN PUSH BUTTONS & SIGNS
R10-X12 SIGN PANEL-ADJACENT TO 3-1
TYPE D SIGN PANEL-OVERHEAD
ONE WAY EVP DETECTOR AND LIGHT (#8,3)
EXTENDED INTO H.H.10:
3"R.S.C.
2-12/c#12
1-6/c#14
2-3/c#12
1-3/c#20
1-3/c#12 (LUM)

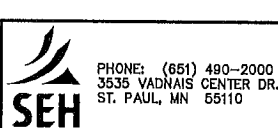
⑤ PEDESTAL FOUNDATION
PEDESTAL POLE, BASE, WIND COLLAR
ONE WAY EVP DETECTOR-MOUNTED ON
TOP OF SLIPFITTER COLLAR (#8,3)
EXTENDED INTO H.H.18:
2"R.S.C.
1-3/c#20

NOTE: THIS PLAN SHEET IS BEING PROVIDED FOR INFORMATIONAL PURPOSES ONLY.

S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10

DESIGNER: JMG	NO.	BY	DATE	REVISIONS
CHECKED BY: JMG				
DESIGN TEAM				

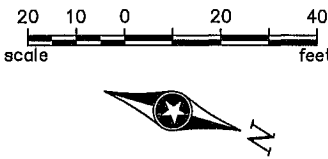
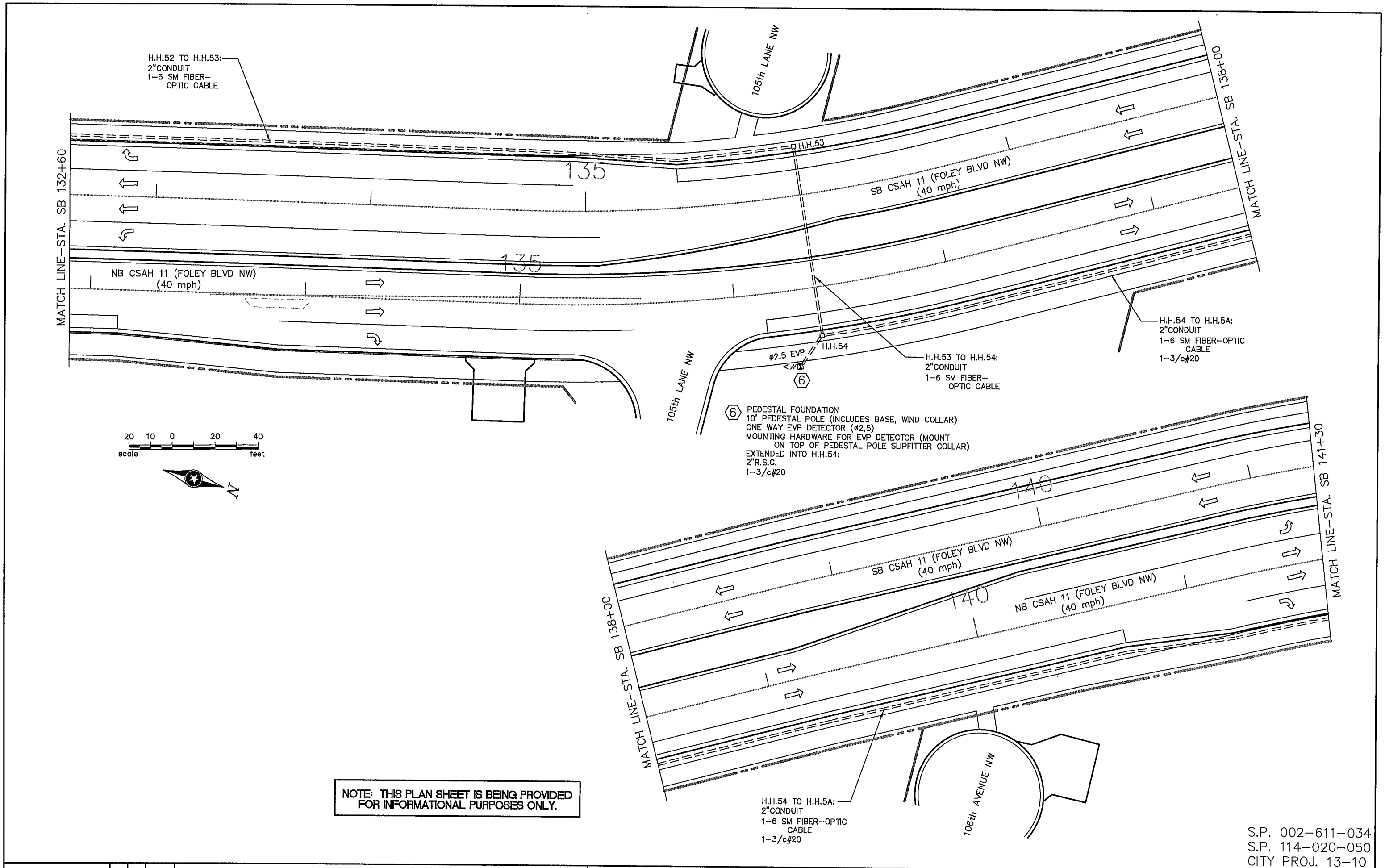
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
Date: April 4, 2016
Name: John M. Gray, PE
Lic. No. 22457



ANOKA COUNTY
CITY OF COON RAPIDS

INPLACE SIGNAL SYSTEM "A"
"FOR INFORMATION ONLY"
CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW

FILE NO. ANOKG 132239
SIGNAL SHEET 25 OF 29
187
298



NOTE: THIS PLAN SHEET IS BEING PROVIDED FOR INFORMATIONAL PURPOSES ONLY.

⑥ PEDESTAL FOUNDATION
 10' PEDESTAL POLE (INCLUDES BASE, WIND COLLAR)
 ONE WAY EVP DETECTOR (Ø2,5)
 MOUNTING HARDWARE FOR EVP DETECTOR (MOUNT ON TOP OF PEDESTAL POLE SLIPFITTER COLLAR) EXTENDED INTO H.H.54:
 2"R.S.C.
 1-3/c#20

DRAWN BY: LC				
DESIGNER: JMG				
CHECKED BY: JMG				
DESIGN TEAM	NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

John M. Gray
 Name: John M. Gray, PE
 Lic. No. 22457
 Date: April 4, 2016

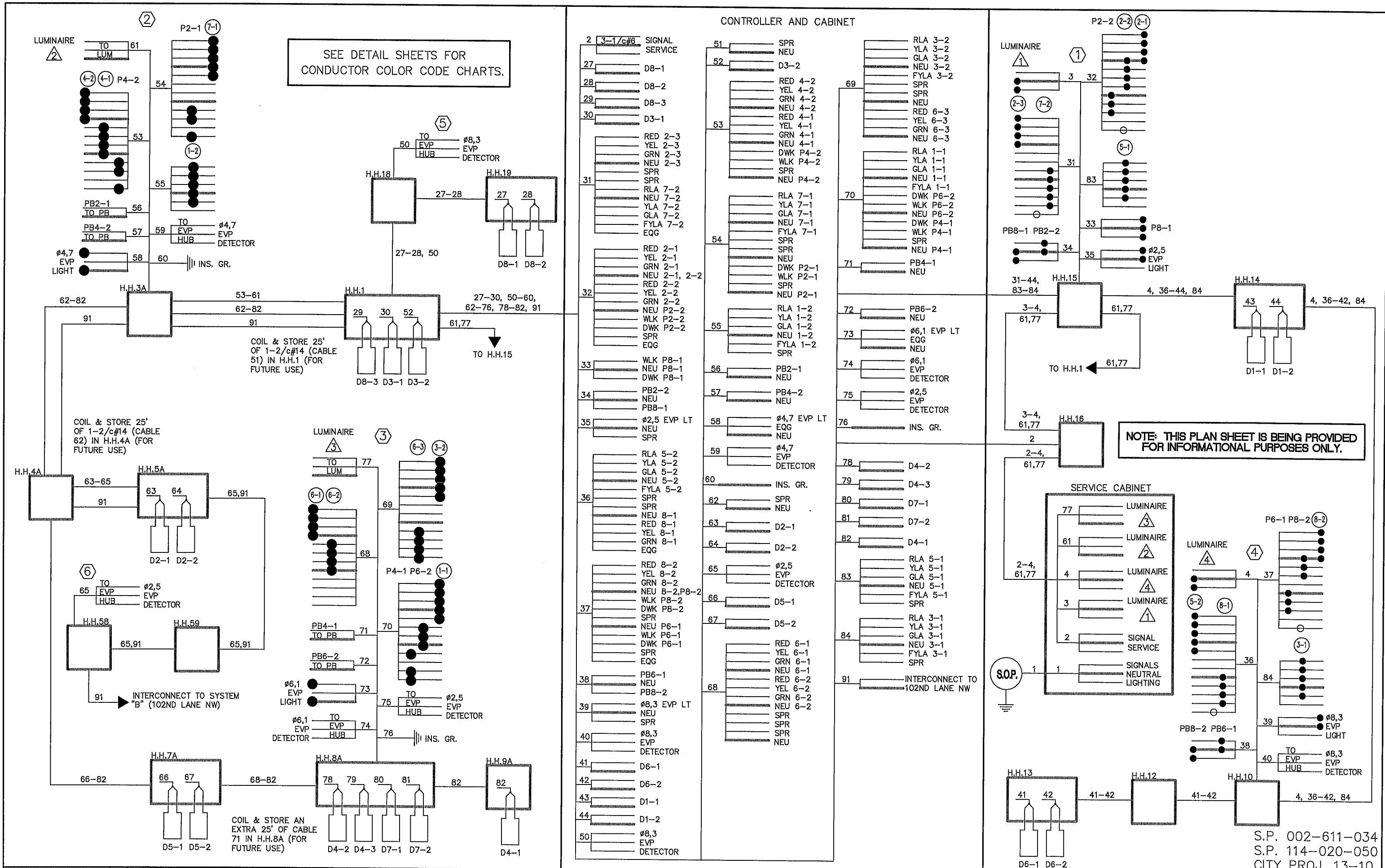
SEH
 PHONE: (651) 490-2000
 3636 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

**ANOKA COUNTY
 CITY OF COON RAPIDS**

**INPLACE SIGNAL SYSTEM 'A'
 'FOR INFORMATION ONLY'
 CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW**

S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10

FILE NO.	188
SIGNAL SHEET	298
26 OF 29	



SEE DETAIL SHEETS FOR
CONDUCTOR COLOR CODE CHARTS.

NOTE: THIS PLAN SHEET IS BEING PROVIDED
FOR INFORMATIONAL PURPOSES ONLY.

- ### CONTROLLER AND CABINET
- 2 3-1/c#6 SIGNAL SERVICE
 - 27 D8-1
 - 28 D8-2
 - 29 D8-3
 - 30 D3-1
 - 31 RED 2-3
YEL 2-3
GRN 2-3
NEU 2-3
SPR
SPR
RLA 7-2
NEU 7-2
YLA 7-2
GLA 7-2
FYLA 7-2
EQG
 - 32 RED 2-1
YEL 2-1
GRN 2-1
NEU 2-1
DWK P2-2
SPR
EQG
 - 33 WLK P8-1
NEU P8-1
DWK P8-1
 - 34 PB2-2
NEU
PB8-1
 - 35 #2,5 EVP LT
NEU
SPR
 - 36 RLA 5-2
YLA 5-2
GLA 5-2
NEU 5-2
FYLA 5-2
SPR
SPR
NEU 8-1
RED 8-1
YEL 8-1
GRN 8-1
EQG
 - 37 RED 8-2
YEL 8-2
GRN 8-2
NEU 8-2, P8-2
WLK P8-2
DWK P8-2
SPR
NEU P6-1
WLK P6-1
DWK P6-1
EQG
 - 38 PB6-1
NEU
PB8-2
 - 39 #8,3 EVP LT
NEU
SPR
 - 40 #8,3
EVP
DETECTOR
 - 41 D6-1
 - 42 D6-2
 - 43 D1-1
 - 44 D1-2
 - 50 #8,3
EVP
DETECTOR
 - 51 SPR
NEU
 - 52 D3-2
 - 53 RED 4-2
YEL 4-2
GRN 4-2
NEU 4-2
RED 4-1
YEL 4-1
GRN 4-1
NEU 4-1
DWK P4-2
WLK P4-2
NEU P4-2
 - 54 RLA 7-1
YLA 7-1
GLA 7-1
NEU 7-1
FYLA 7-1
SPR
SPR
NEU
DWK P2-1
WLK P2-1
SPR
NEU P2-1
 - 55 RLA 1-2
YLA 1-2
GLA 1-2
NEU 1-2
FYLA 1-2
SPR
 - 56 PB2-1
NEU
 - 57 PB4-2
NEU
 - 58 #4,7 EVP LT
EVP
NEU
DETECTOR
 - 59 #4,7
EVP
DETECTOR
 - 60 INS. GR.
 - 62 SPR
NEU
 - 63 D2-1
 - 64 D2-2
 - 65 #2,5
EVP
DETECTOR
 - 66 D5-1
 - 67 D5-2
 - 68 RED 6-1
YEL 6-1
GRN 6-1
NEU 6-1
RED 6-2
YEL 6-2
GRN 6-2
NEU 6-2
SPR
SPR
SPR
NEU
 - 69 RLA 3-2
YLA 3-2
GLA 3-2
NEU 3-2
FYLA 3-2
SPR
SPR
NEU
RED 6-3
YEL 6-3
GRN 6-3
NEU 6-3
 - 70 RLA 1-1
YLA 1-1
GLA 1-1
NEU 1-1
FYLA 1-1
DWK P6-2
WLK P6-2
NEU P6-2
DWK P4-1
WLK P4-1
SPR
NEU P4-1
 - 71 PB4-1
NEU
 - 72 PB6-2
NEU
 - 73 #6,1 EVP LT
EQG
NEU
 - 74 #6,1
EVP
DETECTOR
 - 75 #2,5
EVP
DETECTOR
 - 76 INS. GR.
 - 78 D4-2
 - 79 D4-3
 - 80 D7-1
 - 81 D7-2
 - 82 D4-1
 - 83 RLA 5-1
YLA 5-1
GLA 5-1
NEU 5-1
FYLA 5-1
SPR
 - 84 RLA 3-1
YLA 3-1
GLA 3-1
NEU 3-1
FYLA 3-1
SPR
 - 91 INTERCONNECT TO
102ND LANE NW

DRAWN BY: LC
DESIGNER: JMG
CHECKED BY: JMG

NO.	BY	DATE	REVISIONS

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Date: April 4, 2016 Name: John M. Gray, PE Lic. No. 22457

SEH
PHONE: (651) 490-2000
3535 VADNAIS CENTER DR.
ST. PAUL, MN 55110

ANOKA COUNTY
CITY OF COON RAPIDS

INPLACE SIGNAL SYSTEM "A"
"FOR INFORMATION ONLY"
CSAH 11 (FOLEY BLVD NW) AT EGRET BLVD NW

FILE NO. 189
ANOKG 132239
SIGNAL SHEET 298
27 OF 29

S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10

LED SIGNAL FACES					
SIGNAL FACE	ALL 12"				
	R	Y	G	Y	G
(**) 2-1, 2-3	○	○	○		
2-2	○	○	○		
4-1	○	○	○	←	←
(**) 4-2, 4-3	○	○	○		
6-1, 6-4	○	○	○		
(**) 6-2, 6-3	○	○	○		
8-1	○	○	○		
(**) 8-2, 8-3	○	○	○		

○ ← = INPLACE LED INDICATION, REUSE INPLACE.
 (**)= OPTICALLY PROGRAMMED FACE.

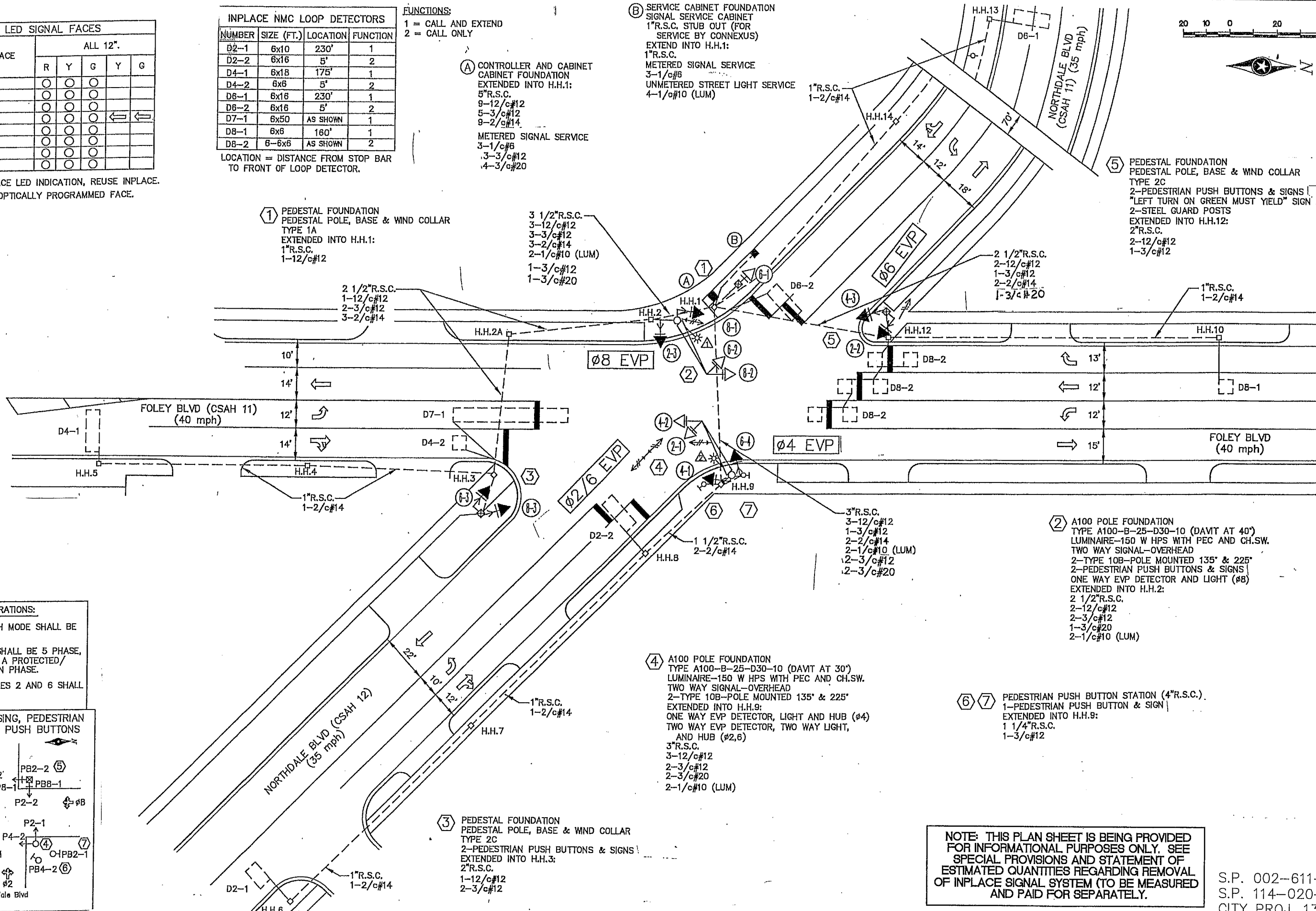
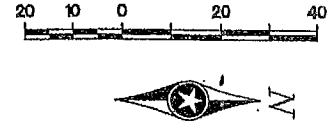
INPLACE NMC LOOP DETECTORS			
NUMBER	SIZE (FT.)	LOCATION	FUNCTION
D2-1	6x10	230'	1
D2-2	6x16	5'	2
D4-1	6x18	175'	1
D4-2	6x6	5'	2
D6-1	6x16	230'	1
D6-2	6x16	5'	2
D7-1	6x50	AS SHOWN	1
D8-1	6x6	160'	1
D8-2	6-6x6	AS SHOWN	2

LOCATION = DISTANCE FROM STOP BAR TO FRONT OF LOOP DETECTOR.

FUNCTIONS:
 1 = CALL AND EXTEND
 2 = CALL ONLY

(A) CONTROLLER AND CABINET CABINET FOUNDATION EXTENDED INTO H.H.1:
 5"R.S.C.
 9-12/c#12
 5-3/c#12
 9-2/c#14
 METERED SIGNAL SERVICE
 3-1/c#6
 3-3/c#12
 4-3/c#20

(B) SERVICE CABINET FOUNDATION SIGNAL SERVICE CABINET 1"R.S.C. STUB OUT (FOR SERVICE BY CONNEXUS) EXTEND INTO H.H.1:
 1"R.S.C.
 METERED SIGNAL SERVICE
 3-1/c#6
 UNMETERED STREET LIGHT SERVICE
 4-1/c#10 (LUM)



(1) PEDESTAL FOUNDATION PEDESTAL POLE, BASE & WIND COLLAR TYPE 1A EXTENDED INTO H.H.1:
 1"R.S.C.
 1-12/c#12

3 1/2"R.S.C.
 3-12/c#12
 3-3/c#12
 3-2/c#14
 2-1/c#10 (LUM)
 1-3/c#12
 1-3/c#20

(5) PEDESTAL FOUNDATION PEDESTAL POLE, BASE & WIND COLLAR TYPE 2C
 2-PEDESTRIAN PUSH BUTTONS & SIGNS
 "LEFT TURN ON GREEN MUST YIELD" SIGN PANEL
 2-STEEL GUARD POSTS EXTENDED INTO H.H.12:
 2"R.S.C.
 2-12/c#12
 1-3/c#12

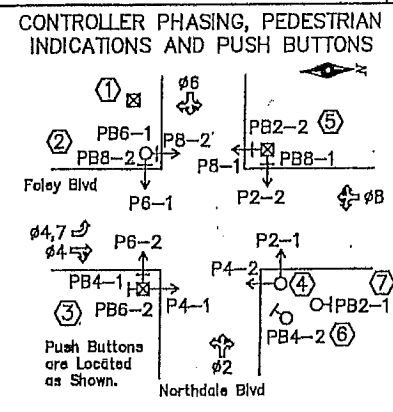
(2) A100 POLE FOUNDATION TYPE A100-B-25-D30-10 (DAVIT AT 40')
 LUMINAIRE-150 W HPS WITH PEC AND CH.SW.
 TWO WAY SIGNAL-OVERHEAD
 2-TYPE 10B-POLE MOUNTED 135' & 225'
 2-PEDESTRIAN PUSH BUTTONS & SIGNS
 ONE WAY EVP DETECTOR AND LIGHT (ø8)
 EXTENDED INTO H.H.2:
 2 1/2"R.S.C.
 2-12/c#12
 2-3/c#12
 1-3/c#20
 2-1/c#10 (LUM)

(4) A100 POLE FOUNDATION TYPE A100-B-25-D30-10 (DAVIT AT 30')
 LUMINAIRE-150 W HPS WITH PEC AND CH.SW.
 TWO WAY SIGNAL-OVERHEAD
 2-TYPE 10B-POLE MOUNTED 135' & 225'
 EXTENDED INTO H.H.9:
 ONE WAY EVP DETECTOR, LIGHT AND HUB (ø4)
 TWO WAY EVP DETECTOR, TWO WAY LIGHT, AND HUB (ø2,6)
 3"R.S.C.
 3-12/c#12
 2-3/c#12
 2-3/c#20
 2-1/c#10 (LUM)

(6) (7) PEDESTRIAN PUSH BUTTON STATION (4"R.S.C.)
 1-PEDESTRIAN PUSH BUTTON & SIGN
 EXTENDED INTO H.H.9:
 1 1/4"R.S.C.
 1-3/c#12

(3) PEDESTAL FOUNDATION PEDESTAL POLE, BASE & WIND COLLAR TYPE 2C
 2-PEDESTRIAN PUSH BUTTONS & SIGNS
 EXTENDED INTO H.H.3:
 2"R.S.C.
 1-12/c#12
 2-3/c#12

SIGNAL SYSTEM OPERATIONS:
 - SIGNAL SYSTEM FLASH MODE SHALL BE ALL RED.
 - NORMAL OPERATION SHALL BE 5 PHASE, WITH PHASE 7 BEING A PROTECTED/PERMISSIVE LEFT TURN PHASE.
 - VEHICLE SIGNAL PHASES 2 AND 6 SHALL OPERATE ON RECALL.



NOTE: THIS PLAN SHEET IS BEING PROVIDED FOR INFORMATIONAL PURPOSES ONLY. SEE SPECIAL PROVISIONS AND STATEMENT OF ESTIMATED QUANTITIES REGARDING REMOVAL OF INPLACE SIGNAL SYSTEM (TO BE MEASURED AND PAID FOR SEPARATELY).

S.P. 002-611-034
 S.P. 114-020-050
 CITY PROJ. 13-10

DRAWN BY: LC
 DESIGNER: JMG
 CHECKED BY: JMG

NO.	BY	DATE	REVISIONS

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 Date: April 4, 2016
 Name: John M. Gray, PE
 Ltc. No. 22457

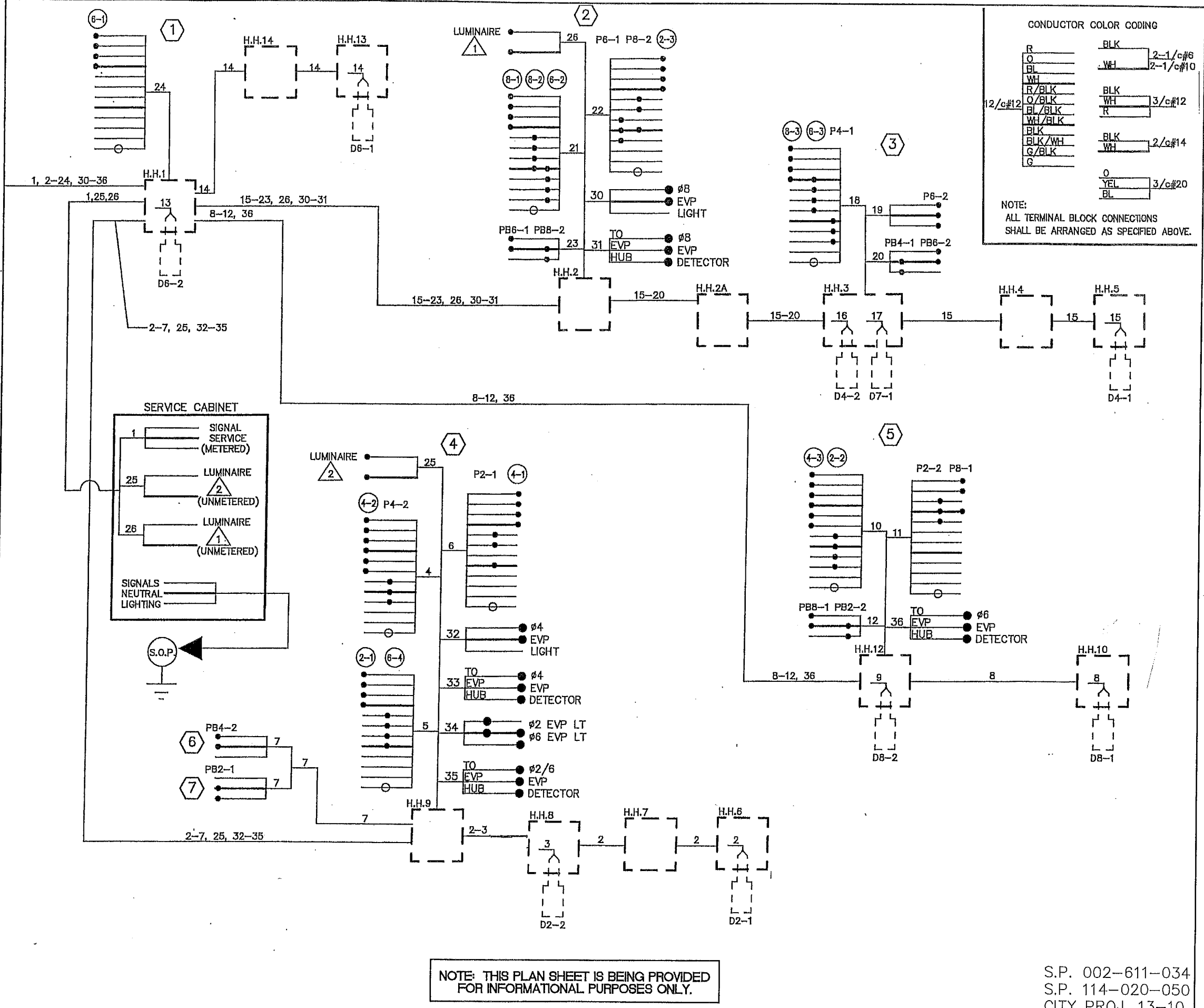
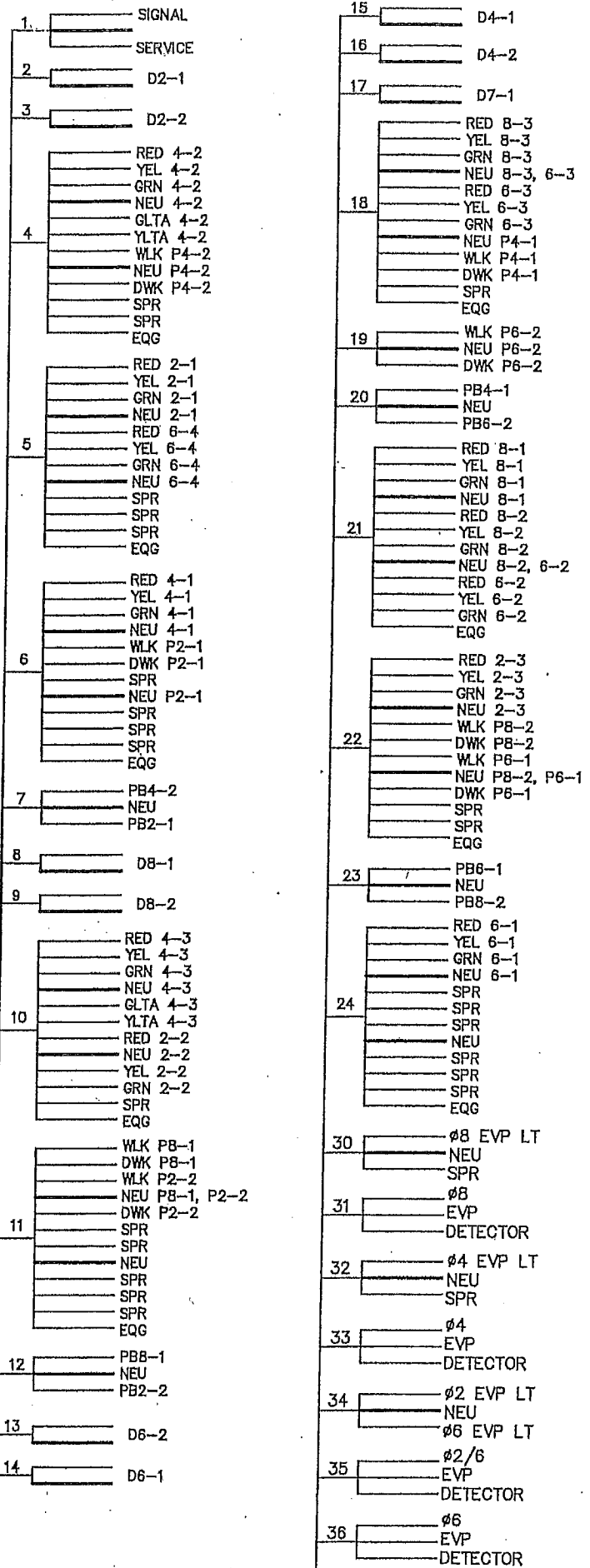
SEH
 PHONE: (651) 490-2000
 3535 VADNAIS CENTER DR.
 ST. PAUL, MN 55110

ANOKA COUNTY
 CITY OF COON RAPIDS

INPLACE SIGNAL SYSTEM "B"
 "FOR INFORMATION ONLY"
 CSAH 11 (FOLEY BLVD NW) AT
 CSAH 11/12 (NORTHDALE BLVD NW)

FILE NO. ANOKC 132239	190
SIGNAL SHEET 2B OF 29	298

CONTROLLER CABINET



CONDUCTOR COLOR CODING

R	BLK	2-1/c#6
O	WH	2-1/c#10
BL	BLK	3/c#12
WH	WH	2/c#14
R/BLK	BLK	3/c#12
O/BLK	WH	2/c#14
BL/BLK	R	
WH/BLK	BLK	2/c#14
BLK/WH	WH	2/c#14
G/BLK		
	O	
	YEL	3/c#20
	BL	

NOTE:
ALL TERMINAL BLOCK CONNECTIONS
SHALL BE ARRANGED AS SPECIFIED ABOVE.

NOTE: THIS PLAN SHEET IS BEING PROVIDED FOR INFORMATIONAL PURPOSES ONLY.

S.P. 002-611-034
S.P. 114-020-050
CITY PROJ. 13-10

DRAWN BY: LC
DESIGNER: JMG
CHECKED BY: JMG

NO.	BY	DATE	REVISIONS

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John M. Gray
Name: John M. Gray, PE
Lic. No. 22457
Date: April 4, 2010

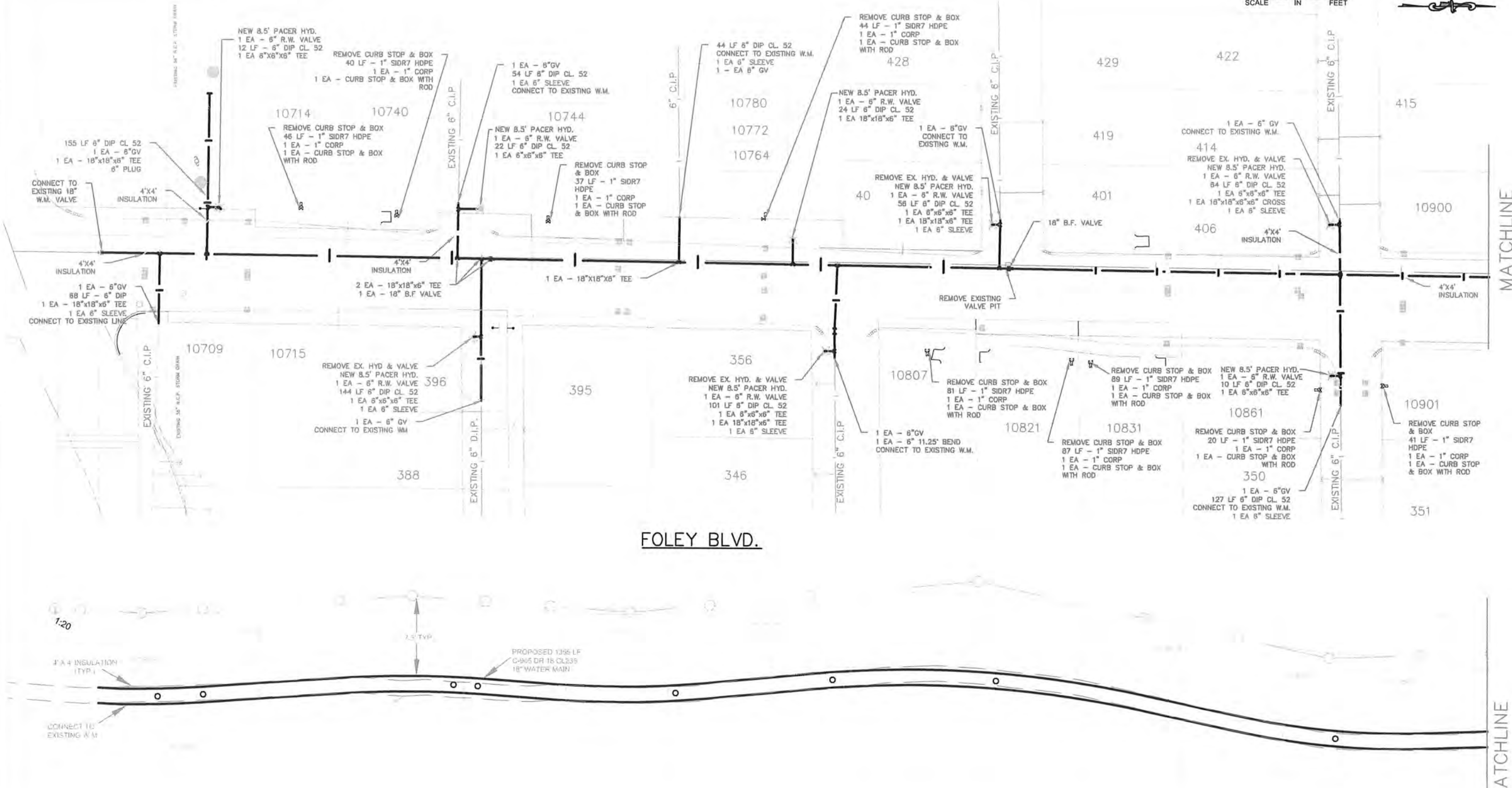
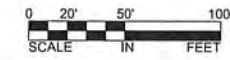
PHONE: (851) 490-2000
3535 VADNAIS CENTER DR.
ST. PAUL, MN 55110

ANOKA COUNTY
CITY OF COON RAPIDS

INPLACE SIGNAL SYSTEM 'B'
'FOR INFORMATION ONLY'
CSAH 11 (FOLEY BLVD NW) AT
CSAH 11/12 (NORTHDALE BLVD NW)

FILE NO. ANOKC 132239
SIGNAL SHEET 29 OF 29

191
298



FOLEY BLVD.

MATCHLINE

MATCHLINE

1. CONTRACTOR TO VERIFY DEPTH OF EXISTING WATER MAIN
2. DISCONNECT EXISTING SERVICE, CUT & RECONNECT TO NEW W.M. (TYP.)
3. INSTALL NEW CURB STOPS AT R/W (TYP.)
4. ALL VALVES SHALL INCLUDE 1.5\"/>

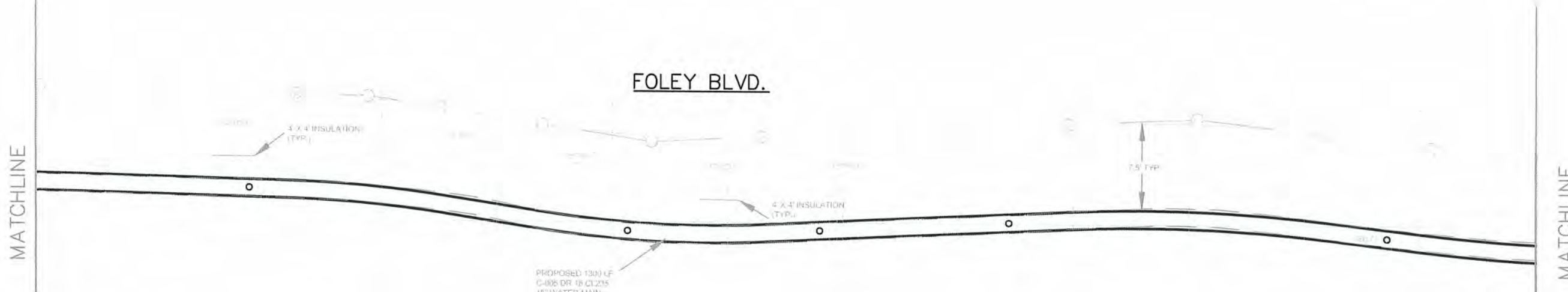
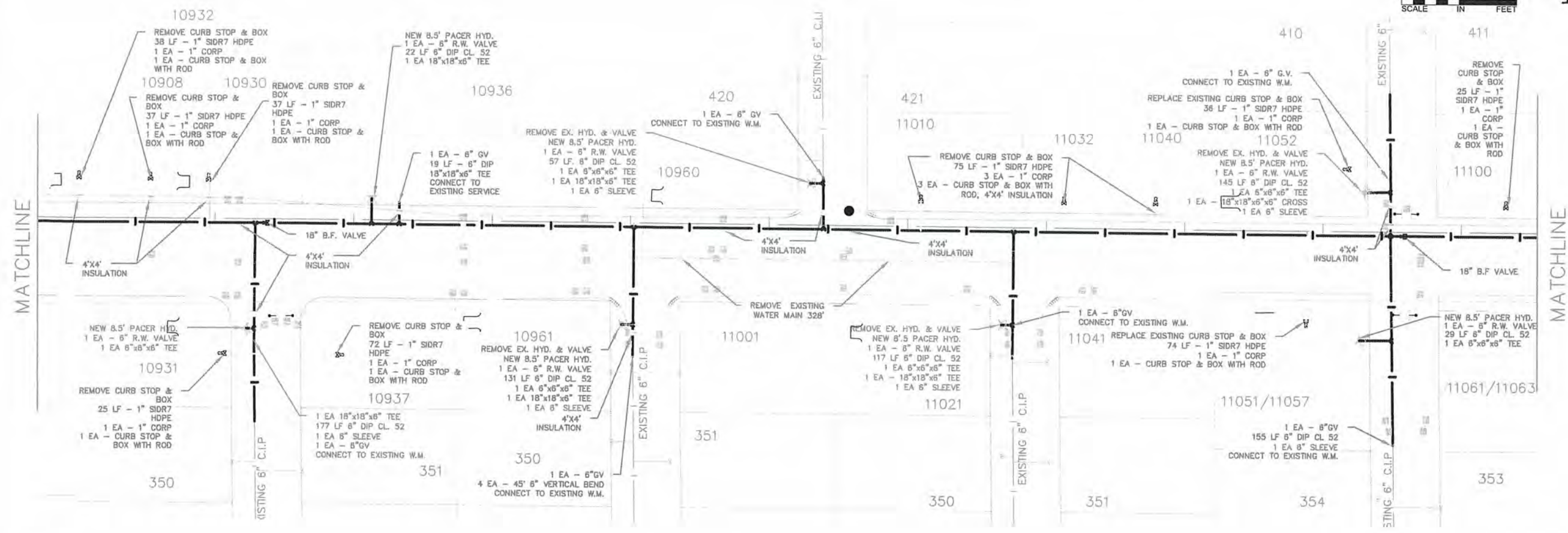
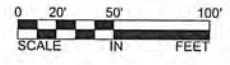
I hereby certify that this plan specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the State of Minnesota.
 Date: 4/27/2016 MN Reg. No.: 43920

INITIAL	
DESIGNED BY:	RT
DRAWN BY:	RT
ORIGINAL DATE:	Apr 27, 2016
SCALE	HORIZ. 1:100 VERT. 1:10

PROJECT 13-10
 FOLEY BOULEVARD



SHEET
192
 OF
298



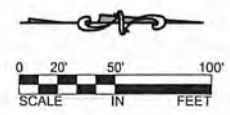
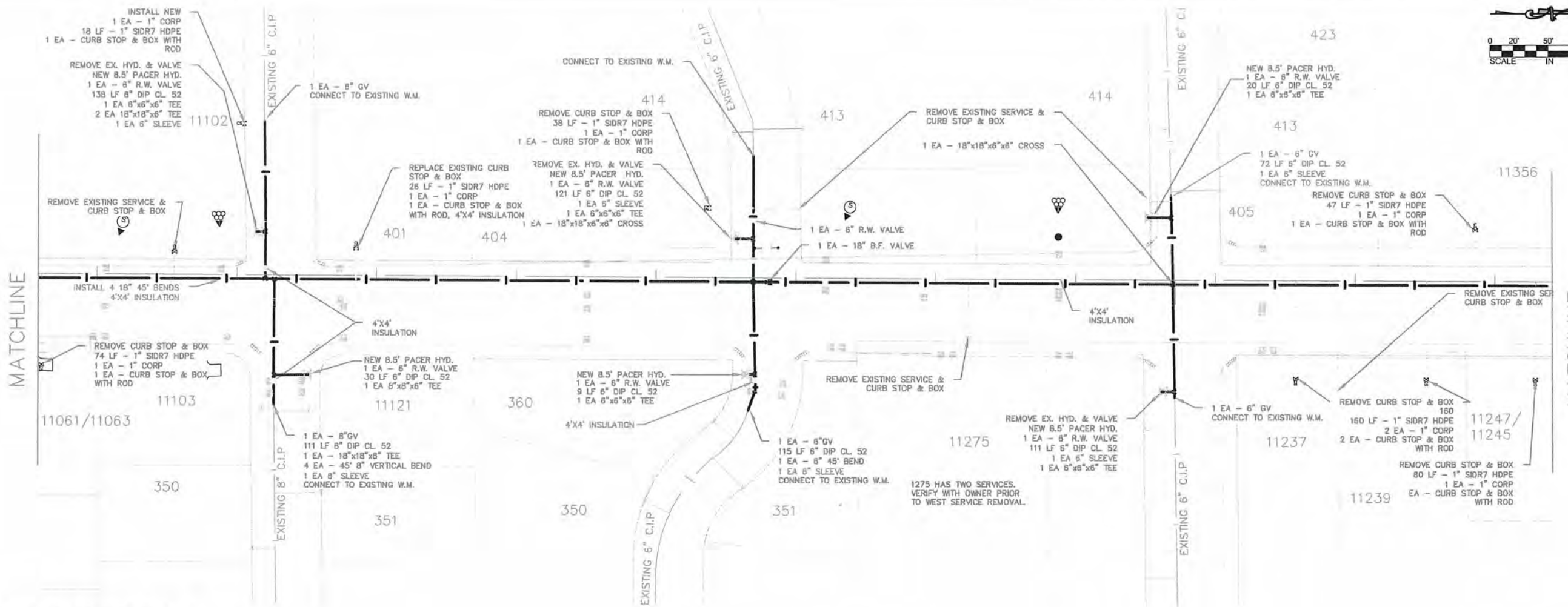
I hereby certify that this plan specification or report was prepared by my direct supervision and that I am a duly Registered Professional Engineer under the State of Minnesota.
 Date: 4/27/2016 PLAN Reg. No.: 43920

INITIAL	REVISIONS	RT	RT	ORIGINAL DATE	SCALE	HORIZ.	VERT.
				Apr 27, 2016	1:100	1:10	

PROJECT 13-10
FOLEY BOULEVARD



- CONTRACTOR TO VERIFY DEPTH OF EXISTING WATER MAIN
- DISCONNECT EXISTING SERVICE, CUT & RECONNECT TO NEW W.M. (TYP.)
- INSTALL NEW CURB STOPS AT R/W (TYP.)
- ALL VALVES SHALL INCLUDE 1.5\"/>



I hereby certify that this plan represents an original design prepared by me or under my direct supervision and that I am a duly registered Professional Engineer under the State of Minnesota.
 Date: 4/27/2016 MN Reg. No.: 43920

INITIAL	
REVISIONS	

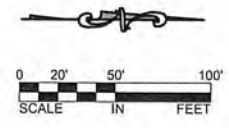
DESIGNED BY:	RT
DRAWN BY:	RT
ORIGINAL DATE:	Apr 27, 2016
SCALE	HORIZ. 1:100 VERT. 1:10

1. CONTRACTOR TO VERIFY DEPTH OF EXISTING WATER MAIN
2. DISCONNECT EXISTING SERVICE, CUT & RECONNECT TO NEW W.M. (TYP.)
3. INSTALL NEW CURB STOPS AT R/W (TYP.)
4. ALL VALVES SHALL INCLUDE 1.5' OPERATING NUT EXTENDERS
5. INSULATE W.M. 4"x4' - 4" THICK POLY STYRENE UNDER STORM SEWER CROSSINGS
6. EXISTING SERVICE PIPE SHALL BE CRIMPED AT THE ENDS & REMAIN IN PLACE AFTER DISCONNECTION
7. CONTRACTOR SHALL REMOVE EXISTING MAINLINE GATE VALVES & REMOVE VALVE BOX
8. CONTRACTOR SHALL PLACE 8 GA. SOLID COPPER TRACER WIRE ALONG W.M., COPPER STRAPS SHALL BE USED FOR CONNECTION TO PIPE

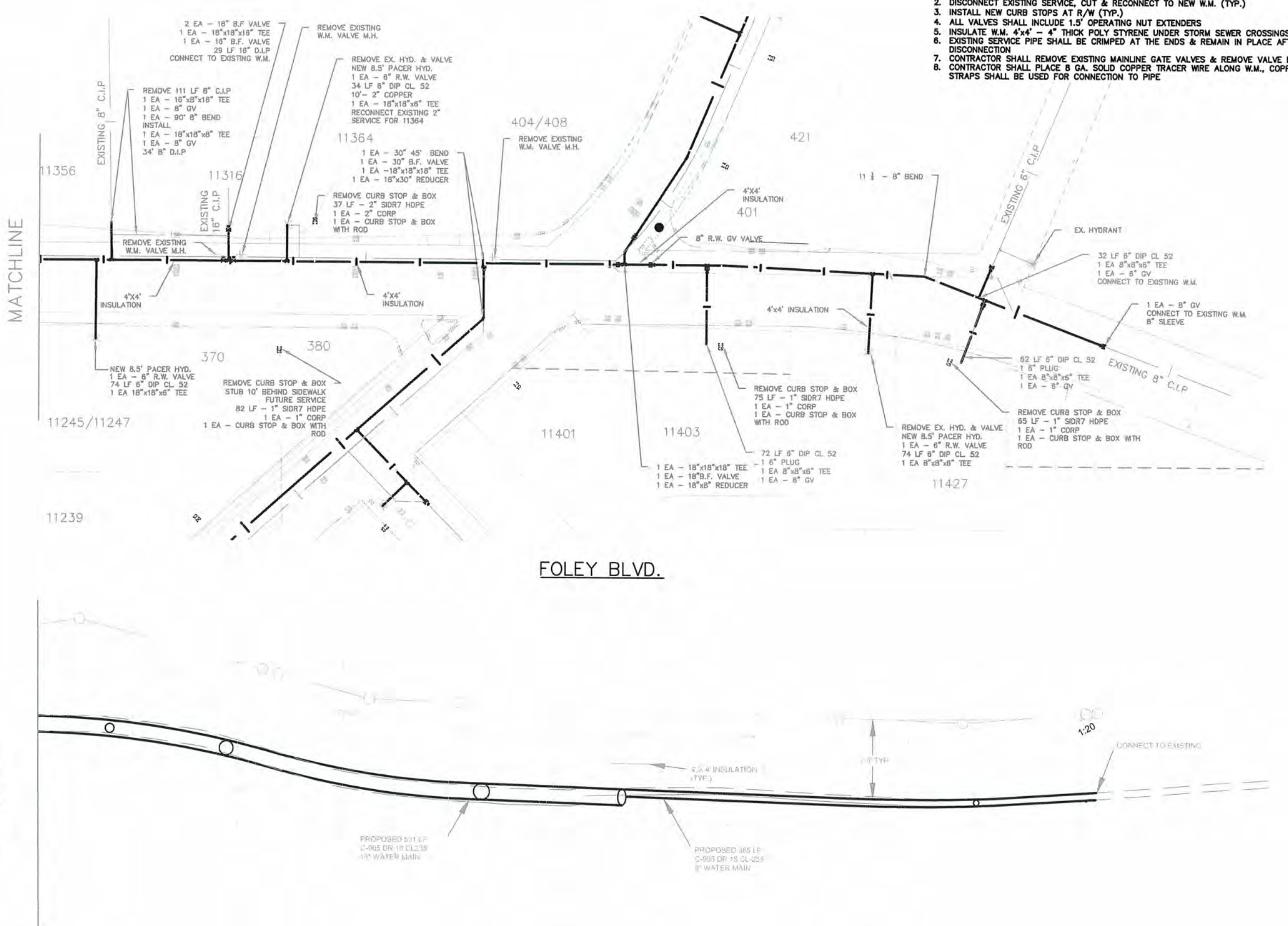
PROJECT 13-10
 FOLEY BOULEVARD

COON RAPIDS
 MINNESOTA
 City of Coon Rapids
 11155 Robinson Dr.
 Coon Rapids, MN 55433-2161
 952-332-2800 Fax 952-767-6491

1. CONTRACTOR TO VERIFY DEPTH OF EXISTING WATER MAIN
2. DISCONNECT EXISTING SERVICE, CUT & RECONNECT TO NEW W.M. (TYP.)
3. INSTALL NEW CURB STOPS AT R/W (TYP.)
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7. CONTRACTOR SHALL REMOVE EXISTING MAINLINE GATE VALVES & REMOVE VALVE BOX
8. CONTRACTOR SHALL PLACE 8 GA. SOLID COPPER TRACER WIRE ALONG W.M., COPPER STRAPS SHALL BE USED FOR CONNECTION TO PIPE



I hereby certify that this plan specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the State of Minnesota.
 Date: 4/27/2016 Reg. No. 43920



INITIAL	REVISIONS	DESIGNED BY:	RT
		DRAWN BY: <td>RT</td>	RT
		ORIGINAL DATE: <td>Apr 27, 2016</td>	Apr 27, 2016
		SCALE <td>HORIZ. 1:100 VERT. 1:10</td>	HORIZ. 1:100 VERT. 1:10

PROJECT 13-10
 FOLEY BOULEVARD





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Date: 4/27/2016 MN Reg. No.: 43920

REMOVE EXISTING VALVE PITS (2)
29 LF 18" WATER MAIN
1 EA - 18" BF
1 EA - 18"x18"x18" TEE
CONNECT TO EXISTING W.M.

33 LF - 1" SDR7 HDPE
1 EA - 1" CORP
1 EA - CURB STOP & BOX
WITH ROD

35 LF - 1" SDR7 HDPE
1 EA - 1" CORP
1 EA - CURB STOP & BOX
WITH ROD

NEW B.5' PACER HYD.
1 EA - 6" R.W. VALVE
86 LF 6" DIP CL. S2
1 EA 18"x18"x6" TEE

REMOVE CURB STOP & BOX
35 LF - 1" SDR7 HDPE
1 EA - 1" CORP
1 EA - CURB STOP & BOX
WITH ROD

REMOVE CURB STOP & BOX
75 LF - 1" SDR7 HDPE
1 EA - 1" CORP
1 EA - CURB STOP & BOX
WITH ROD

72 LF 6" DIP CL. S2
1 EA - 6" R.W. VALVE
1 EA - 18" B.F. VALVE
1 EA - 18"x18" REDUCER
1 EA - 18"x6"

1 EA - 18" B.F. VALVE
2 EA - 18" - 22.5" BENDS
CONNECT TO EXISTING W.M.

REMOVE CURB STOP & BOX
64 LF - 1" SDR7 HDPE
1 EA - 1" CORP
1 EA - CURB STOP & BOX
WITH ROD

NEW B.5' PACER HYD.
1 EA - 6" R.W. VALVE
57 LF 6" DIP CL. S2
1 EA 18"x18"x6" TEE

18" B.F. VALVE
18" 45" BEND

1 EA - 18" 11" BEND

EXISTING WATER SERVICE
FED TO BACK OF BUILDING.

1. CONTRACTOR TO VERIFY DEPTH OF EXISTING WATER MAIN
2. DISCONNECT EXISTING SERVICE, CUT & RECONNECT TO NEW W.M. (TYP.)
3. INSTALL NEW CURB STOPS AT R/W (TYP.)
4. ALL VALVES SHALL INCLUDE 1.5" OPERATING NUT EXTENDERS
5. INSULATE W.M. 4"x4" - 4" THICK POLY STYRENE UNDER STORM SEWER CROSSINGS
6. EXISTING SERVICE PIPE SHALL BE CRIMPED AT THE ENDS & REMAIN IN PLACE AFTER DISCONNECTION
7. CONTRACTOR SHALL REMOVE EXISTING MAINLINE GATE VALVES & REMOVE VALVE BOX
8. CONTRACTOR SHALL PLACE 8 GA. SOLID COPPER TRACER WIRE ALONG W.M., COPPER STRAPS SHALL BE USED FOR CONNECTION TO PIPE

INITIAL

DESIGNED BY:	RT
DRAWN BY:	RT
ORIGINAL DATE:	Apr 27, 2016

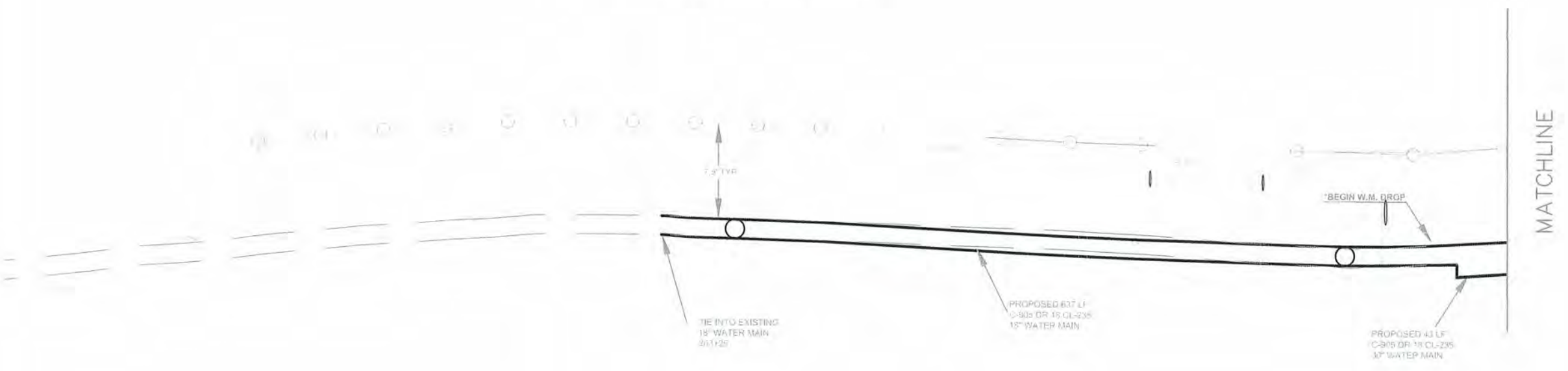
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	1:100	1:10

PROJECT 13-10
FOLEY BOULEVARD

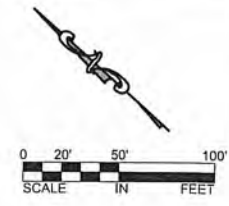
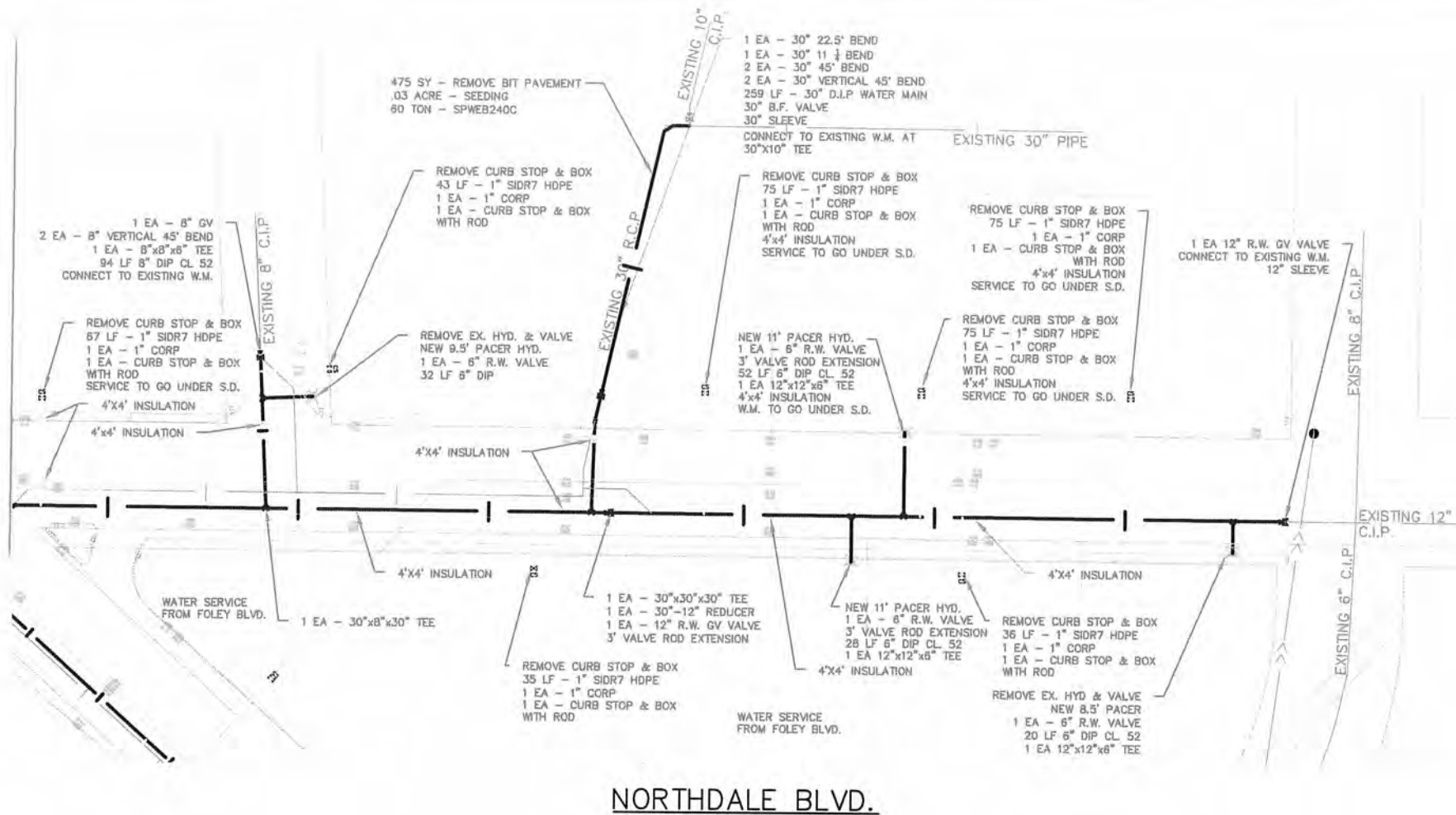
COON RAPIDS Minnesota
City of Coon Rapids
11155 Robinson Dr
Coon Rapids, MN 55433-3761
762-25-5881 Fax 762-25-5651

NORTHDAL BLVD.

MATCHLINE



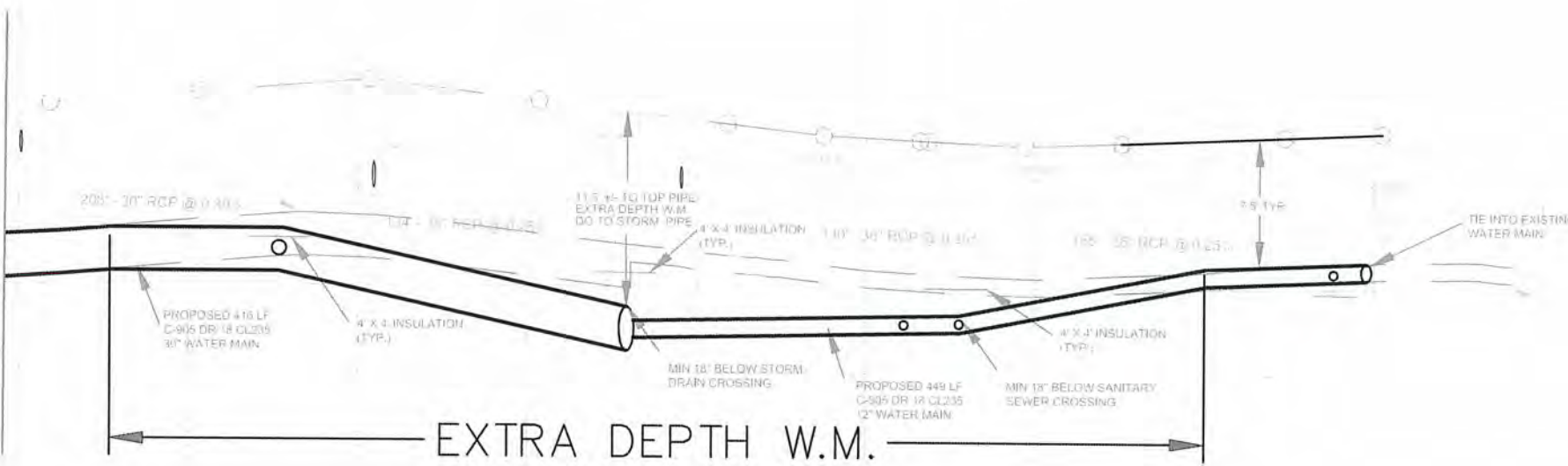
MATCHLINE



NOTE: 295 NORTHDAL BLVD. HAS 6" WATER SERVICE FROM THE REAR OF THE BUILDING AS WELL.

NORTHDAL BLVD.

MATCHLINE



EXTRA DEPTH W.M.

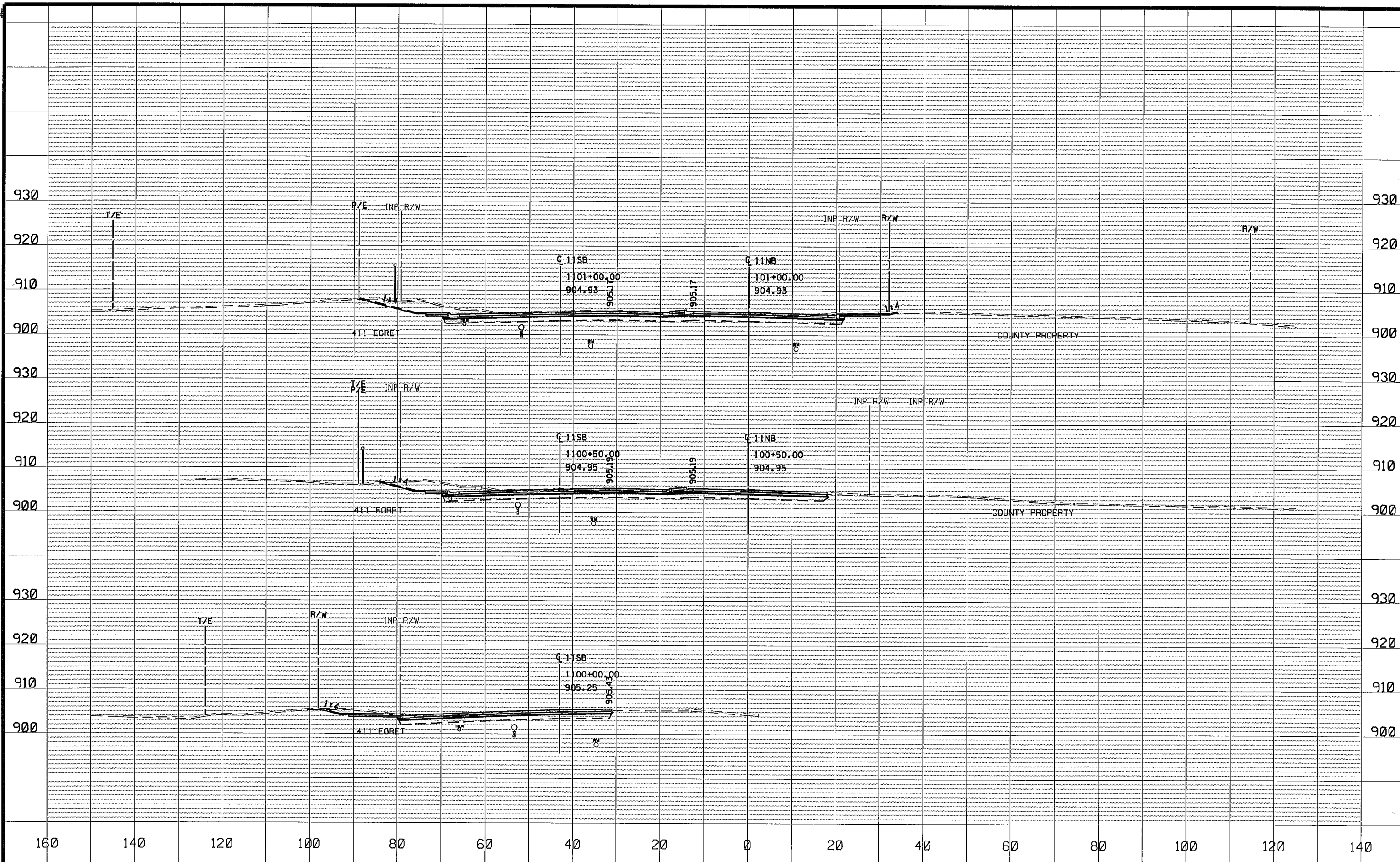
1. CONTRACTOR TO VERIFY DEPTH OF EXISTING WATER MAIN
2. DISCONNECT EXISTING SERVICE, CUT & RECONNECT TO NEW W.M. (TYP.)
3. INSTALL NEW CURB STOPS AT R/W (TYP.)
4. ALL VALVES SHALL INCLUDE 1.5" OPERATING NUT EXTENDERS
5. INSULATE W.M. 4"x4" - 4" THICK POLY STYRENE UNDER STORM SEWER CROSSINGS
6. EXISTING SERVICE PIPE SHALL BE CRIMPED AT THE ENDS & REMAIN IN PLACE AFTER DISCONNECTION
7. CONTRACTOR SHALL REMOVE EXISTING MAINLINE GATE VALVES & REMOVE VALVE BOX
8. CONTRACTOR SHALL PLACE 8 GA. SOLID COPPER TRACER WIRE ALONG W.M., COPPER STRAPS SHALL BE USED FOR CONNECTION TO PIPE

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly licensed Professional Engineer under the State of Minnesota.
 Date: 5/11/2016 Min. Reg. No.: 43920

INITIAL	
REVISIONS	
DESIGNED BY:	RT
DRAWN BY:	RT
ORIGINAL DATE:	May 11, 2016
SCALE	HORIZ. 1:100 VERT. 1:10

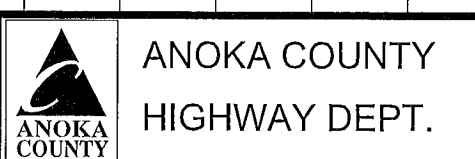
PROJECT 13-10
FOLEY BOULEVARD





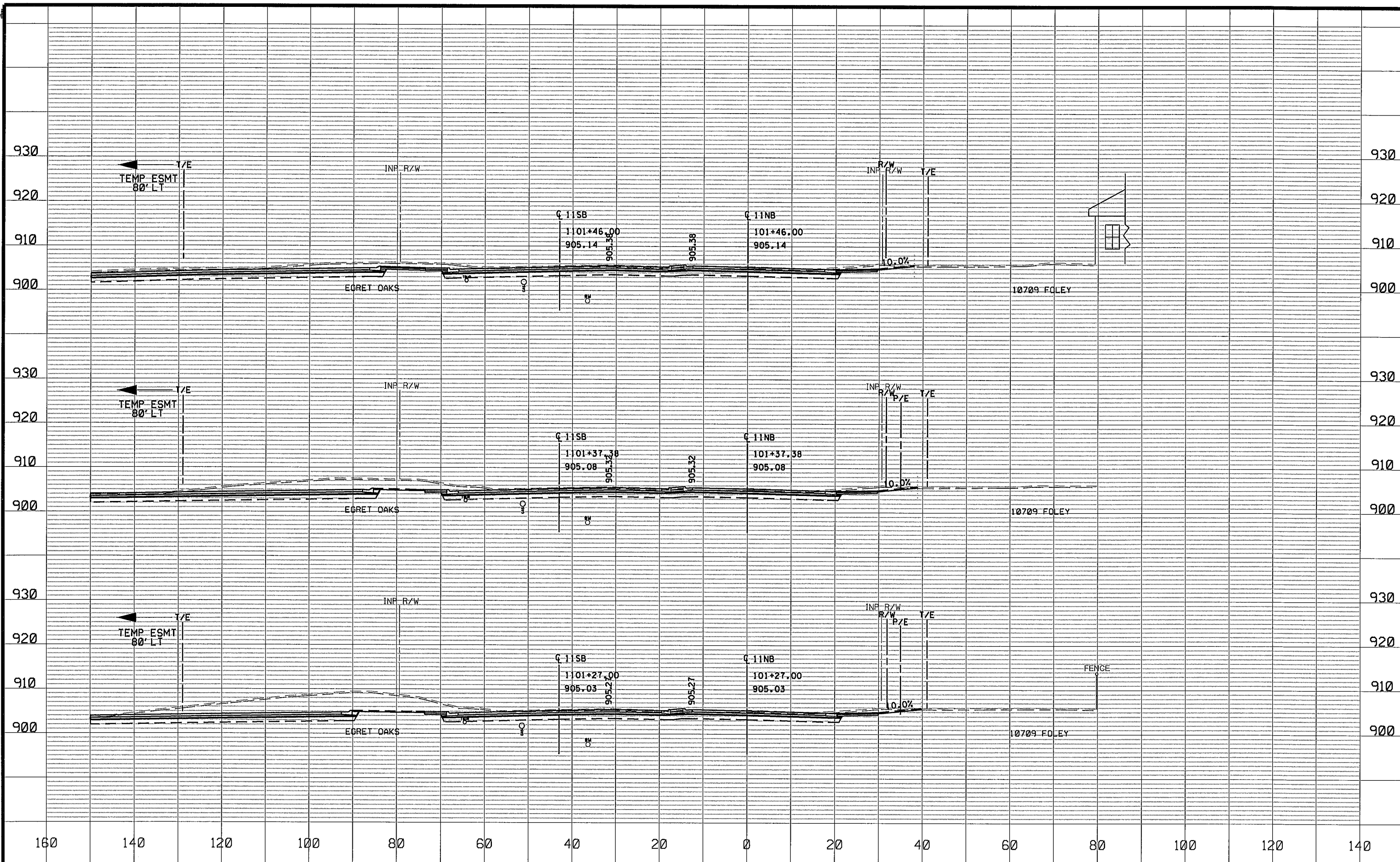
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 DESIGN BY DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



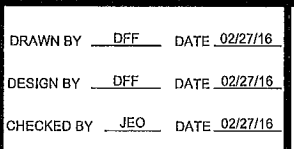
SP 002-611-034
 SP 114-020-050
 CP 13-10

CROSS SECTIONS
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 Sheet 198 of 298 Sheets



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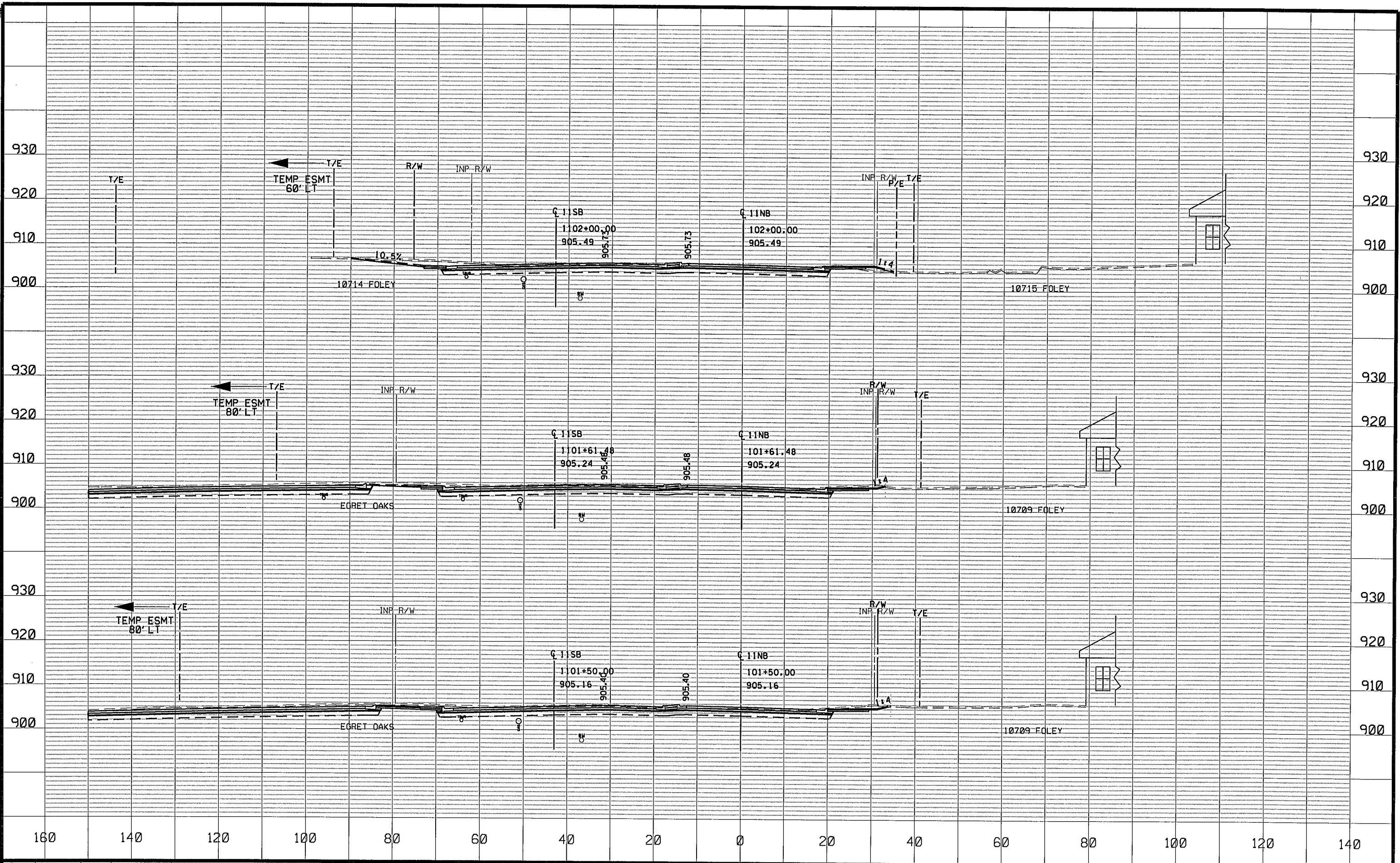
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DESIGN BY	DFP	DATE	02/27/16
CHECKED BY	JEO	DATE	02/27/16



ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
SP 114-020-050
CP 13-10

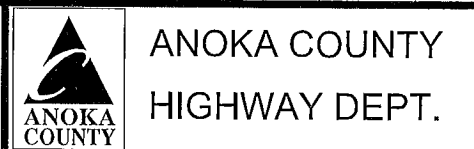
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Sheet 199 of 298 Sheets



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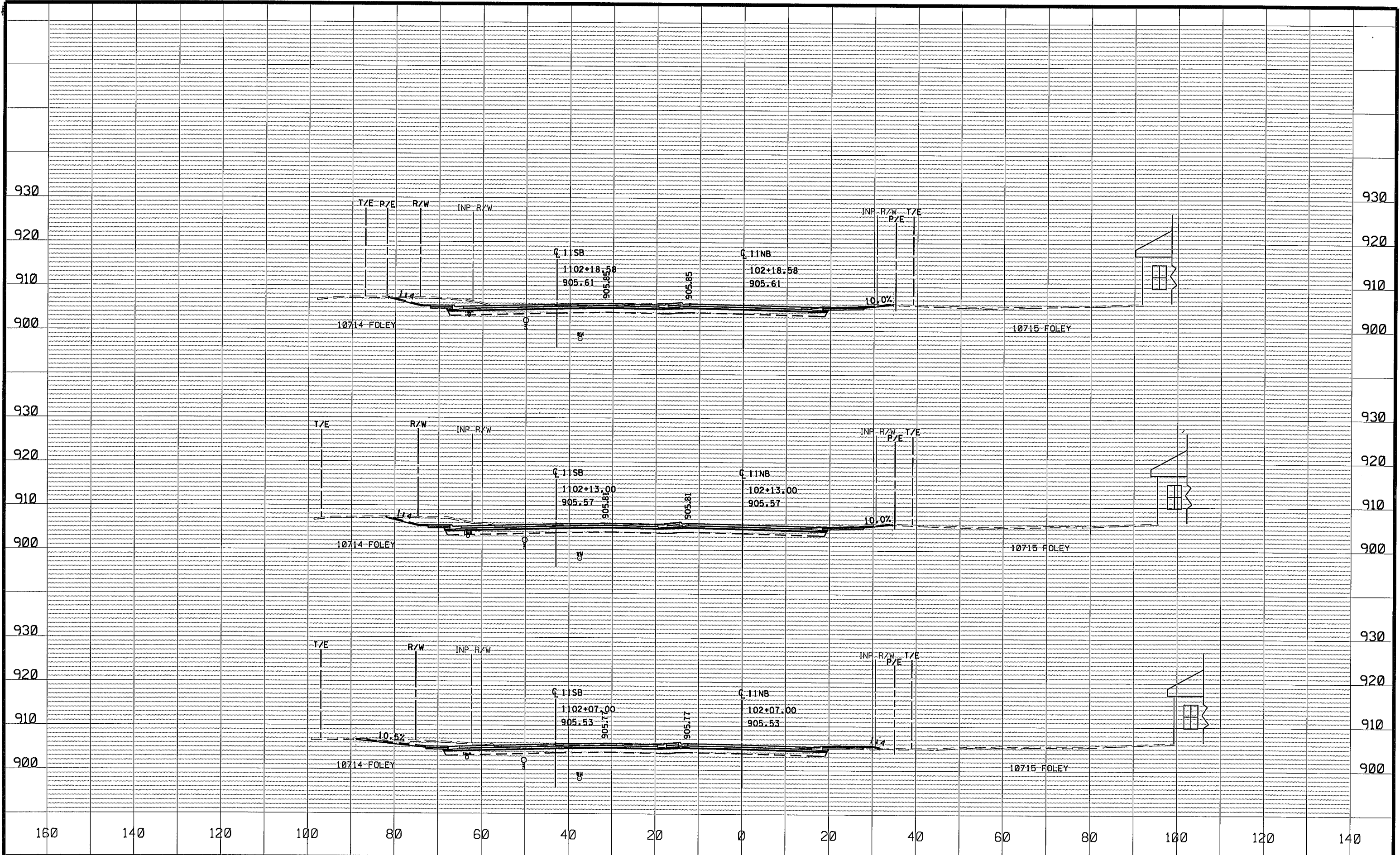
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 DESIGN BY DF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



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CROSS SECTIONS
 STA 101+50.00 TO 102+00.00
 Sheet 200 of 298 Sheets



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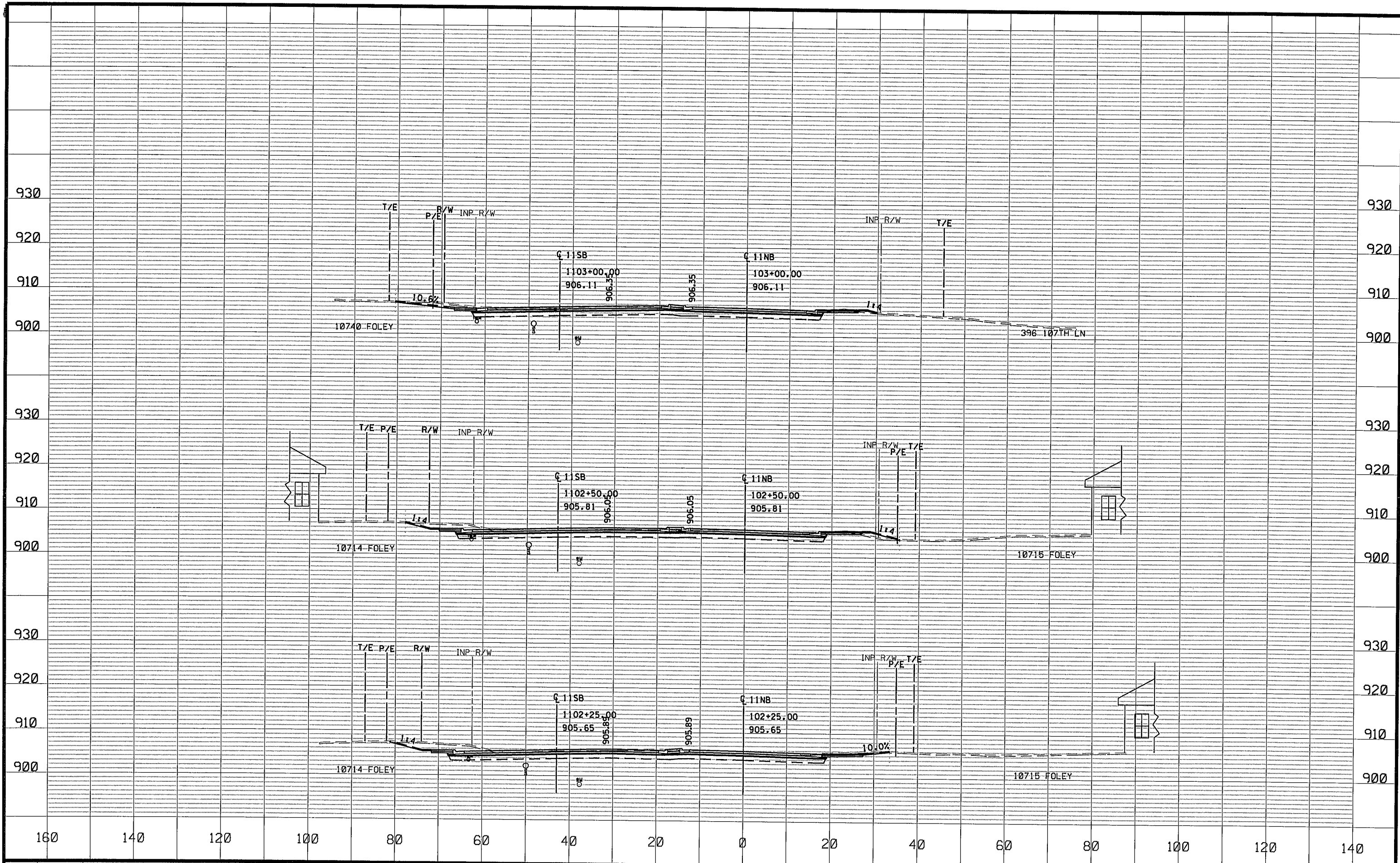
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 DESIGN BY DFP DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



ANOKA COUNTY
HIGHWAY DEPT.

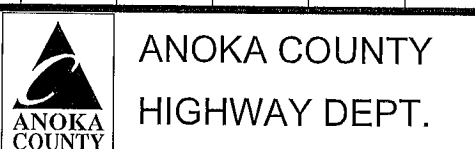
SP 002-611-034
 SP 114-020-050
 CP 13-10

CROSS SECTIONS
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 Sheet 201 of 298 Sheets



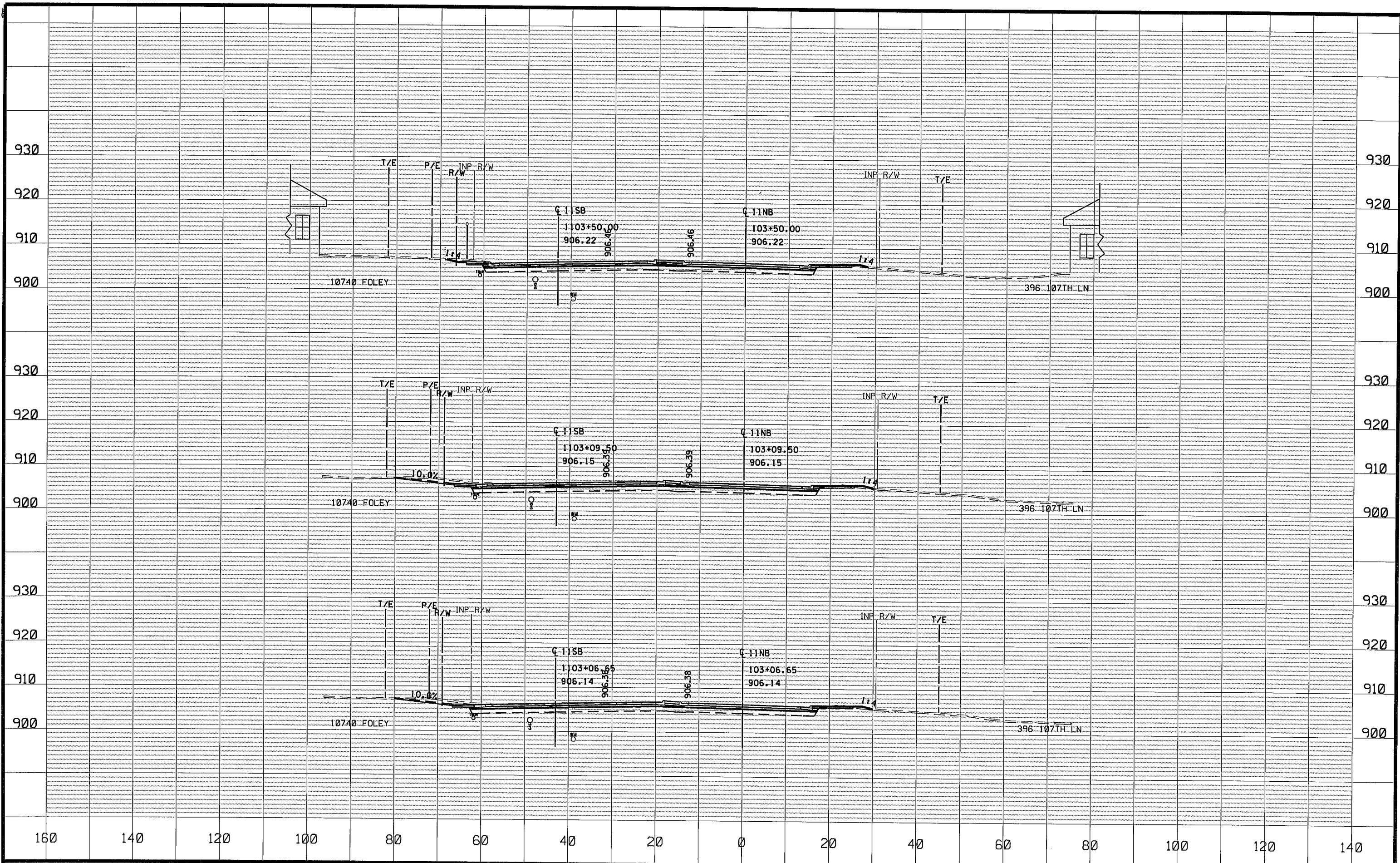
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 DESIGN BY DFF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



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 CP 13-10

CROSS SECTIONS
 STA 102+25.00 TO 103+00.00
 Sheet 202 of 298 Sheets



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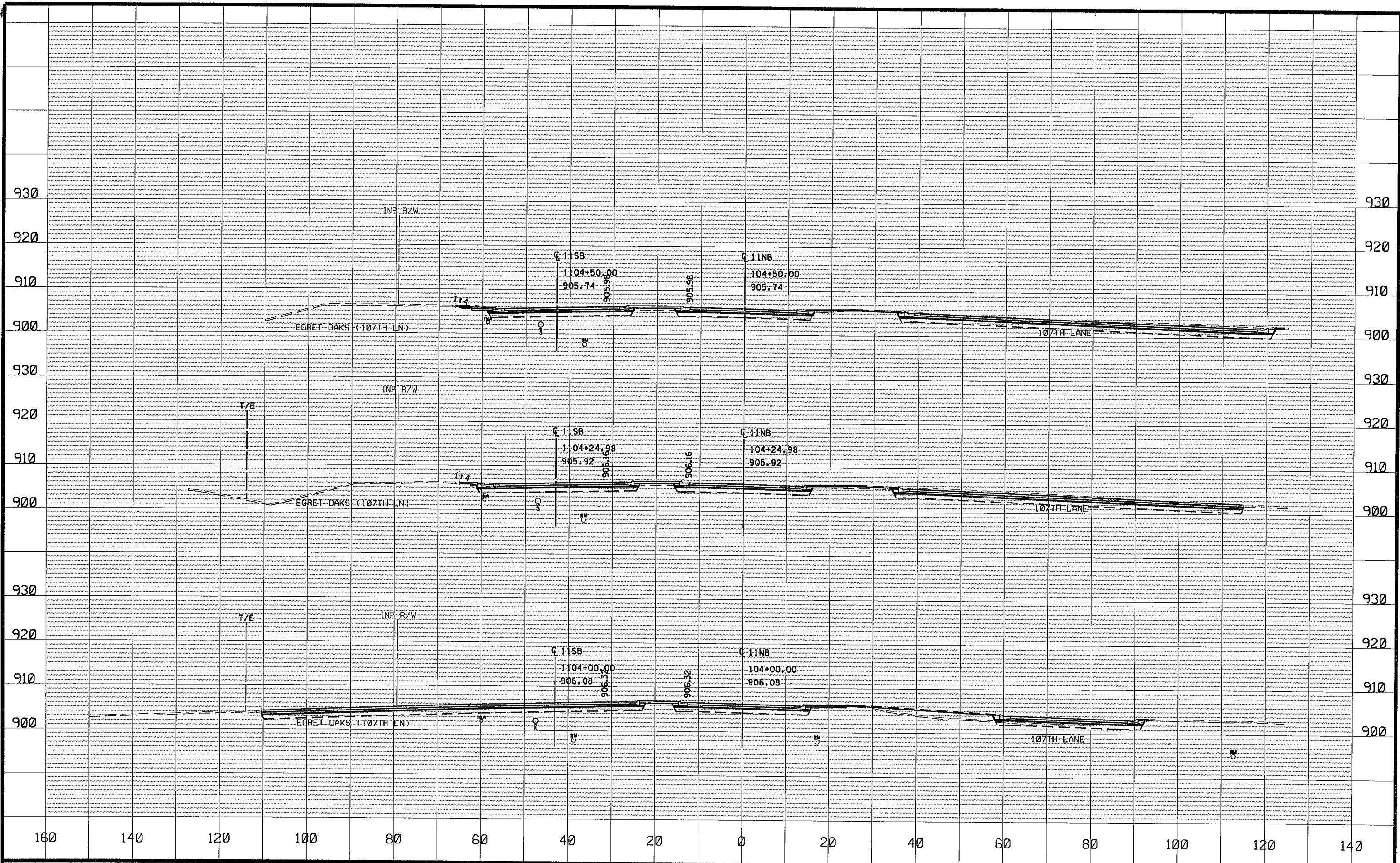
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 DESIGN BY DFF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
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CROSS SECTIONS
 STA 103+06.65 TO 103+50.00
 Sheet 203 of 298 Sheets



NO	DATE	BY	CKD	APPR	REVISION

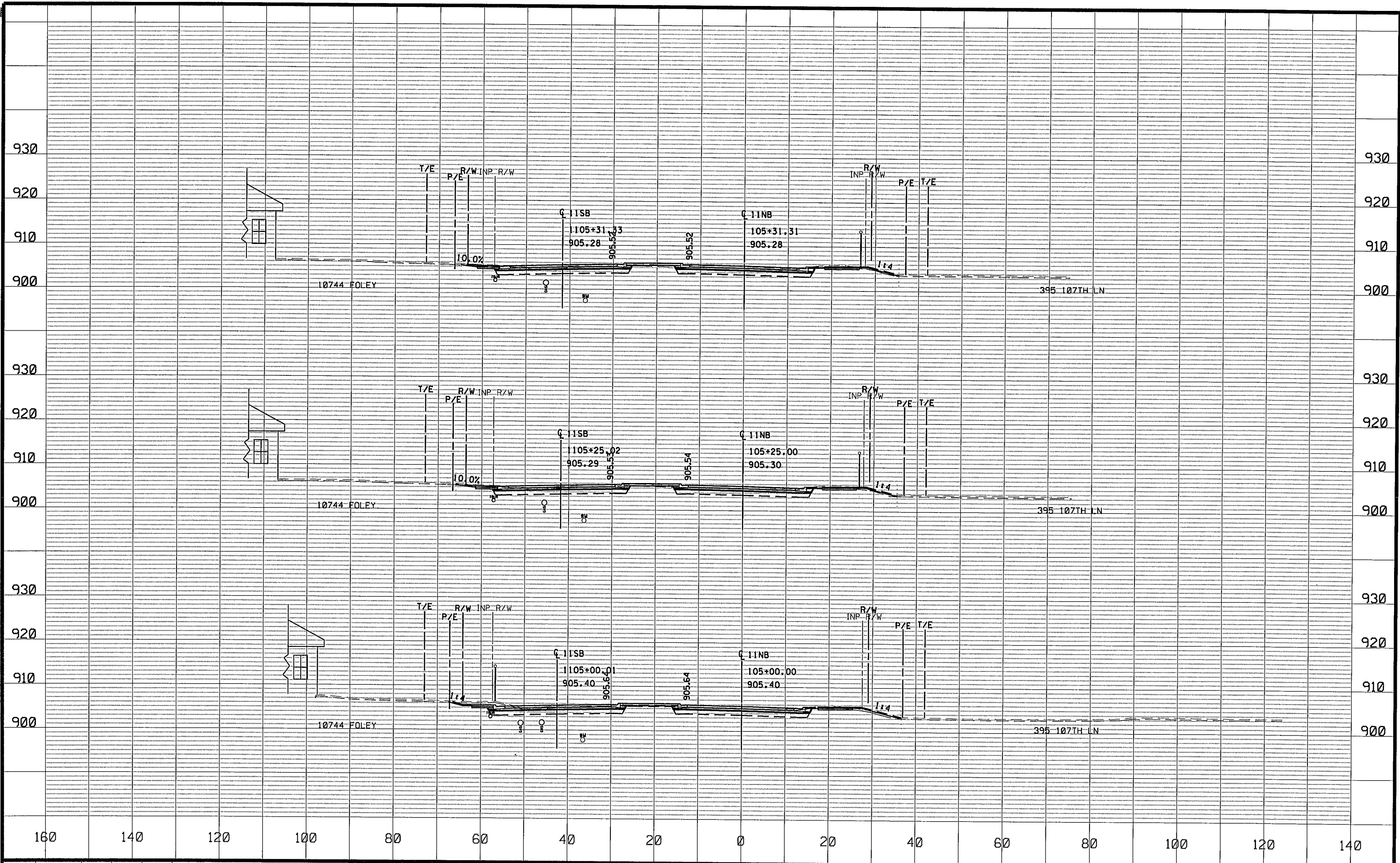
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DESIGN BY	DFD	DATE	02/27/16
CHECKED BY	JEO	DATE	02/27/16

ANOKA COUNTY
HIGHWAY DEPT.

SP 002-611-034
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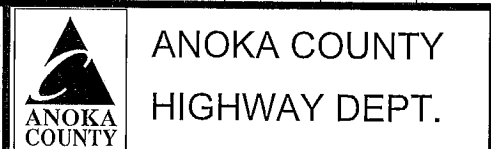
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Sheet 204 of 298 Sheets



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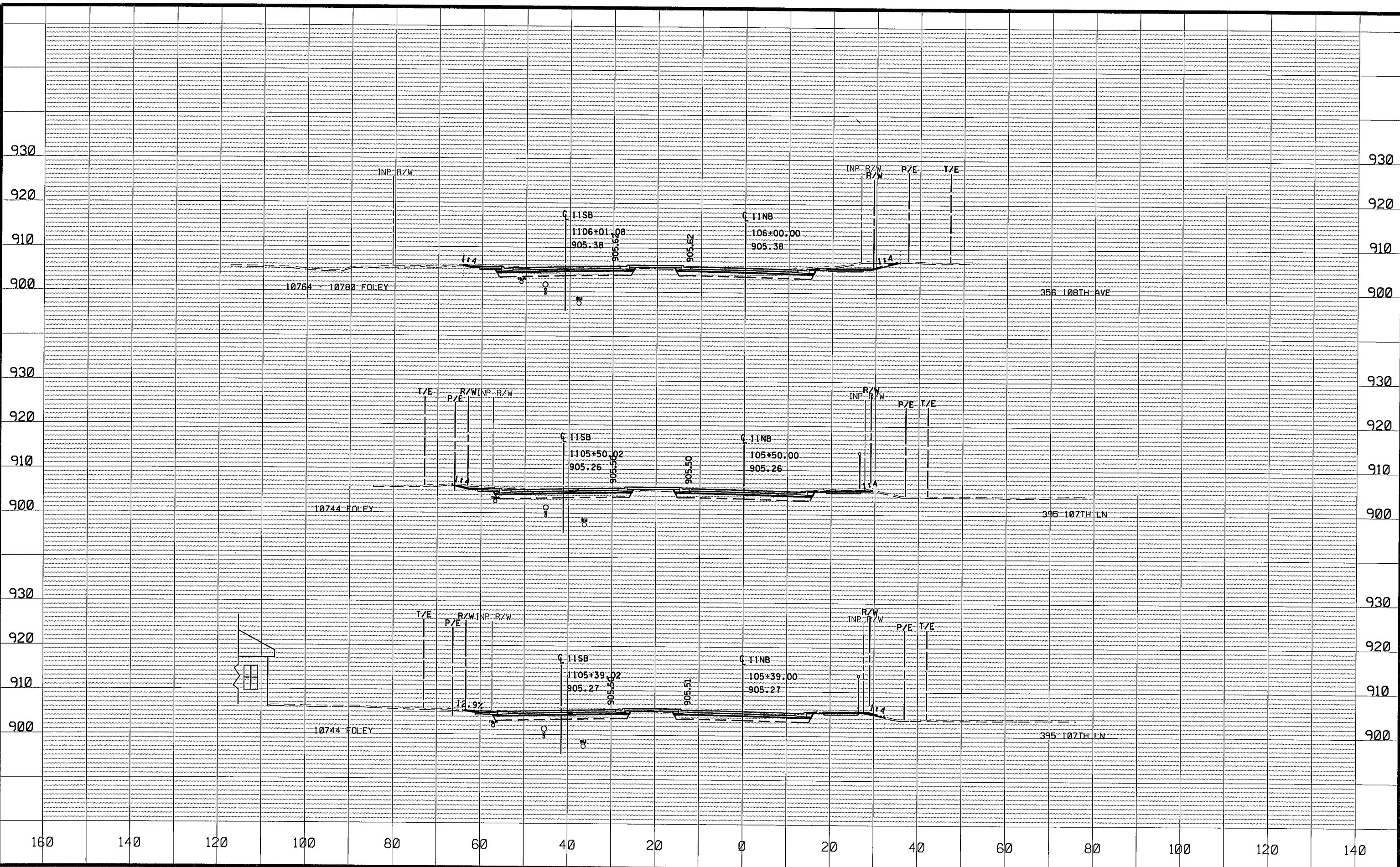
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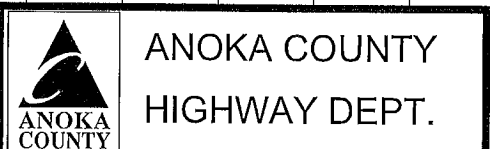
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 SP 114-020-050
 CP 13-10

CROSS SECTIONS
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 Sheet 205 of 298 Sheets



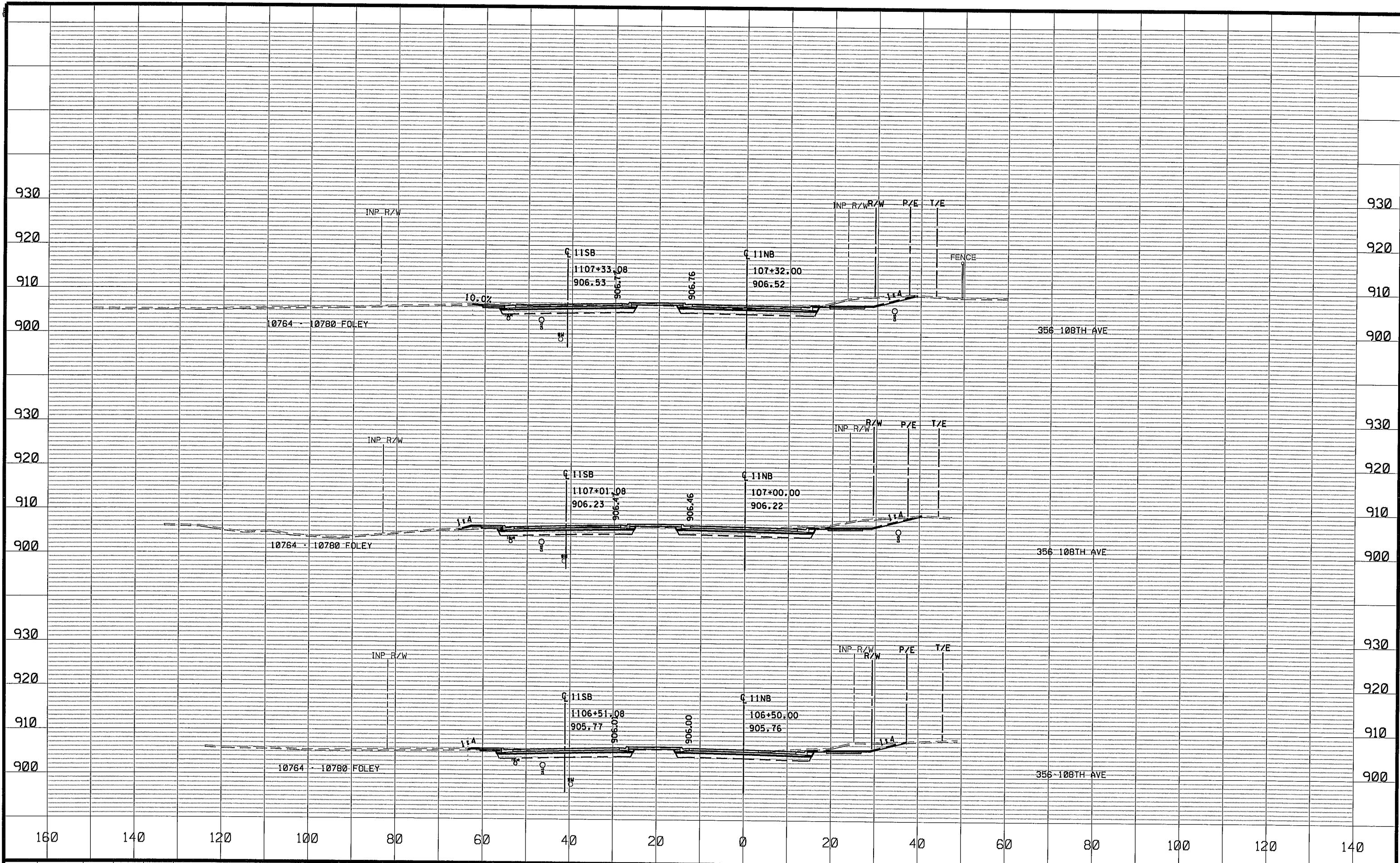
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 CHECKED BY JEO DATE 02/27/16



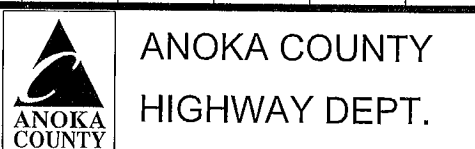
SP 002-611-034
 SP 114-020-050
 CP 13-10

CROSS SECTIONS
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 Sheet 206 of 298 Sheets



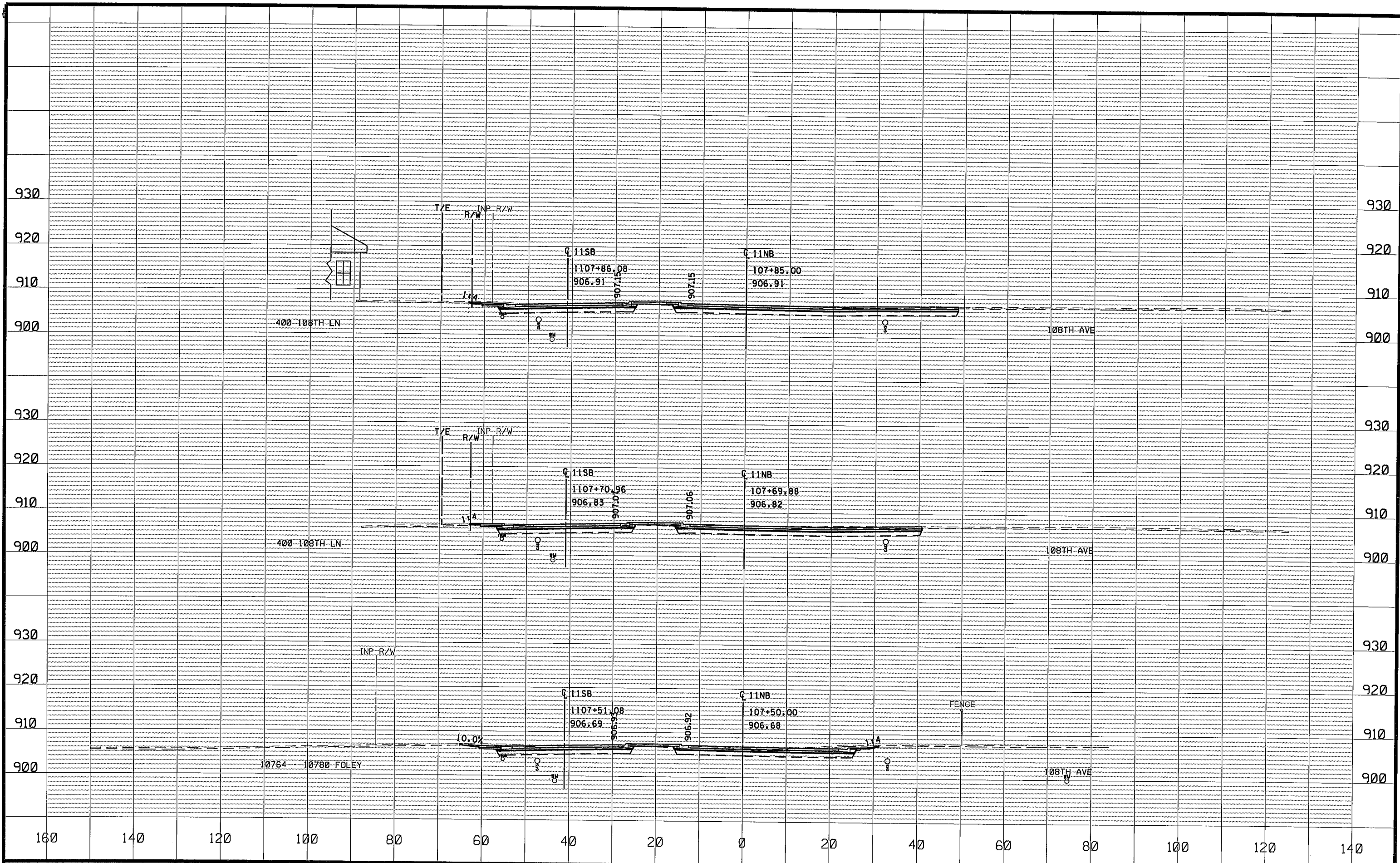
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SP 002-611-034
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 CP 13-10

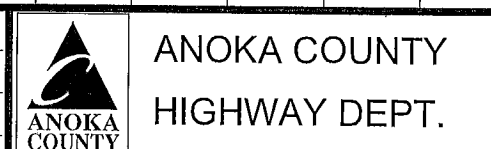
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 Sheet 207 of 298 Sheets



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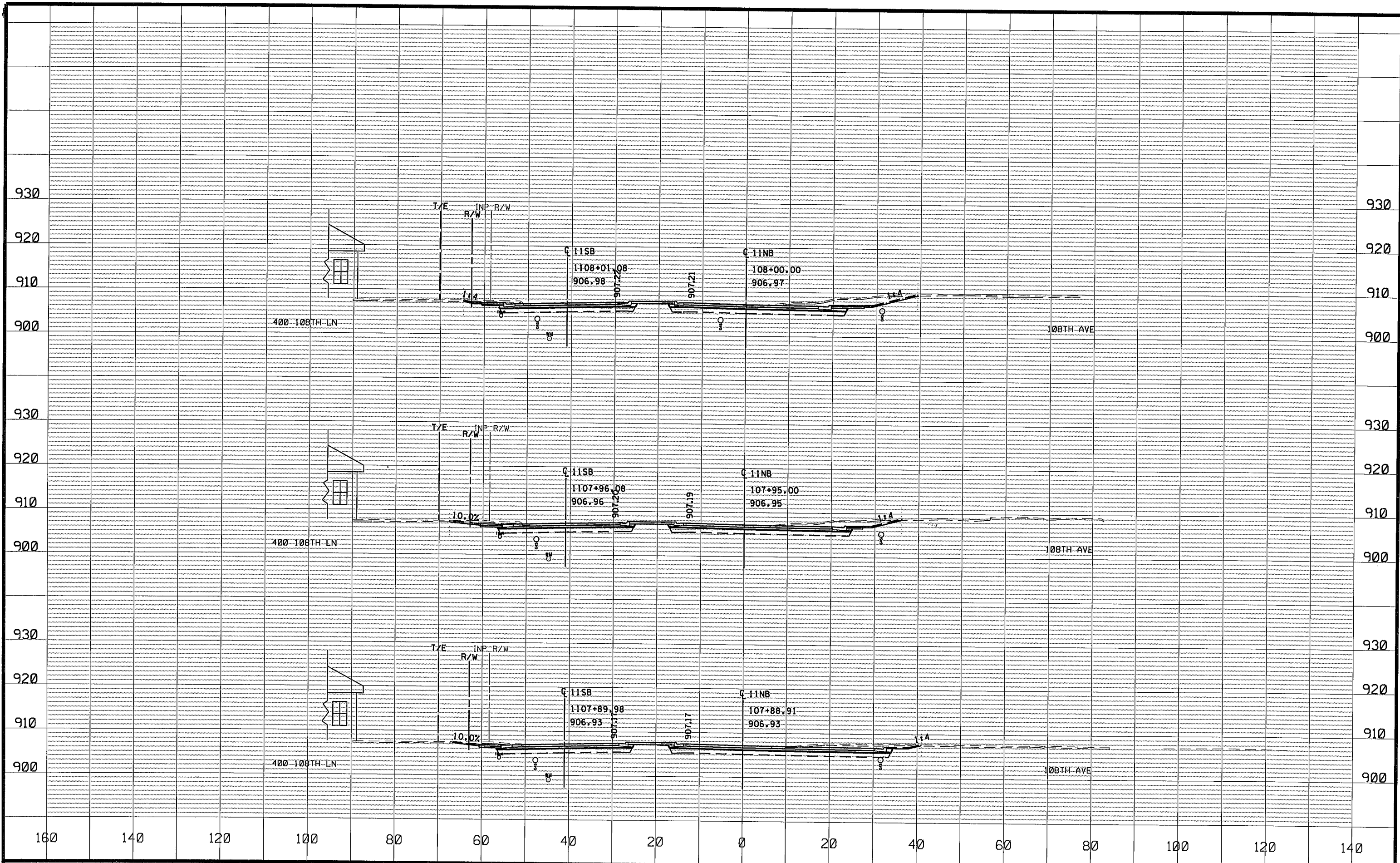
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 CHECKED BY JEO DATE 02/27/16



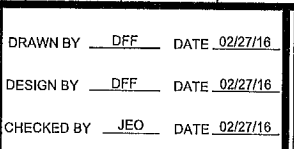
SP 002-611-034
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CROSS SECTIONS
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 Sheet 208 of 298 Sheets



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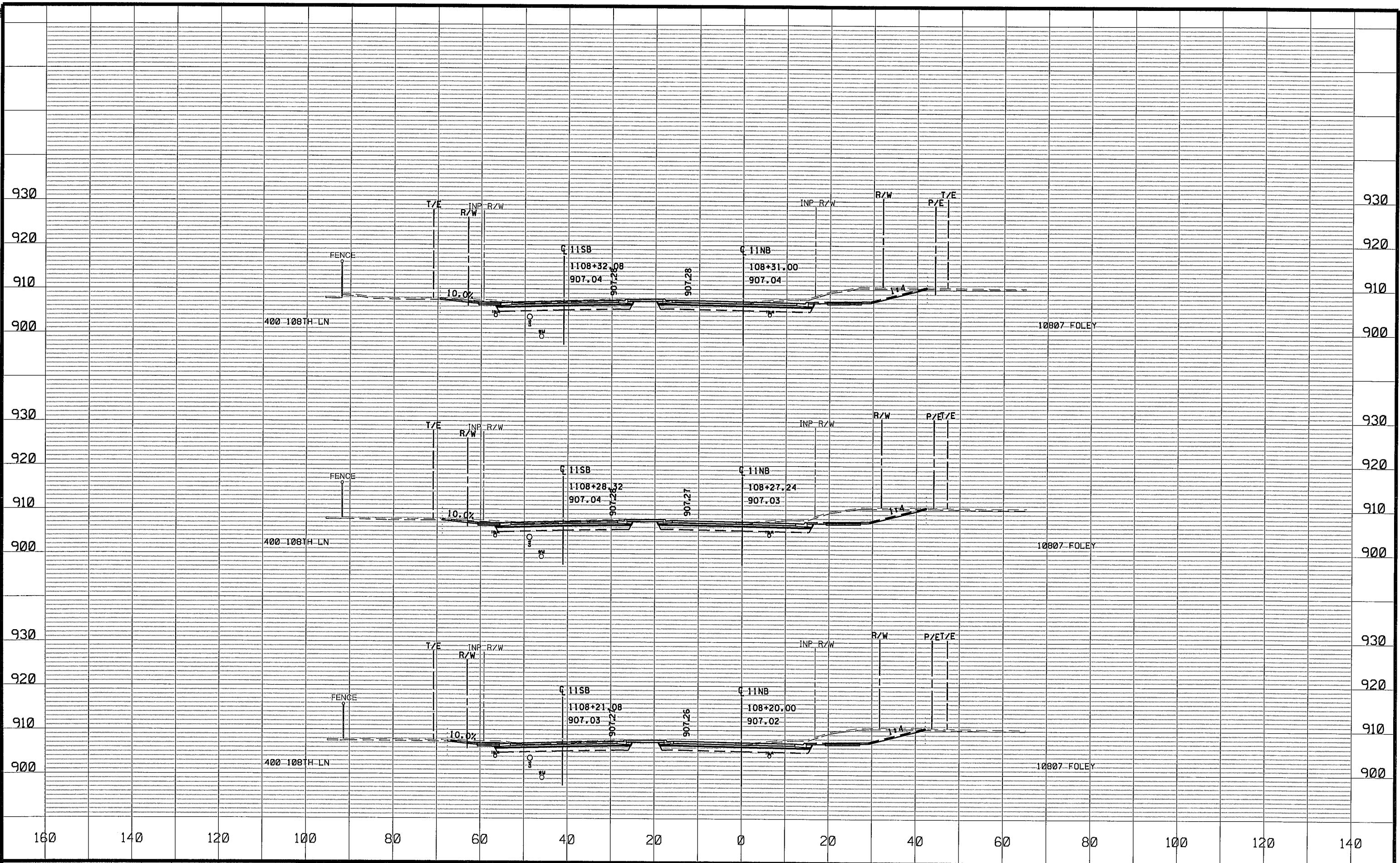
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**ANOKA COUNTY
HIGHWAY DEPT.**

SP 002-611-034
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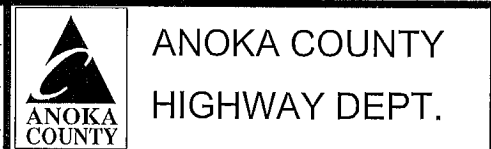
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Sheet 209 of 298 Sheets



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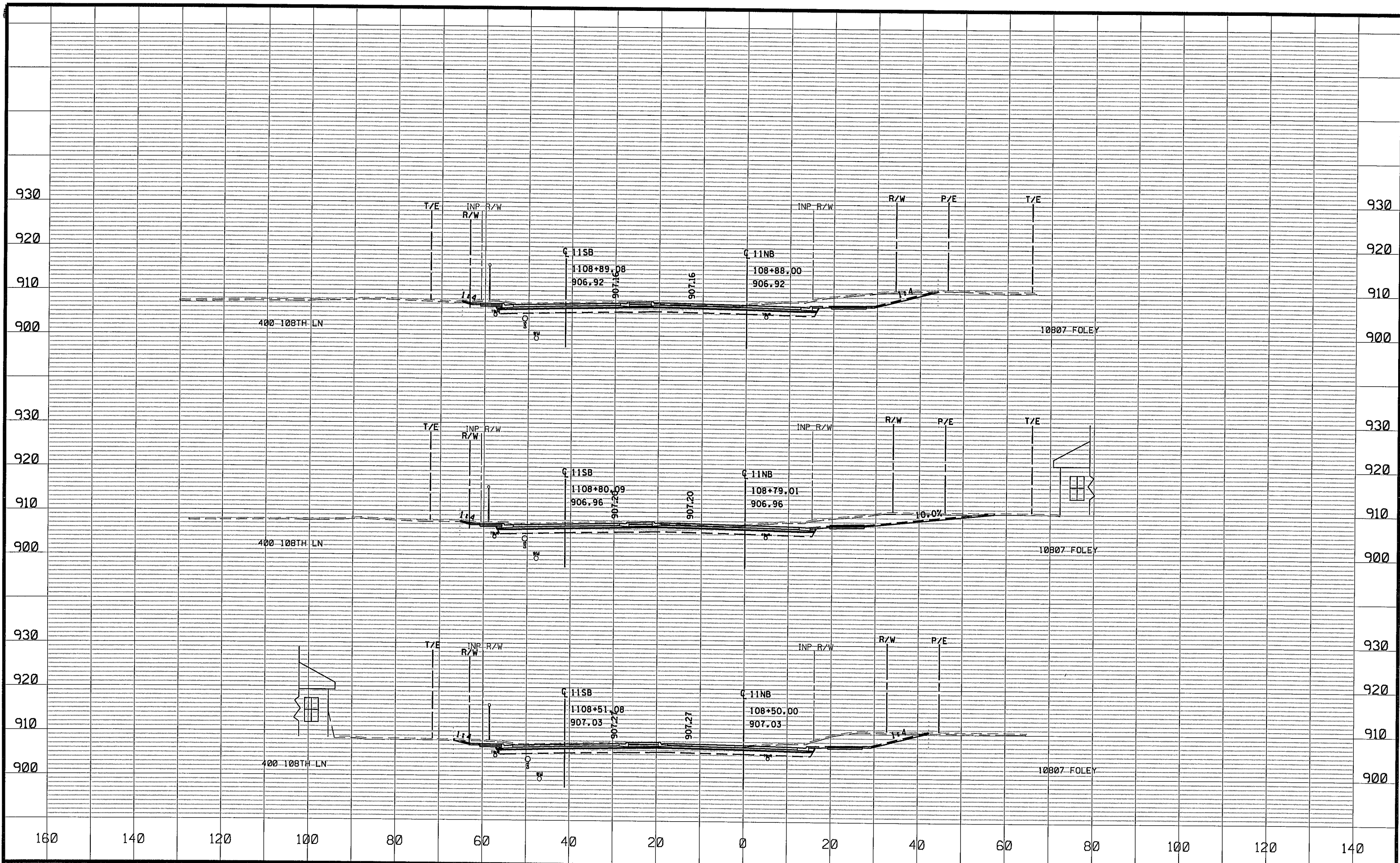
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SP 002-611-034
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 CP 13-10

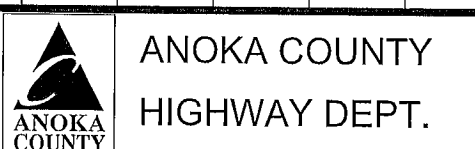
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 Sheet 210 of 298 Sheets



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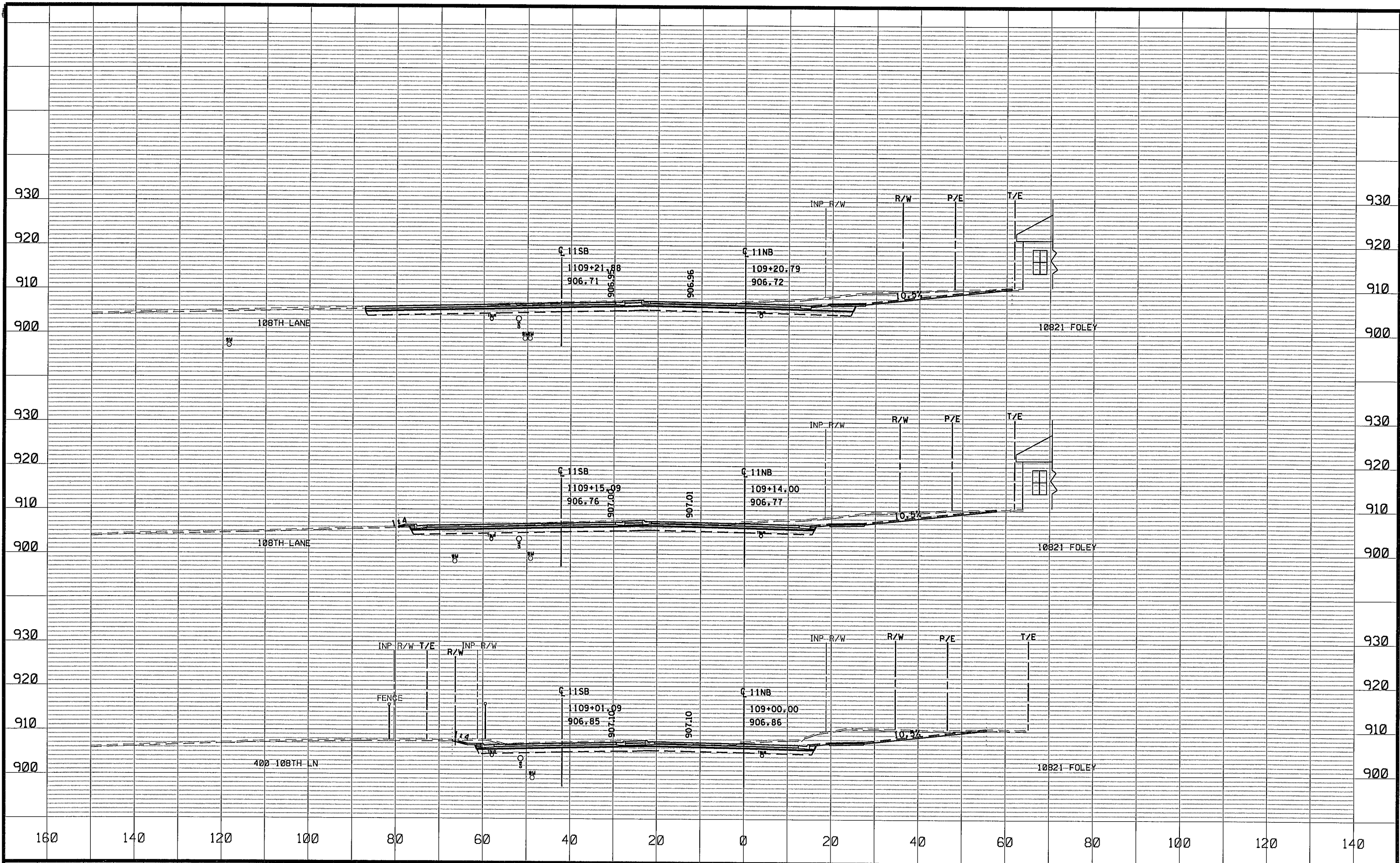
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 CHECKED BY: JEO DATE: 02/27/16



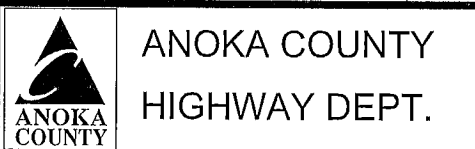
SP 002-611-034
 SP 114-020-050
 CP 13-10

CROSS SECTIONS
 STA 108+50.00 TO 108+88.00
 Sheet 211 of 298 Sheets



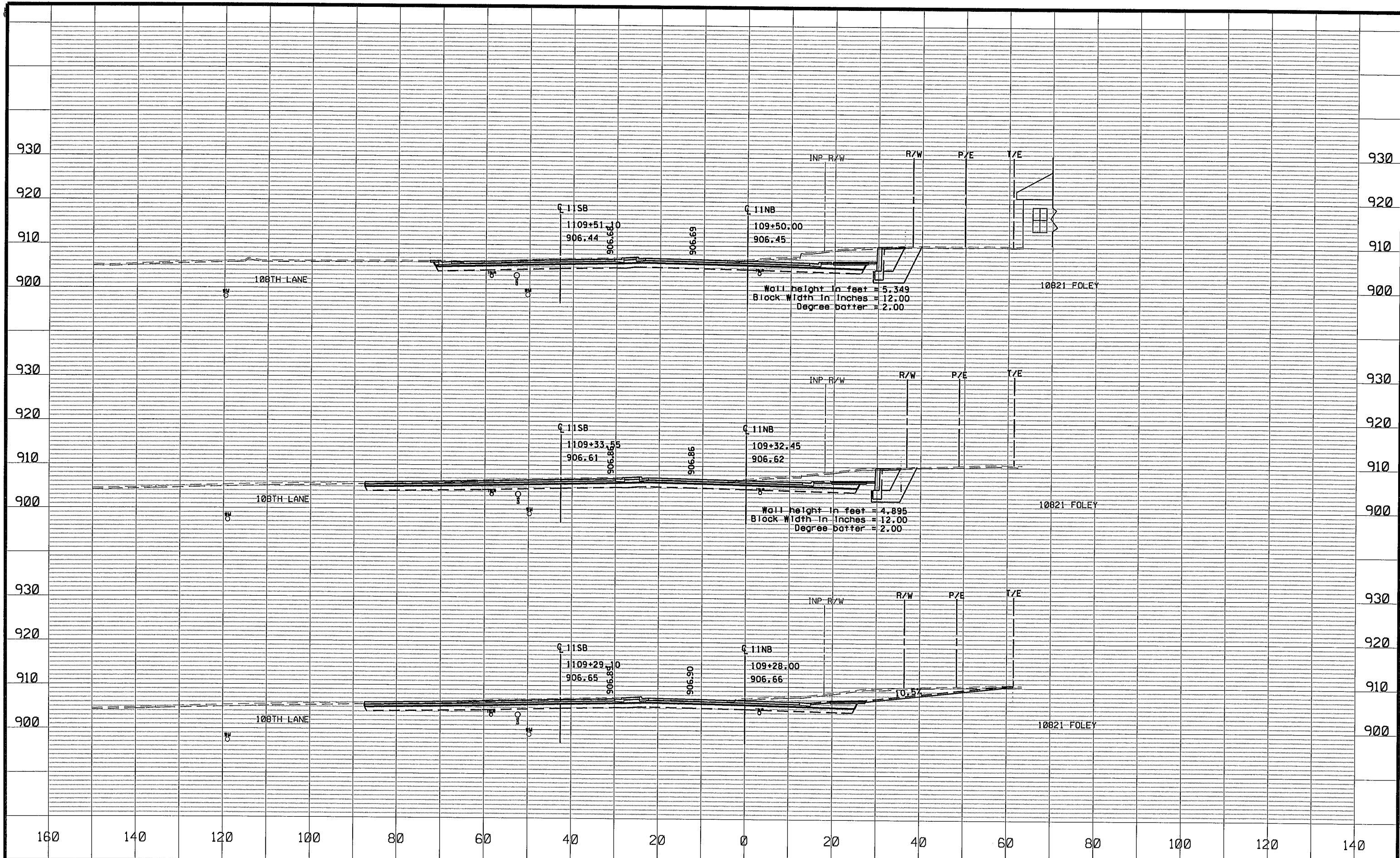
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 CHECKED BY JEO DATE 02/27/16



SP 002-611-034
 SP 114-020-050
 CP 13-10

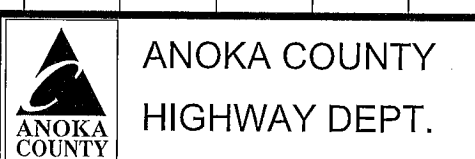
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 Sheet 212 of 298 Sheets



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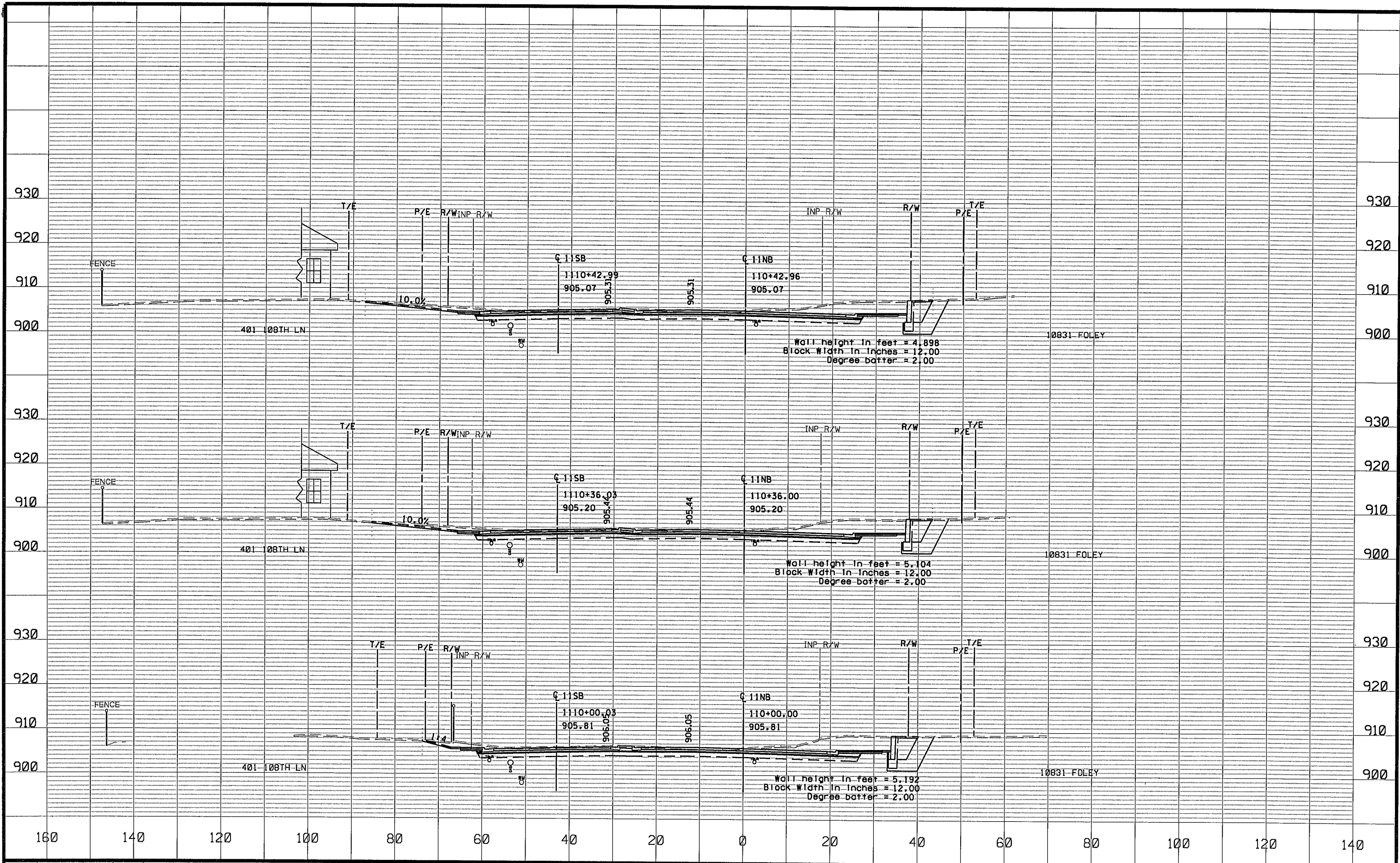
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SP 002-611-034
 SP 114-020-050
 CP 13-10

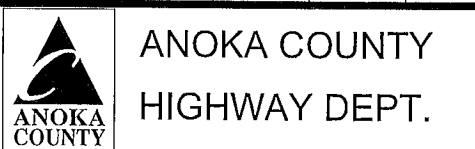
CROSS SECTIONS
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 Sheet 213 of 298 Sheets



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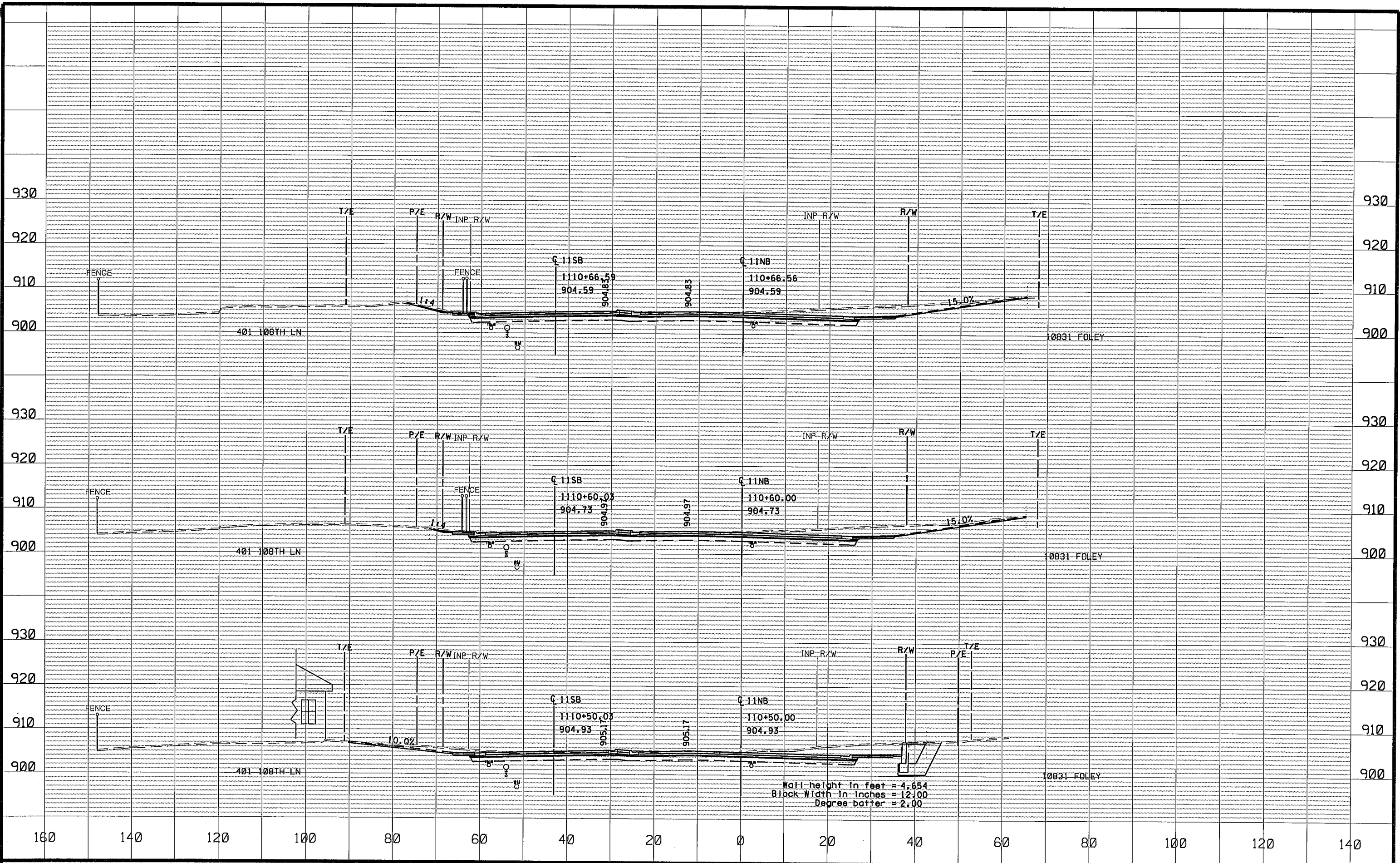
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SP 002-611-034
 SP 114-020-050
 CP 13-10

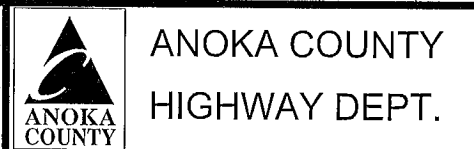
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 Sheet 214 of 298 Sheets



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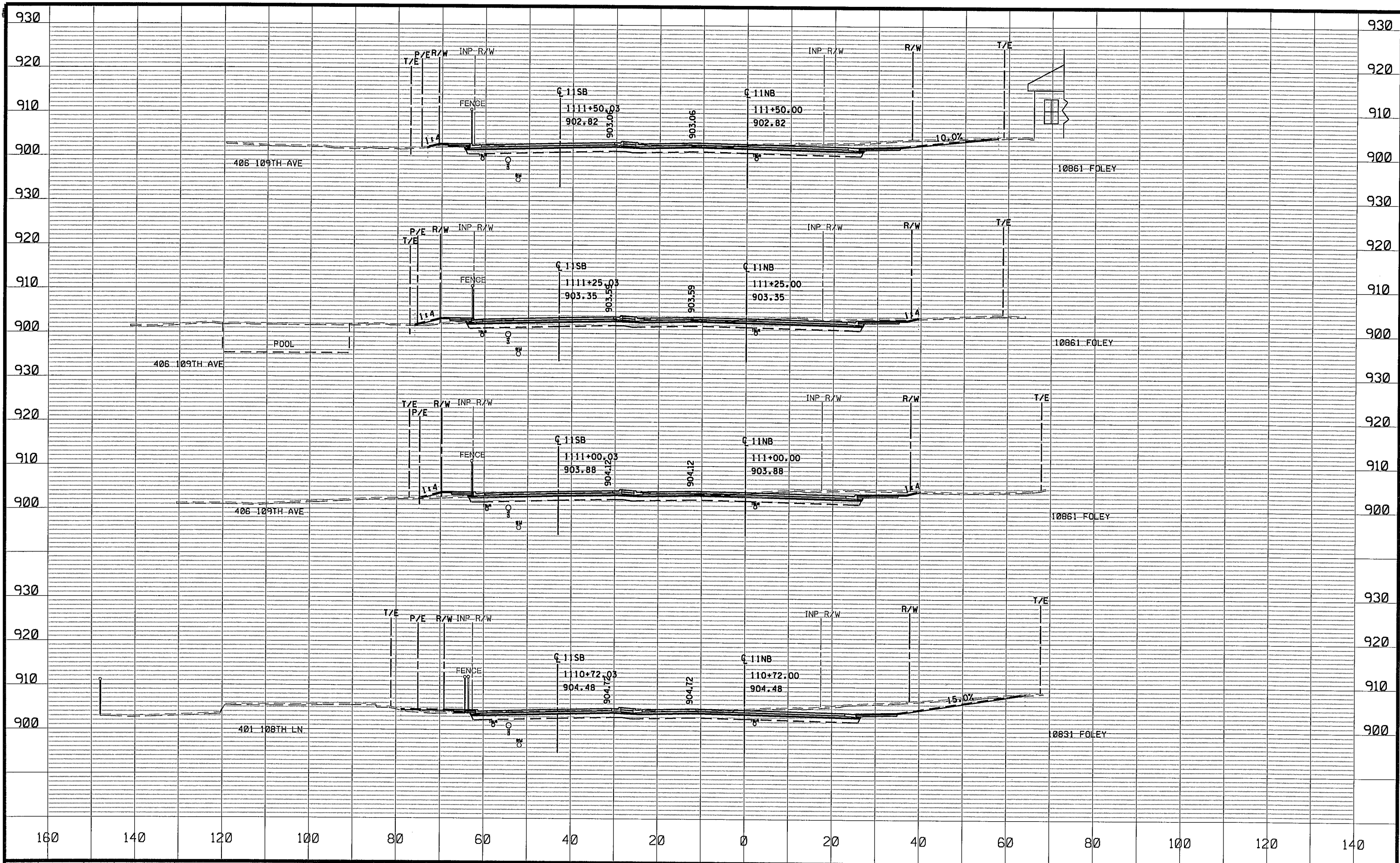
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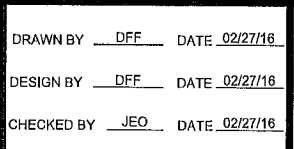
SP 002-611-034
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 CP 13-10

CROSS SECTIONS
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 Sheet 215 of 298 Sheets



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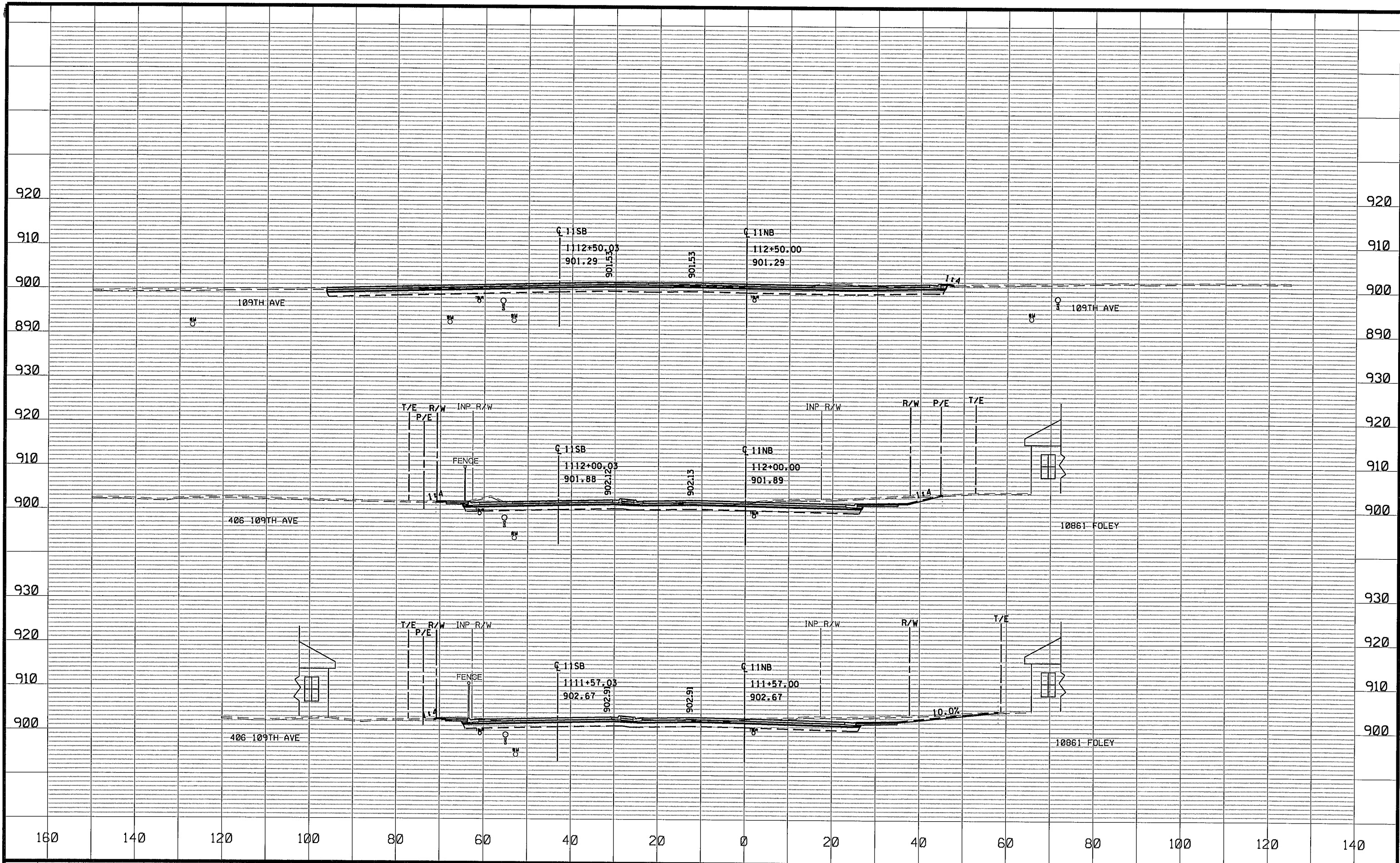
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ANOKA COUNTY
HIGHWAY DEPT.

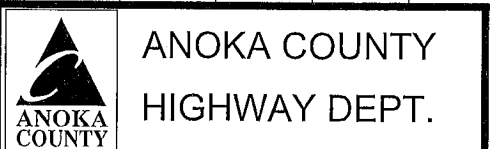
SP 002-611-034
SP 114-020-050
CP 13-10

CROSS SECTIONS
STA 110+72.00 TO 111+50.00
Sheet 216 of 298 Sheets



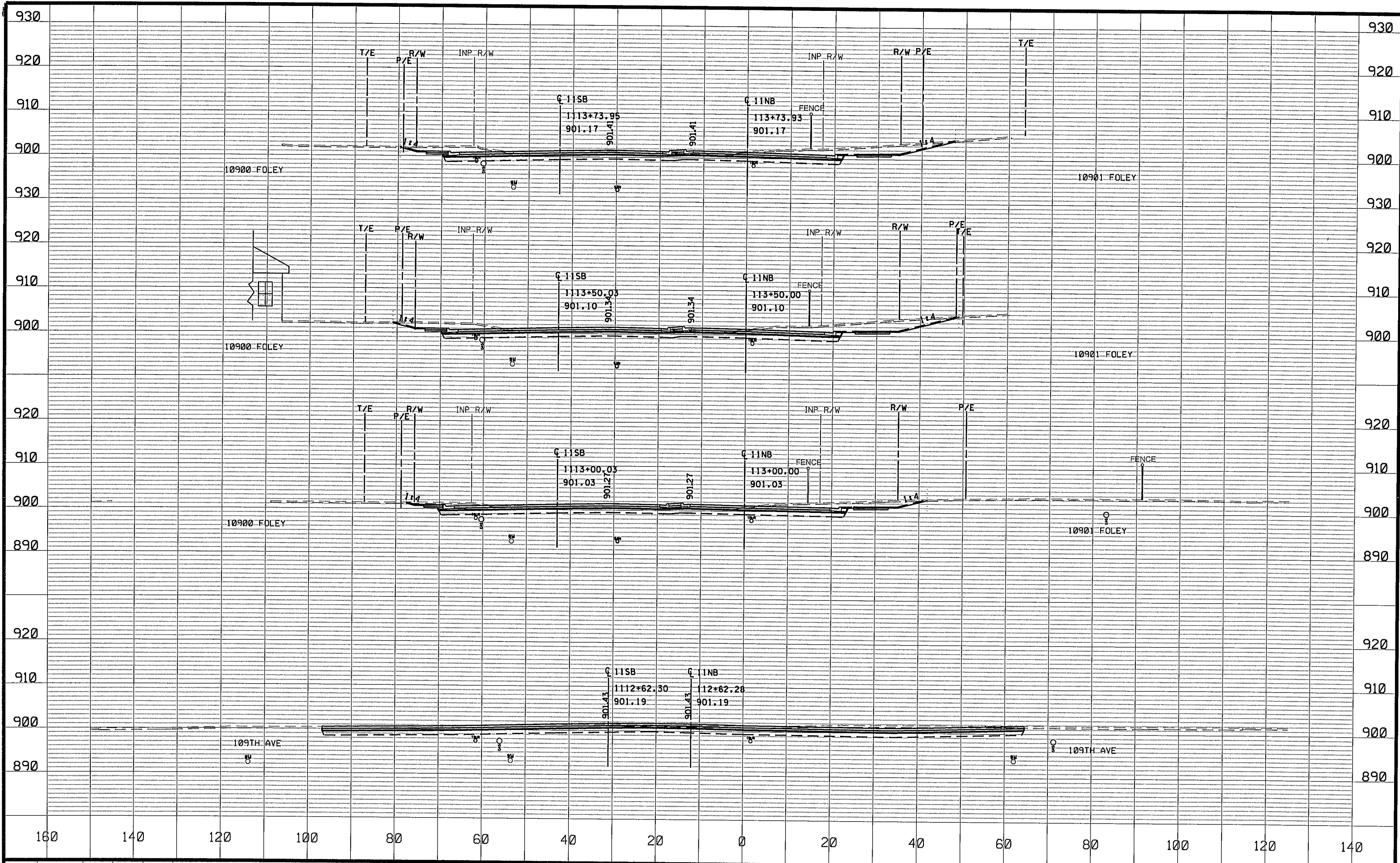
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SP 002-611-034
 SP 114-020-050
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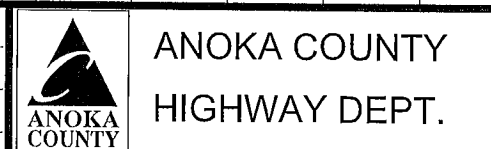
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 Sheet 217 of 298 Sheets



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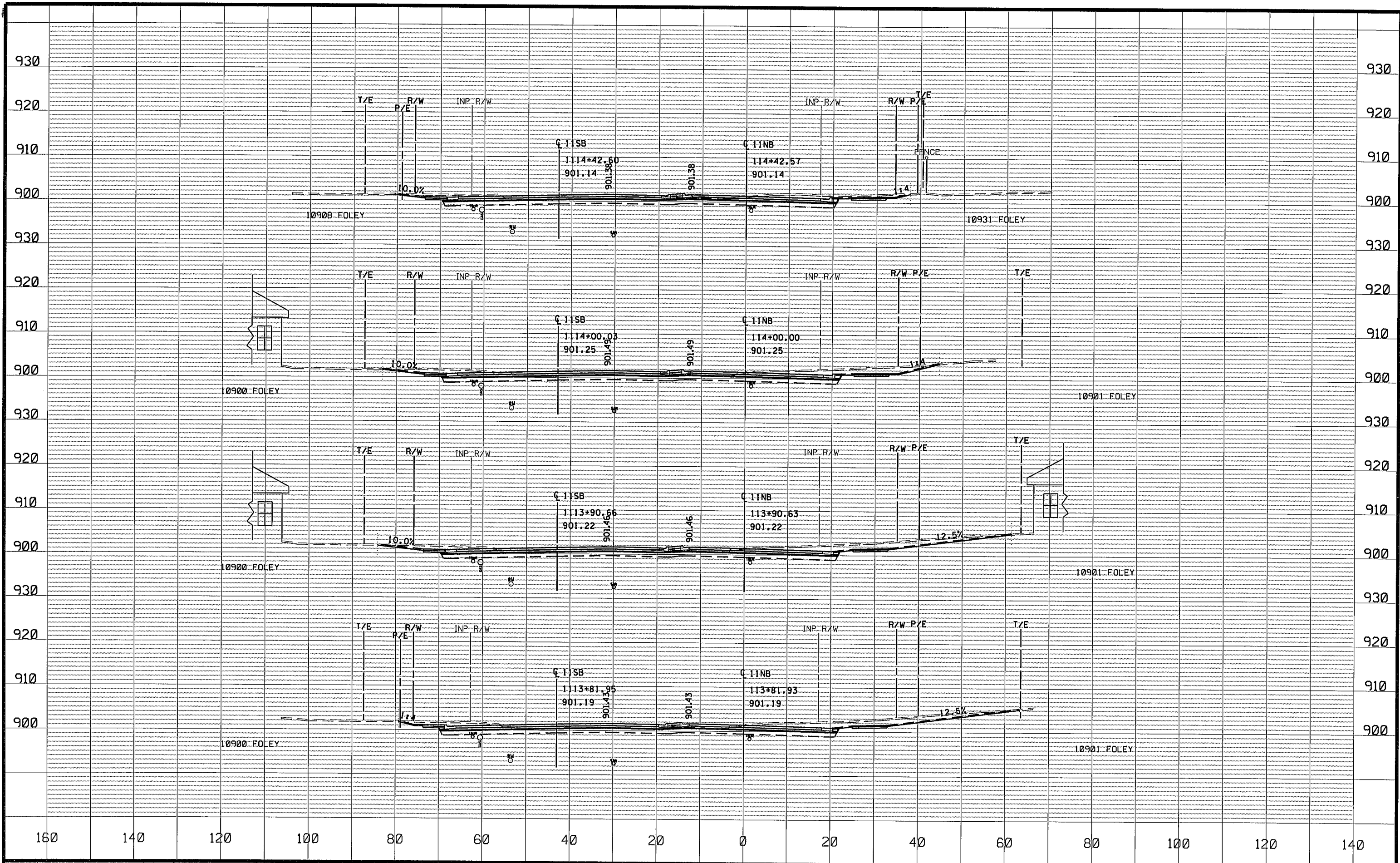
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 CHECKED BY: JEO DATE: 02/27/16



SP 002-611-034
 SP 114-020-050
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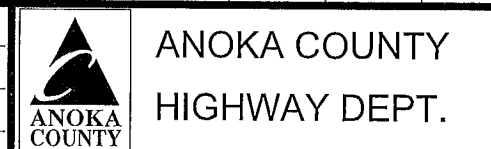
CROSS SECTIONS
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 Sheet 218 of 298 Sheets



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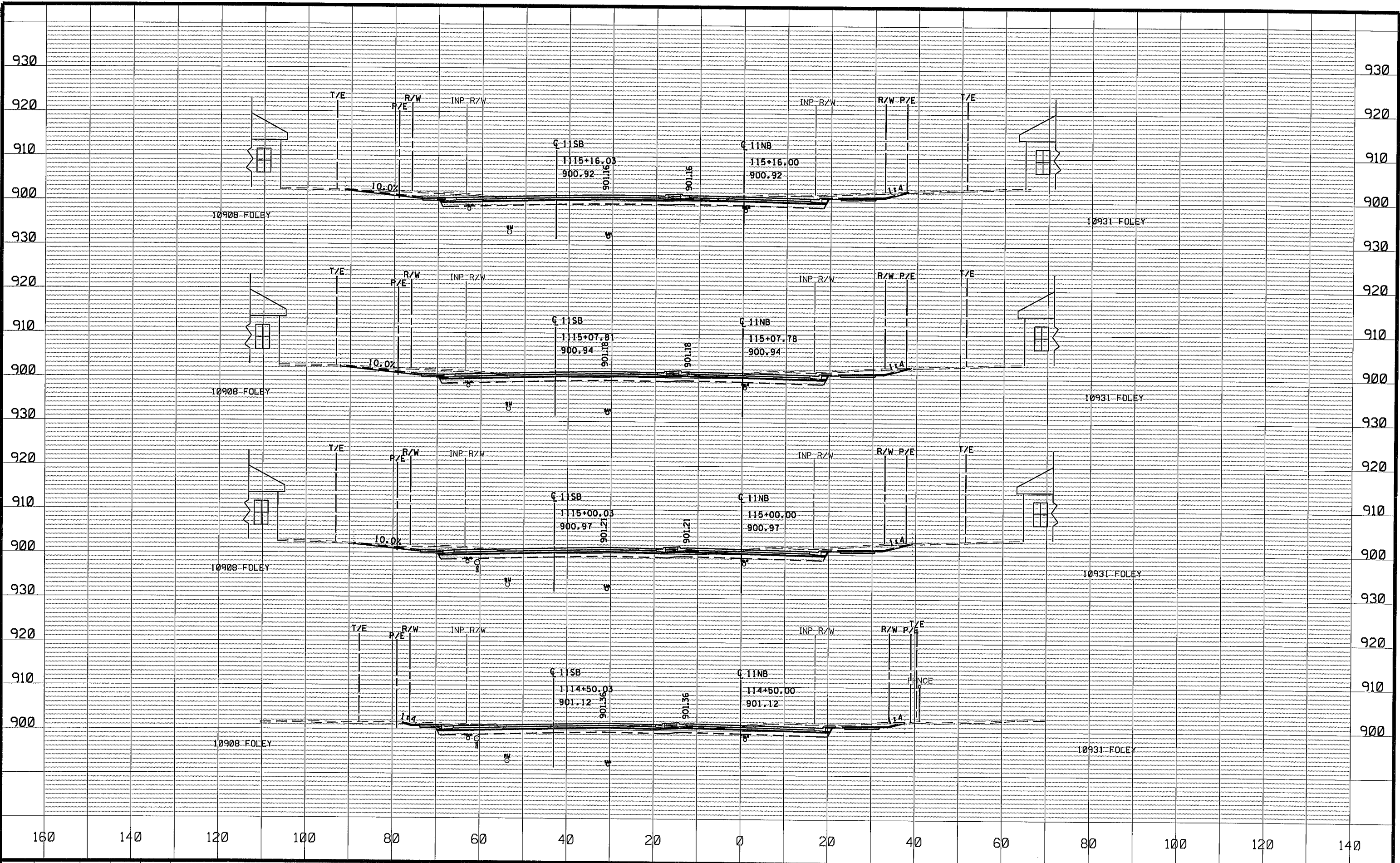
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SP 002-611-034
 SP 114-020-050
 CP 13-10

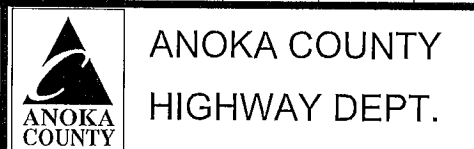
CROSS SECTIONS
 STA 113+81.93 TO 114+42.57
 Sheet 219 of 298 Sheets



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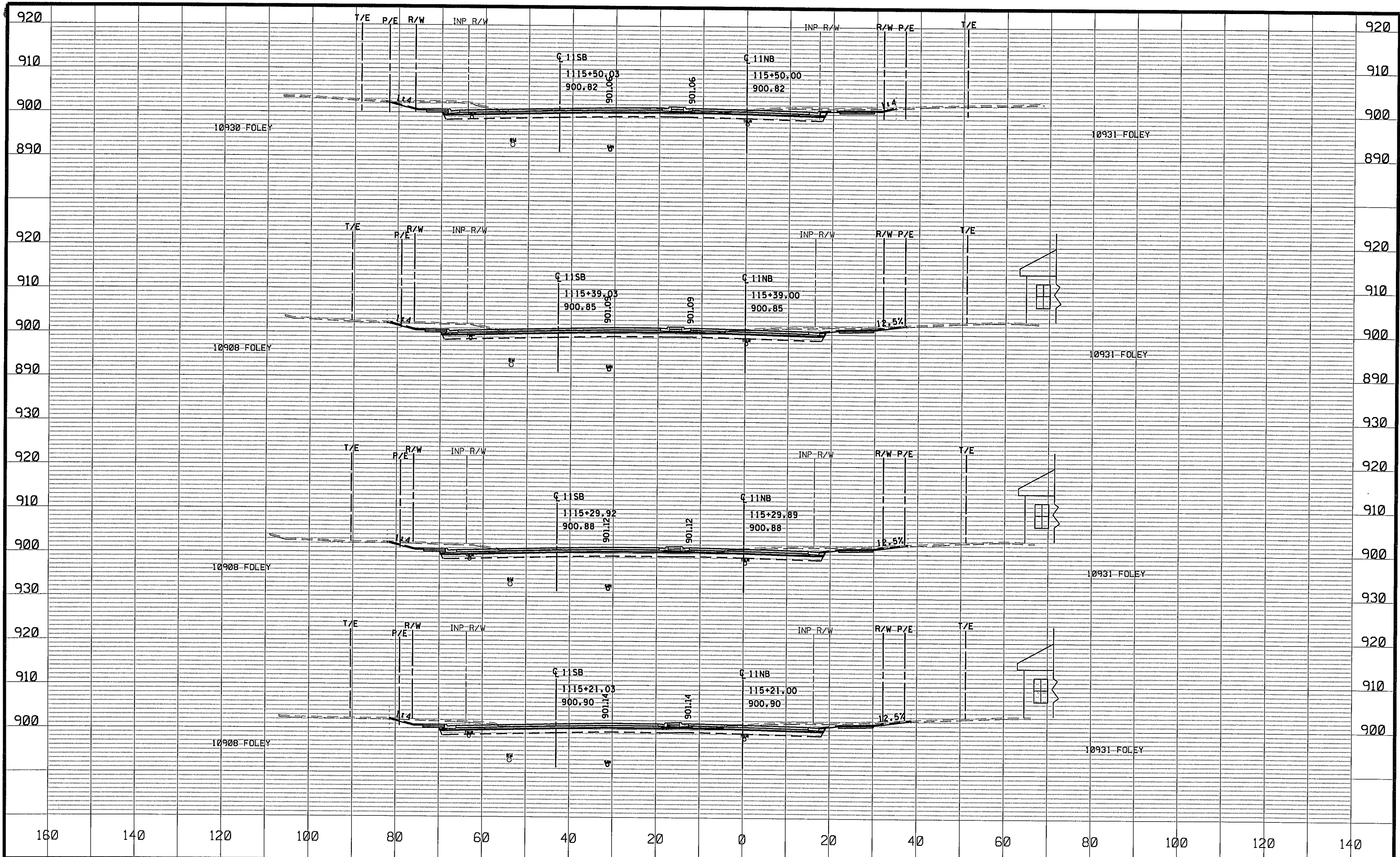
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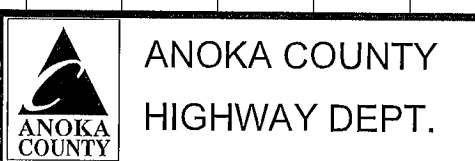
CROSS SECTIONS
 STA 114+50.00 TO 115+16.00
 Sheet 220 of 298 Sheets



NO	DATE	BY	CKD	APPR	REVISION

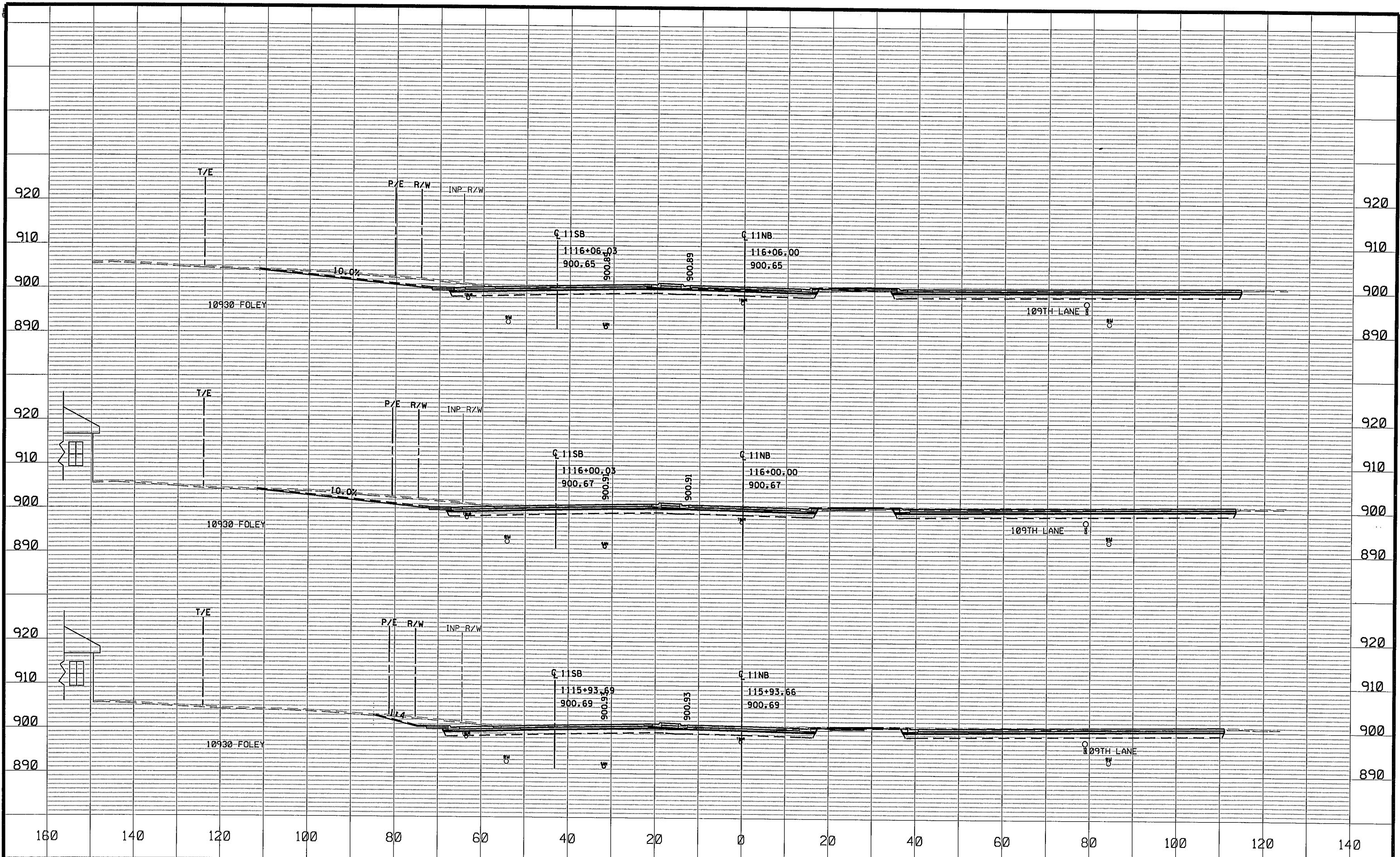
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 DESIGN BY DFF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



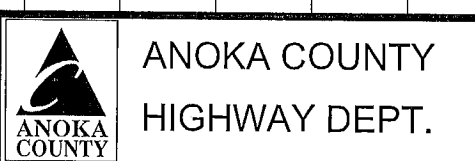
SP 002-611-034
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CROSS SECTIONS
 STA 115+21.00 TO 115+50.00
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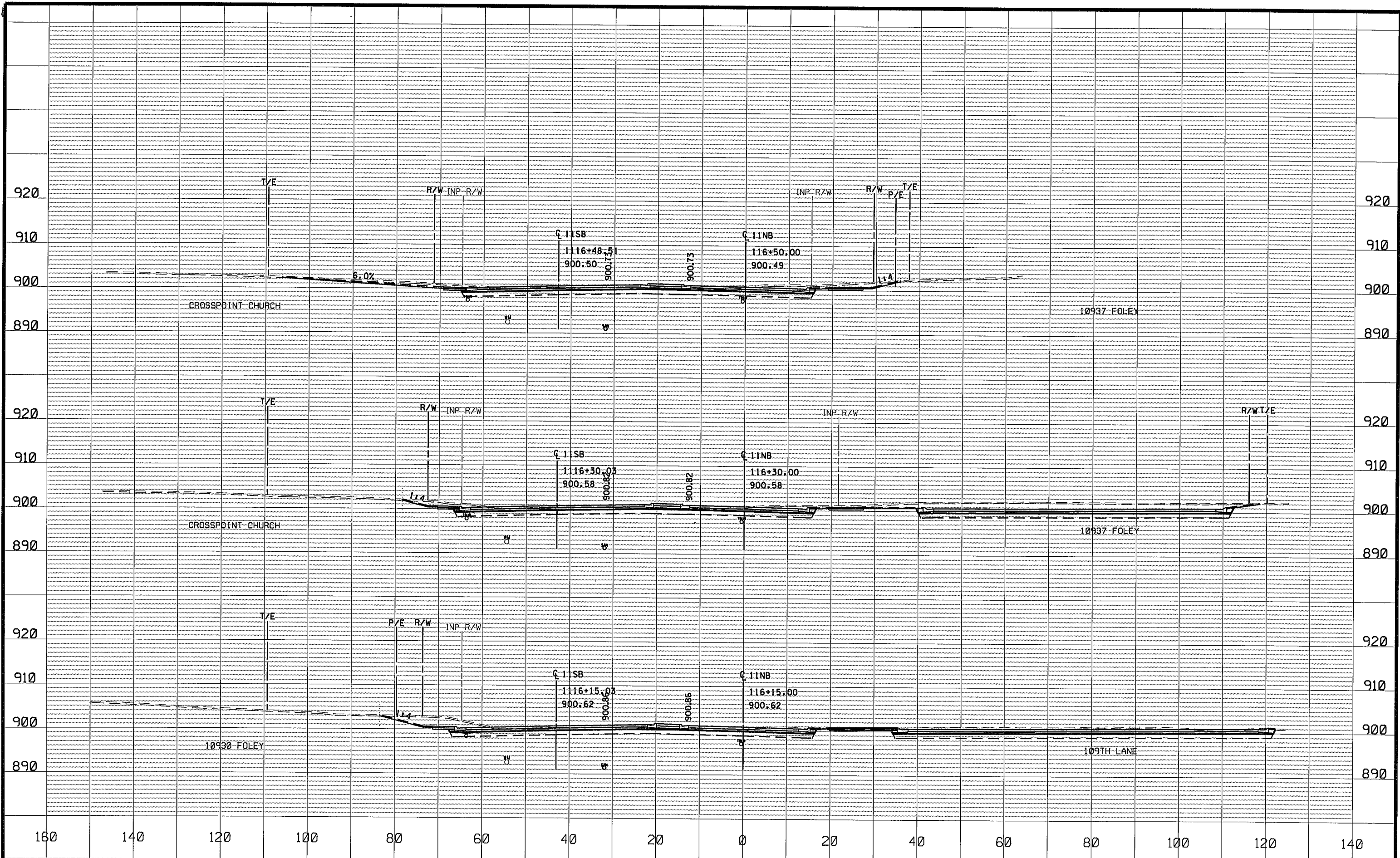
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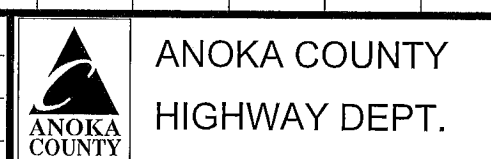
CROSS SECTIONS
 STA 115+93.66 TO 116+06.00
 Sheet 222 of 298 Sheets



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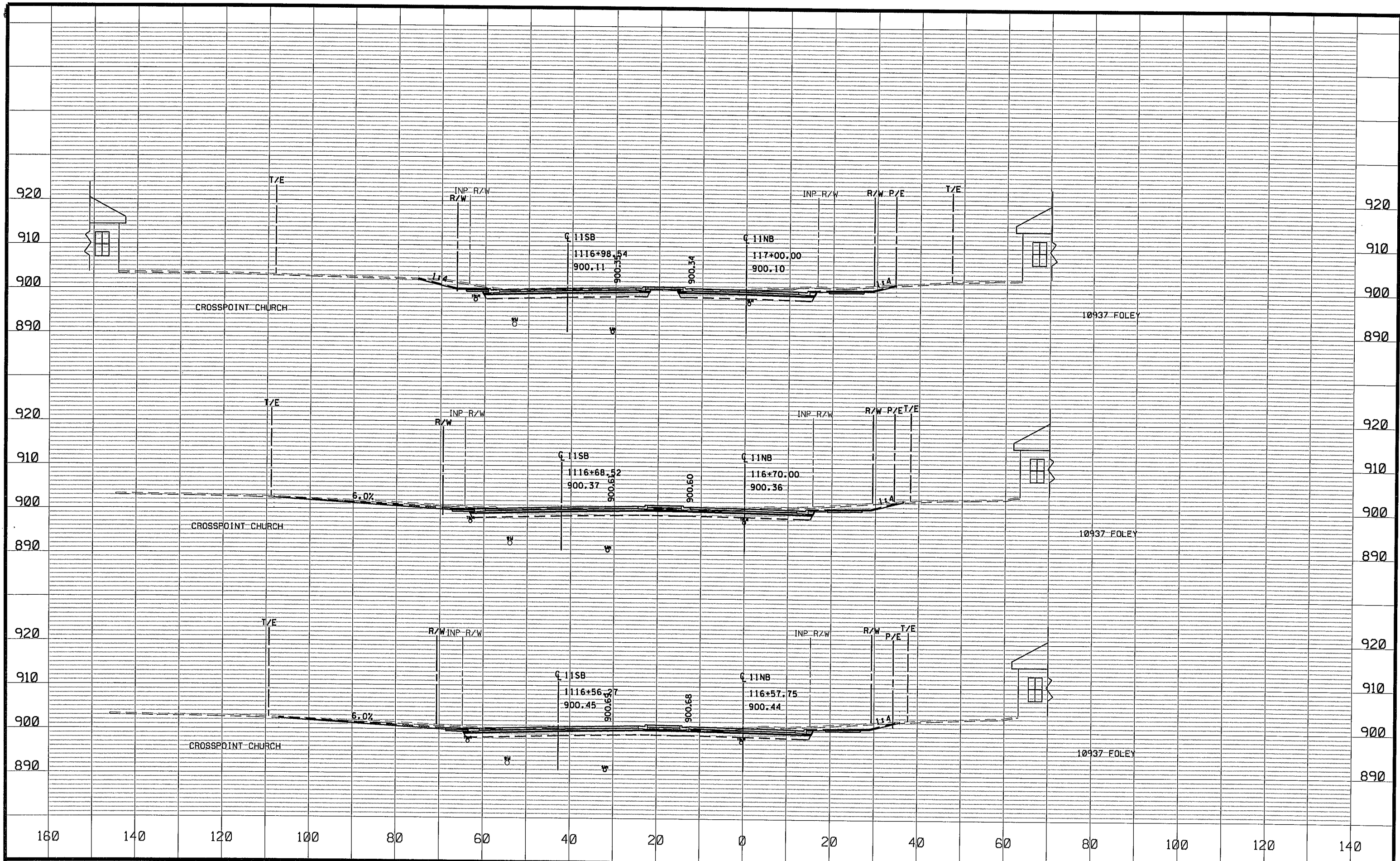
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 CHECKED BY JEO DATE 02/27/16



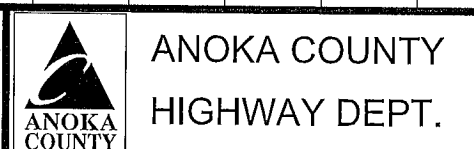
SP 002-611-034
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CROSS SECTIONS
 STA 116+15.00 TO 116+50.00
 Sheet 223 of 298 Sheets



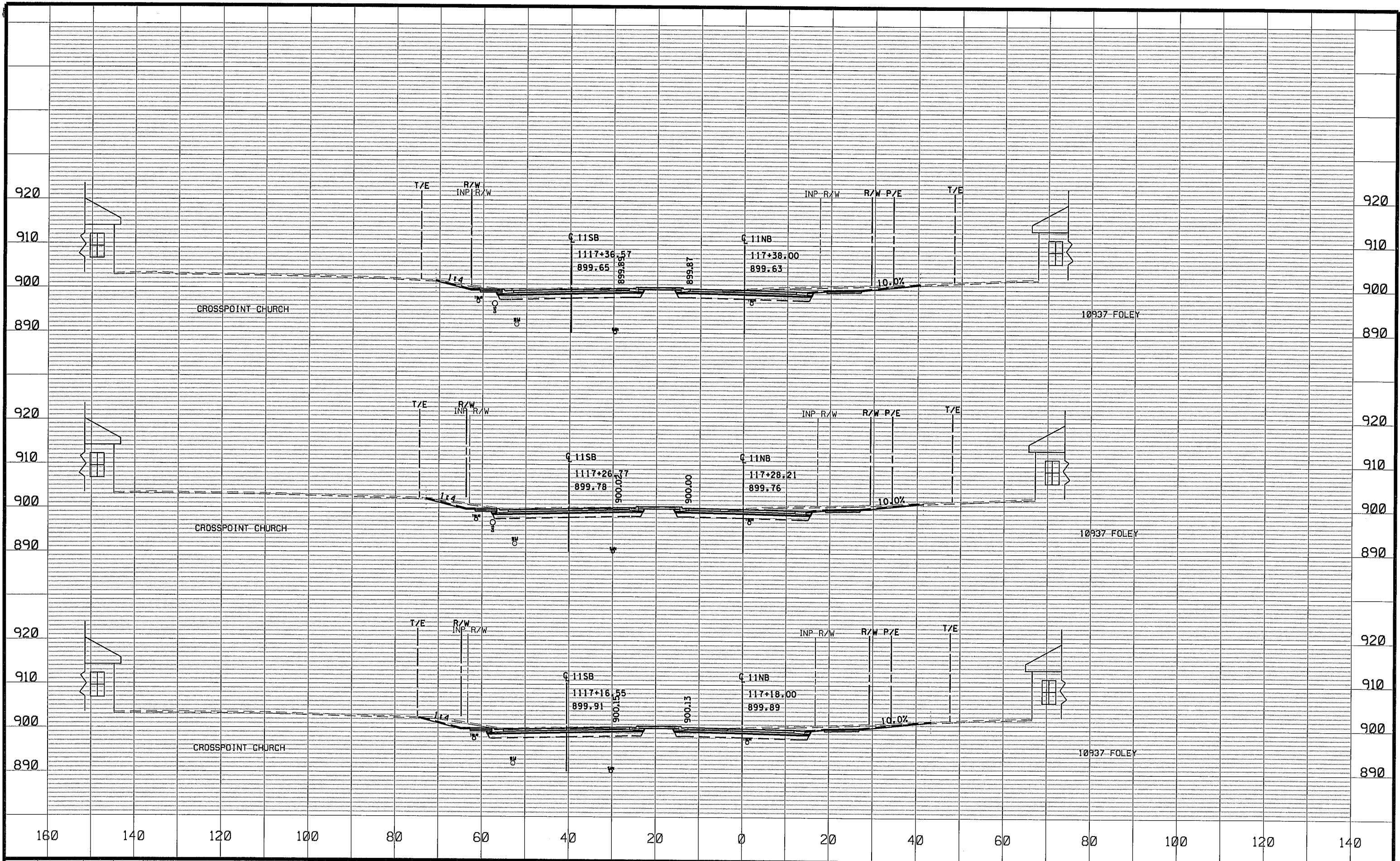
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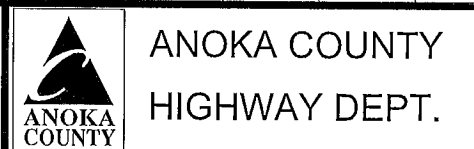
CROSS SECTIONS
 STA 116+57.75 TO 117+00.00
 Sheet 224 of 298 Sheets



NO	DATE	BY	CKD	APPR	REVISION

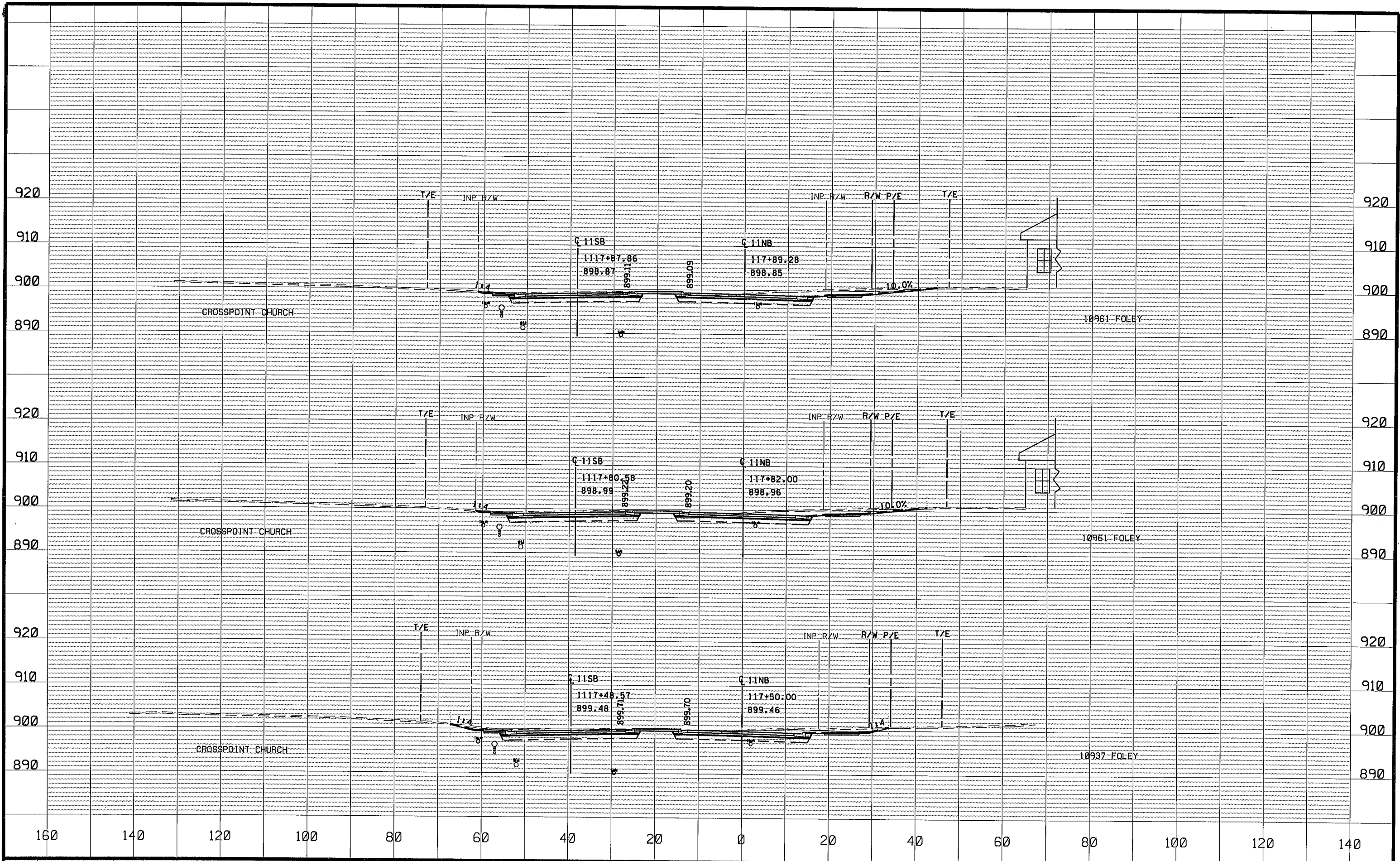
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 CHECKED BY JEO DATE 02/27/16



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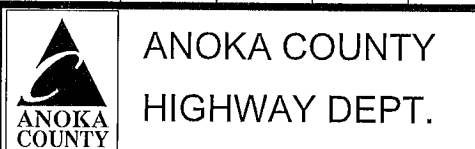
CROSS SECTIONS
 STA 117+18.00 TO 117+38.00
 Sheet 225 of 298 Sheets



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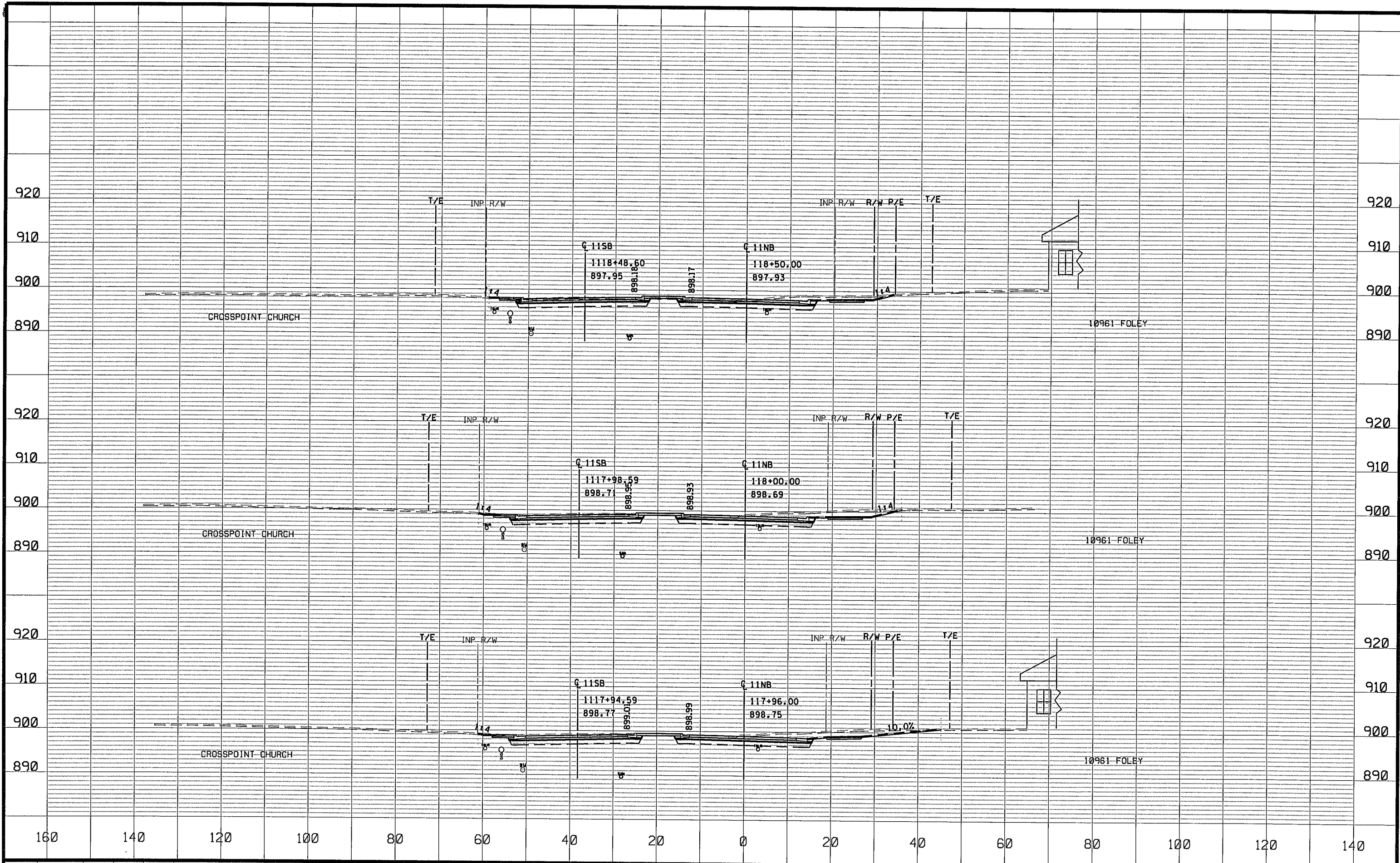
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CROSS SECTIONS
 STA 117+50.00 TO 117+89.28
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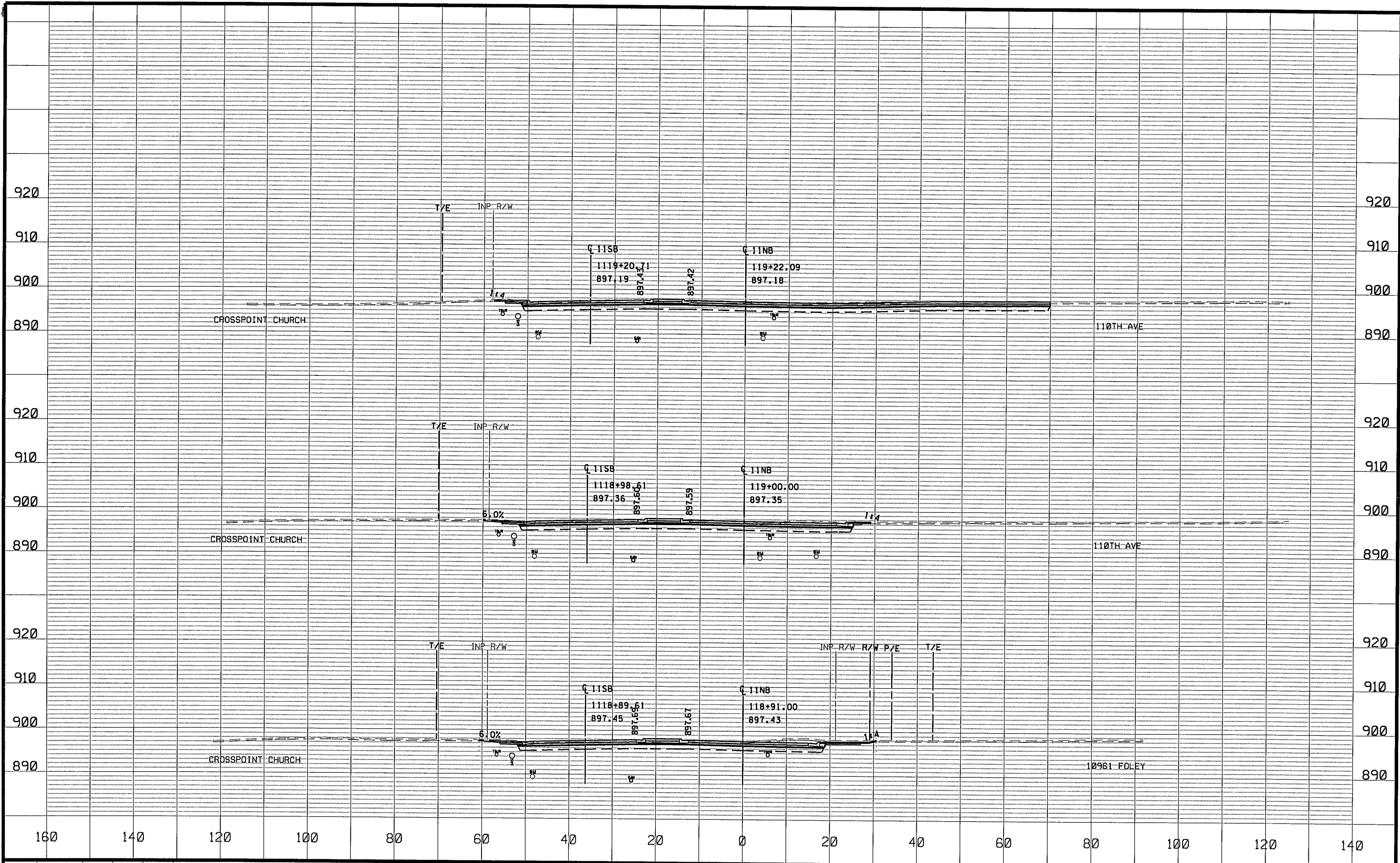
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DESIGN BY	DEF	DATE	02/27/16
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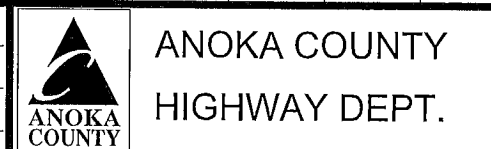
CROSS SECTIONS
 STA 117+96.00 TO 118+50.00
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NO	DATE	BY	CKD	APPR	REVISION

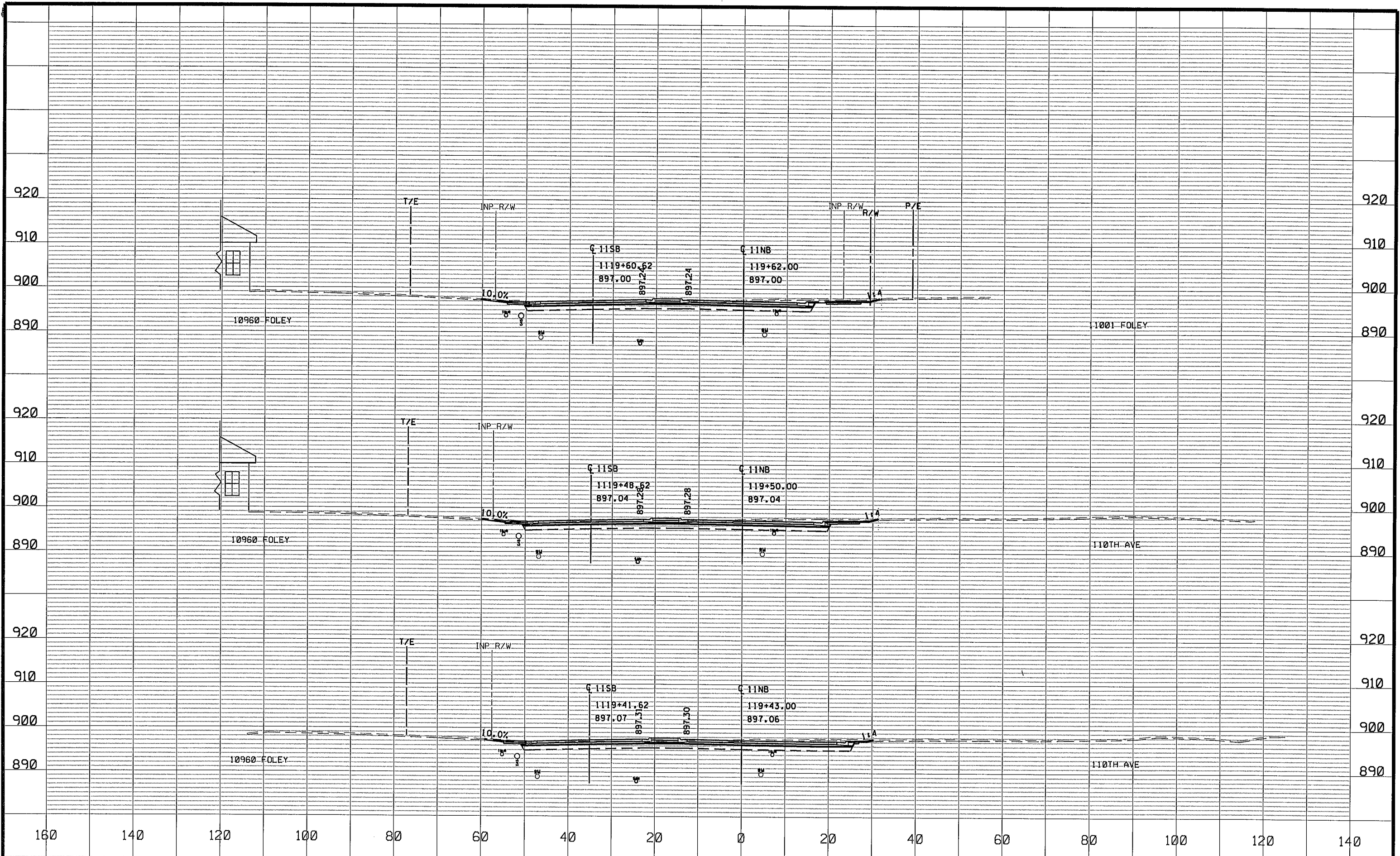
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 CHECKED BY: JEO DATE: 02/27/16



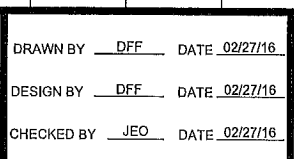
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CROSS SECTIONS
 STA 118+91.00 TO 119+22.09
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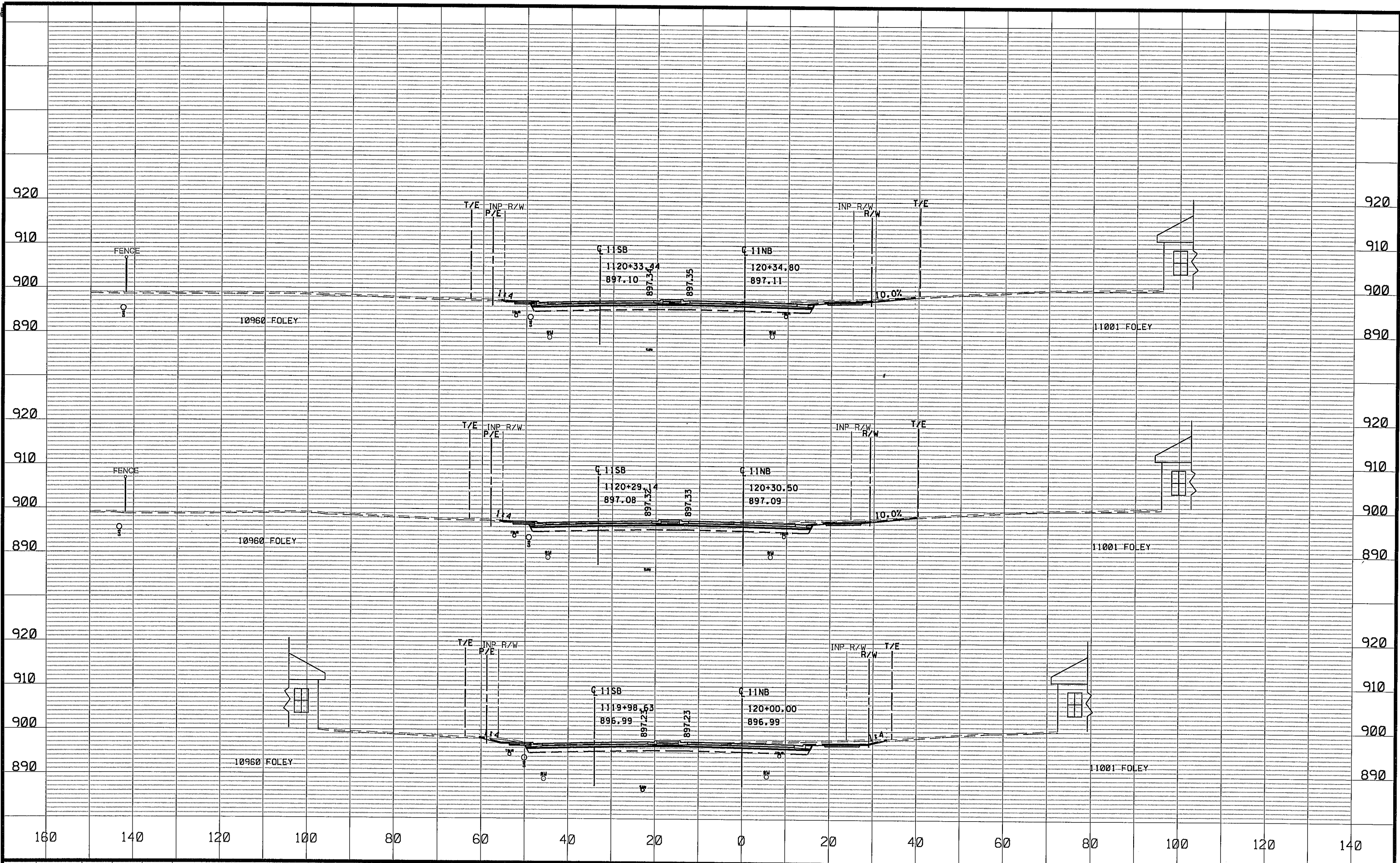
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DESIGN BY	DFP	DATE	02/27/16
CHECKED BY	JEO	DATE	02/27/16



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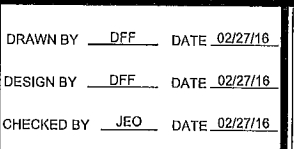
CROSS SECTIONS
STA 119+43.00 TO 119+62.00
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05/13/2016 1:00:20 PM

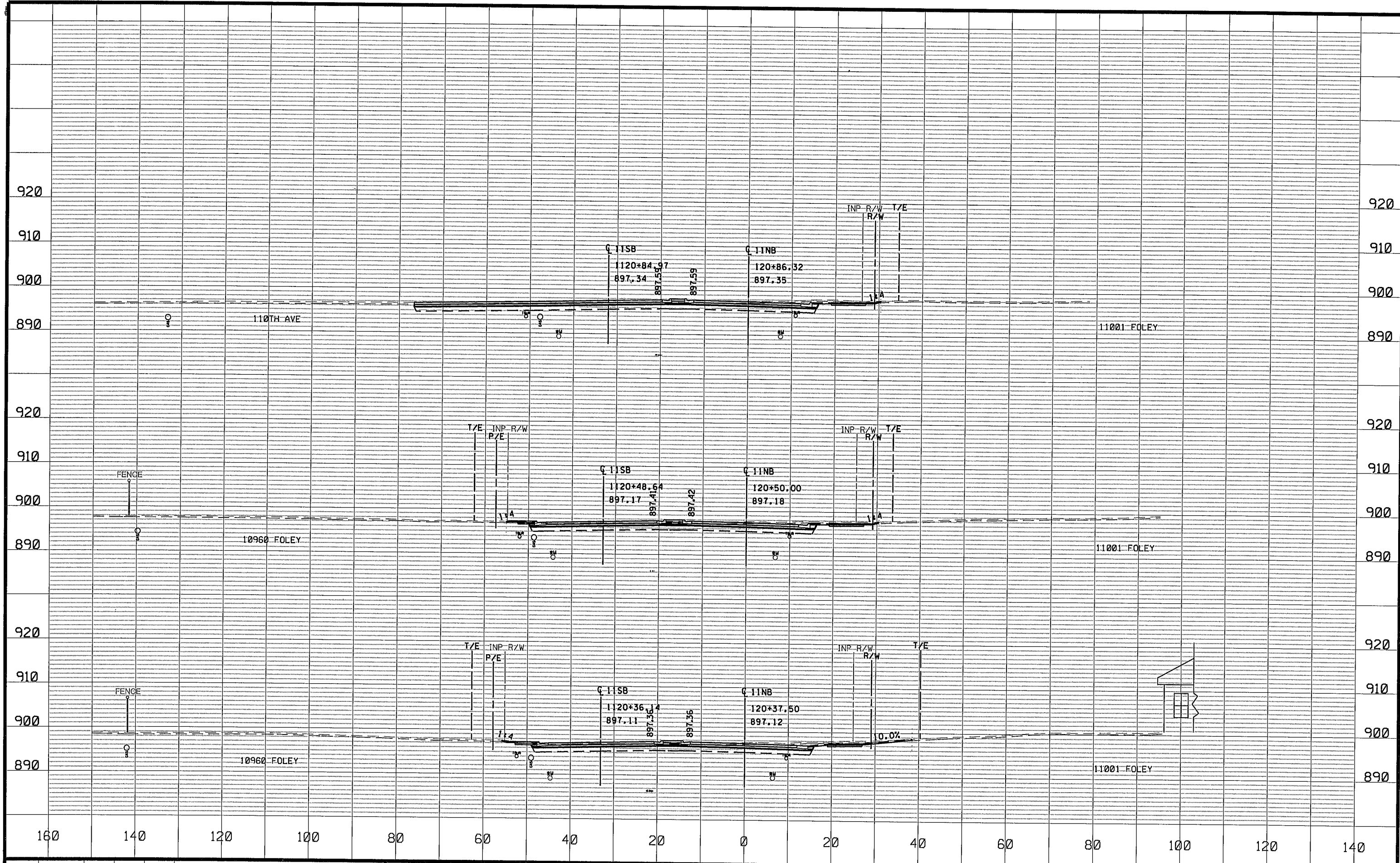
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CHECKED BY	___ JEO ___	DATE	02/27/16



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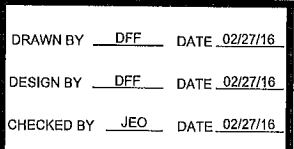
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CROSS SECTIONS
STA 120+00.00 TO 120+34.80
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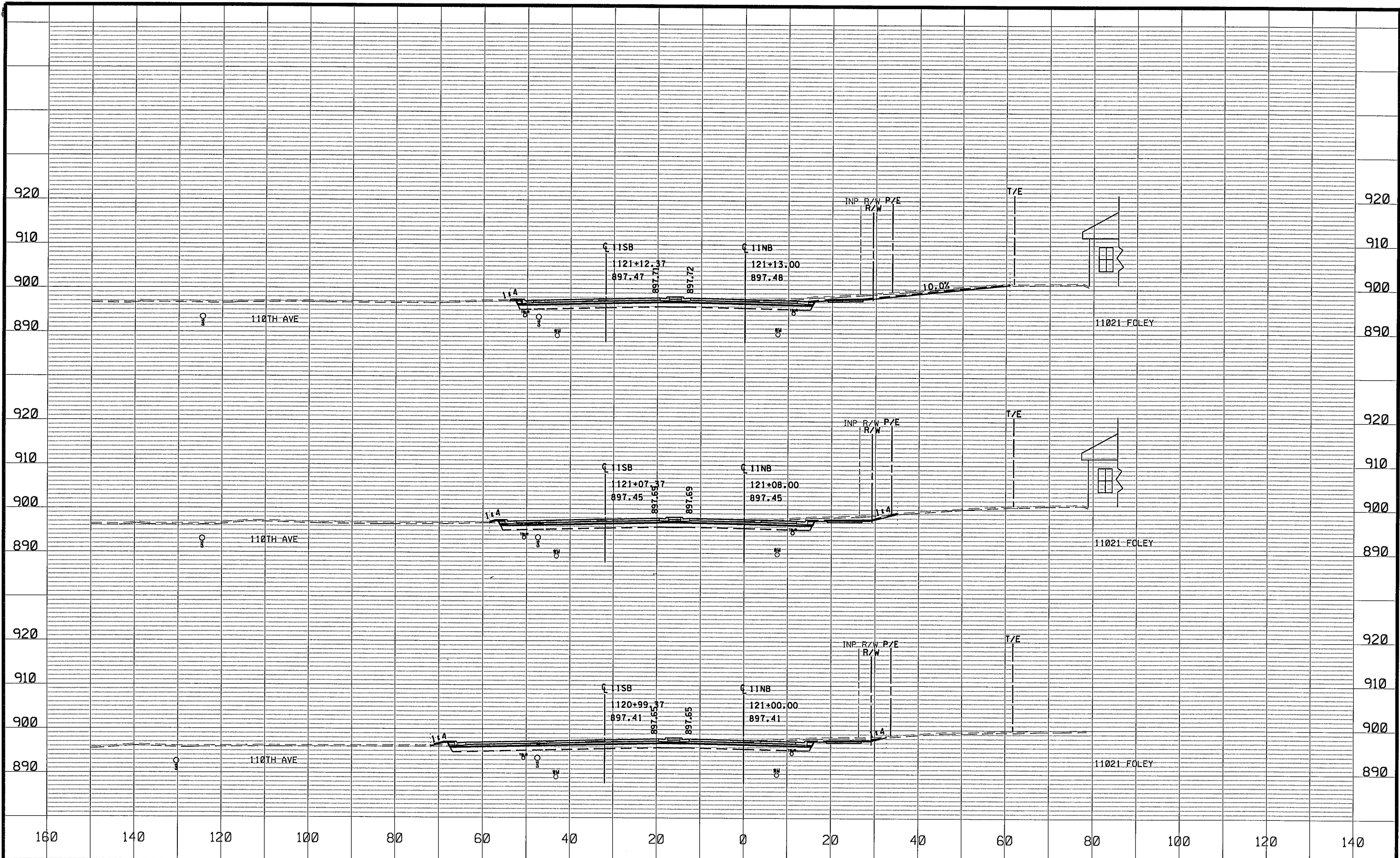
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DESIGN BY	DFE	DATE	02/27/16
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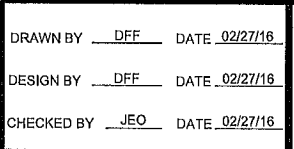
CROSS SECTIONS
STA 120+37.50 TO 120+86.32
Sheet 231 of 298 Sheets



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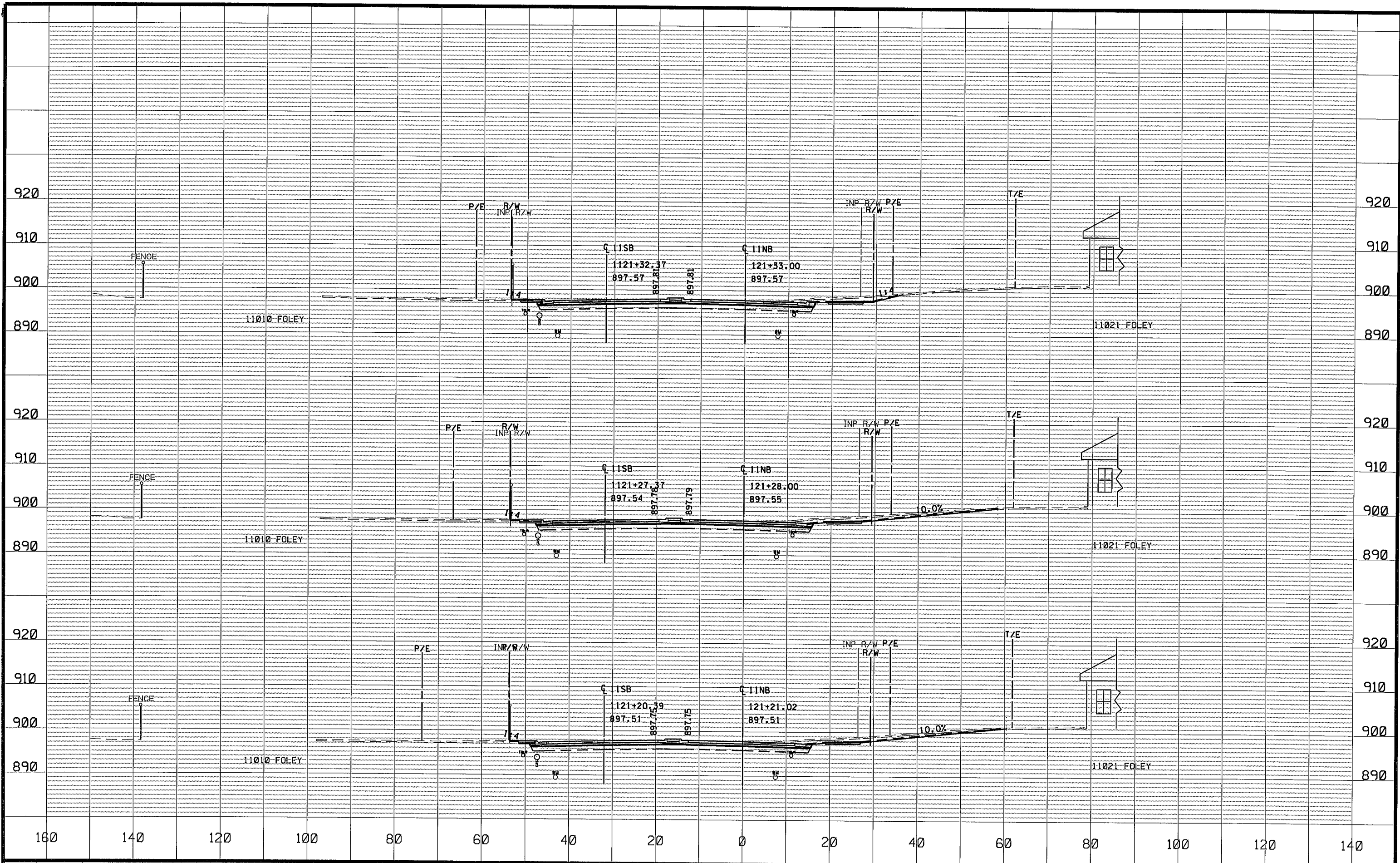
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CHECKED BY	JEO	DATE	02/27/16



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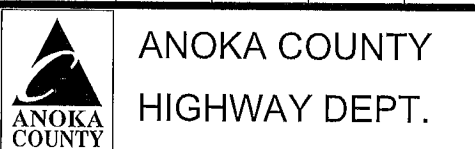
SP 002-611-034
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CROSS SECTIONS
 STA 121+00.00 TO 121+13.00
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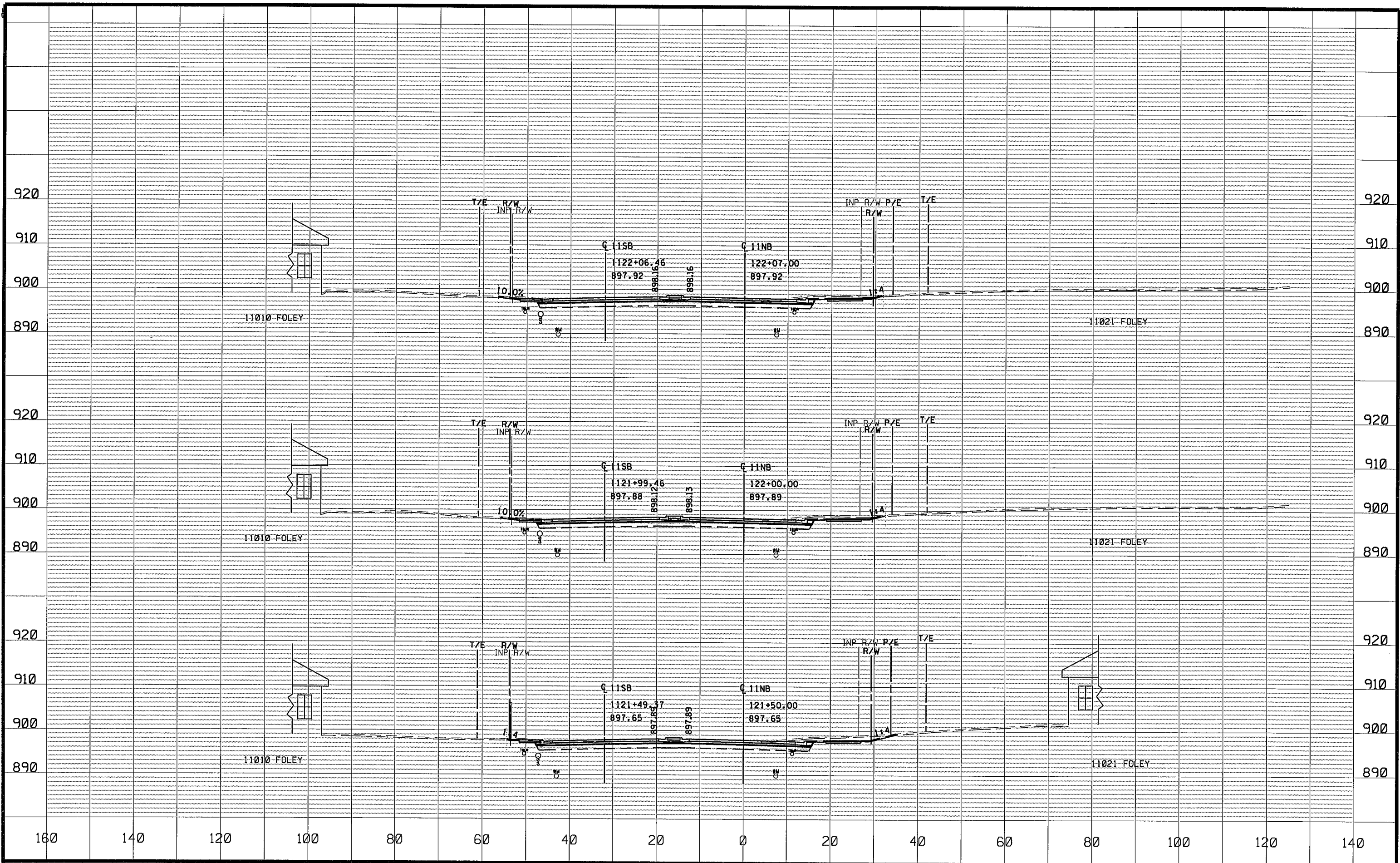
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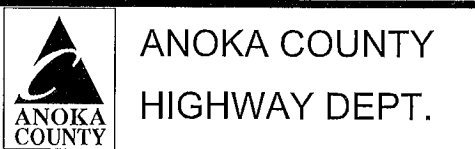
CROSS SECTIONS
 STA 121+21.02 TO 121+33.00
 Sheet 233 of 298 Sheets



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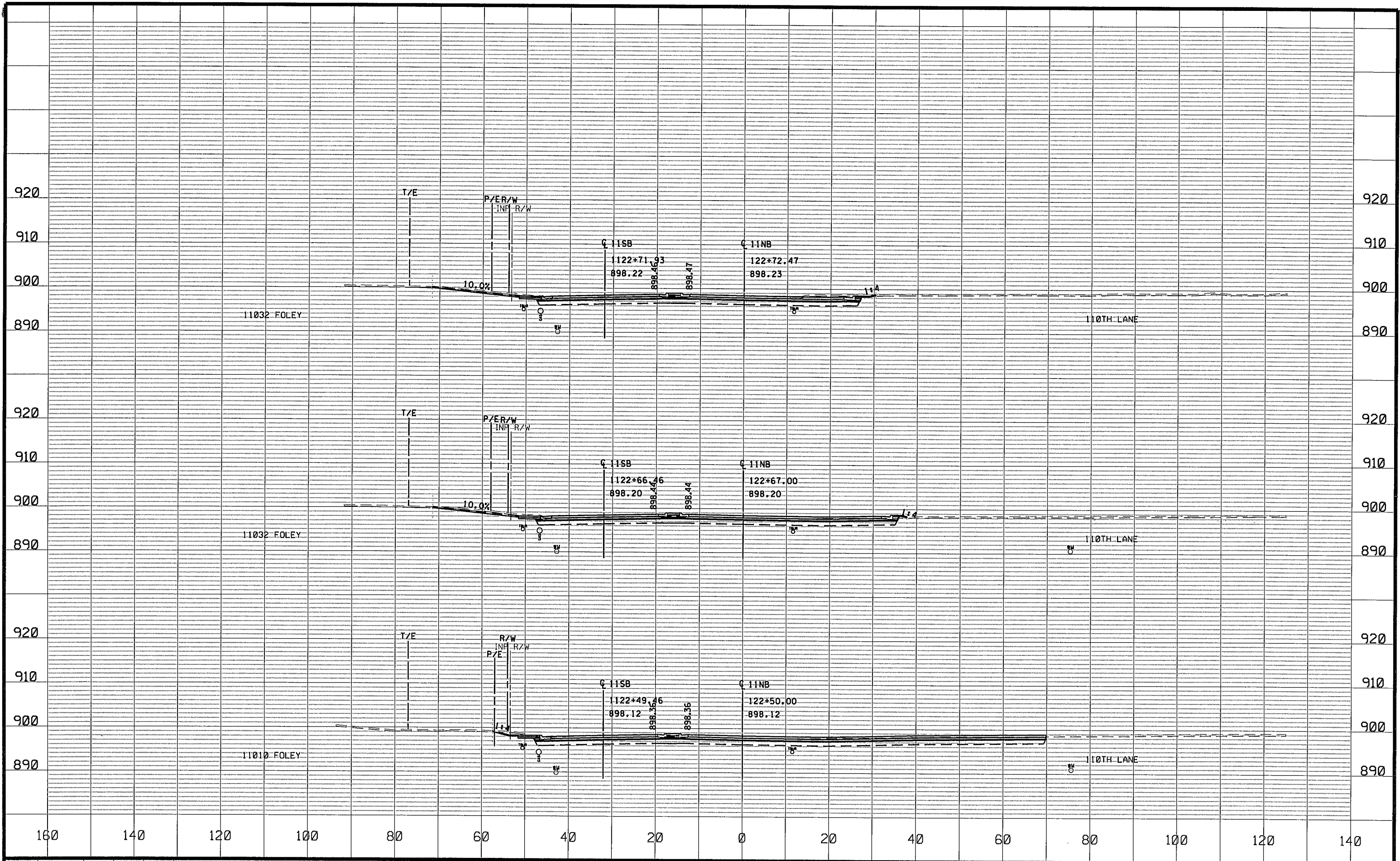
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 CHECKED BY JEO DATE 02/27/16



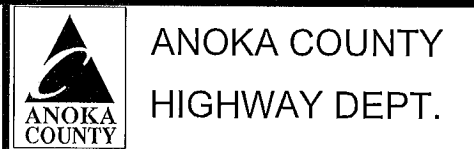
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CROSS SECTIONS
 STA 121+50.00 TO 122+07.00
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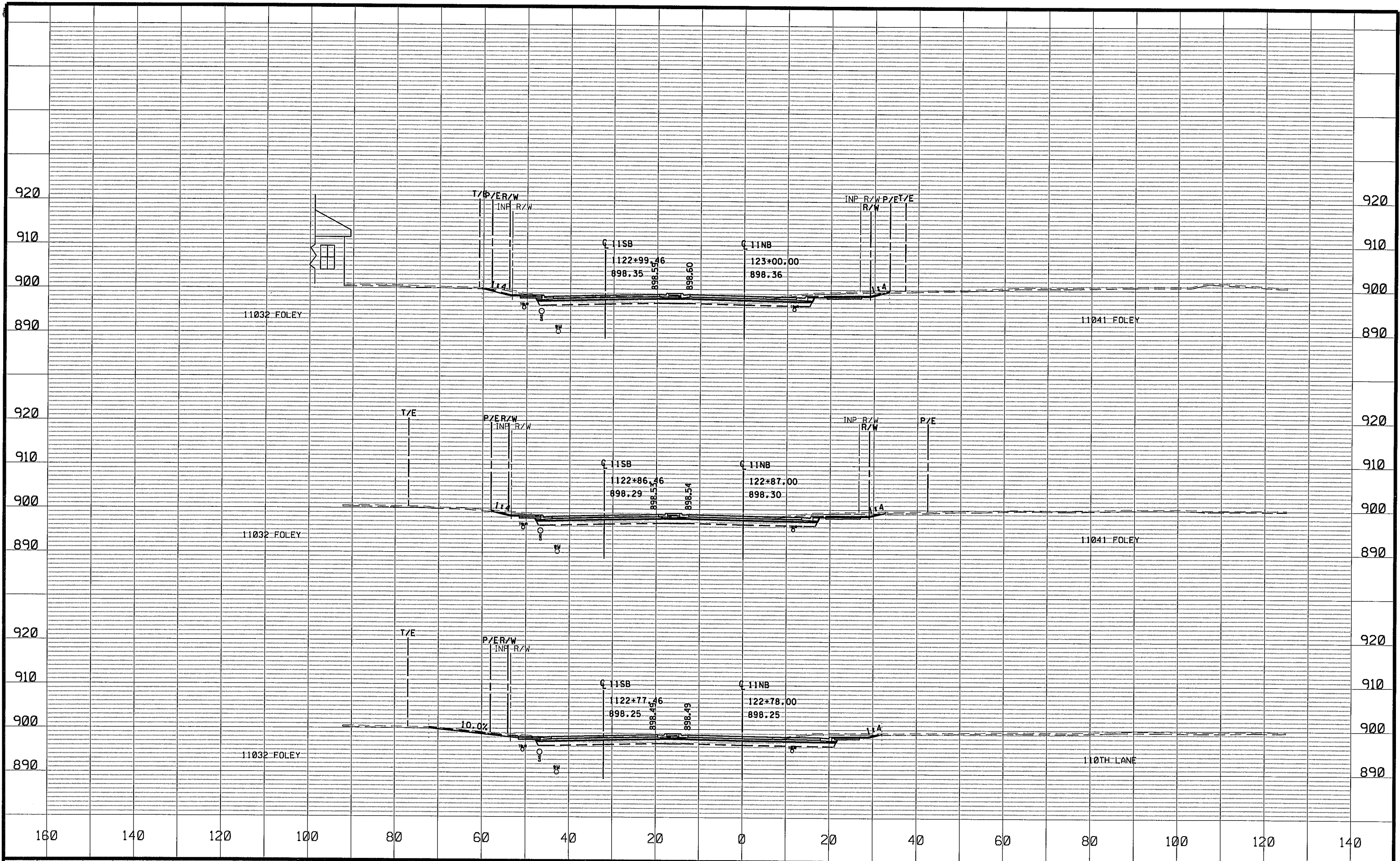
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CROSS SECTIONS
 STA 122+50.00 TO 122+72.47
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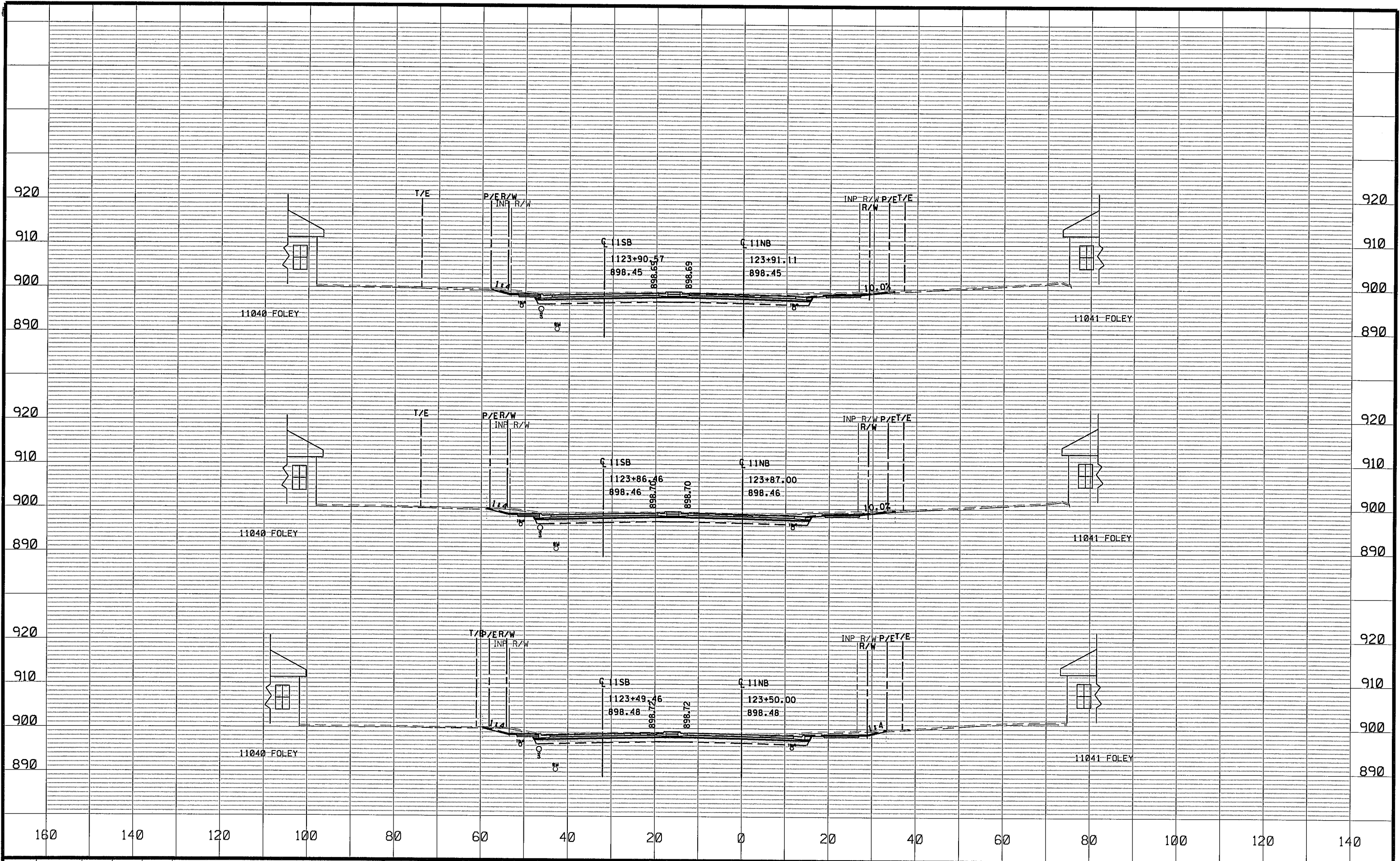
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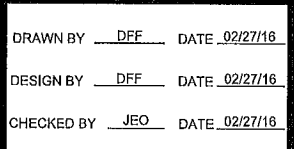
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CROSS SECTIONS
STA 122+78.00 TO 123+00.00
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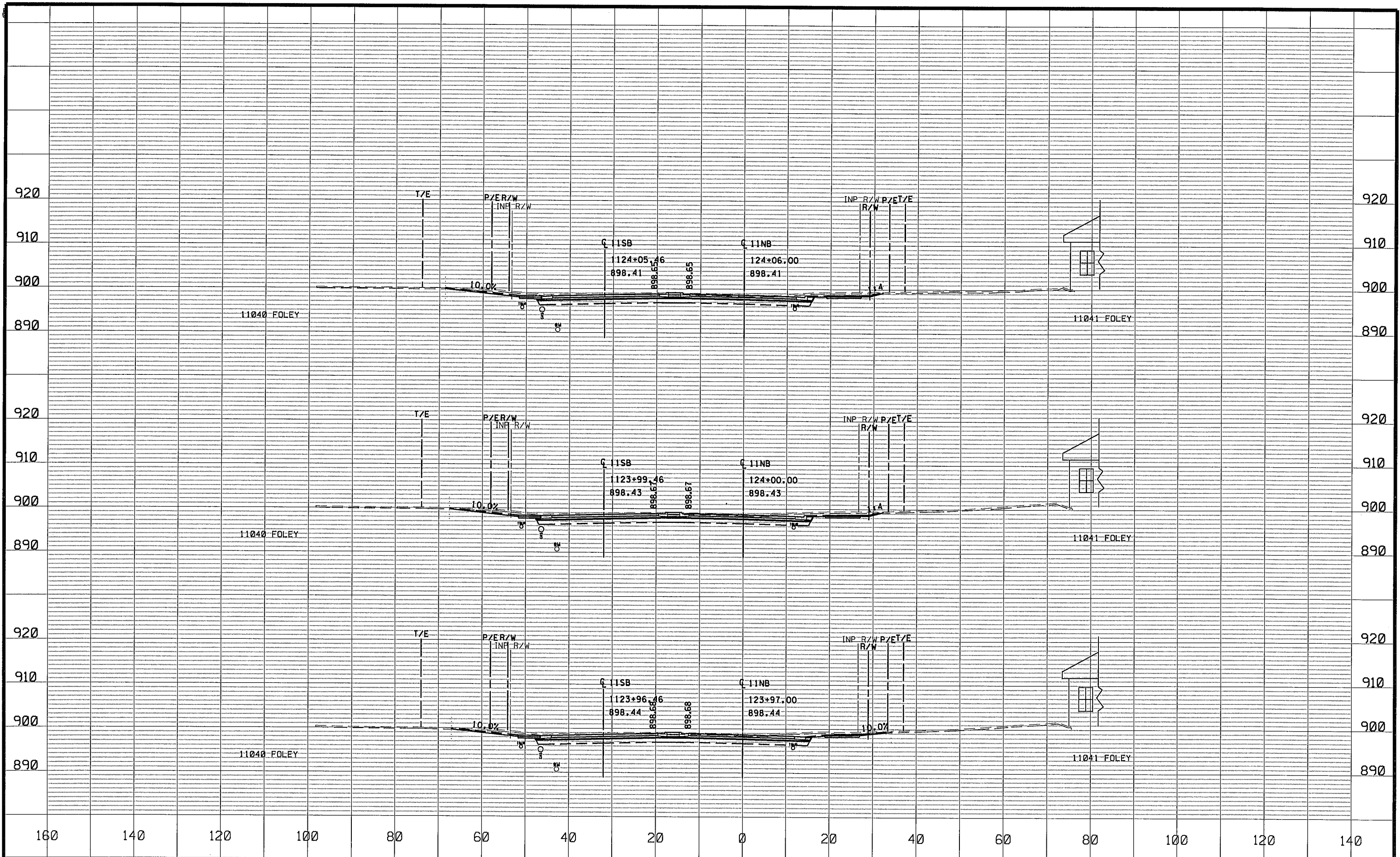
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DESIGN BY	DFP	DATE	02/27/16
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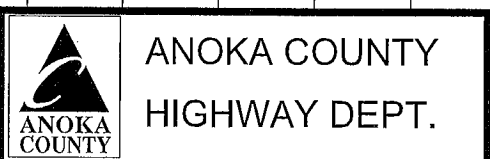
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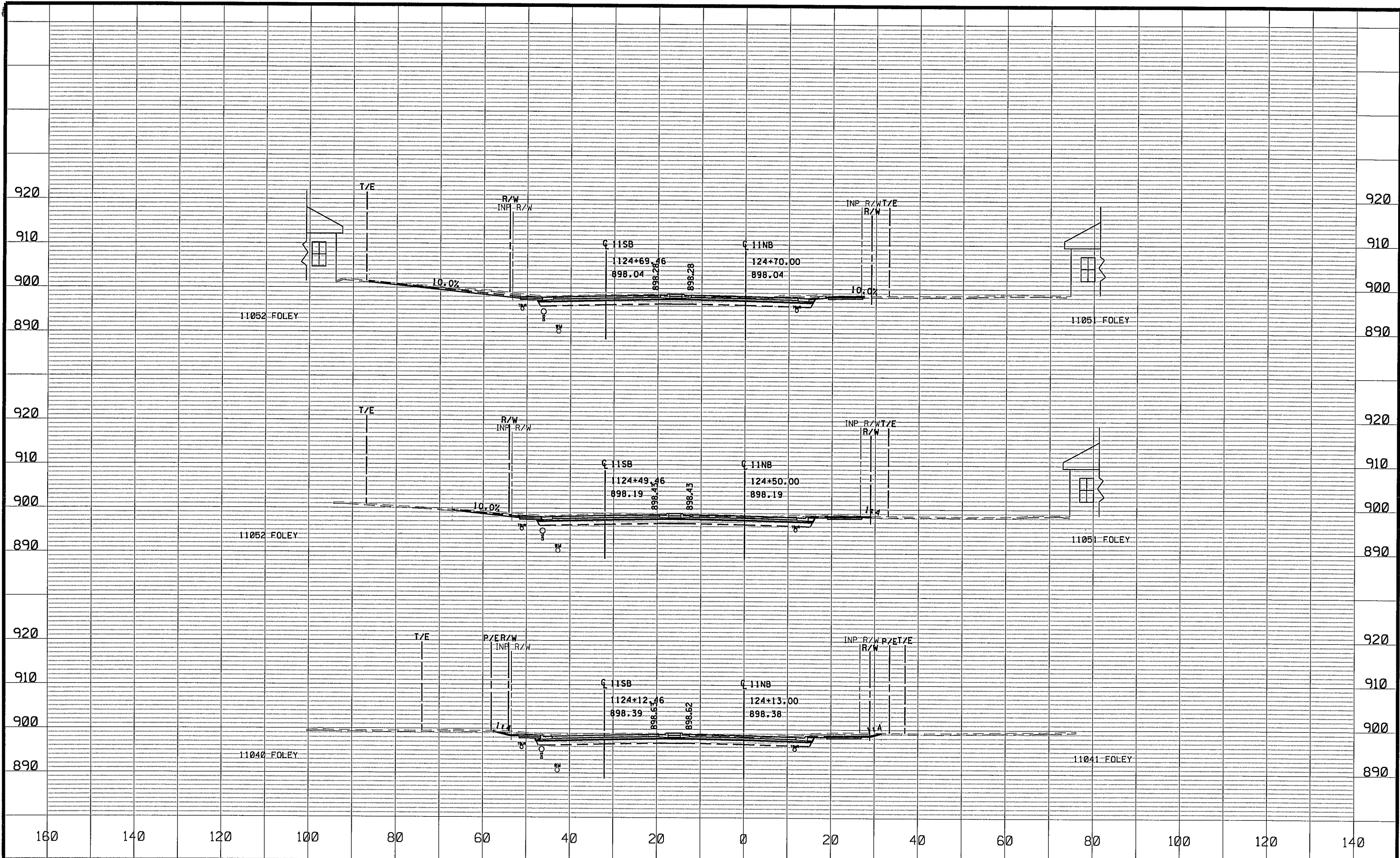
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 CHECKED BY JEO DATE 02/27/16



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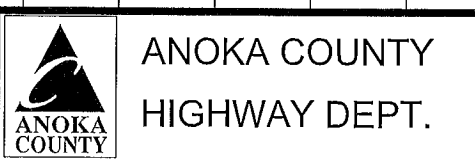
CROSS SECTIONS
 STA 123+97.00 TO 124+06.00
 Sheet 238 of 298 Sheets



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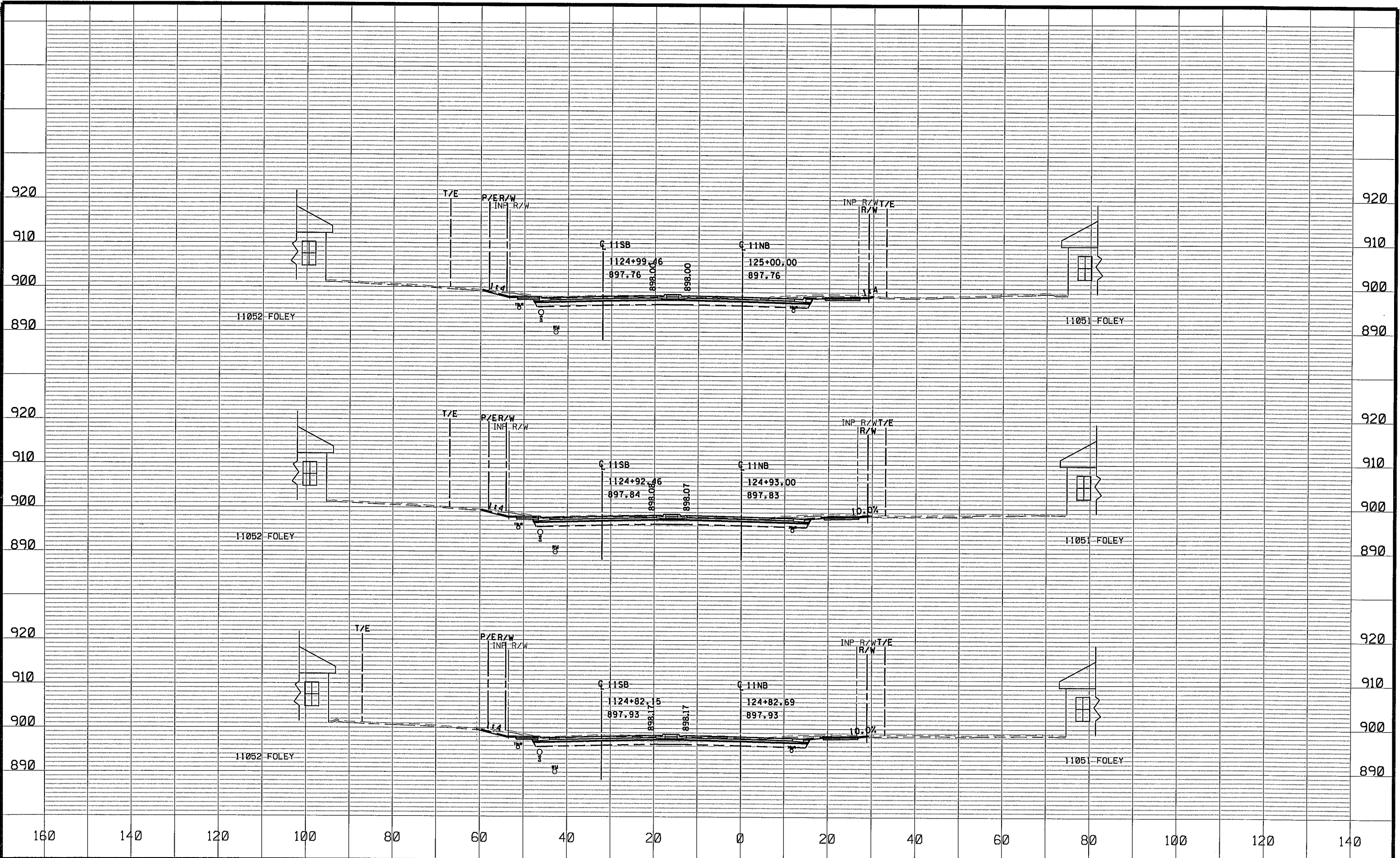
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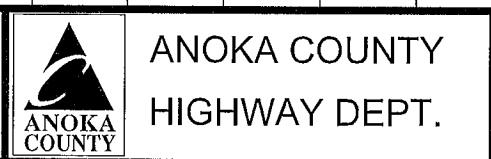
CROSS SECTIONS
 STA 124+13.00 TO 124+70.00
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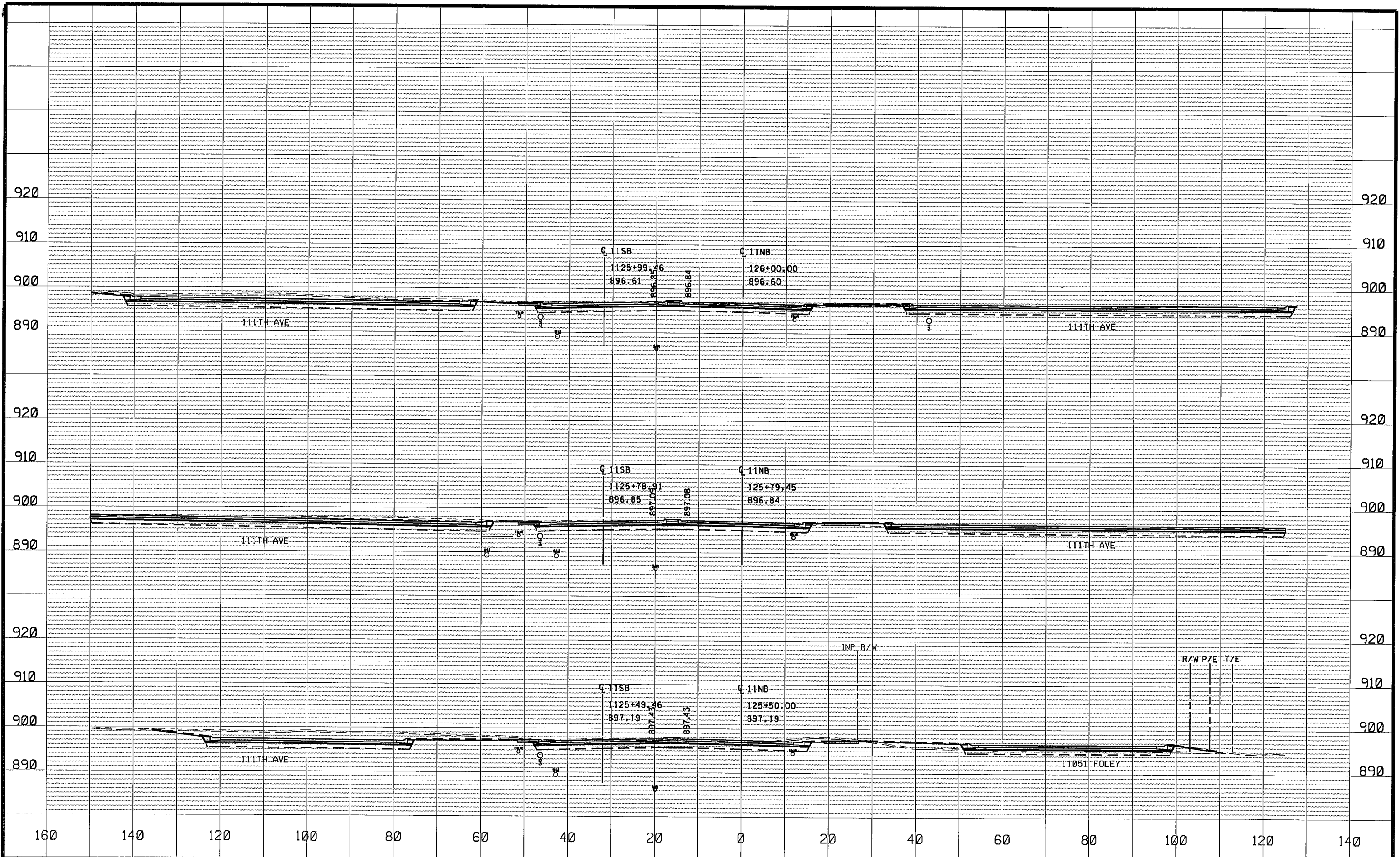
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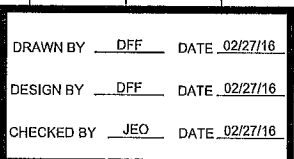
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CROSS SECTIONS
 STA 124+82.69 TO 125+00.00
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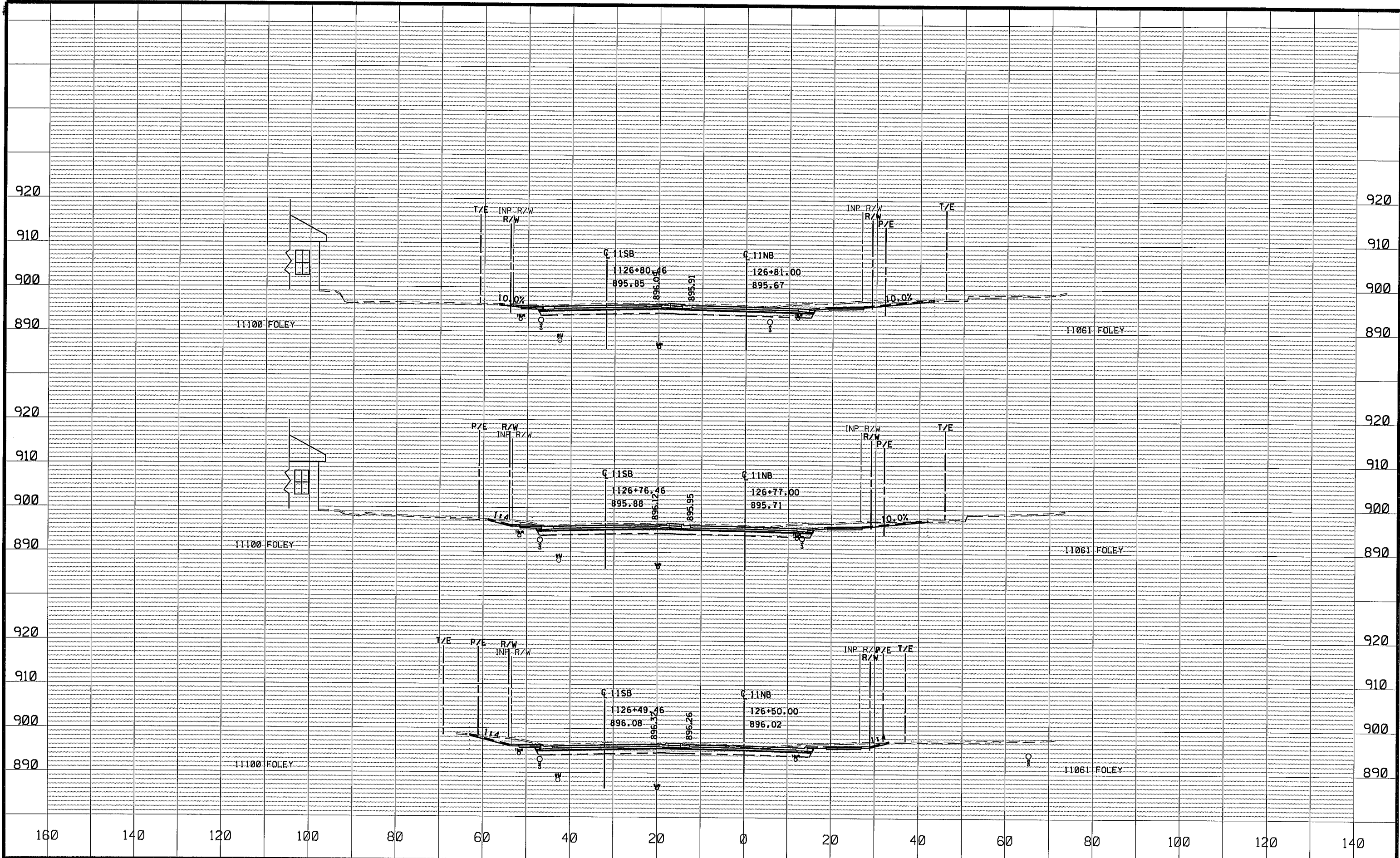
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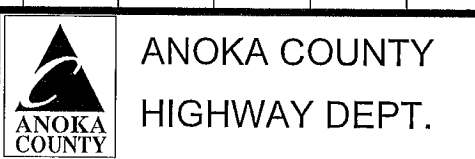
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CROSS SECTIONS
 STA 125+50.00 TO 126+00.00
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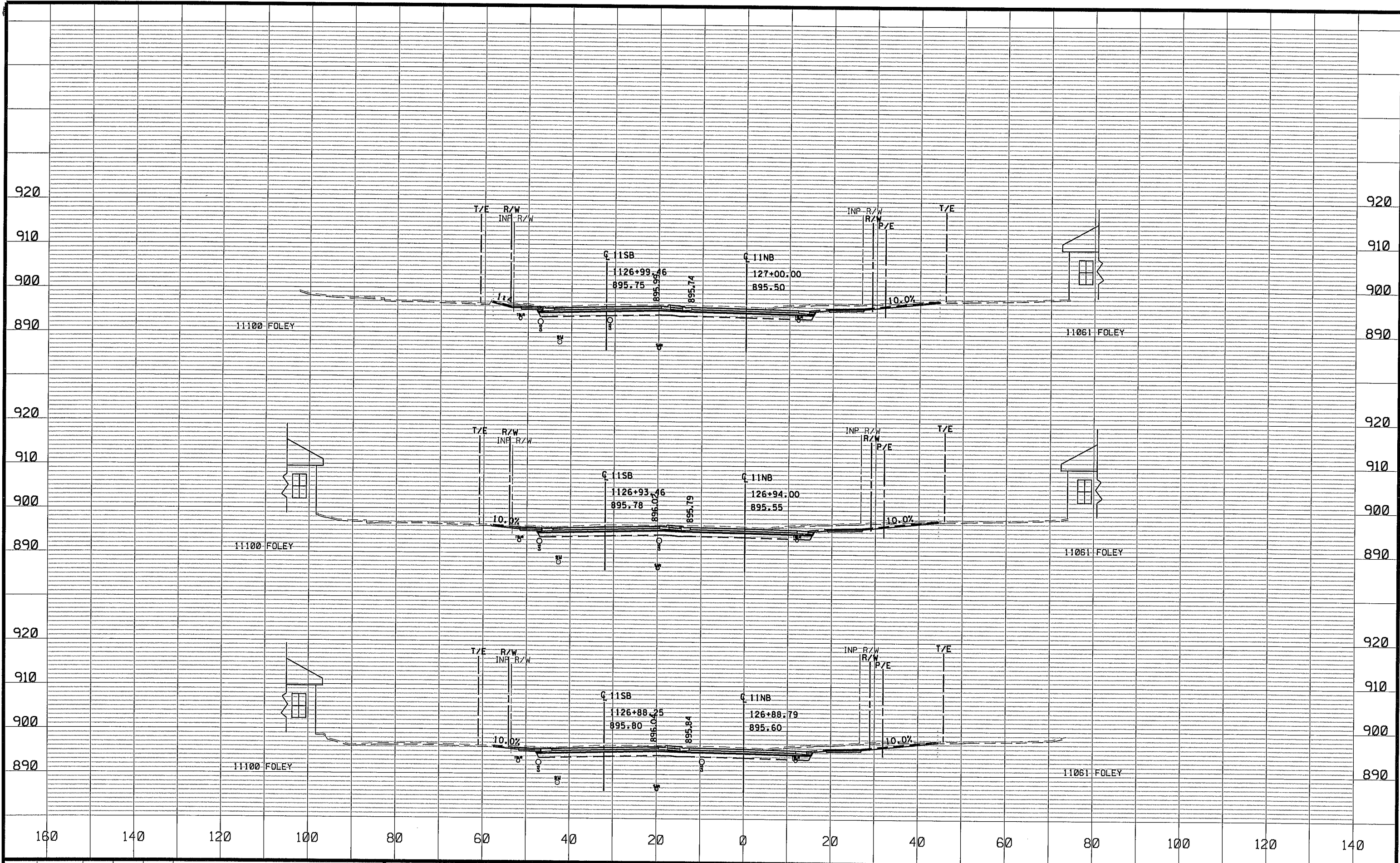
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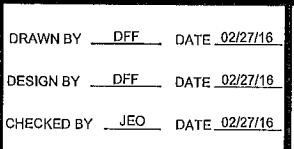
CROSS SECTIONS
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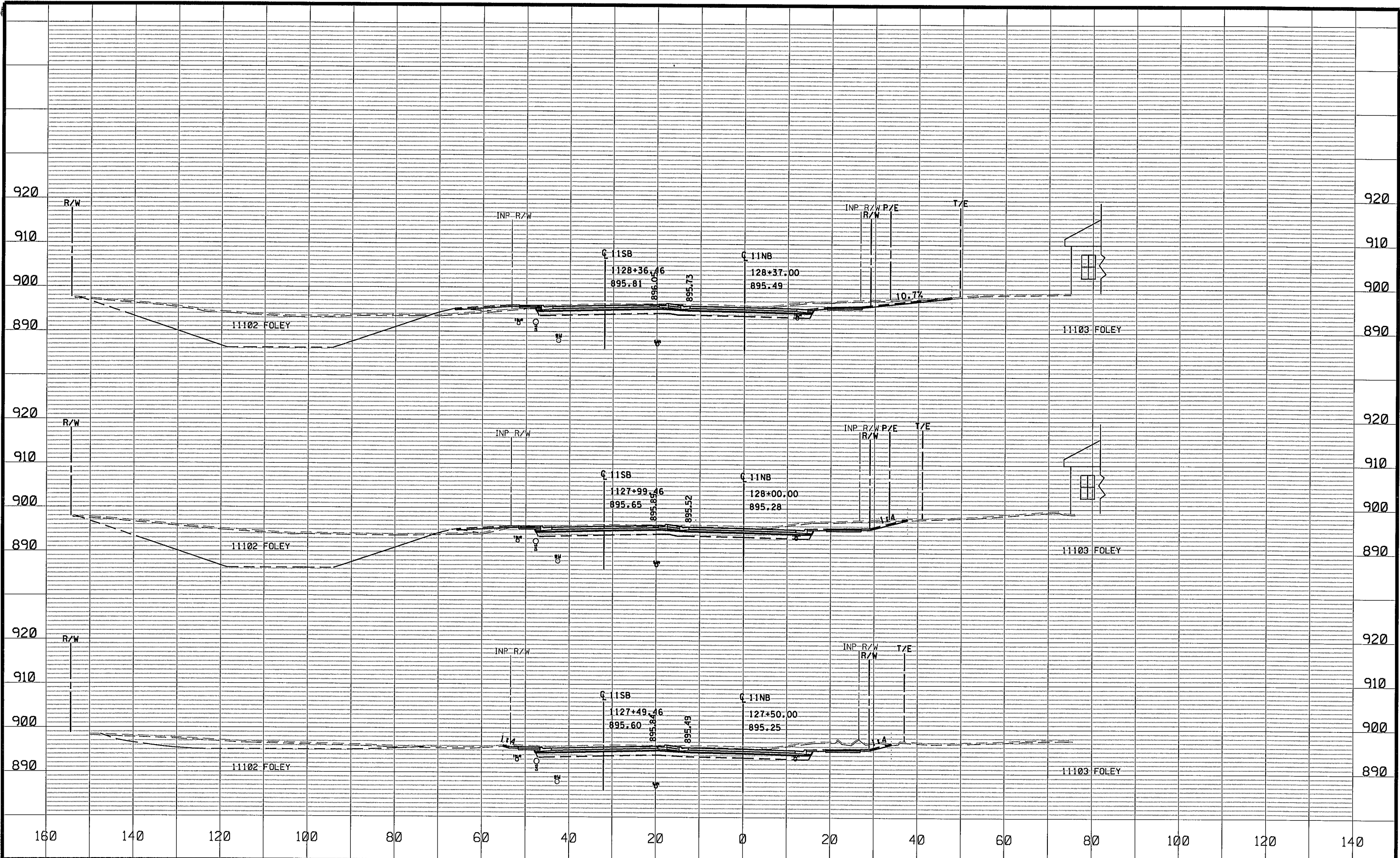
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DESIGN BY	DFP	DATE	02/27/16
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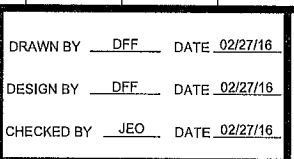
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CROSS SECTIONS
STA 126+88.79 TO 127+00.00
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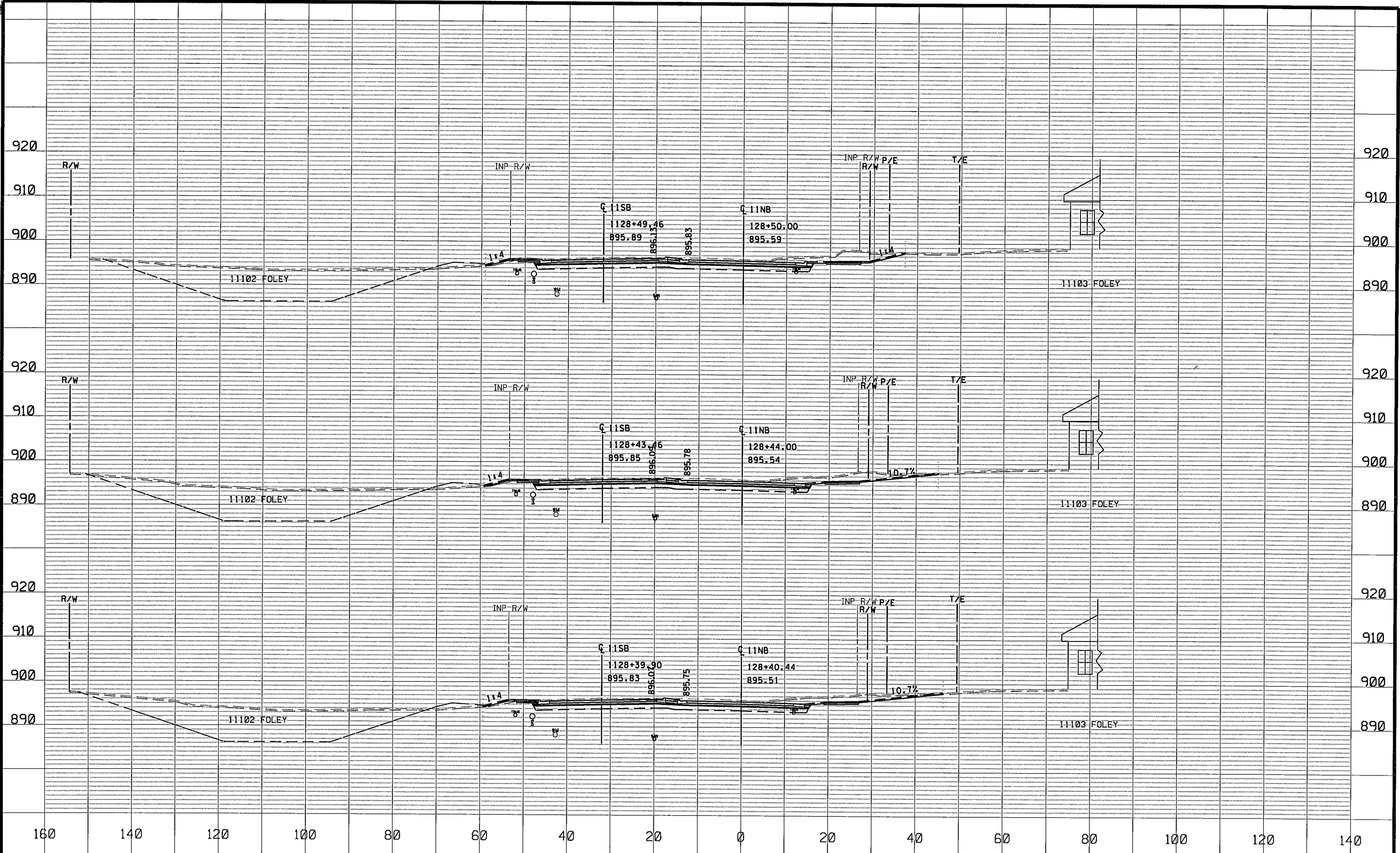
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CHECKED BY	JEO	DATE	02/27/16



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CROSS SECTIONS
 STA 127+50.00 TO 128+37.00
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NO	DATE	BY	CKD	APPR	REVISION
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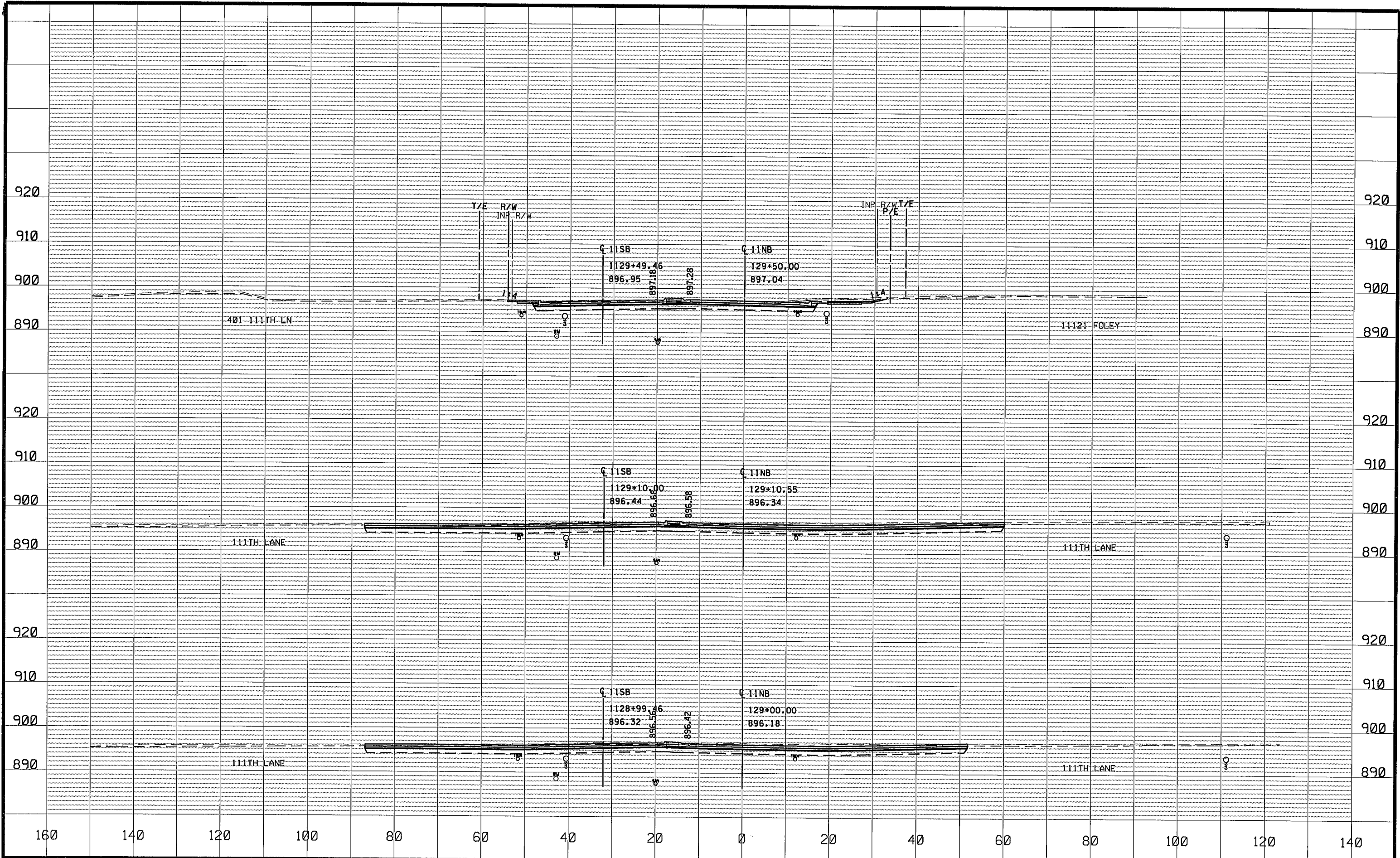
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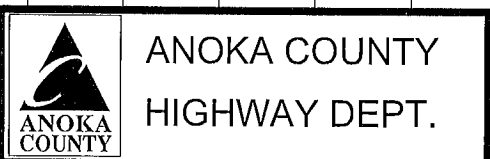
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CROSS SECTIONS
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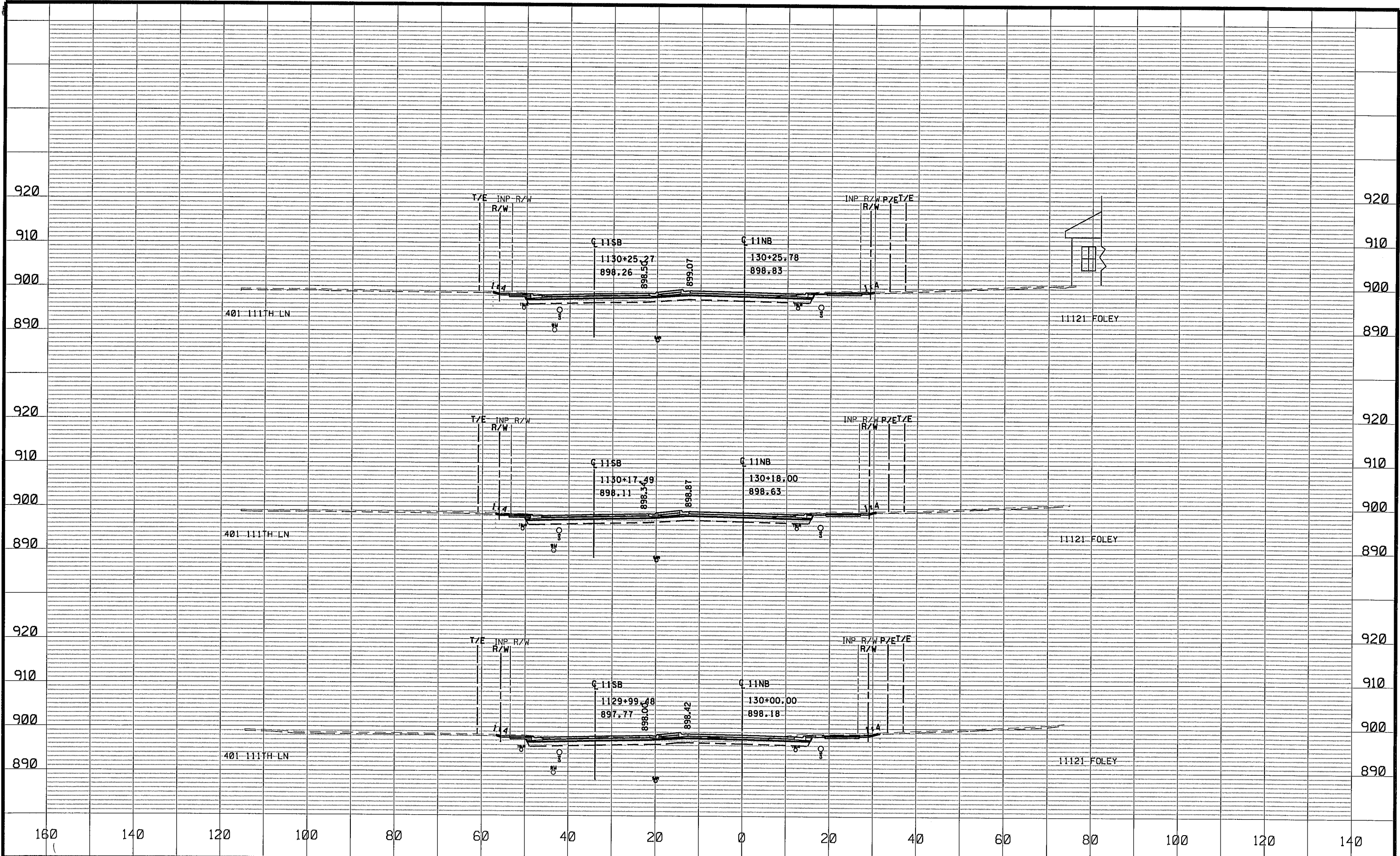
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 DESIGN BY DFF DATE 02/27/16
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CROSS SECTIONS
 STA 129+00.00 TO 129+50.00
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NO	DATE	BY	CKD	APPR	REVISION
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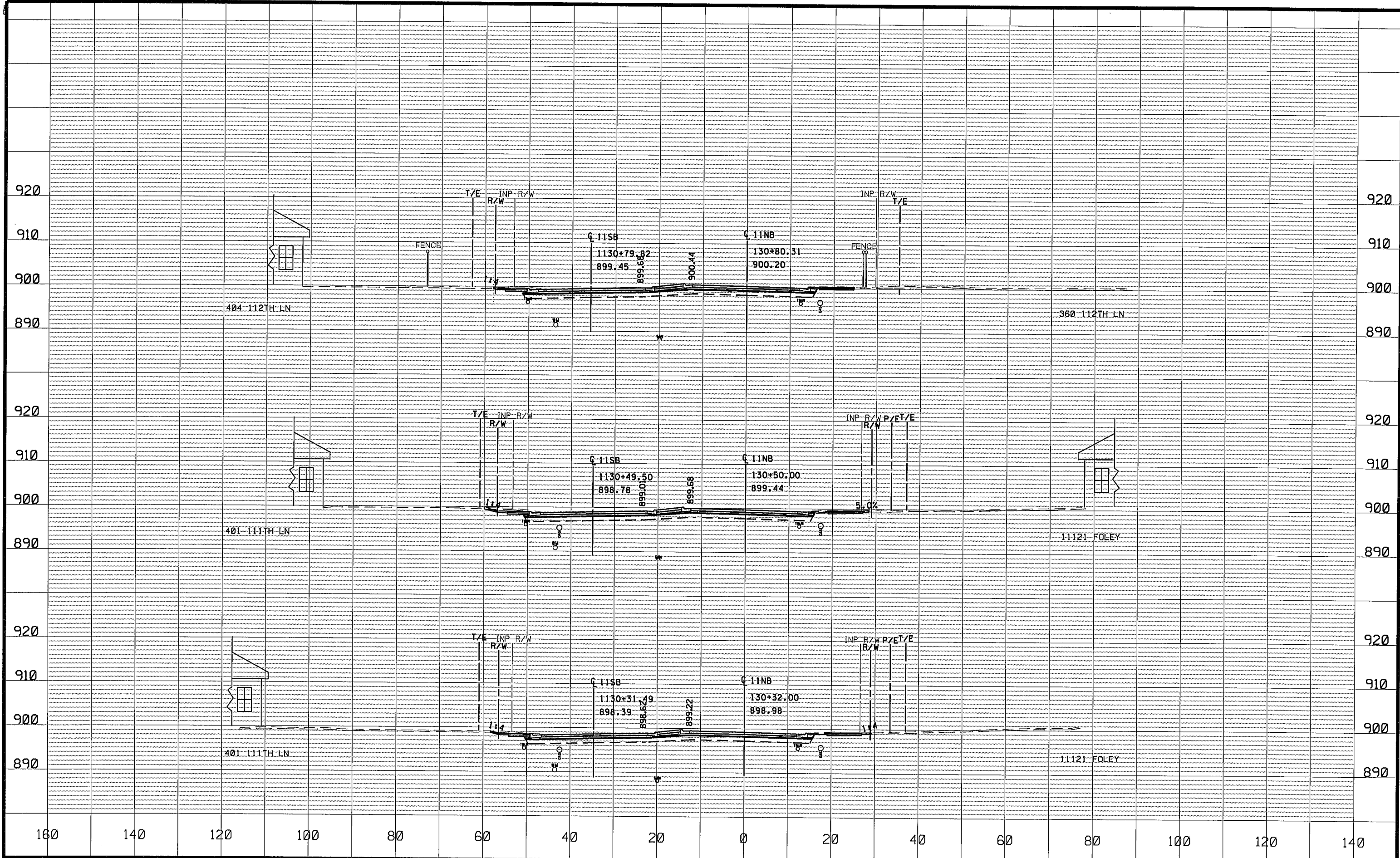
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 DESIGN BY DFF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



ANOKA COUNTY
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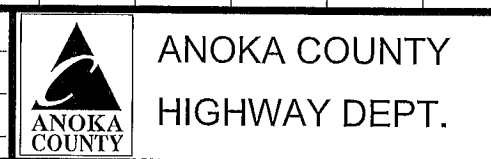
SP 002-611-034
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CROSS SECTIONS
 STA 130+00.00 TO 130+25.78
 Sheet 247 of 298 Sheets



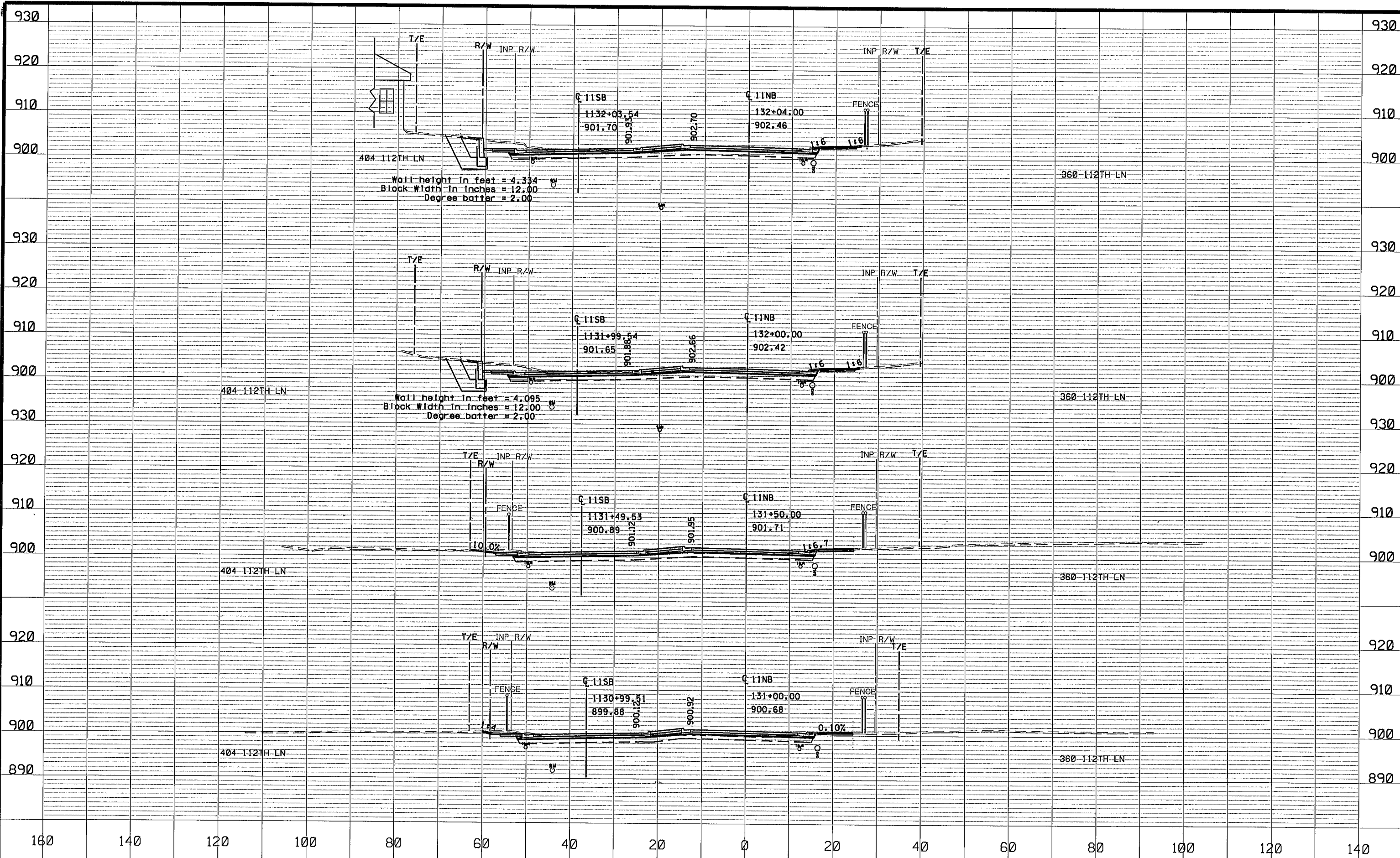
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 CHECKED BY JEO DATE 02/27/16



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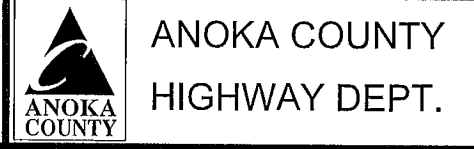
CROSS SECTIONS
 STA 130+32.00 TO 130+80.31
 Sheet 248 of 298 Sheets



NO	DATE	BY	CHKD	APPR	REVISION

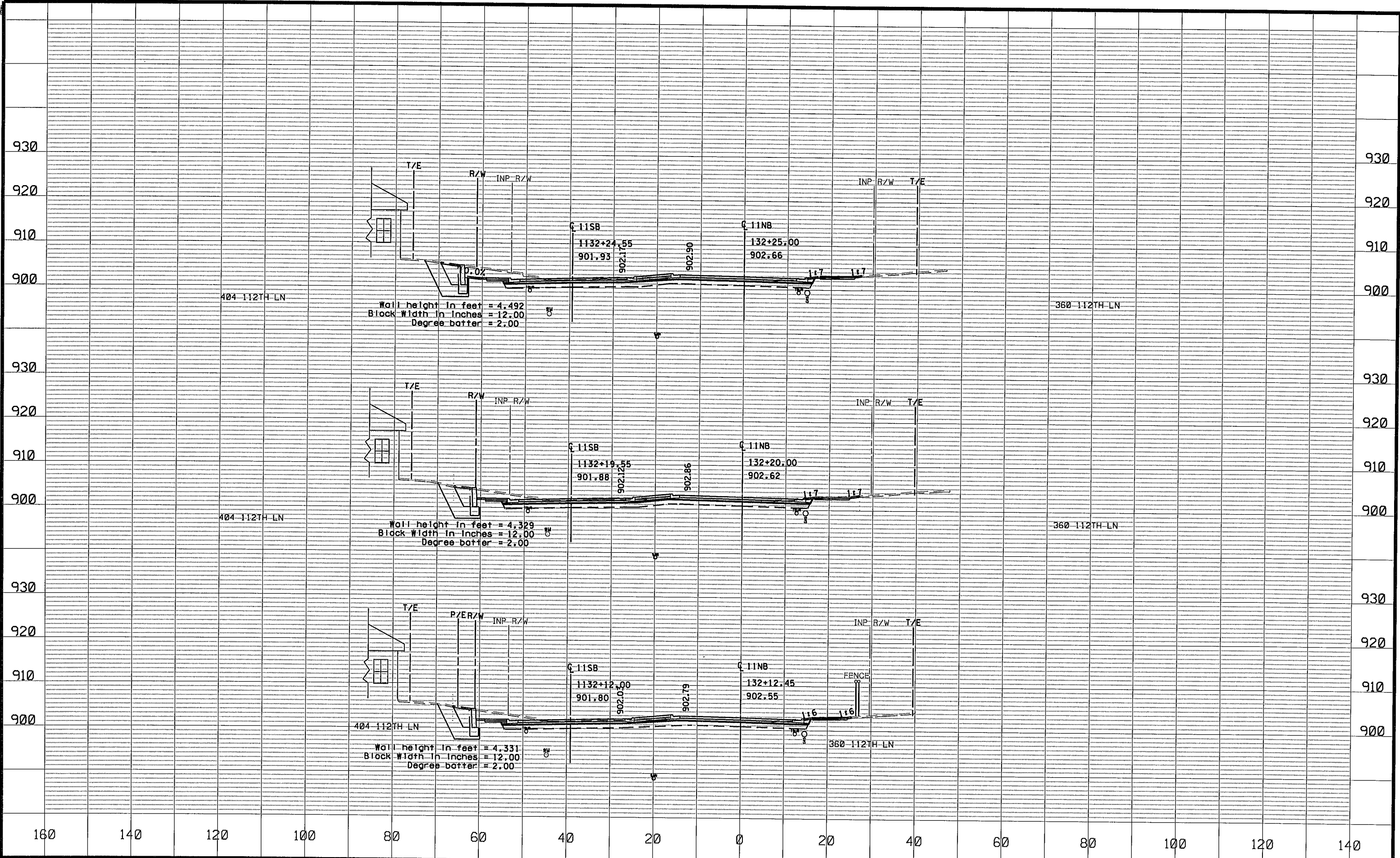
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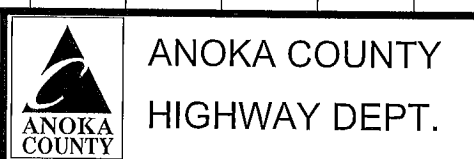
CROSS SECTIONS
 STA 131+00.00 TO 132+04.00
 Sheet 249 of 298 Sheets



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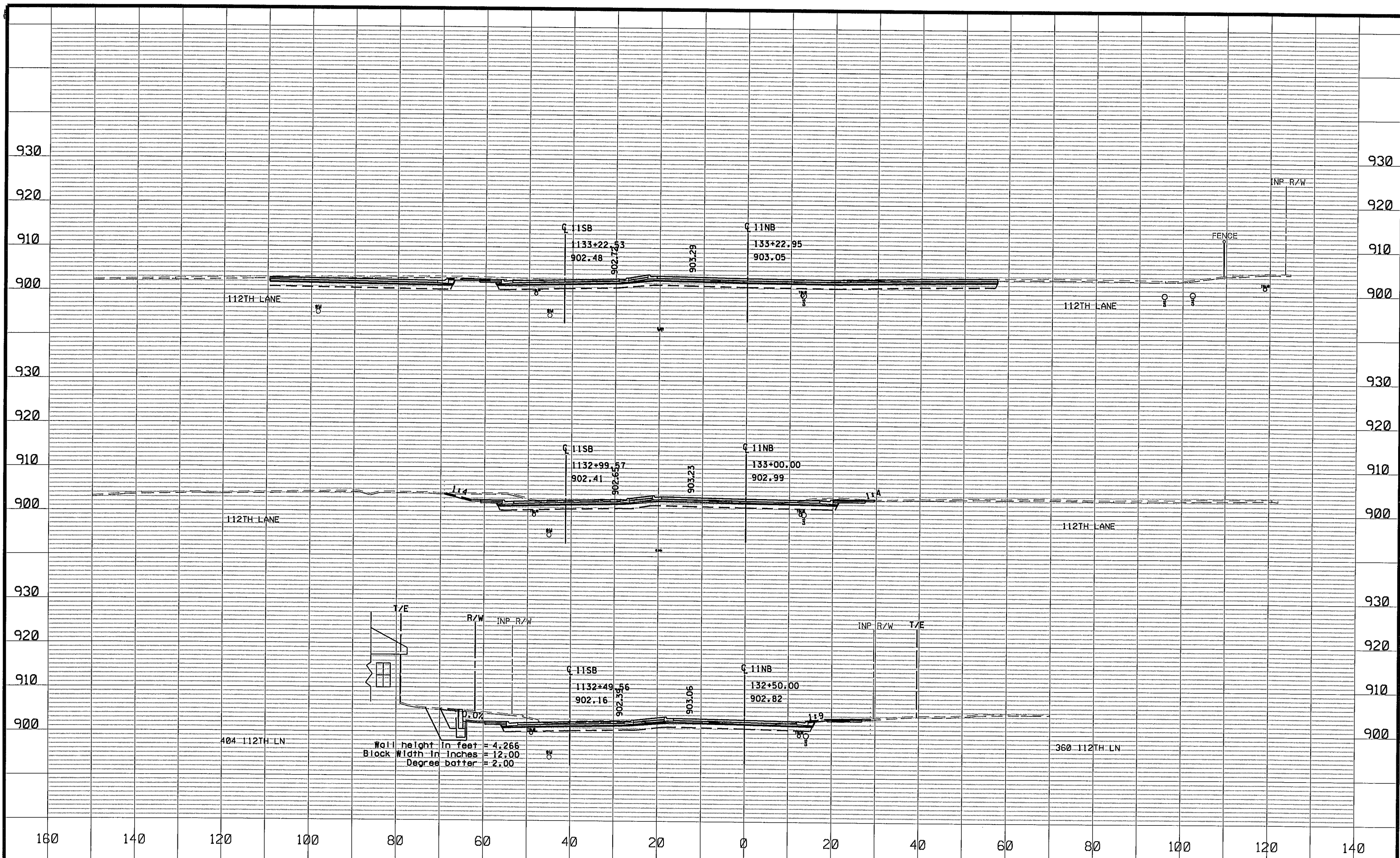
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CROSS SECTIONS
 STA 132+12.45 TO 132+25.00
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NO	DATE	BY	CHKD	APPR	REVISION
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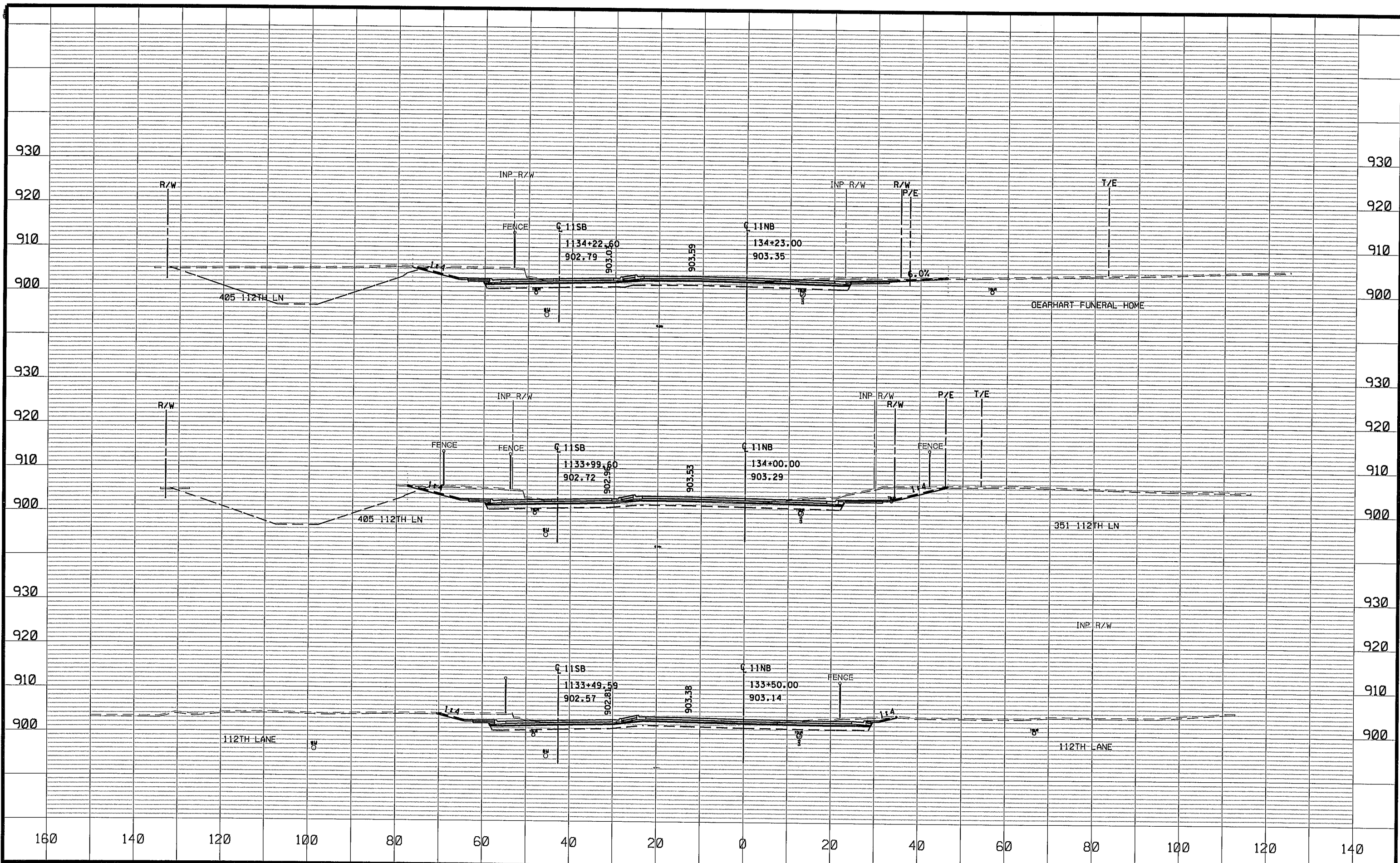
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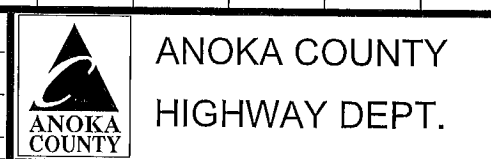
SP 002-611-034
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CROSS SECTIONS
 STA 132+50.00 TO 133+22.95
 Sheet 251 of 298 Sheets



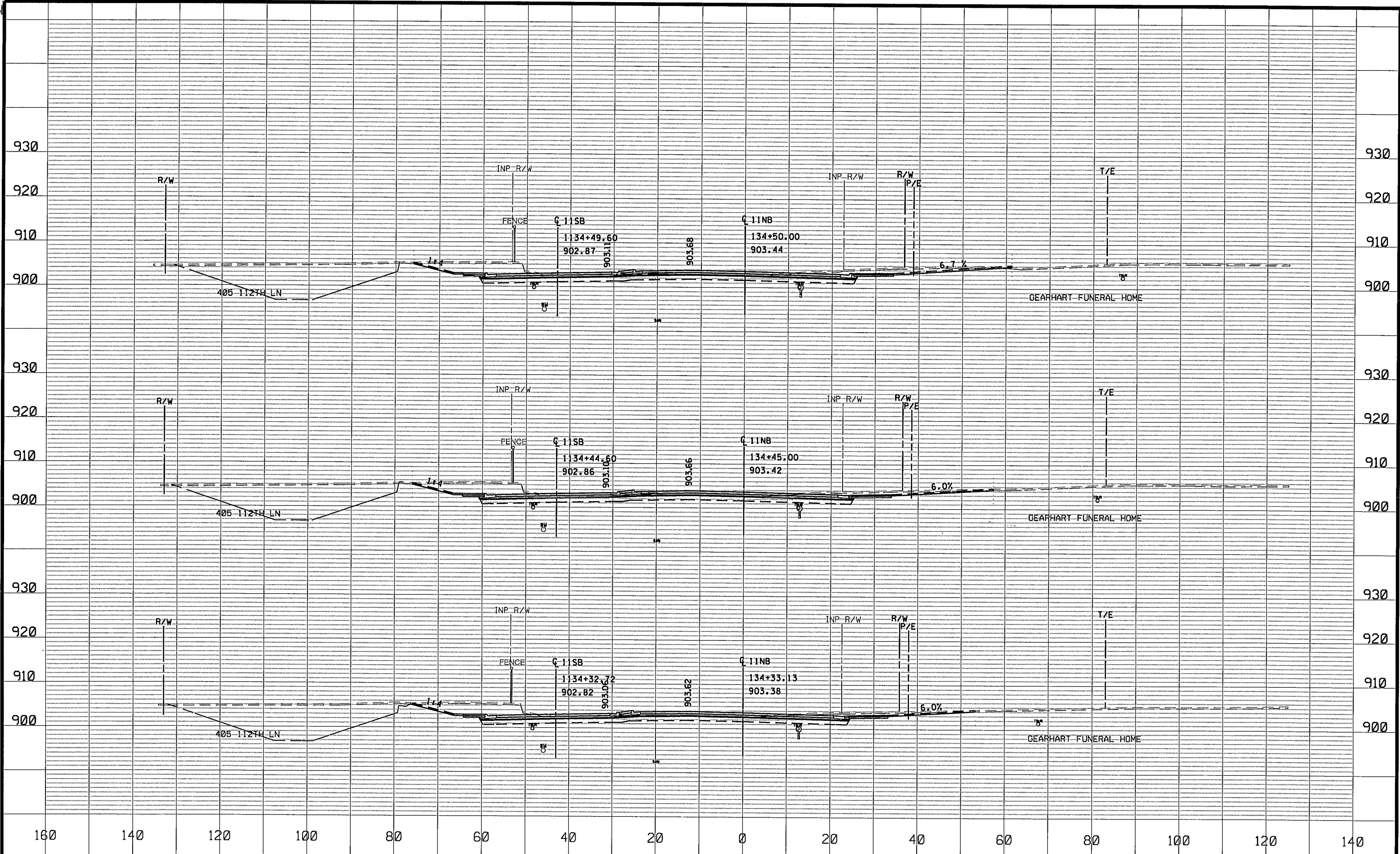
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CROSS SECTIONS
 STA 133+50.00 TO 134+23.00
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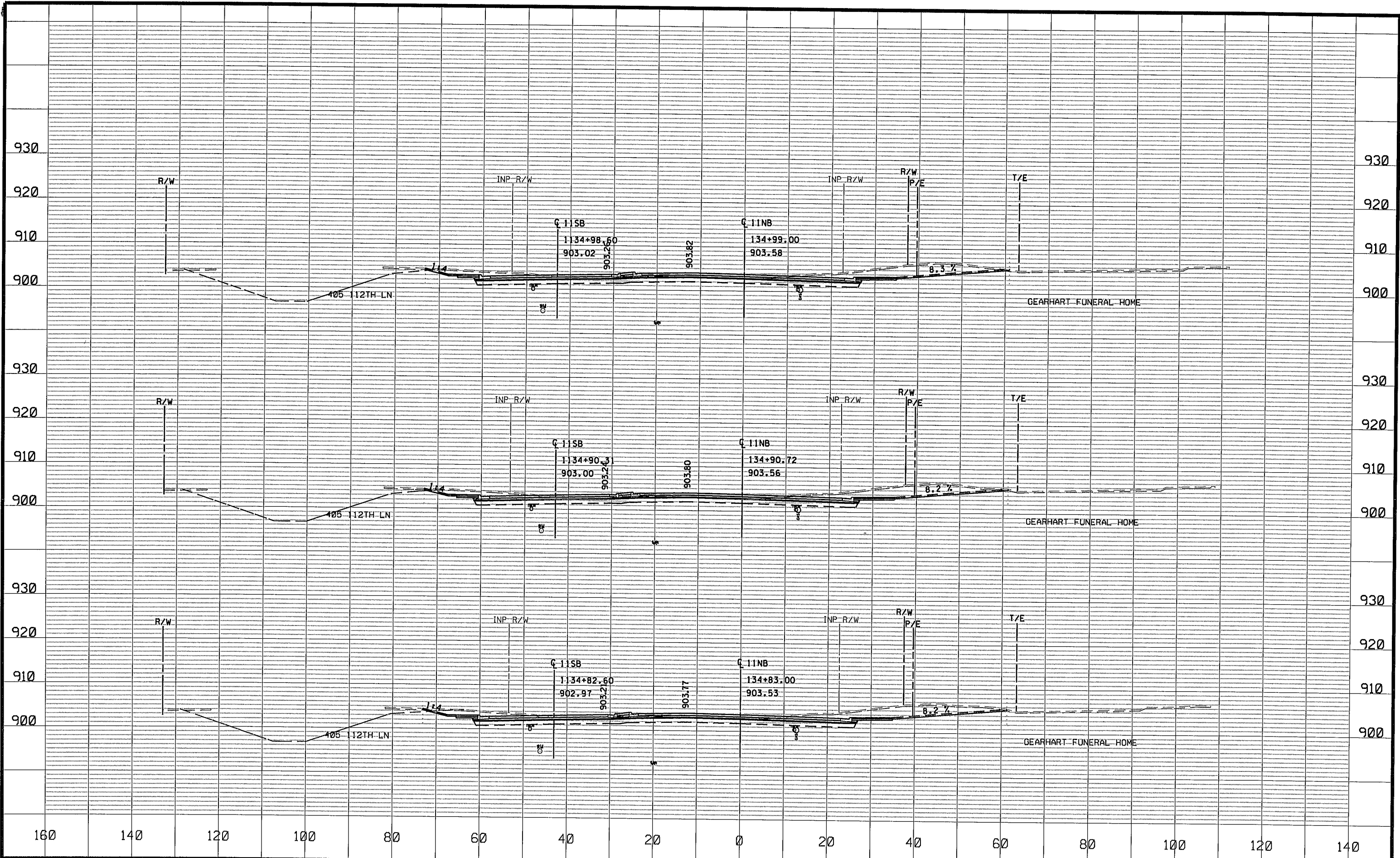
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 DESIGN BY DFF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16

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HIGHWAY DEPT.

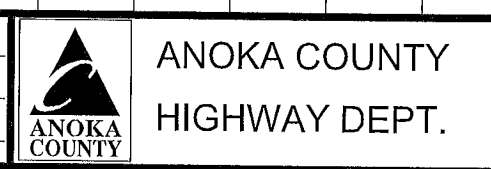
SP 002-611-034
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CROSS SECTIONS
 STA 134+33.13 TO 134+50.00
 Sheet 253 of 298 Sheets



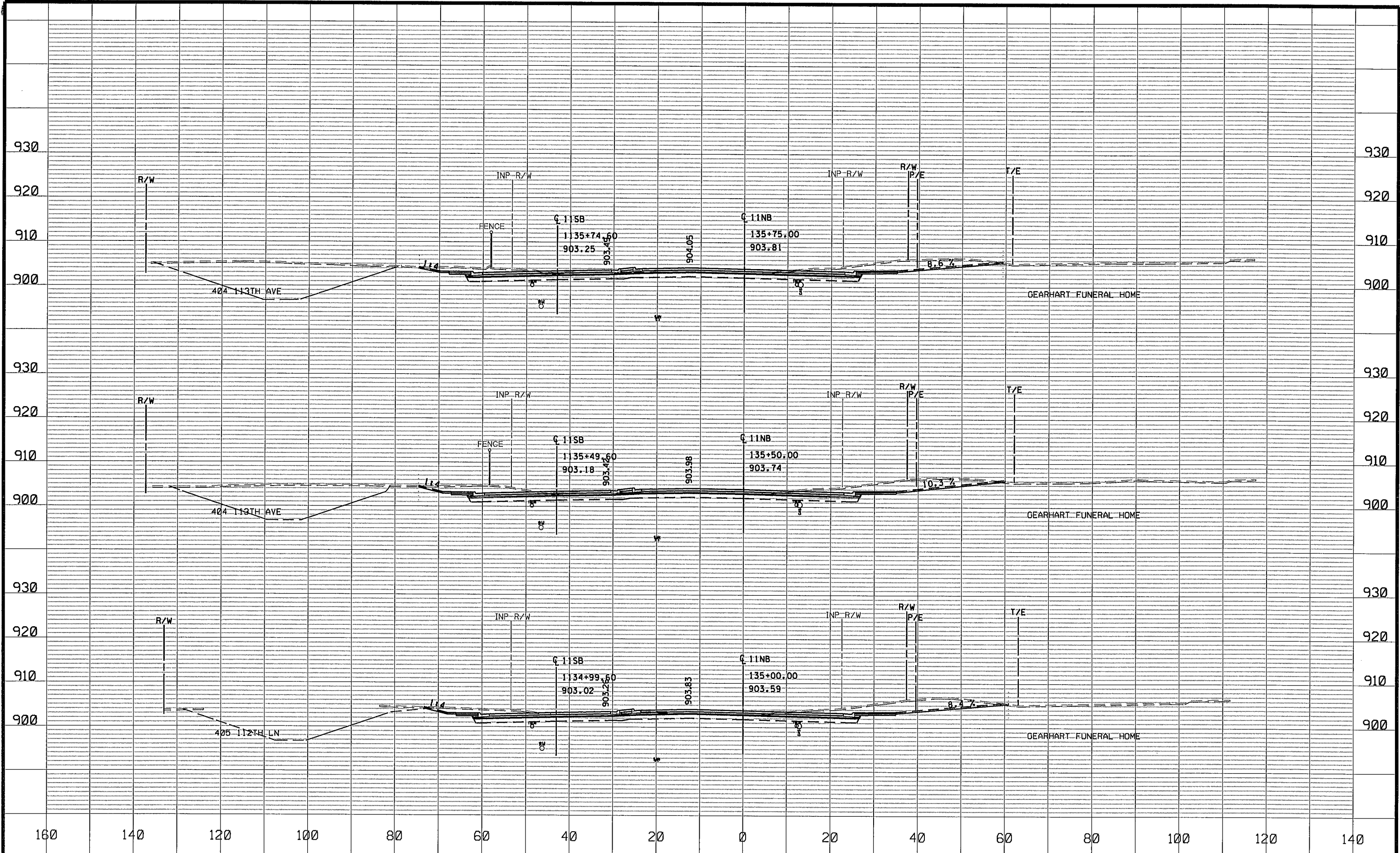
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 CHECKED BY JEO DATE 02/27/16



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CROSS SECTIONS
 STA 134+83.00 TO 134+99.00
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NO	DATE	BY	CKD	APPR	REVISION

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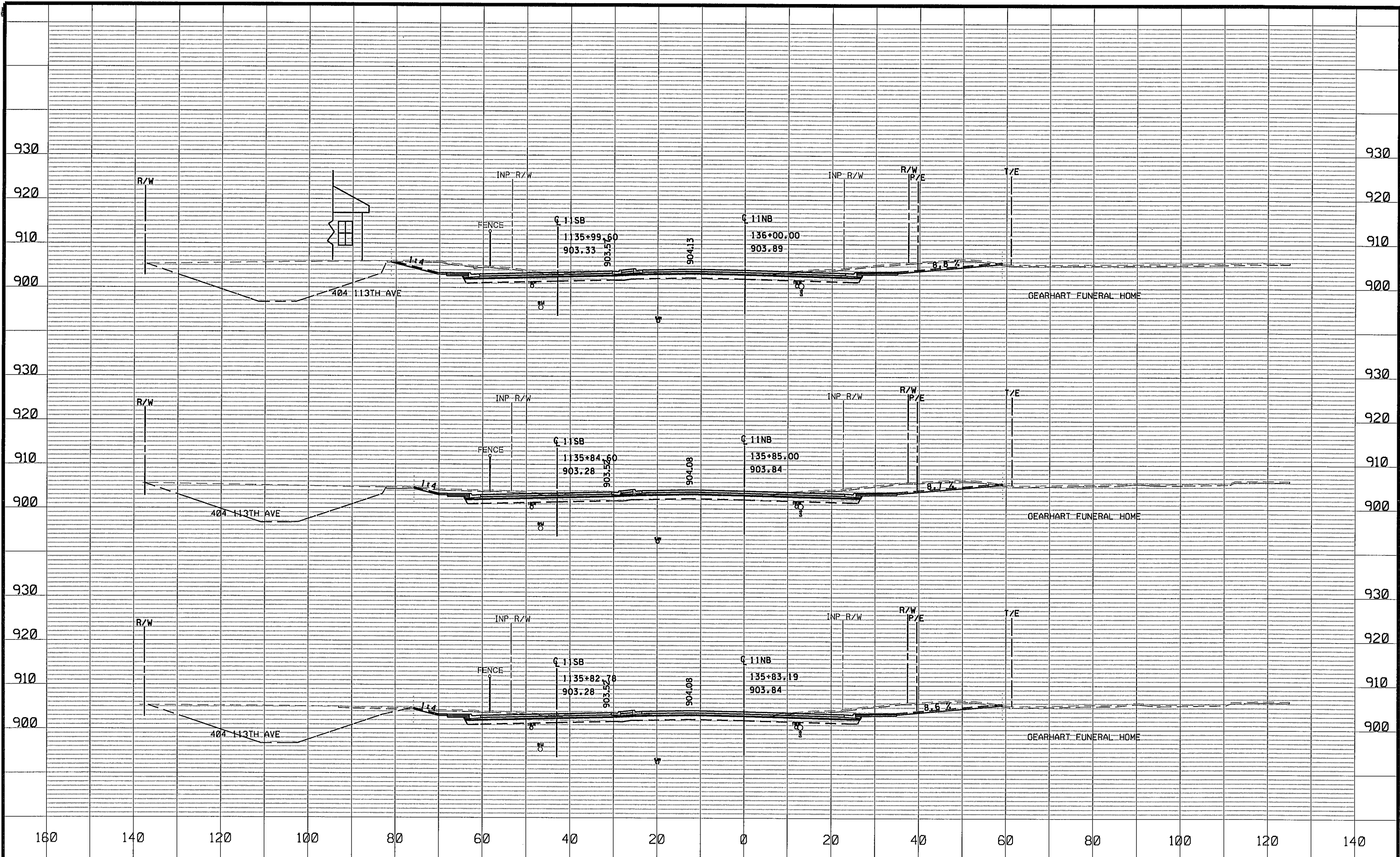
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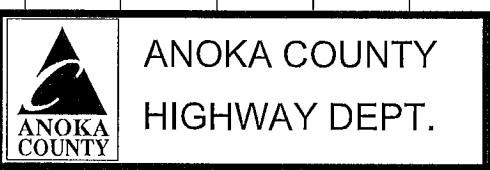
SP 002-611-034
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CROSS SECTIONS
 STA 135+00.00 TO 135+75.00
 Sheet 255 of 298 Sheets



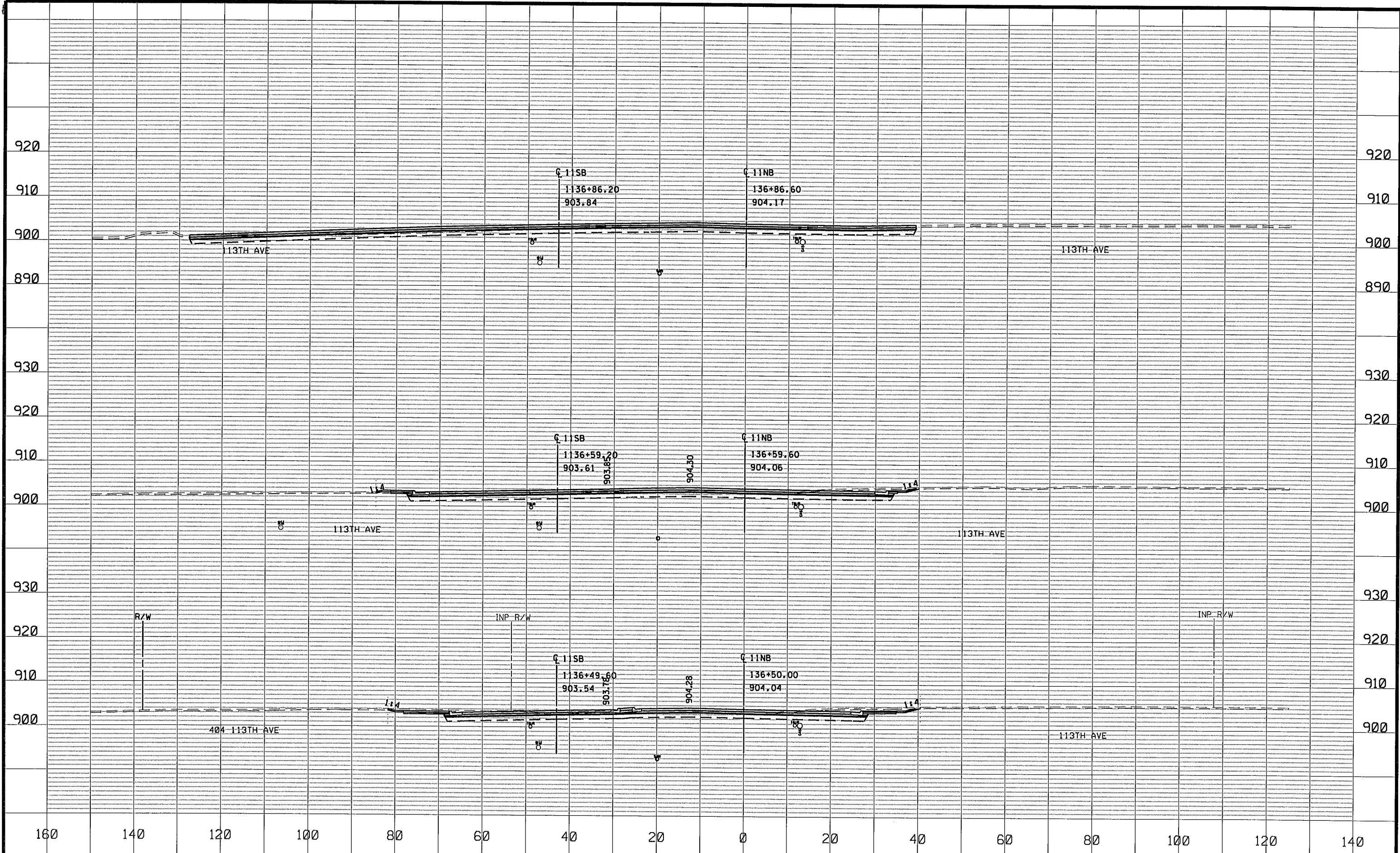
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CROSS SECTIONS
 STA 135+83.19 TO 136+00.00
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NO	DATE	BY	CKD	APPR	REVISION

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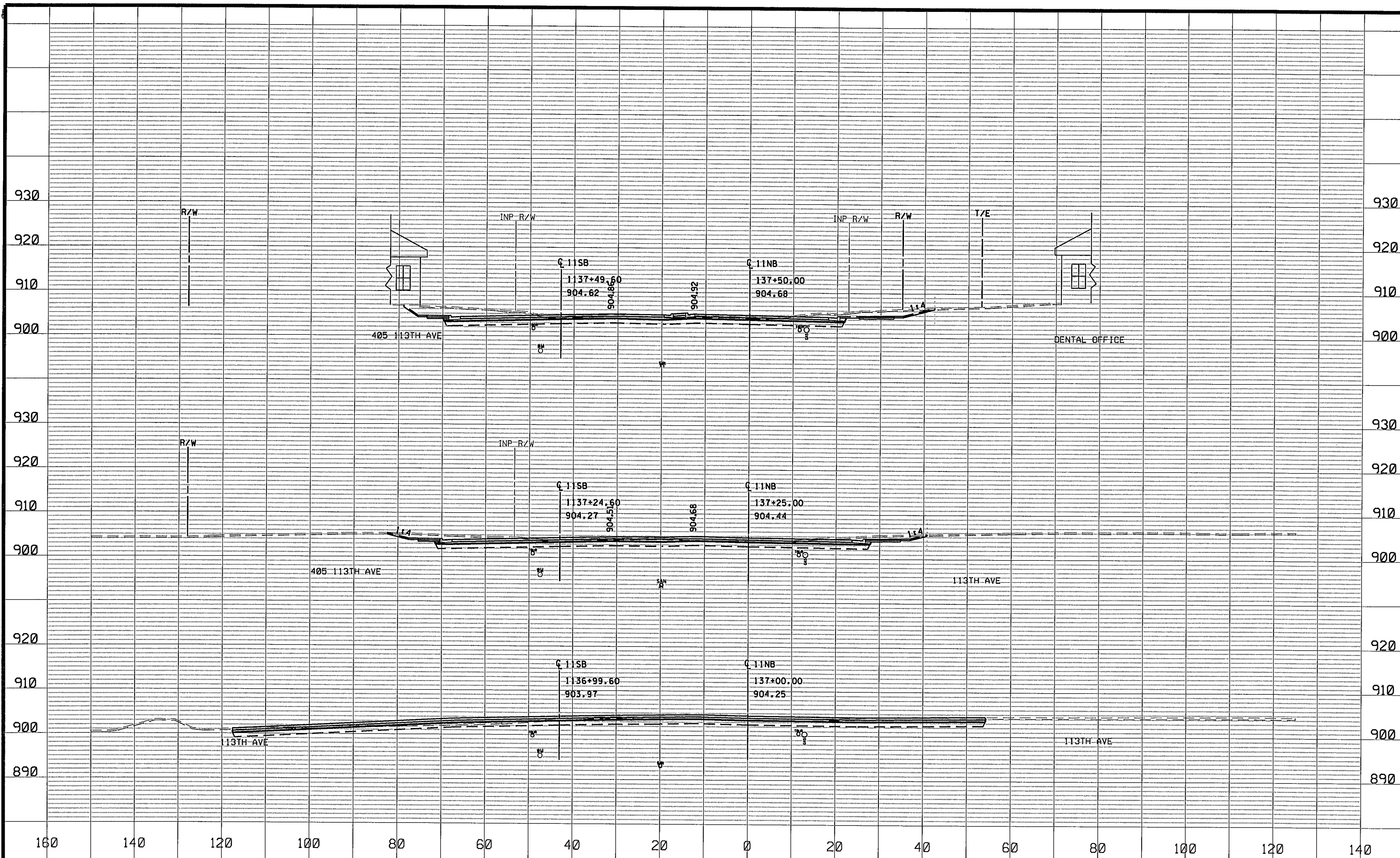
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CROSS SECTIONS
 STA 136+50.00 TO 136+86.60
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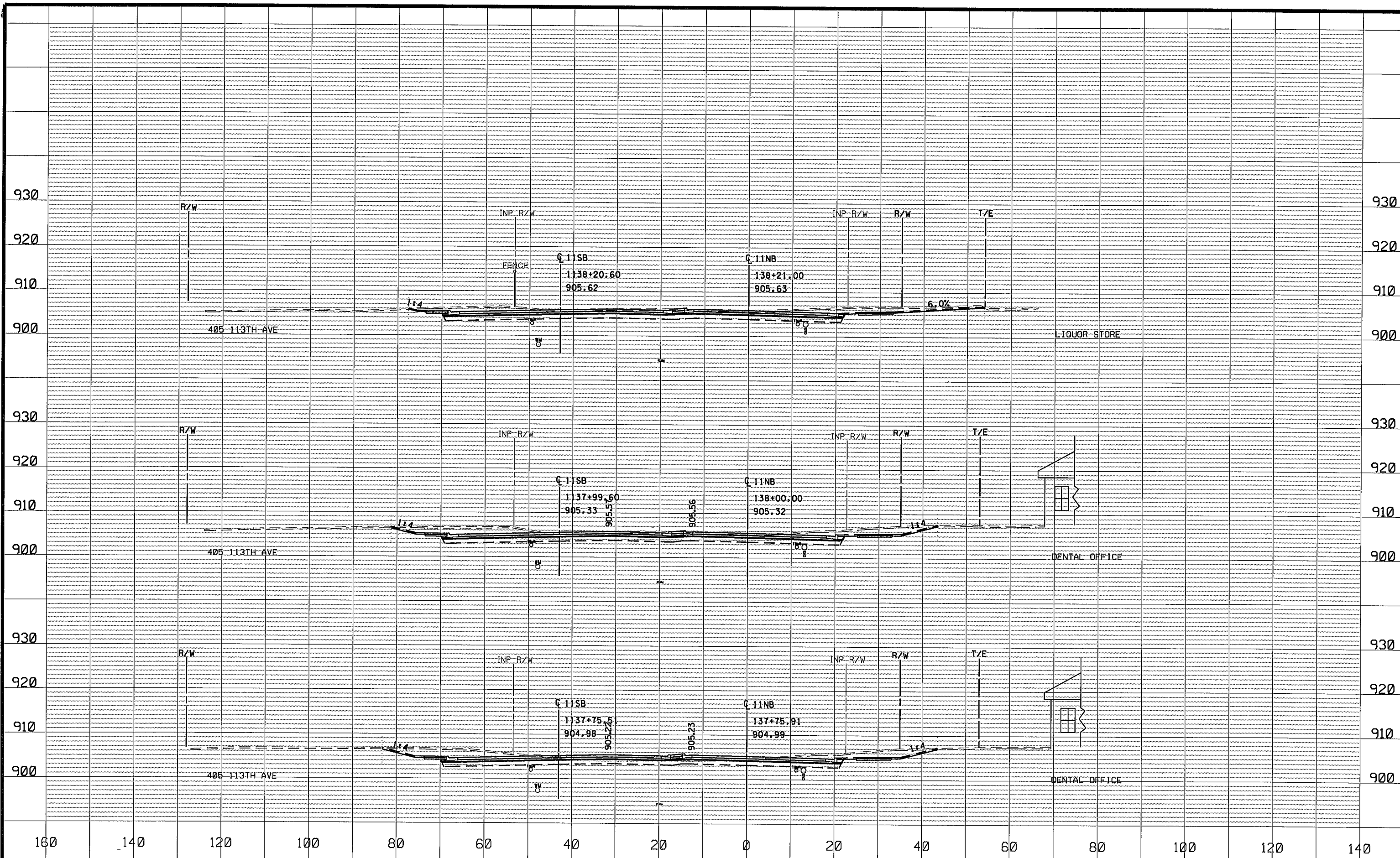
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CROSS SECTIONS
 STA 137+00.00 TO 137+50.00
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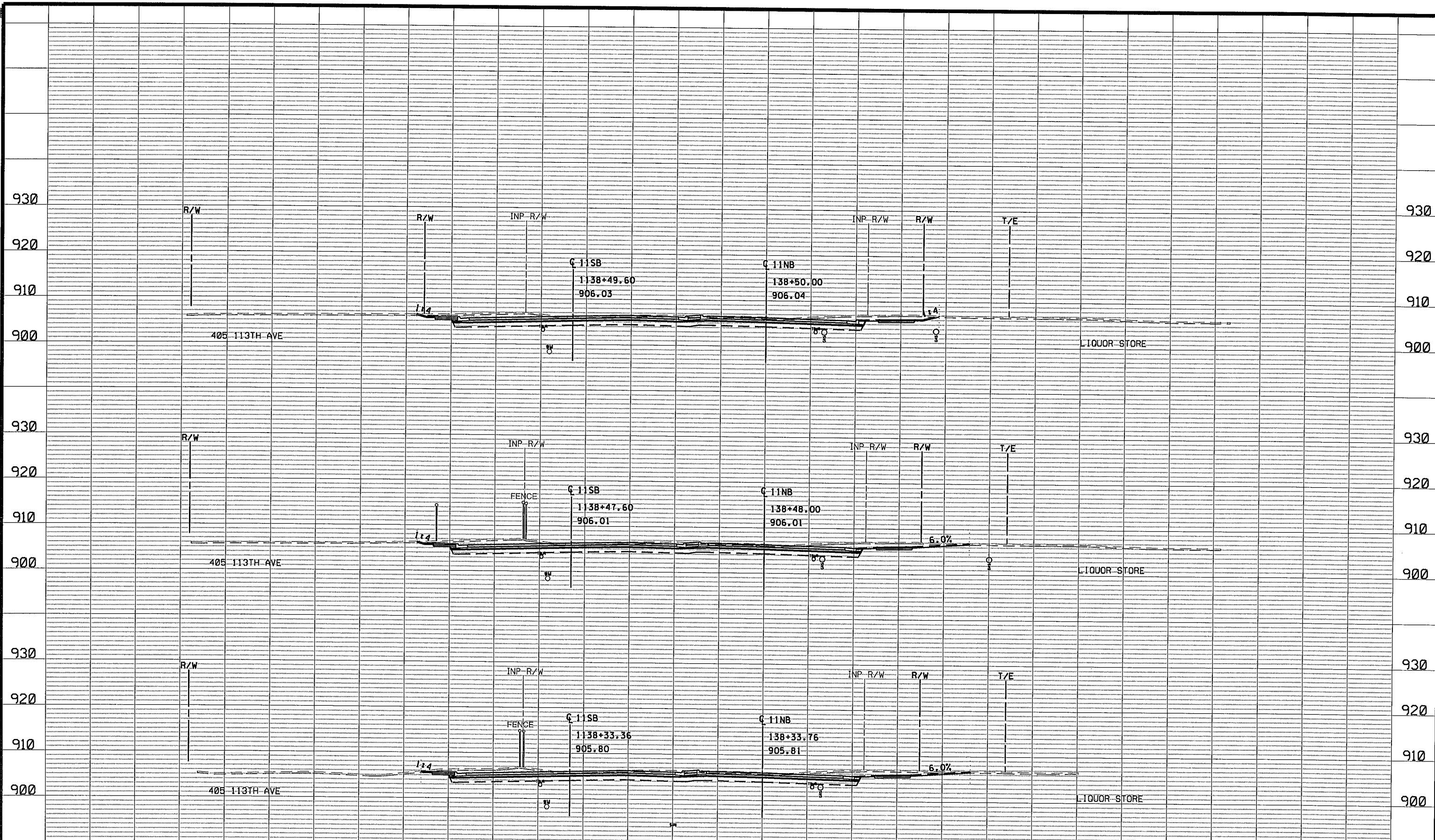
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CROSS SECTIONS
STA 137+75.91 TO 138+21.00
Sheet 259 of 298 Sheets

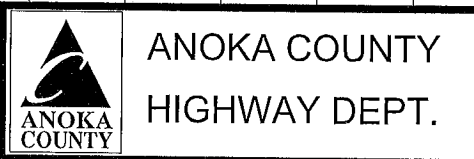


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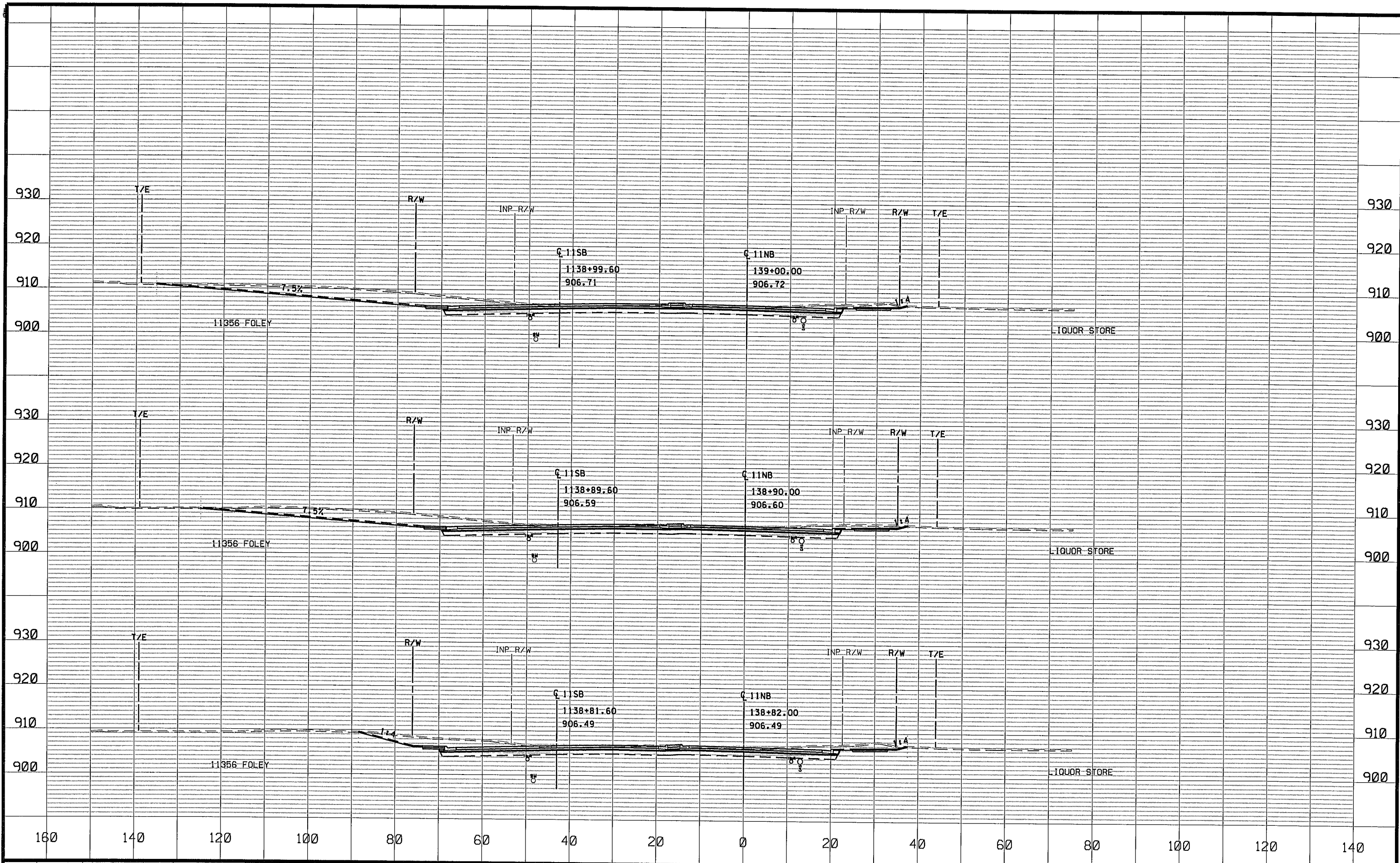
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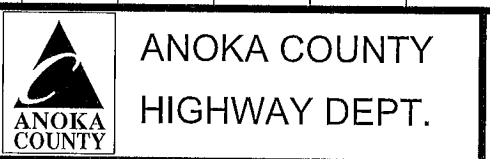
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 Sheet 260 of 298 Sheets



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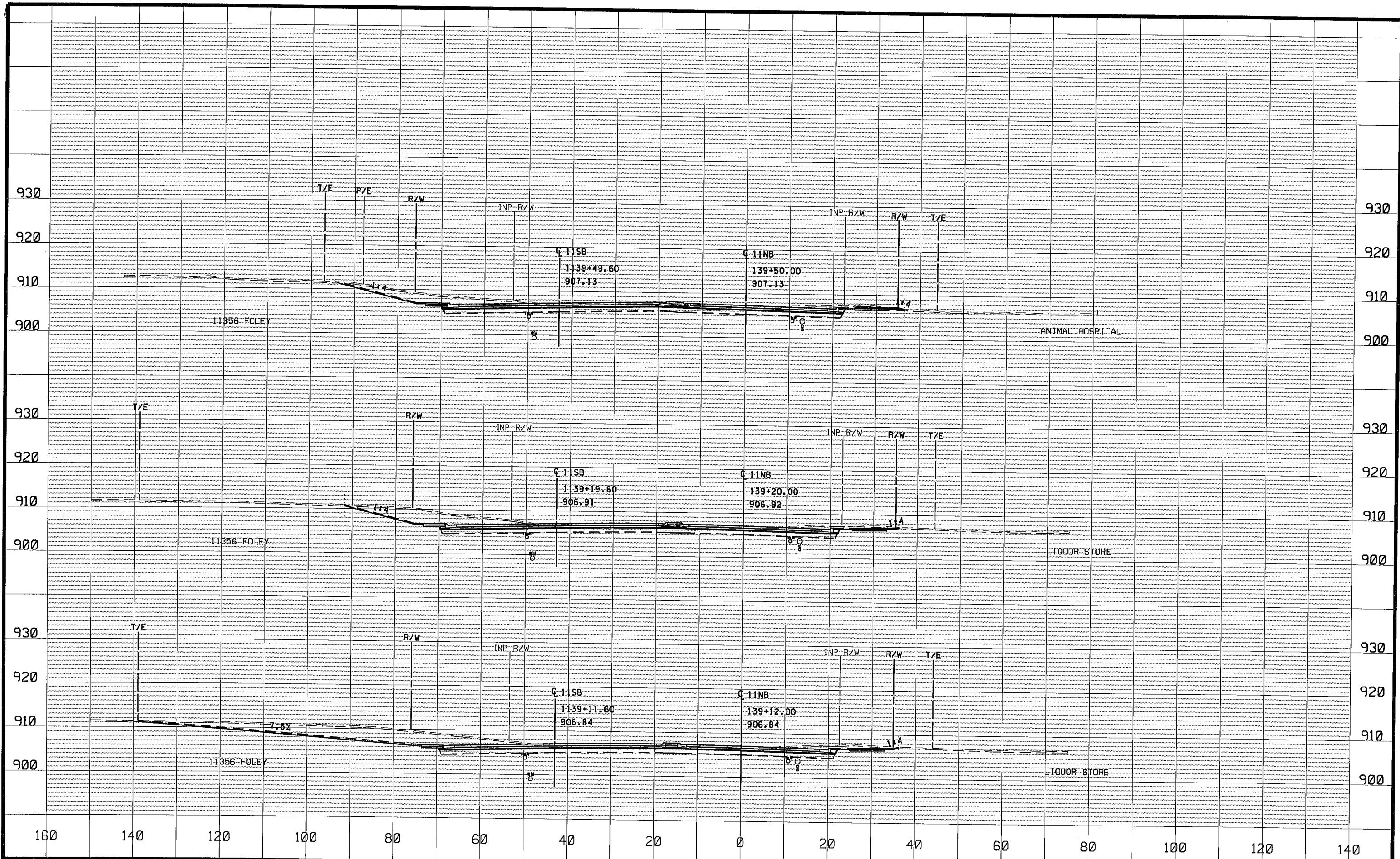
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 CHECKED BY JEO DATE 02/27/16



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CROSS SECTIONS
 STA 138+82.00 TO 139+00.00
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NO	DATE	BY	CKD	APPR	REVISION
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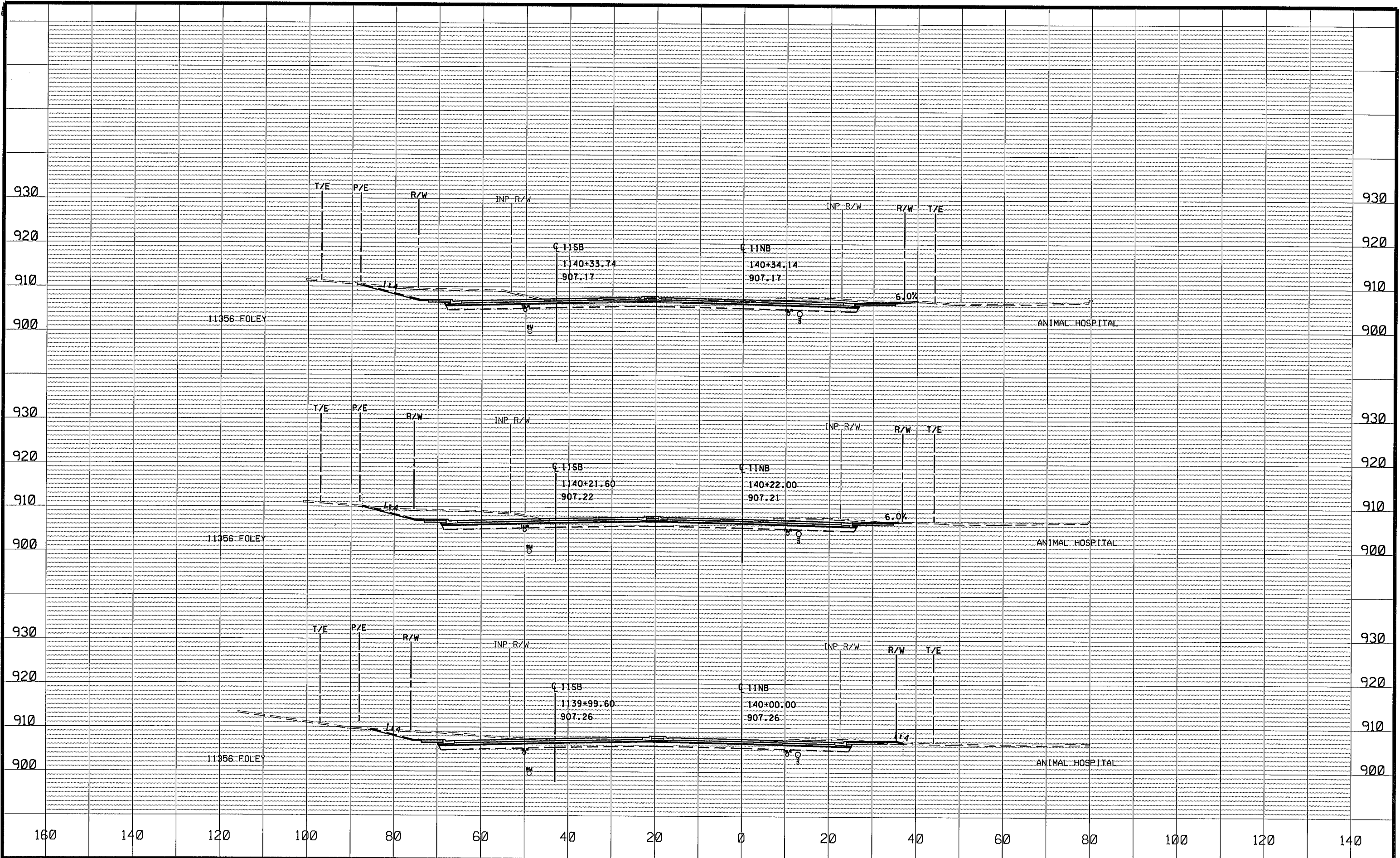
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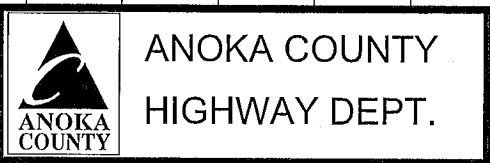
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Sheet 262 of 298 Sheets



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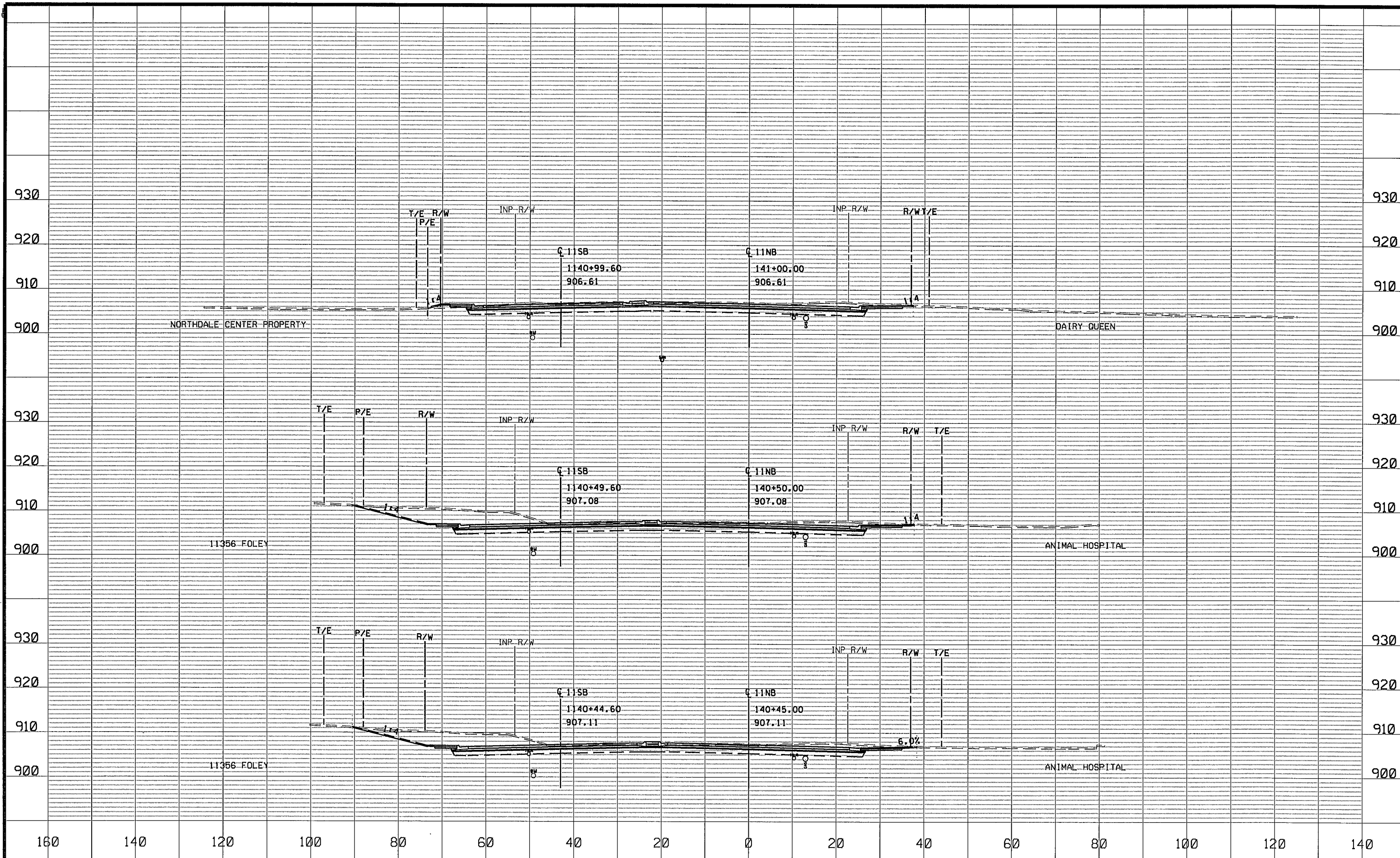
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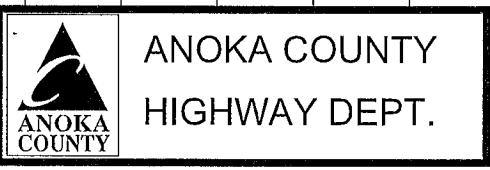
SP 002-611-034
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CROSS SECTIONS
STA 140+00.00 TO 140+34.14
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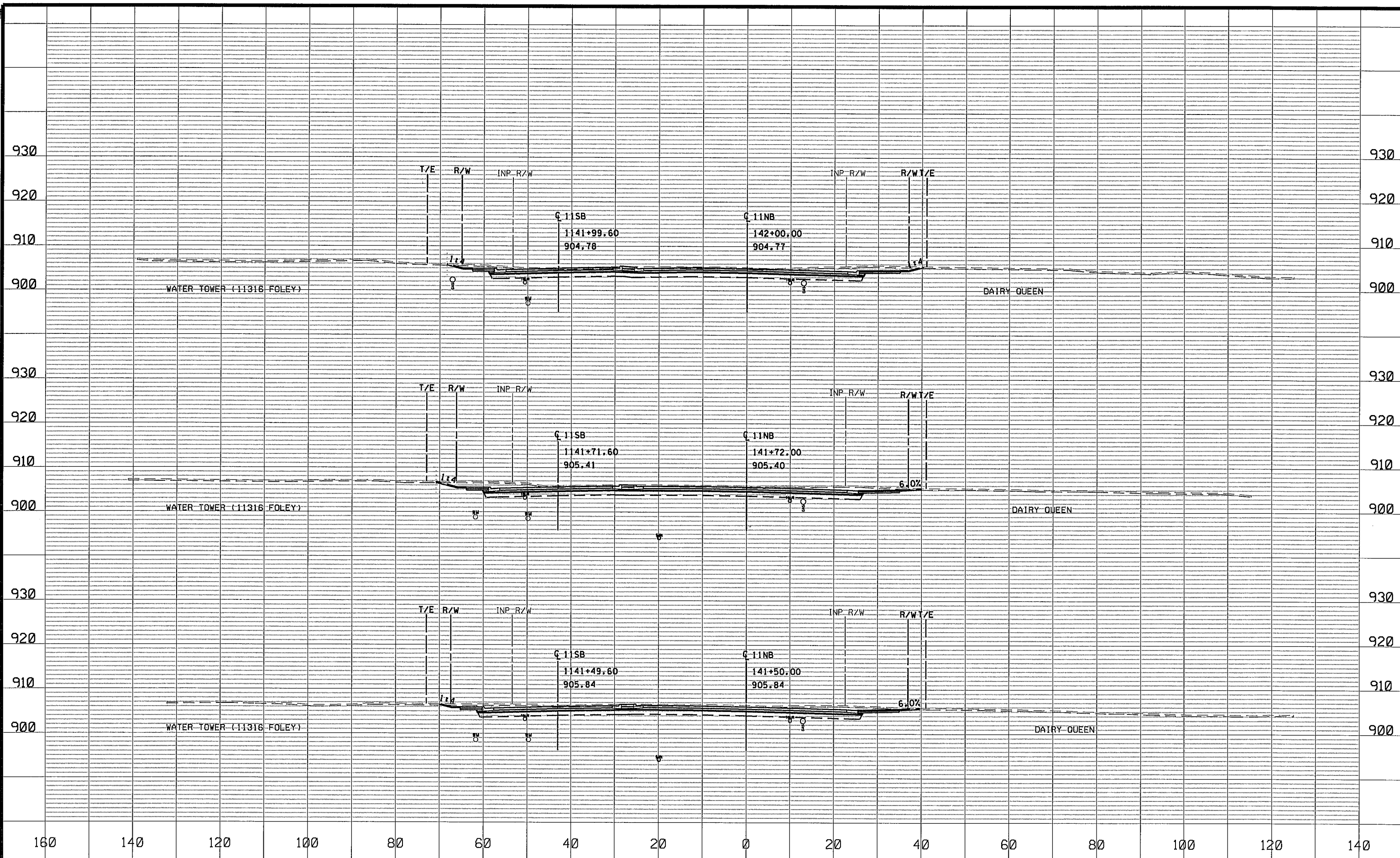
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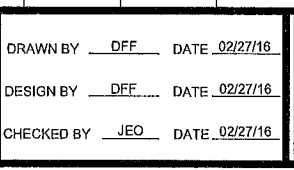
SP 002-611-034
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CROSS SECTIONS
 STA 140+45.00 TO 141+00.00
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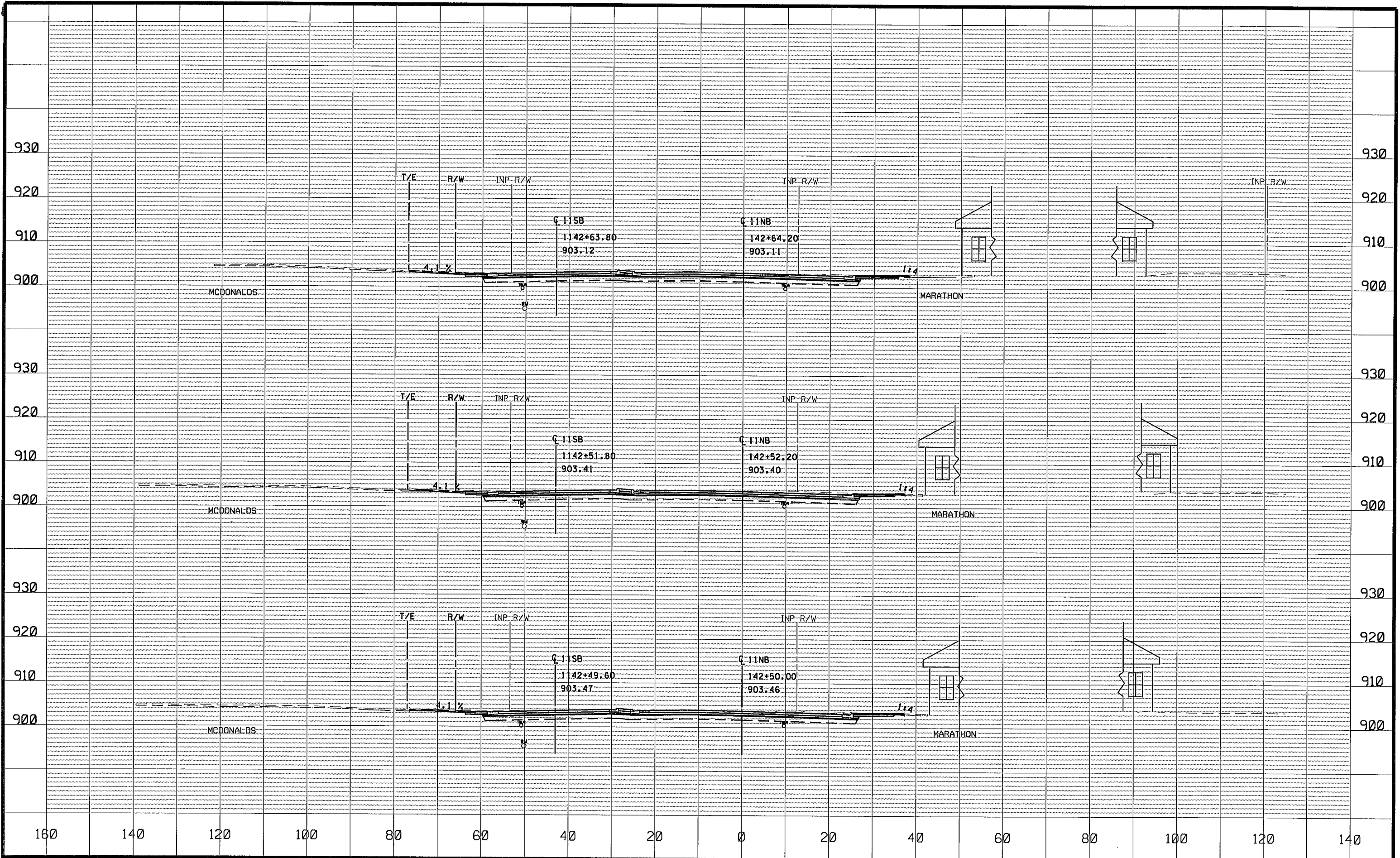
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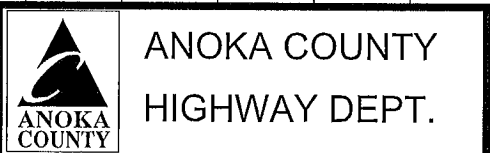
CROSS SECTIONS
STA 141+50.00 TO 142+00.00
Sheet 265 of 298 Sheets



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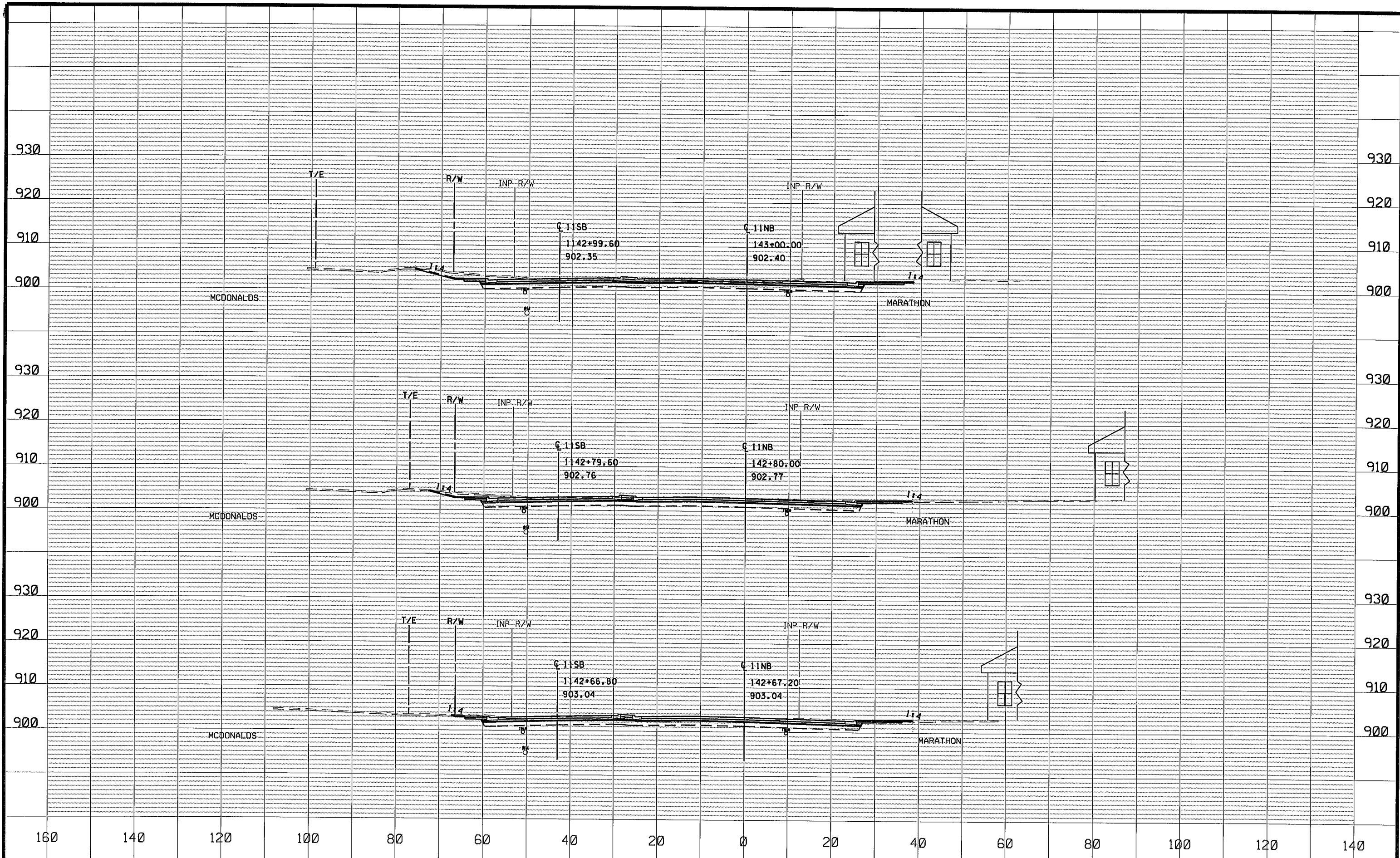
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DRAWN BY DFF DATE 02/27/16
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CROSS SECTIONS
 STA 142+50.00 TO 142+64.20
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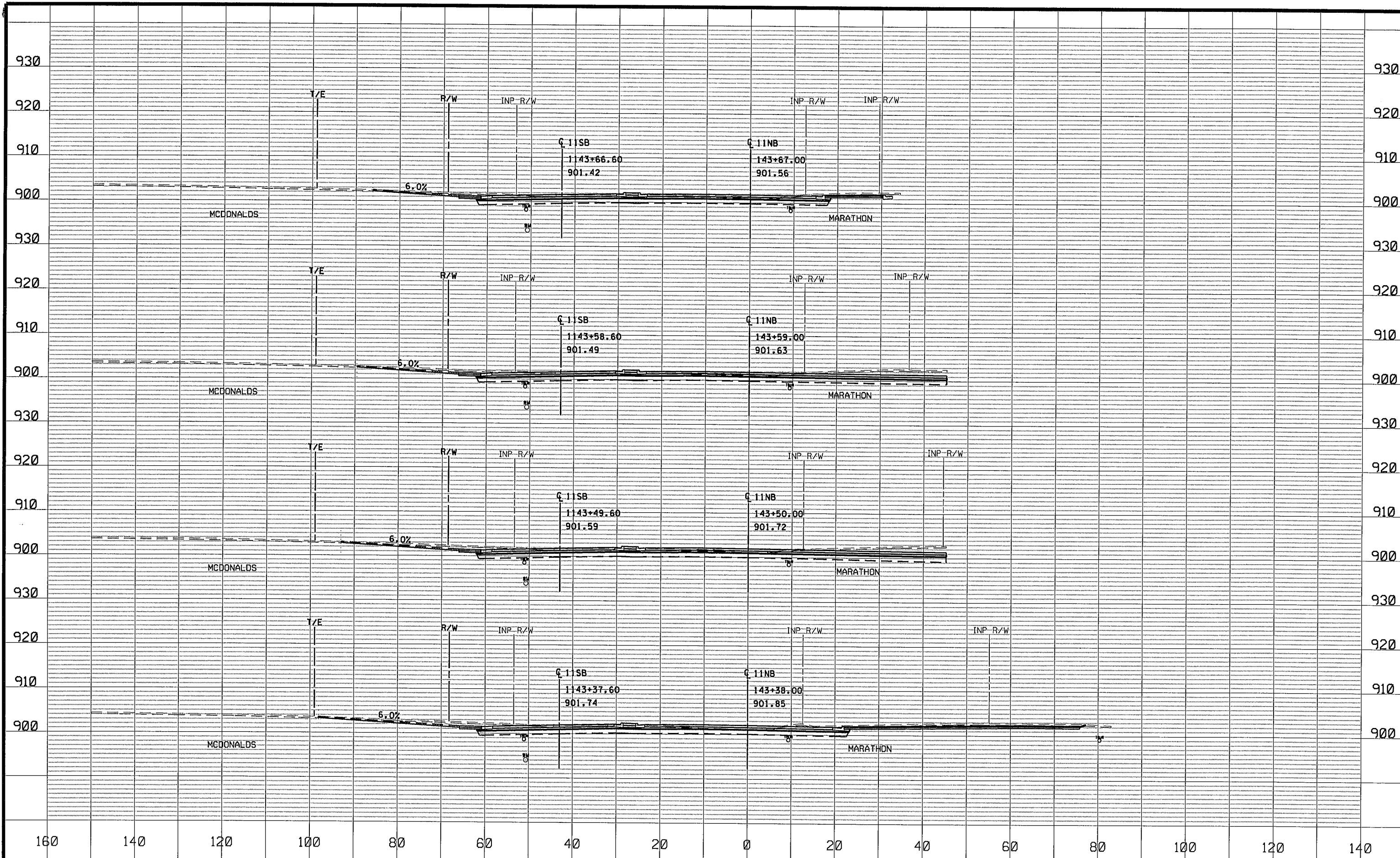
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CROSS SECTIONS
 STA 142+67.20 TO 143+00.00
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NO	DATE	BY	CKD	APPR	REVISION

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NO	DATE	BY	CKD	APPR	REVISION

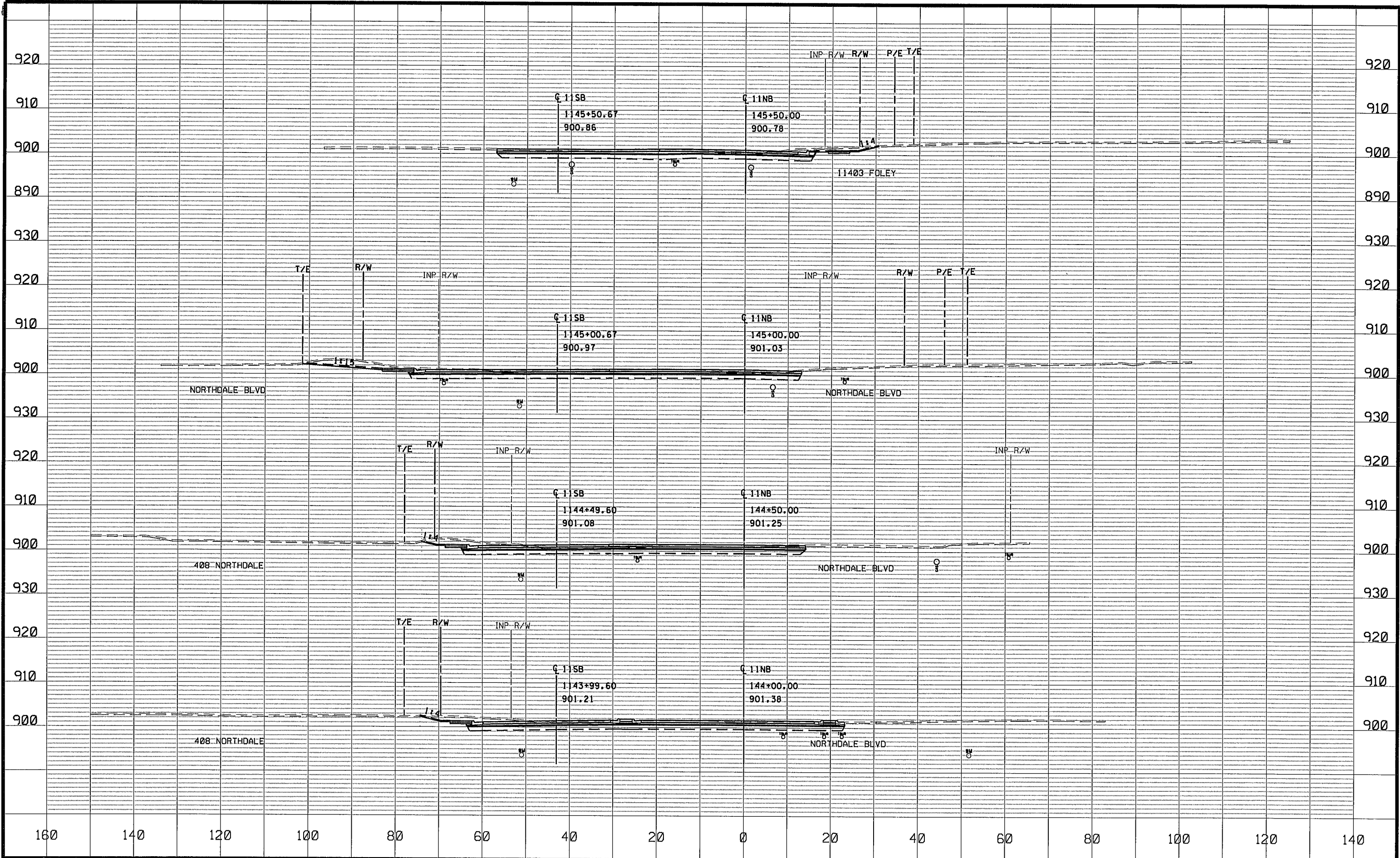
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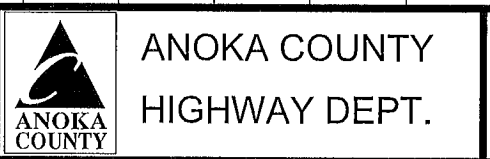
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STA 143+38.00 TO 143+67.00
Sheet 268 of 298 Sheets



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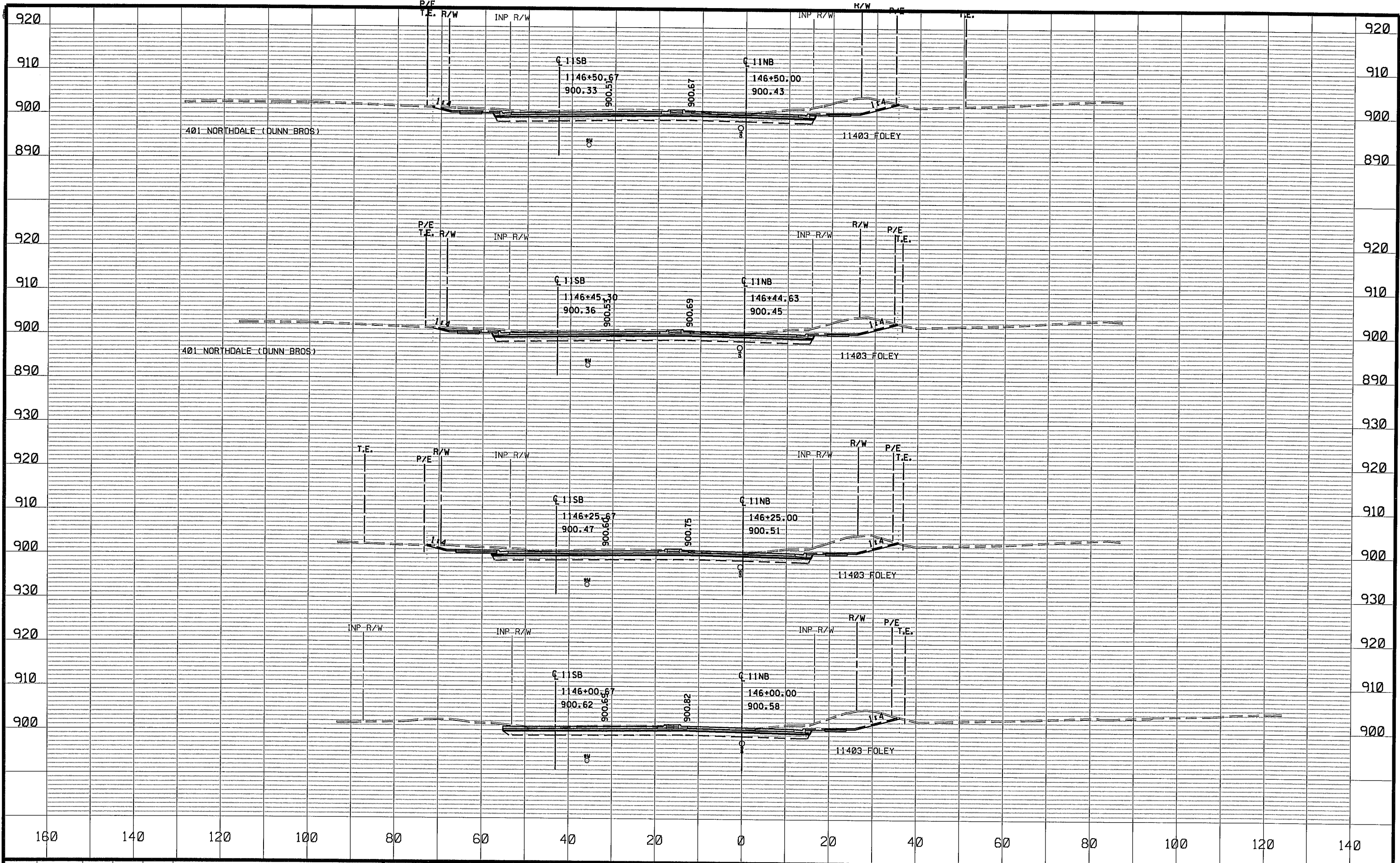
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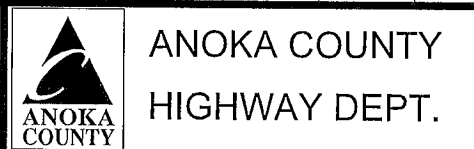
CROSS SECTIONS
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 Sheet 269 of 298 Sheets



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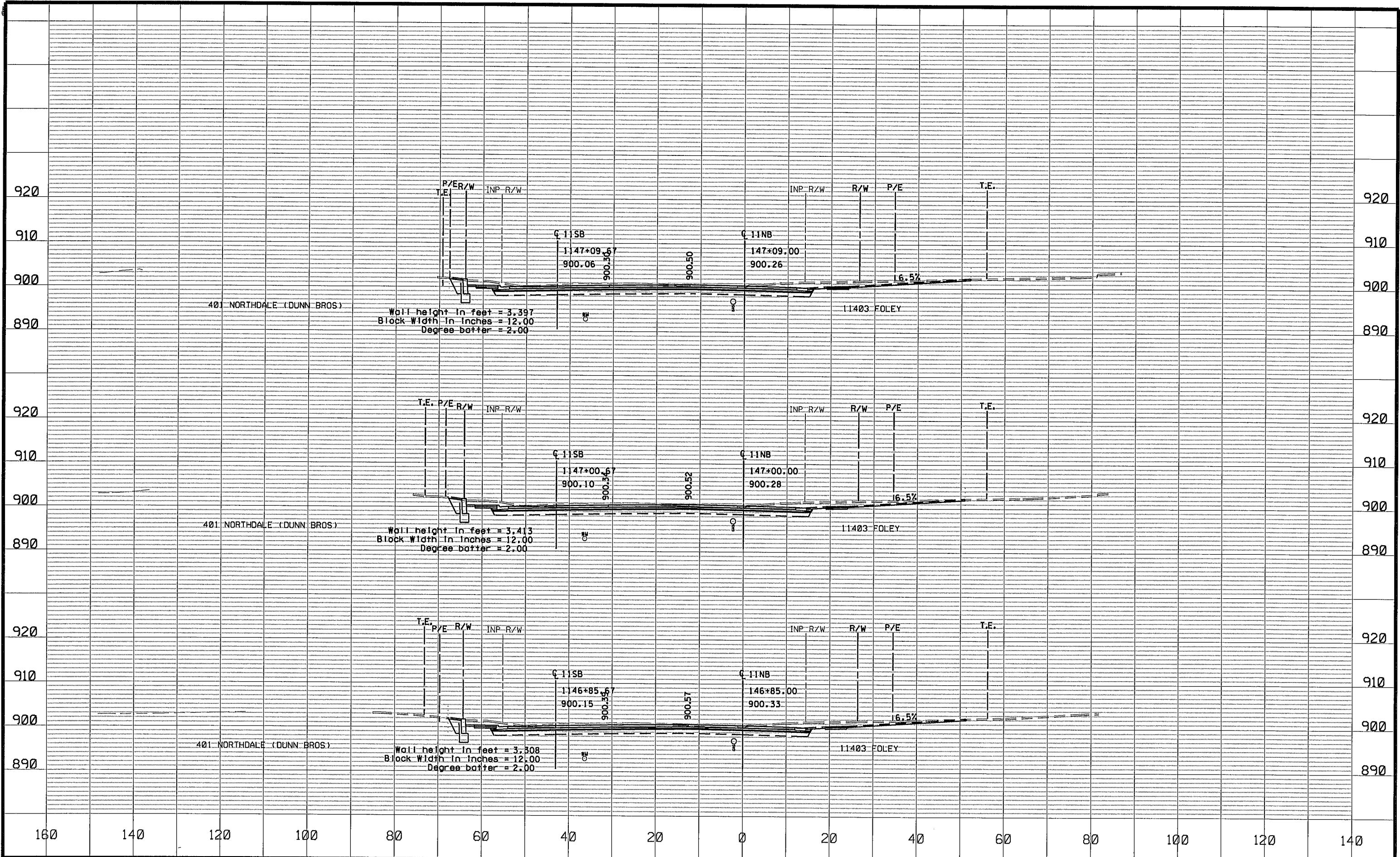
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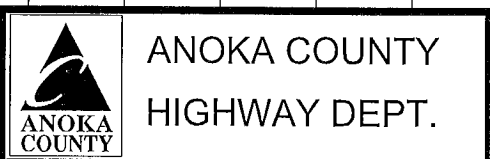
SP 002-611-034
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CROSS SECTIONS
 STA 146+00.00 TO 146+50.00
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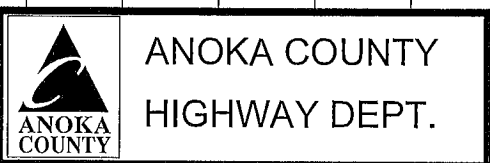
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CROSS SECTIONS
 STA 146+85.00 TO 147+09.00
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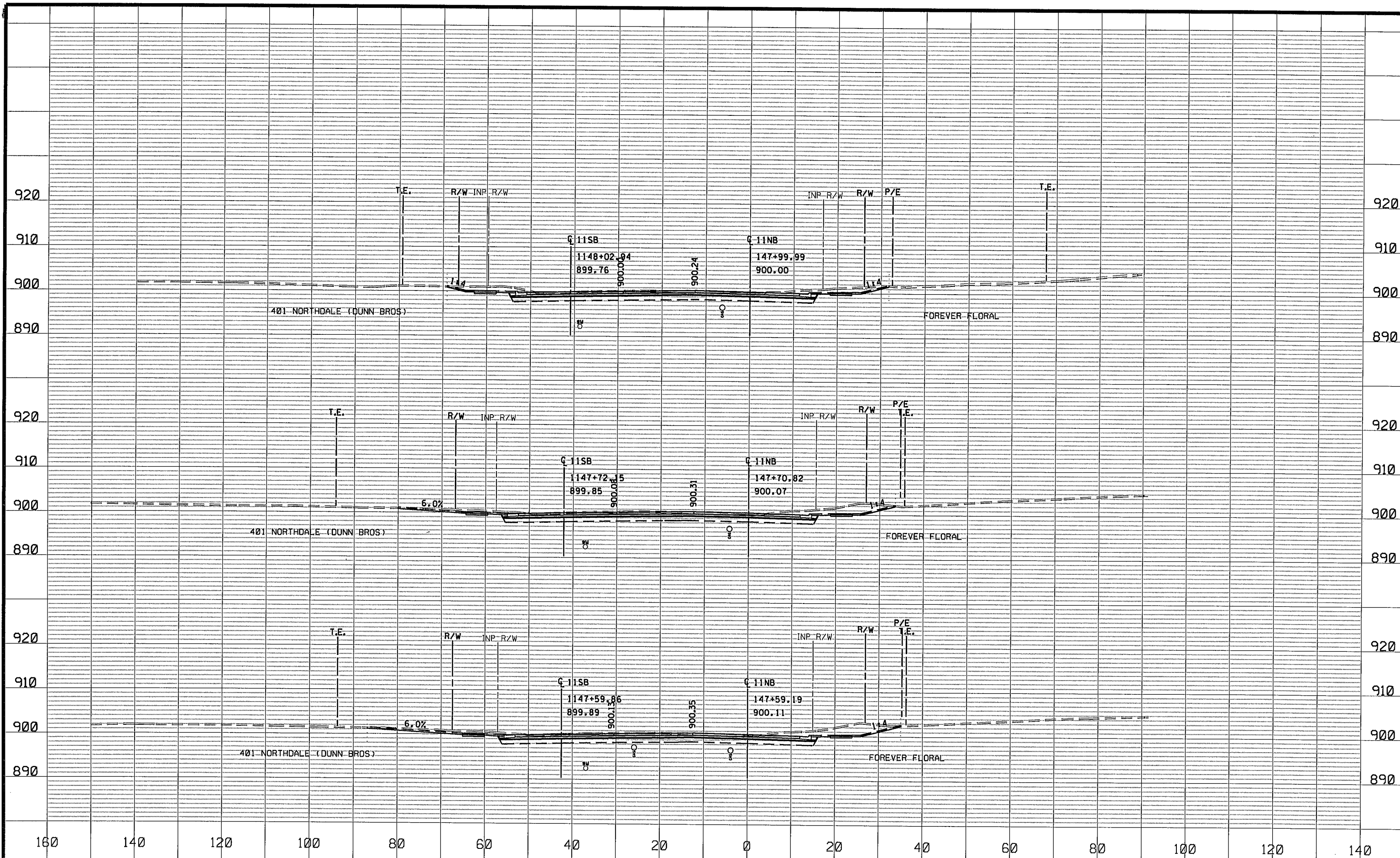
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CROSS SECTIONS
 STA 147+13.21 TO 147+50.00
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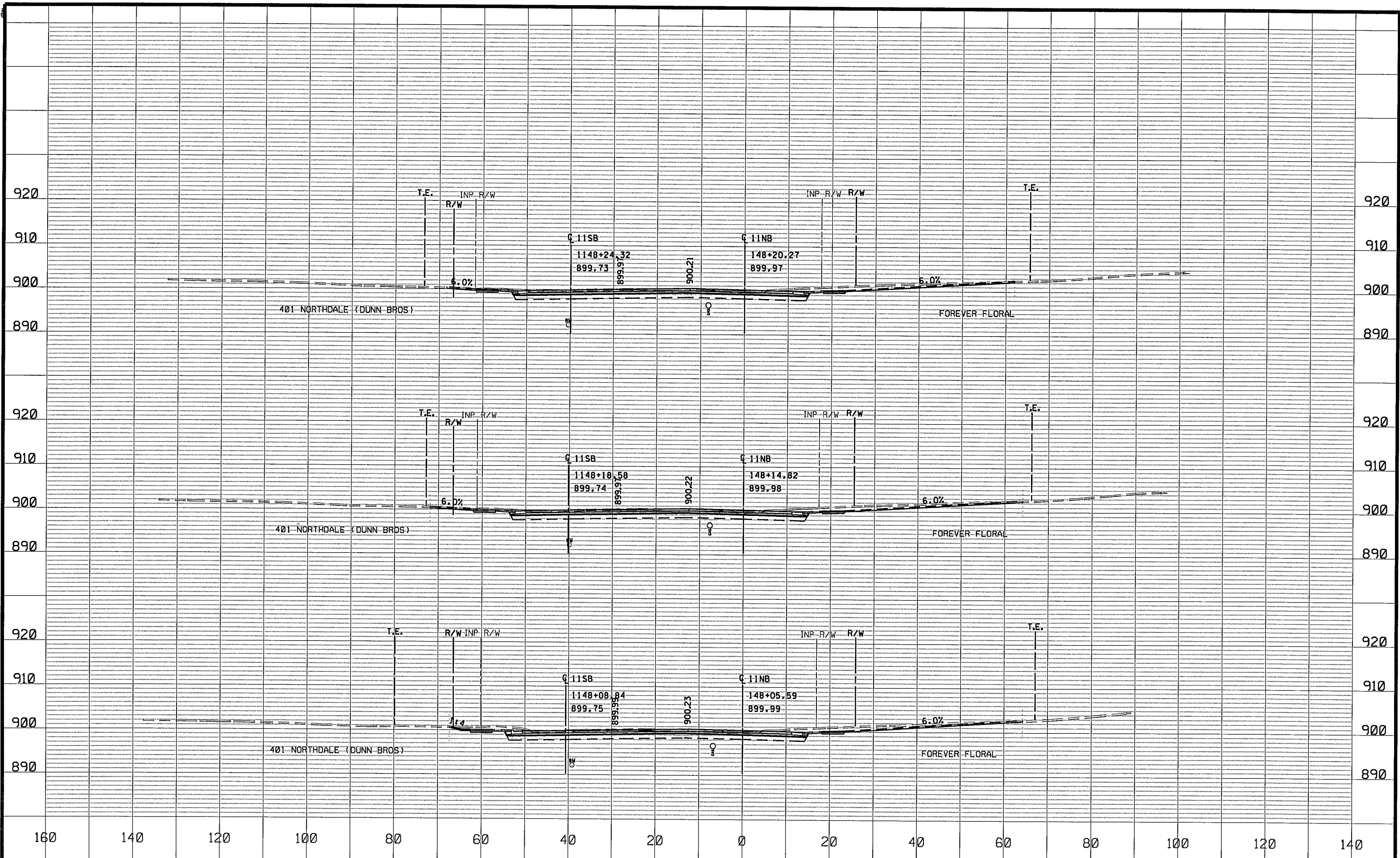
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CROSS SECTIONS
 STA 147+59.19 TO 147+99.99
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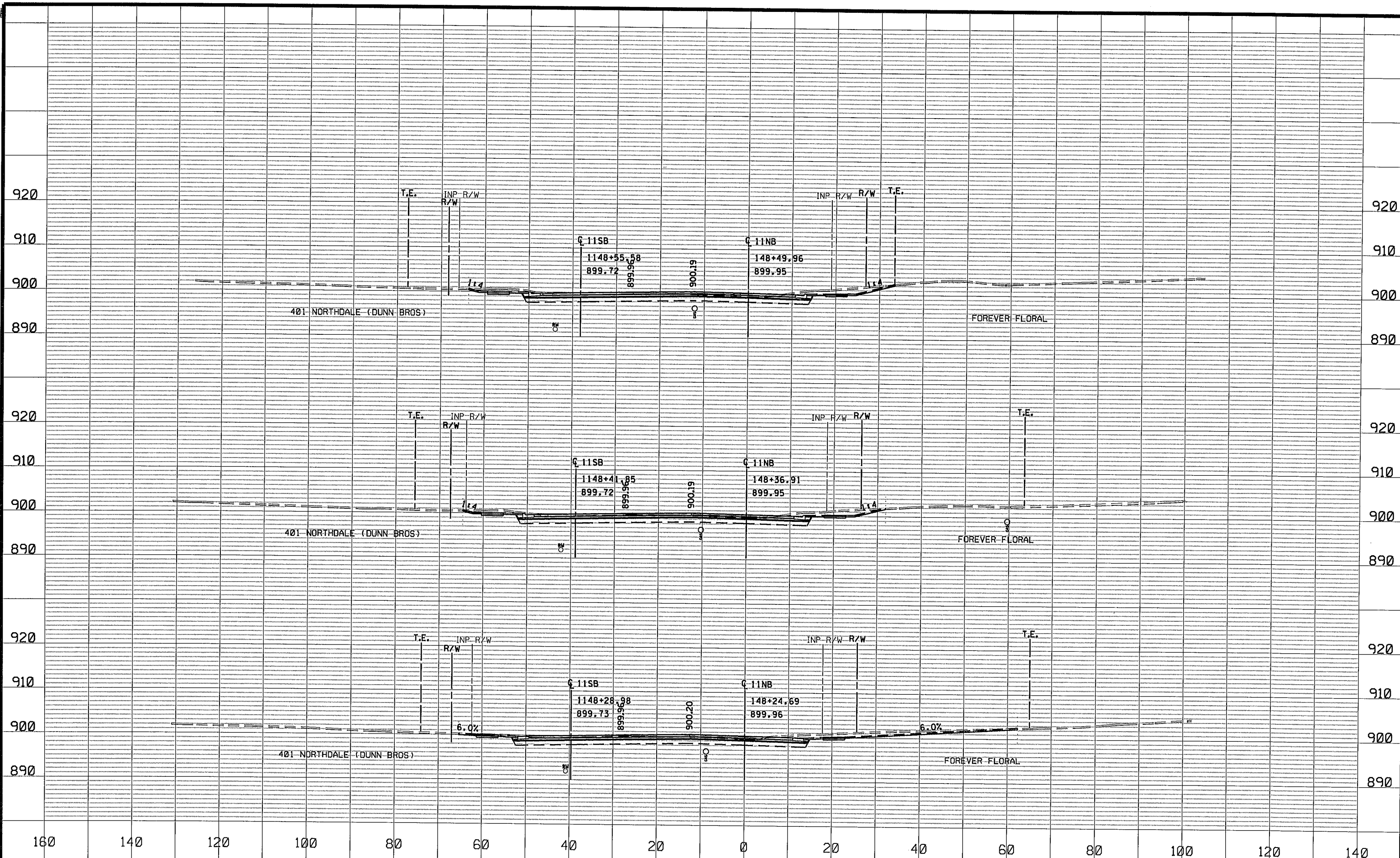
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 DESIGN BY DFF DATE 02/27/16
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CROSS SECTIONS
 STA 148+05.59 TO 148+20.27
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NO	DATE	BY	CHKD	APPR	REVISION
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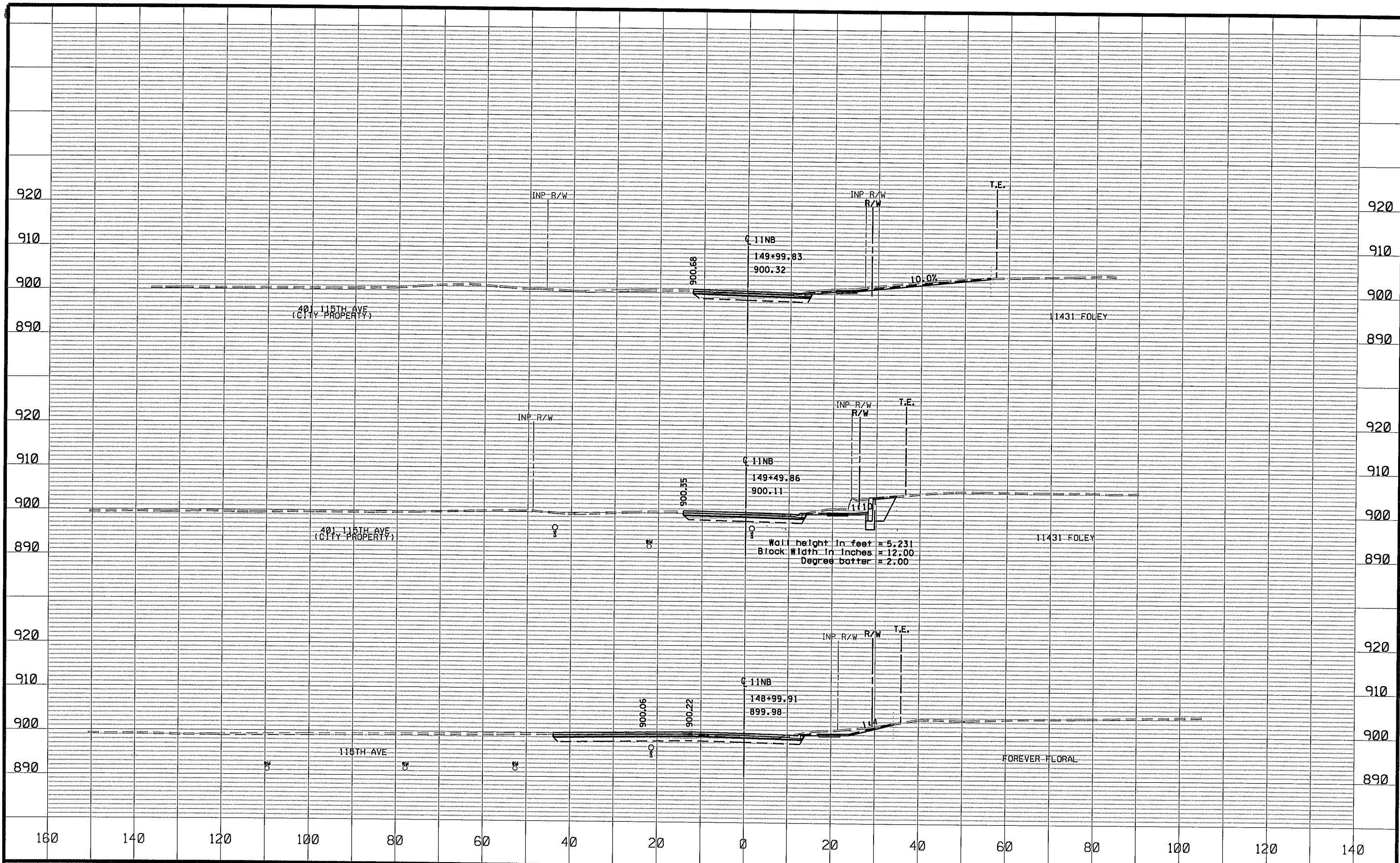
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 CHECKED BY JEO DATE 02/27/16



ANOKA COUNTY
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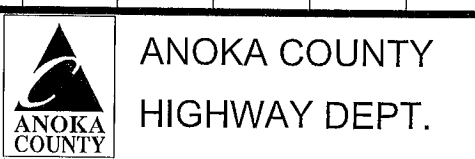
SP 002-611-034
 SP 114-020-050
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CROSS SECTIONS
 STA 148+24.69 TO 148+49.96
 Sheet 275 of 298 Sheets



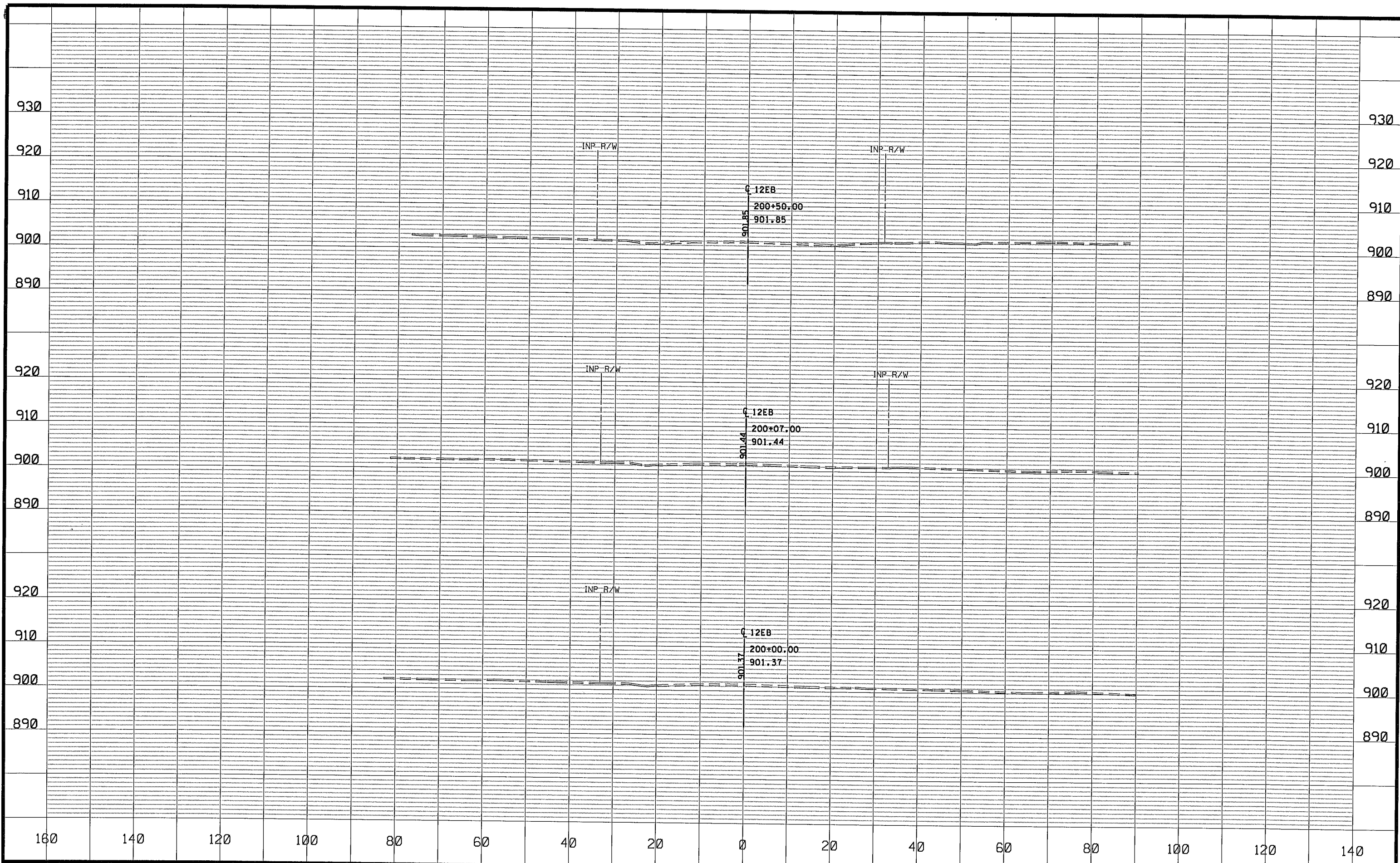
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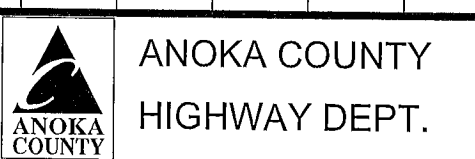
SP 002-611-034
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CROSS SECTIONS
 STA 148+99.91 TO 149+99.83
 Sheet 276 of 298 Sheets



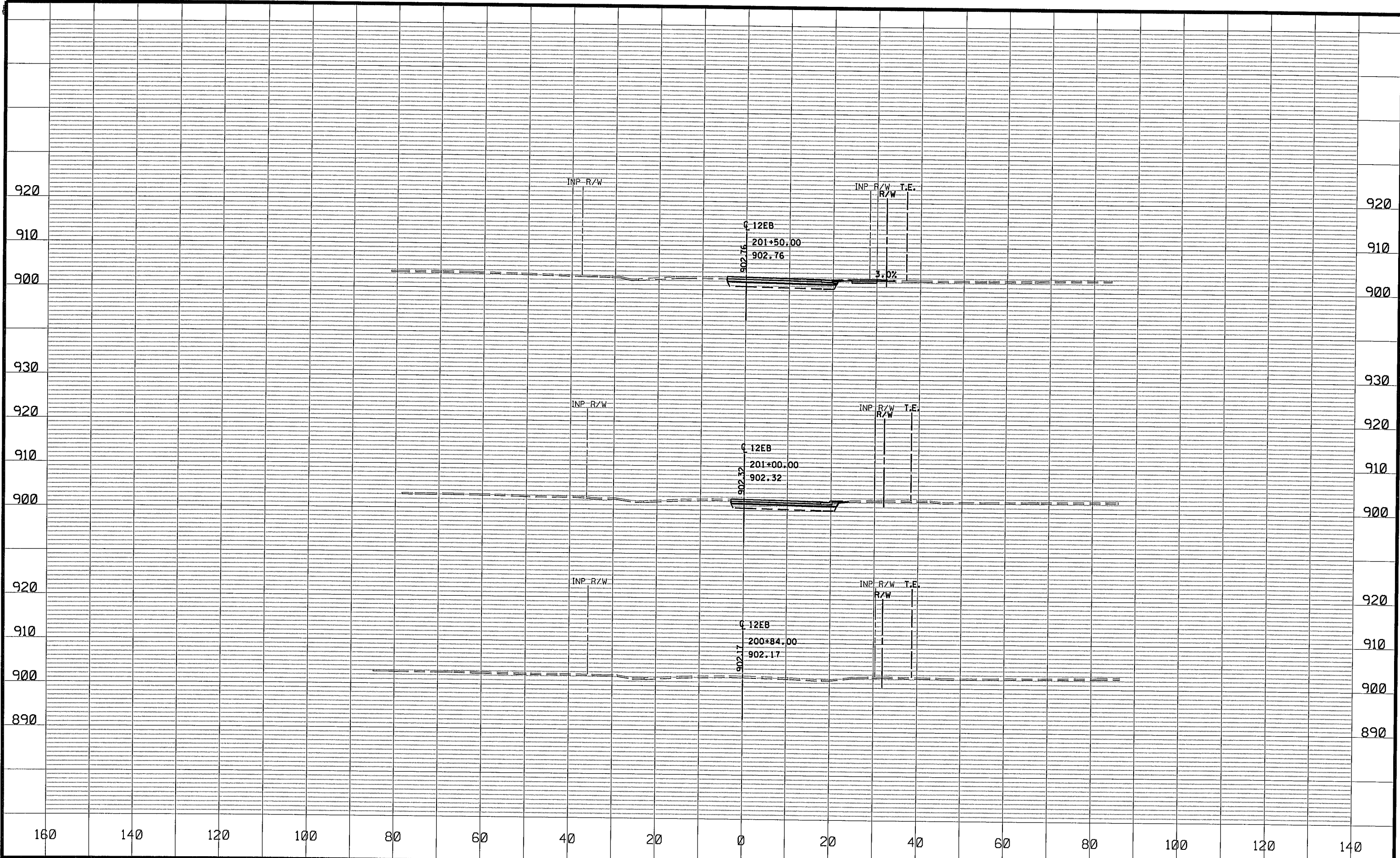
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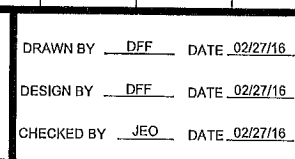
CROSS SECTIONS
 STA 200+00.00 TO 200+50.00
 Sheet 277 of 298 Sheets



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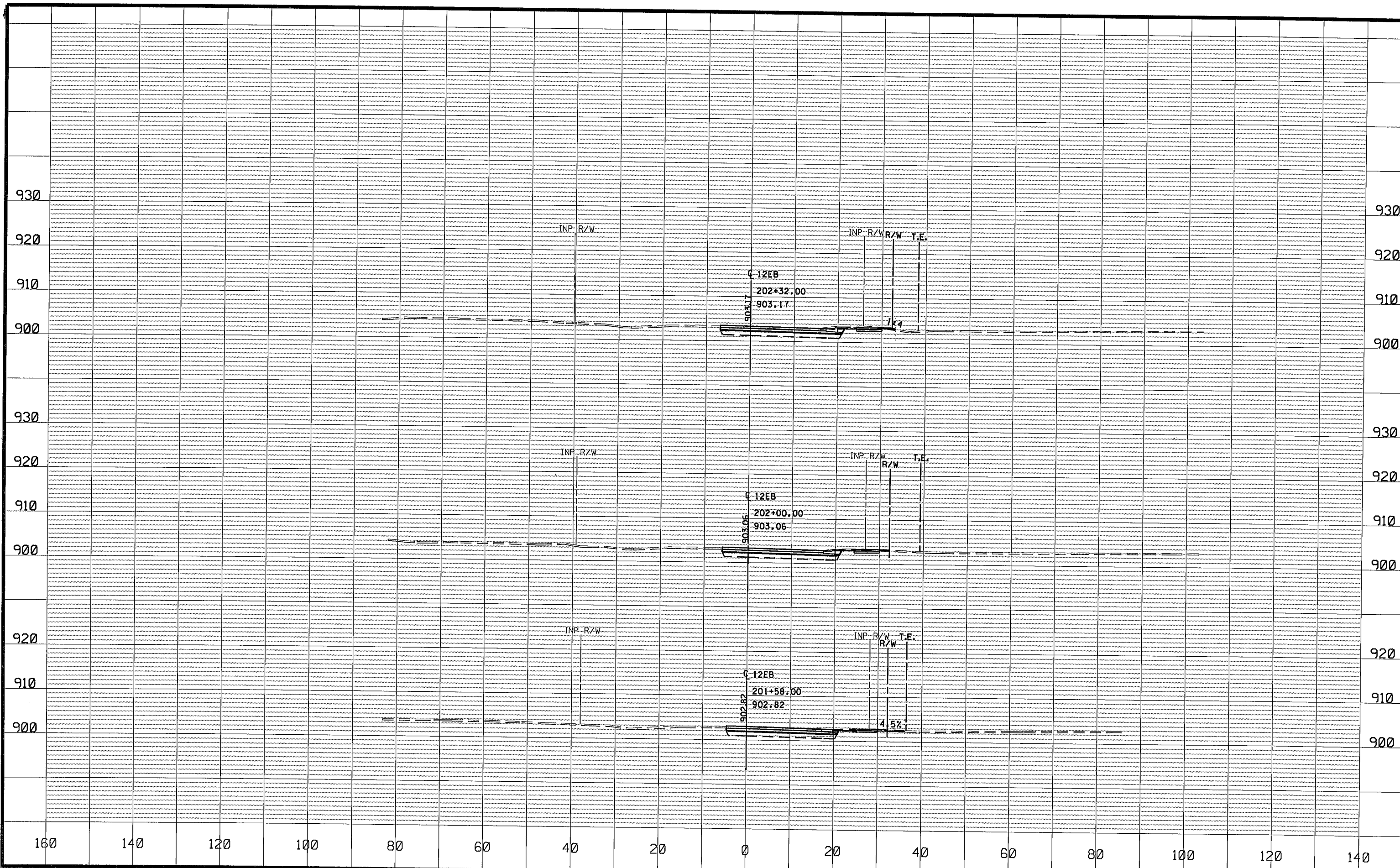
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DESIGN BY	DFP	DATE	02/27/16
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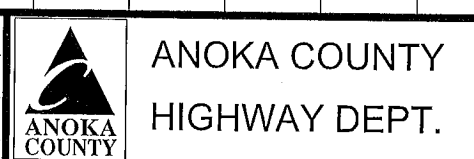
CROSS SECTIONS
STA 200+84.00 TO 201+50.00
Sheet 278 of 298 Sheets



NO	DATE	BY	CHKD	APPR	REVISION

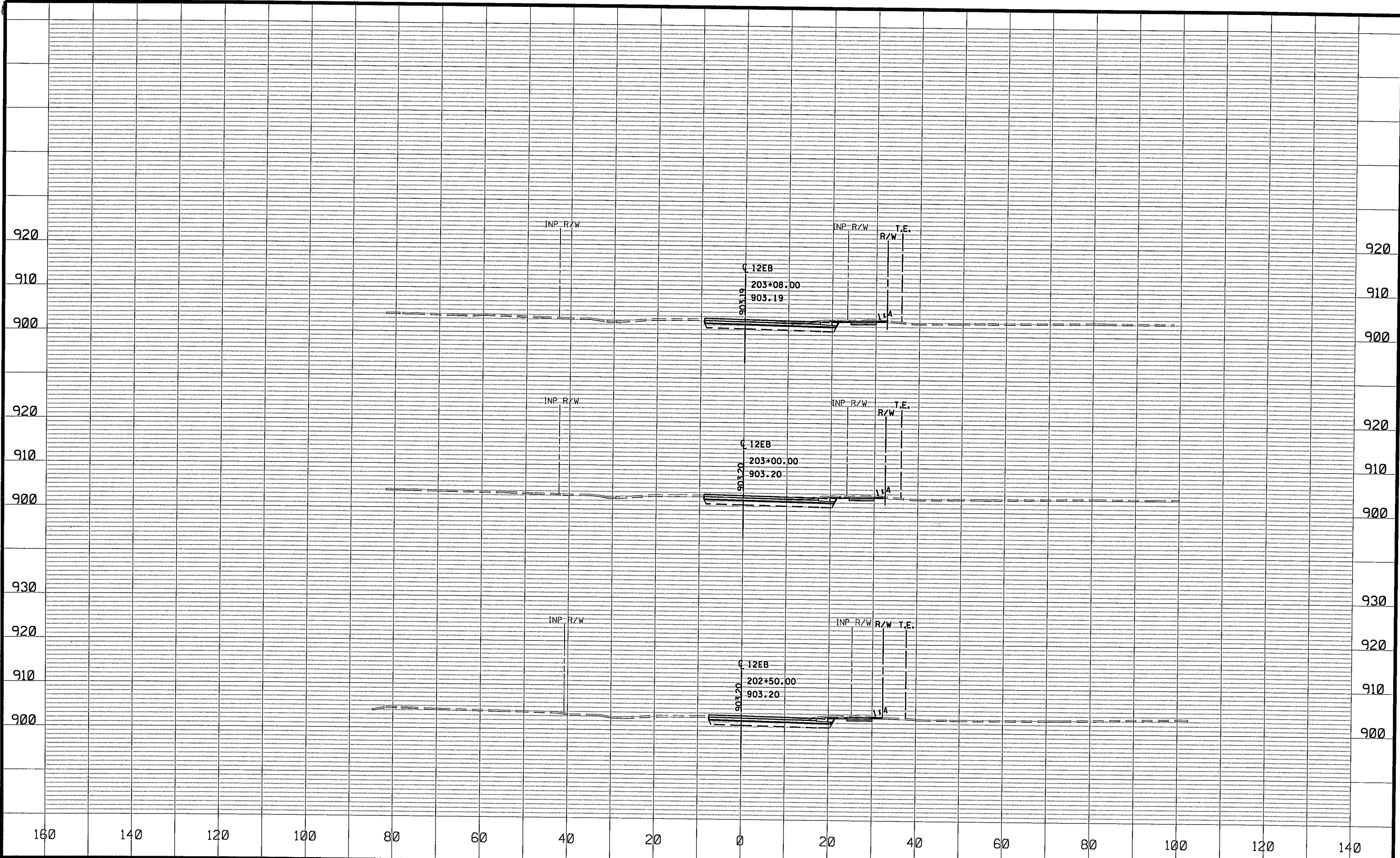
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 CHECKED BY: JEO DATE: 02/27/16



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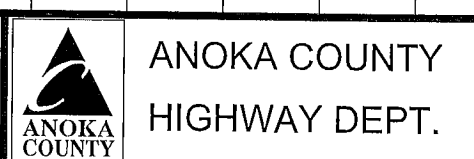
CROSS SECTIONS
 STA 201+58.00 TO 202+32.00
 Sheet 279 of 298 Sheets



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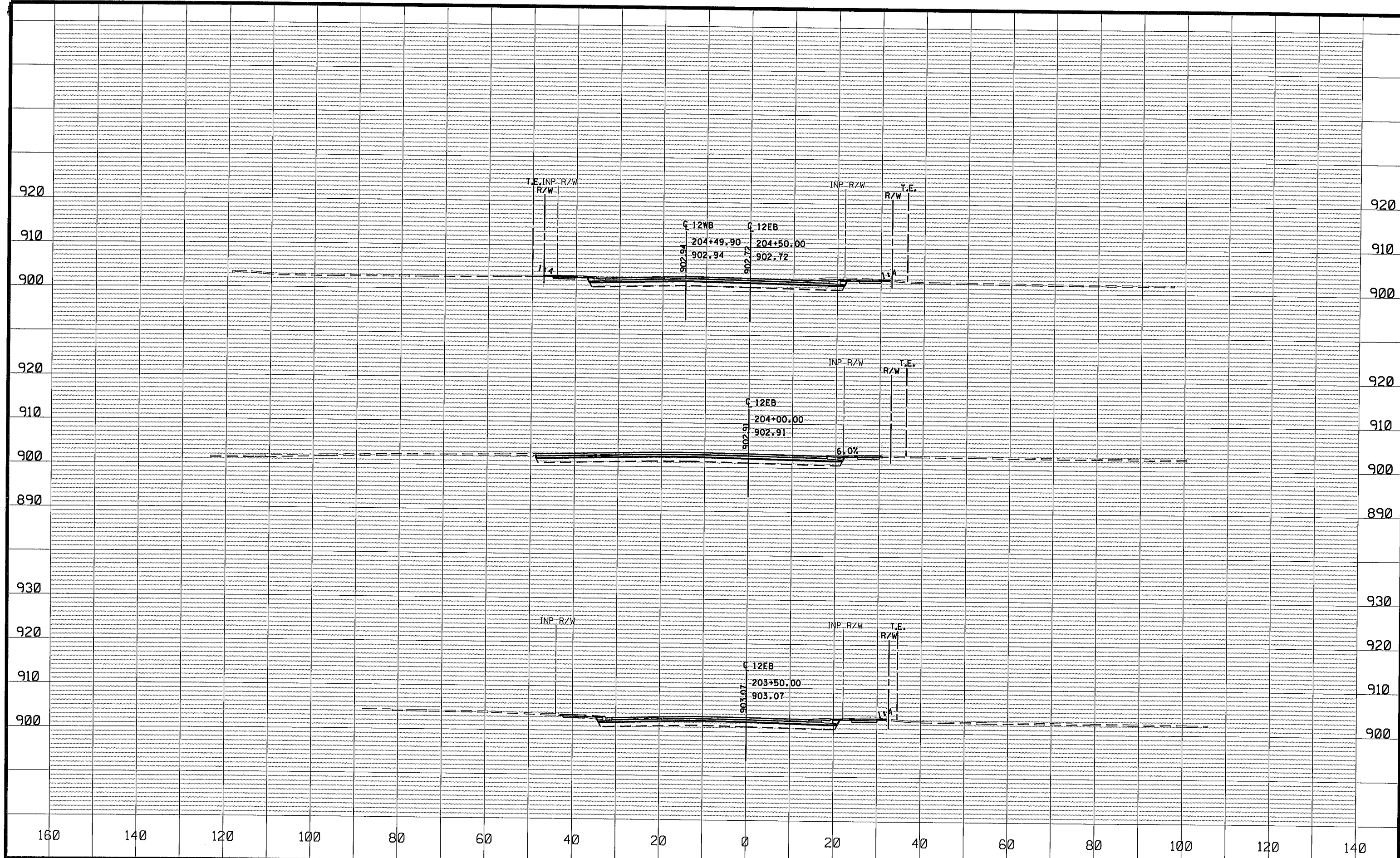
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 CHECKED BY JEO DATE 02/27/16



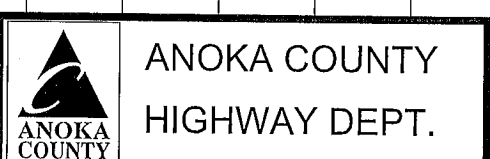
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CROSS SECTIONS
 STA 202+50.00 TO 203+08.00
 Sheet 280 of 298 Sheets



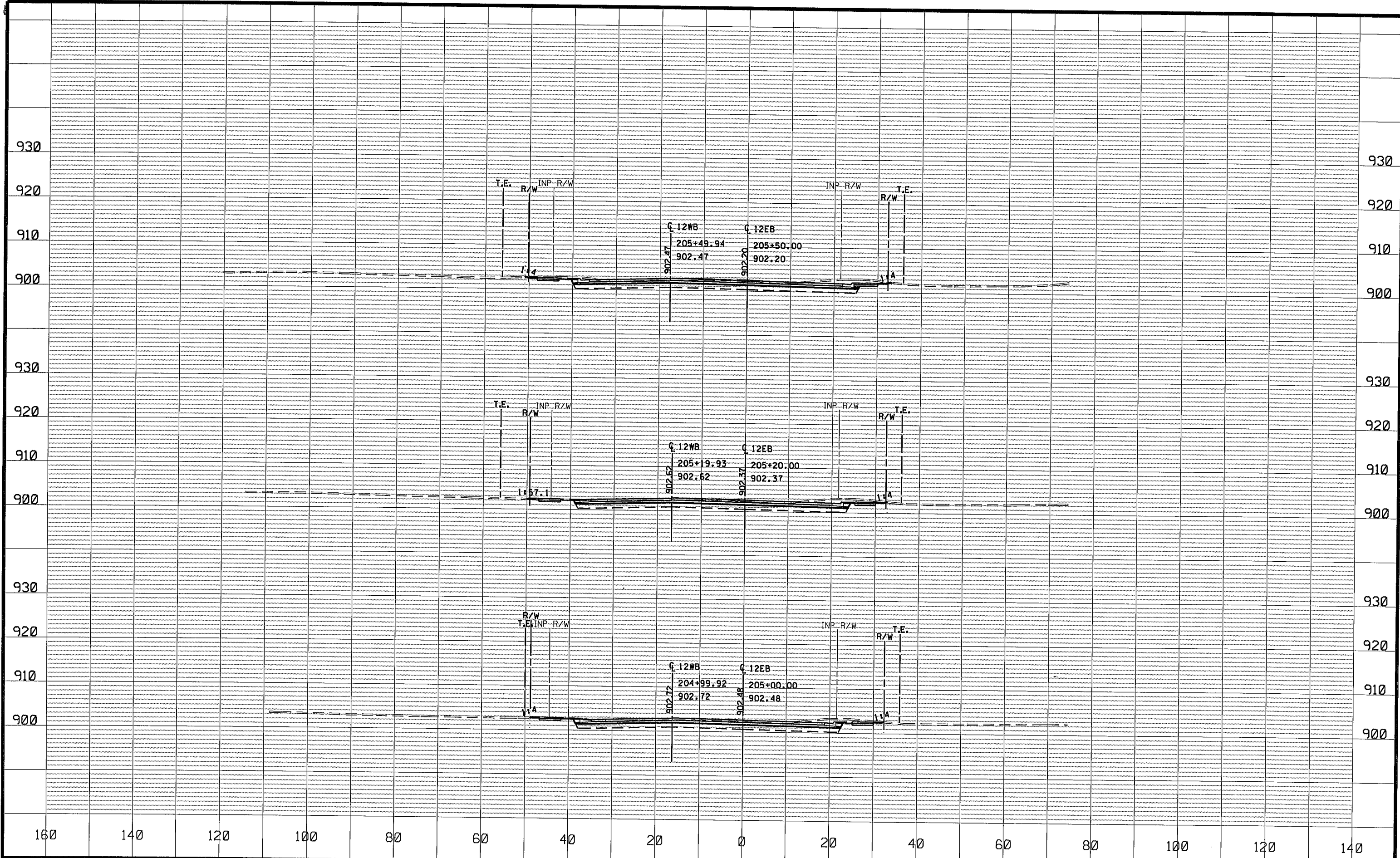
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 CHECKED BY JEO DATE 02/27/16



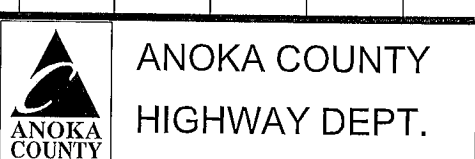
SP 002-611-034
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CROSS SECTIONS
 STA 203+50.00 TO 204+50.00
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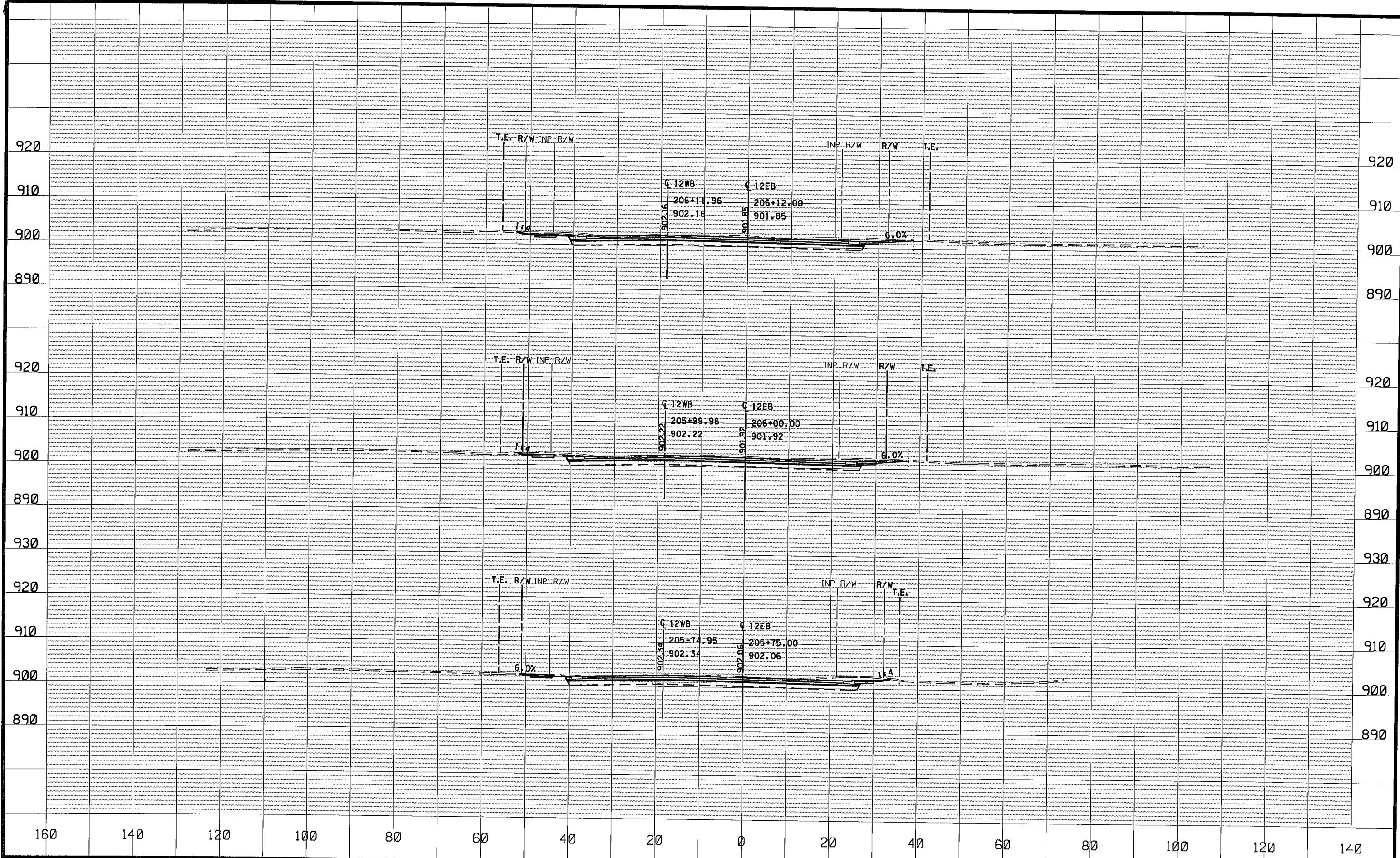
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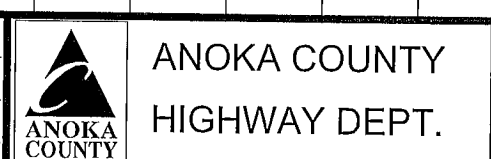
SP 002-611-034
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CROSS SECTIONS
 STA 205+00.00 TO 205+50.00
 Sheet 282 of 298 Sheets



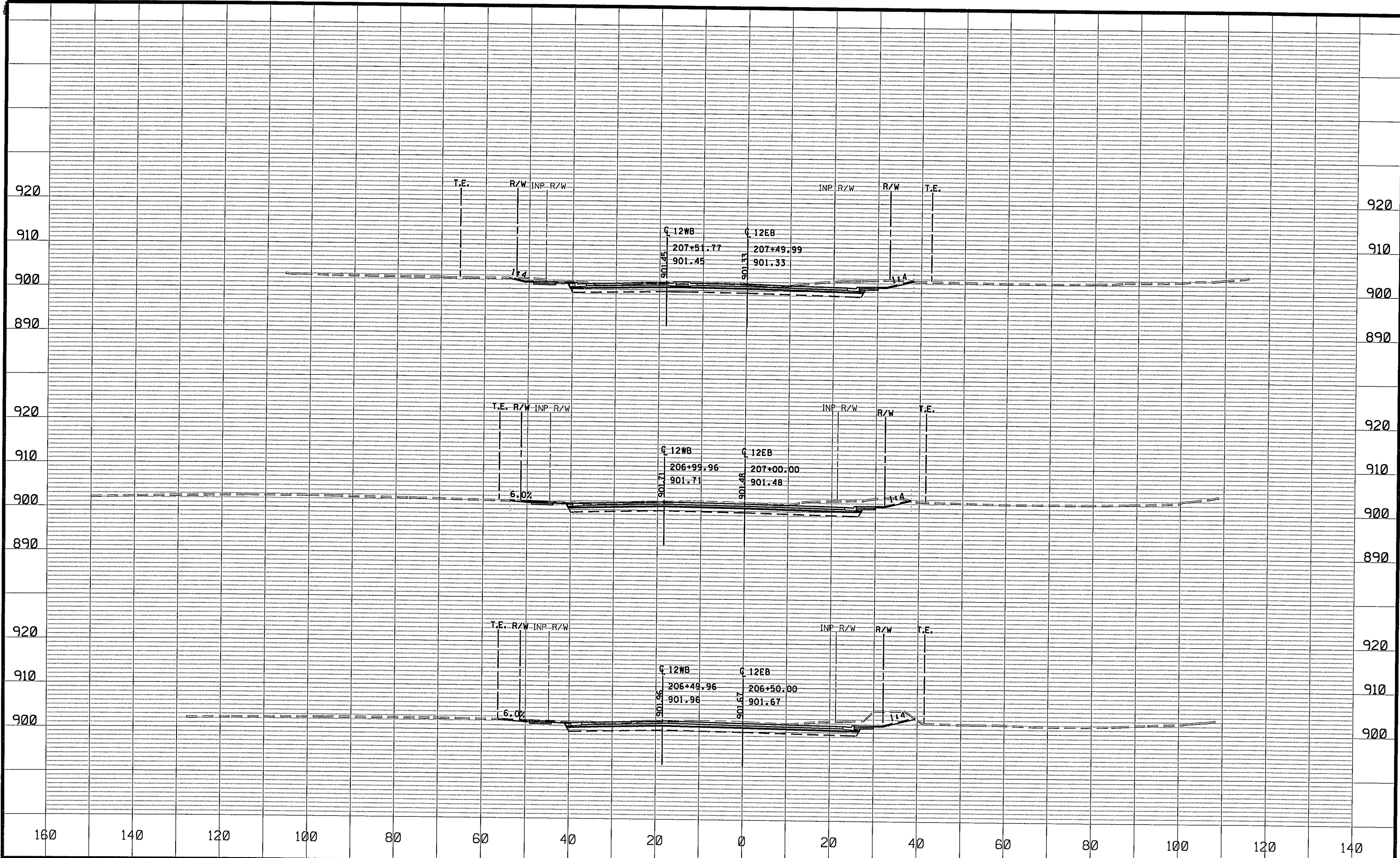
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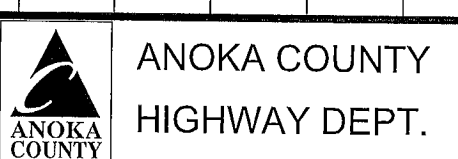
CROSS SECTIONS
 STA 205+75.00 TO 206+12.00
 Sheet 283 of 298 Sheets



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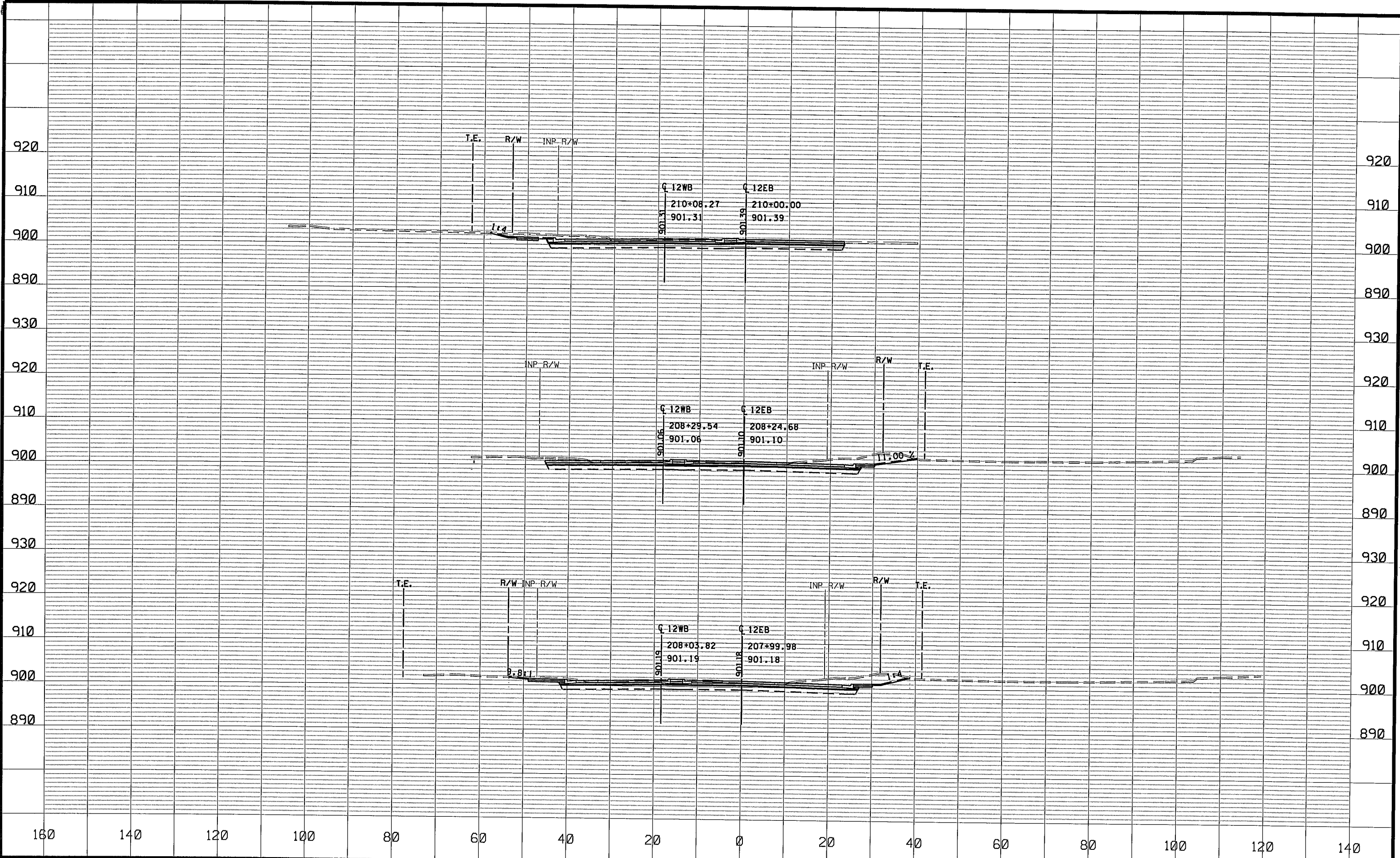
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 CHECKED BY: JEO DATE: 02/27/16



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CROSS SECTIONS
 STA 206+50.00 TO 207+49.99
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NO	DATE	BY	CKD	APPR	REVISION

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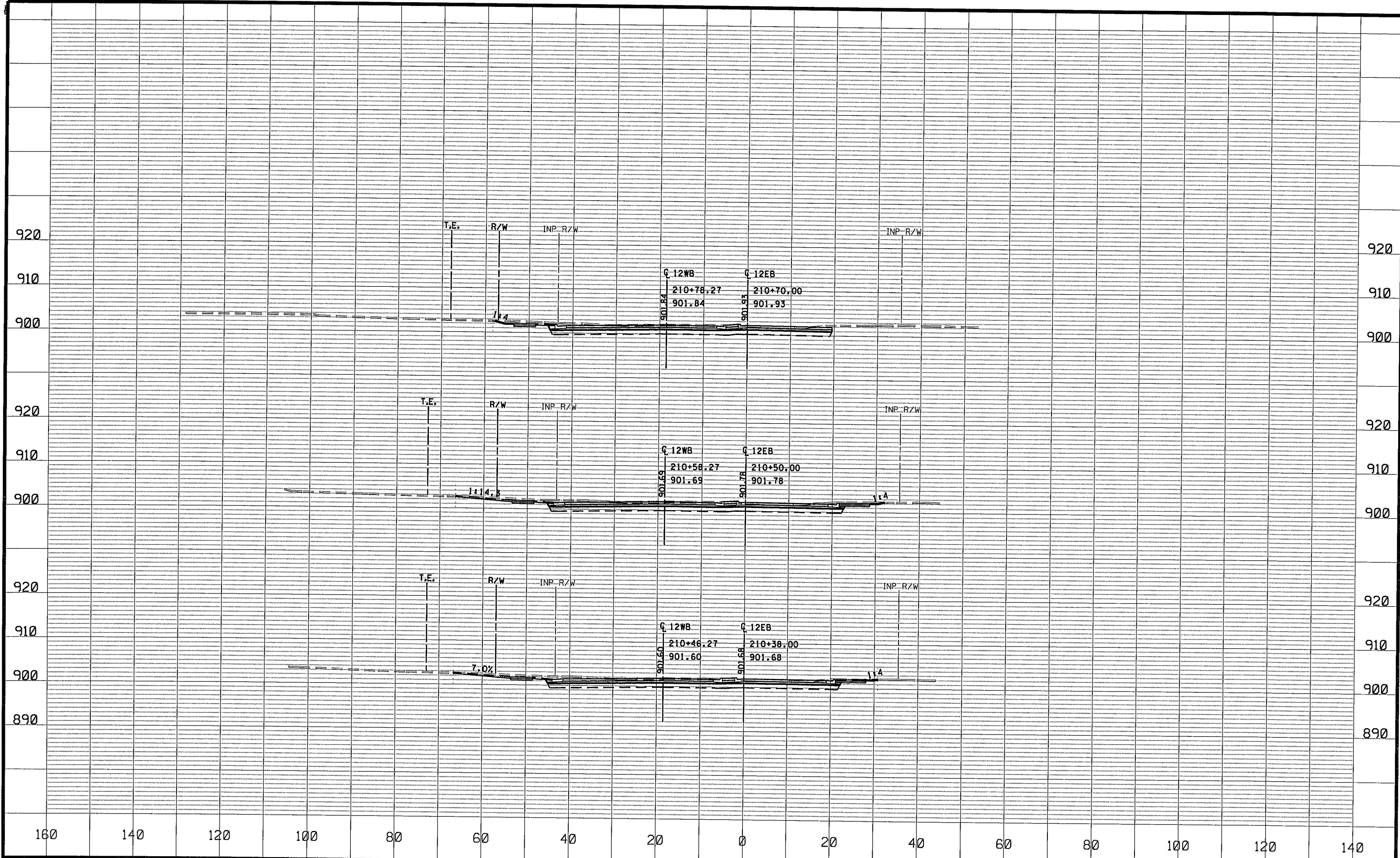
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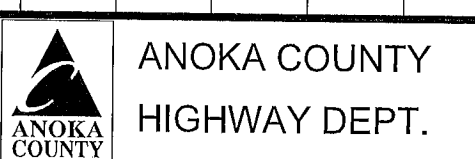
CROSS SECTIONS
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 Sheet 285 of 298 Sheets



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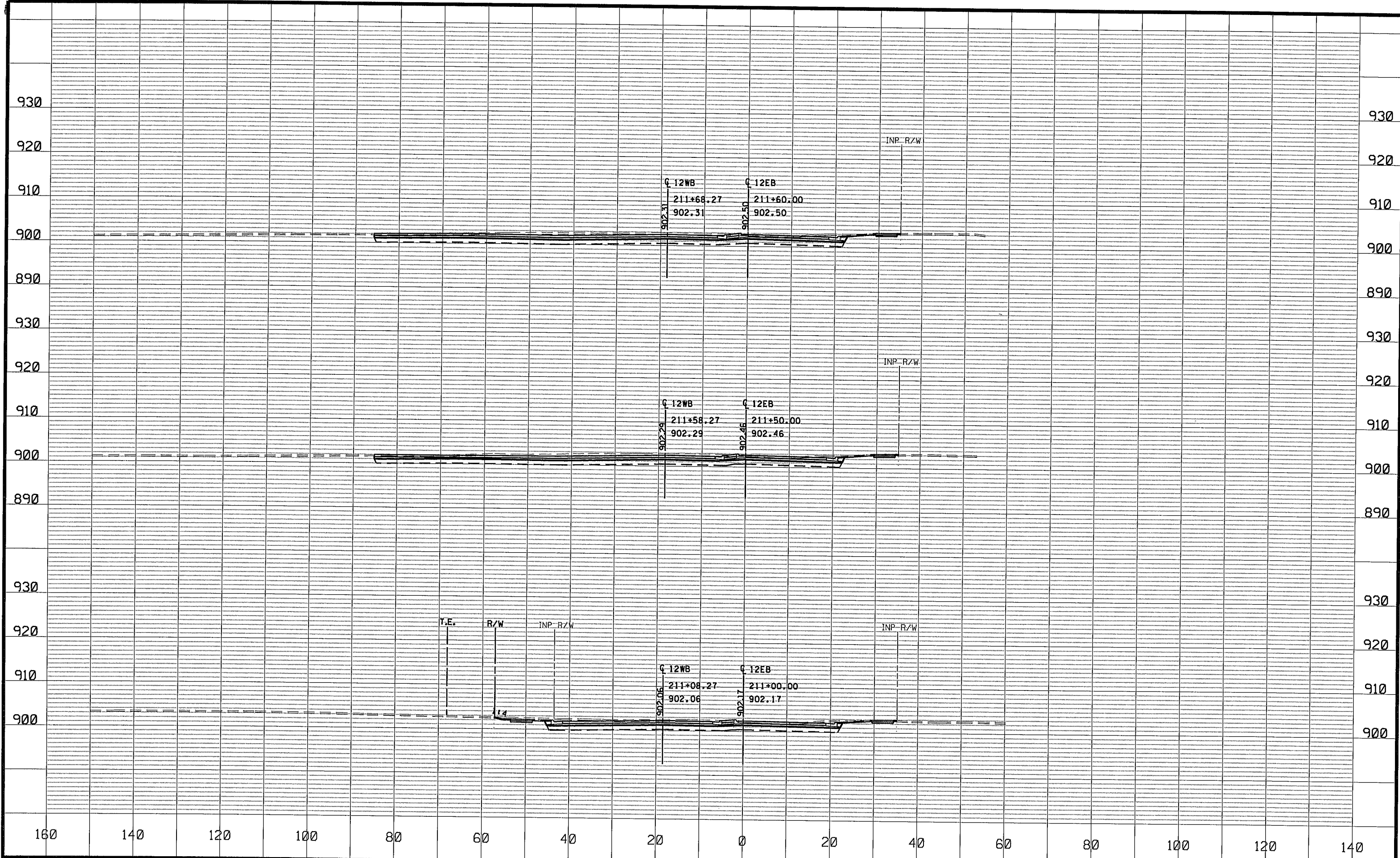
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 CHECKED BY: JEO DATE: 02/27/16



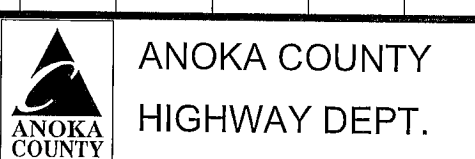
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CROSS SECTIONS
 STA 210+38.00 TO 210+70.00
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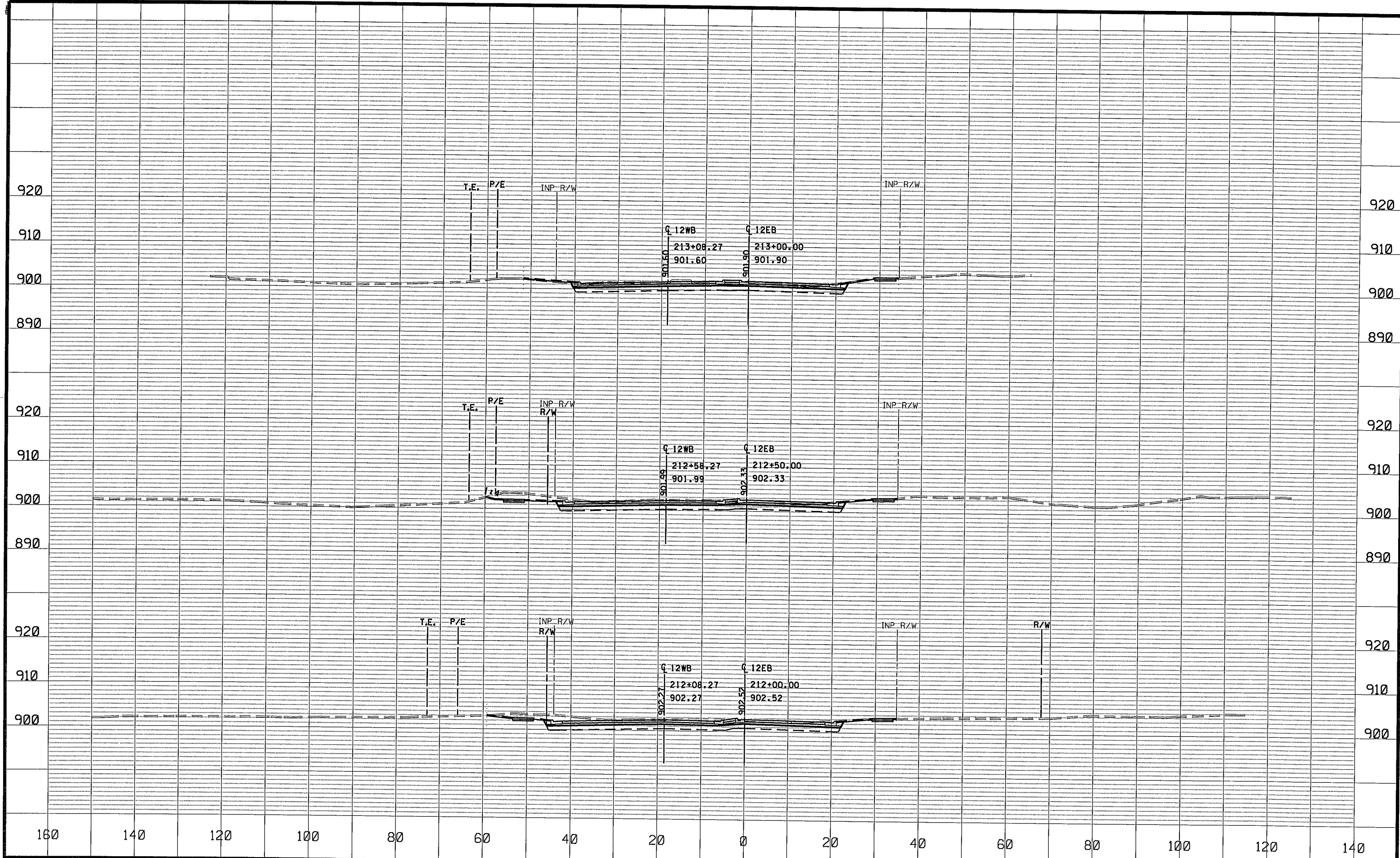
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 CHECKED BY JEO DATE 02/27/16



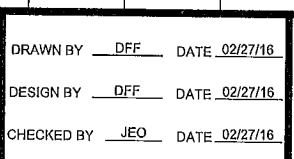
SP 002-611-034
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CROSS SECTIONS
 STA 211+00.00 TO 211+60.00
 Sheet 287 of 298 Sheets



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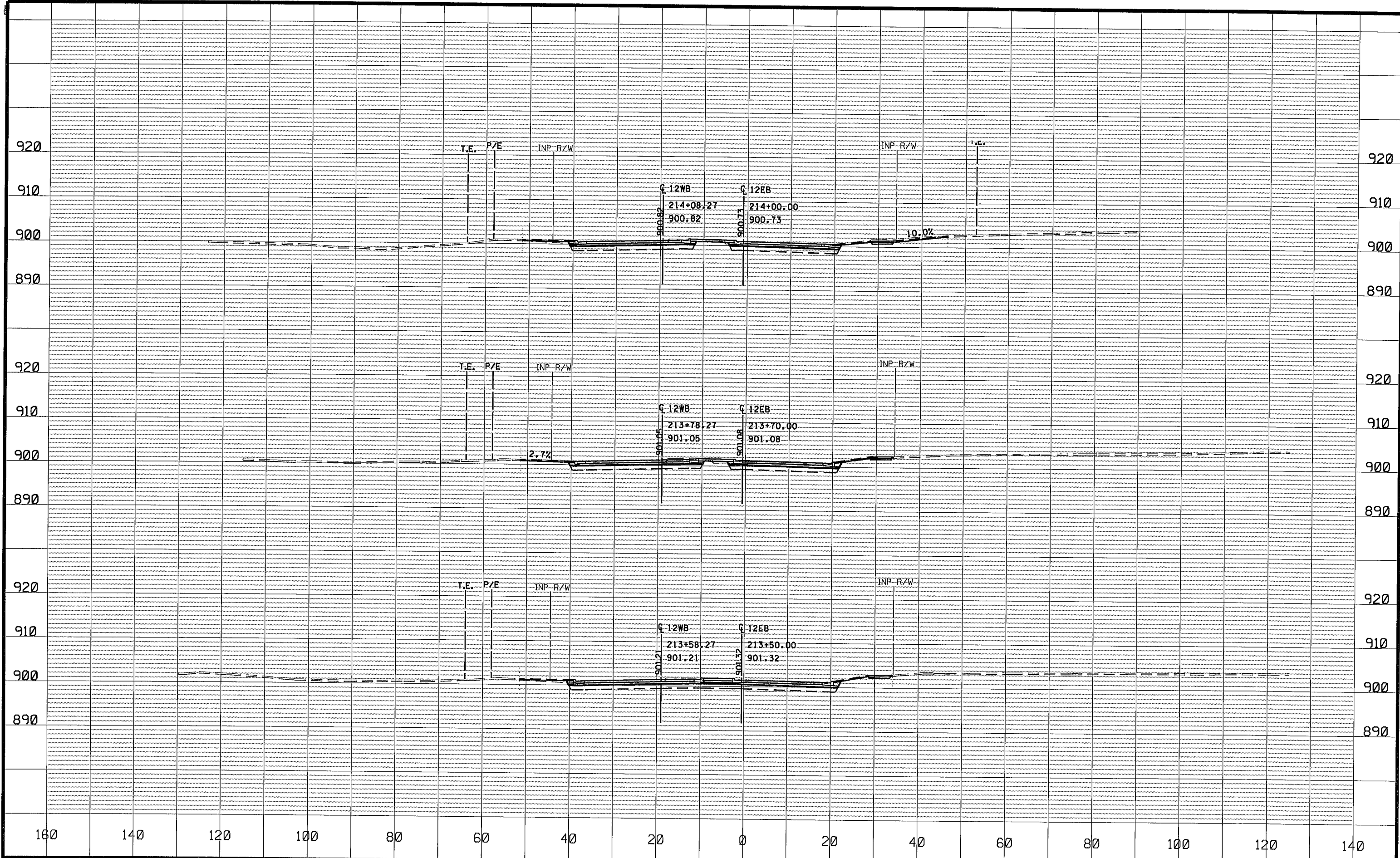
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DESIGN BY	DFP	DATE	02/27/16
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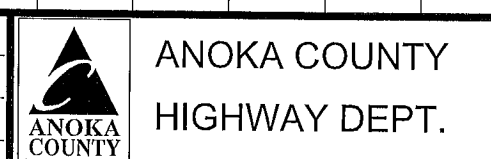
CROSS SECTIONS
STA 212+00.00 TO 213+00.00
Sheet 288 of 298 Sheets



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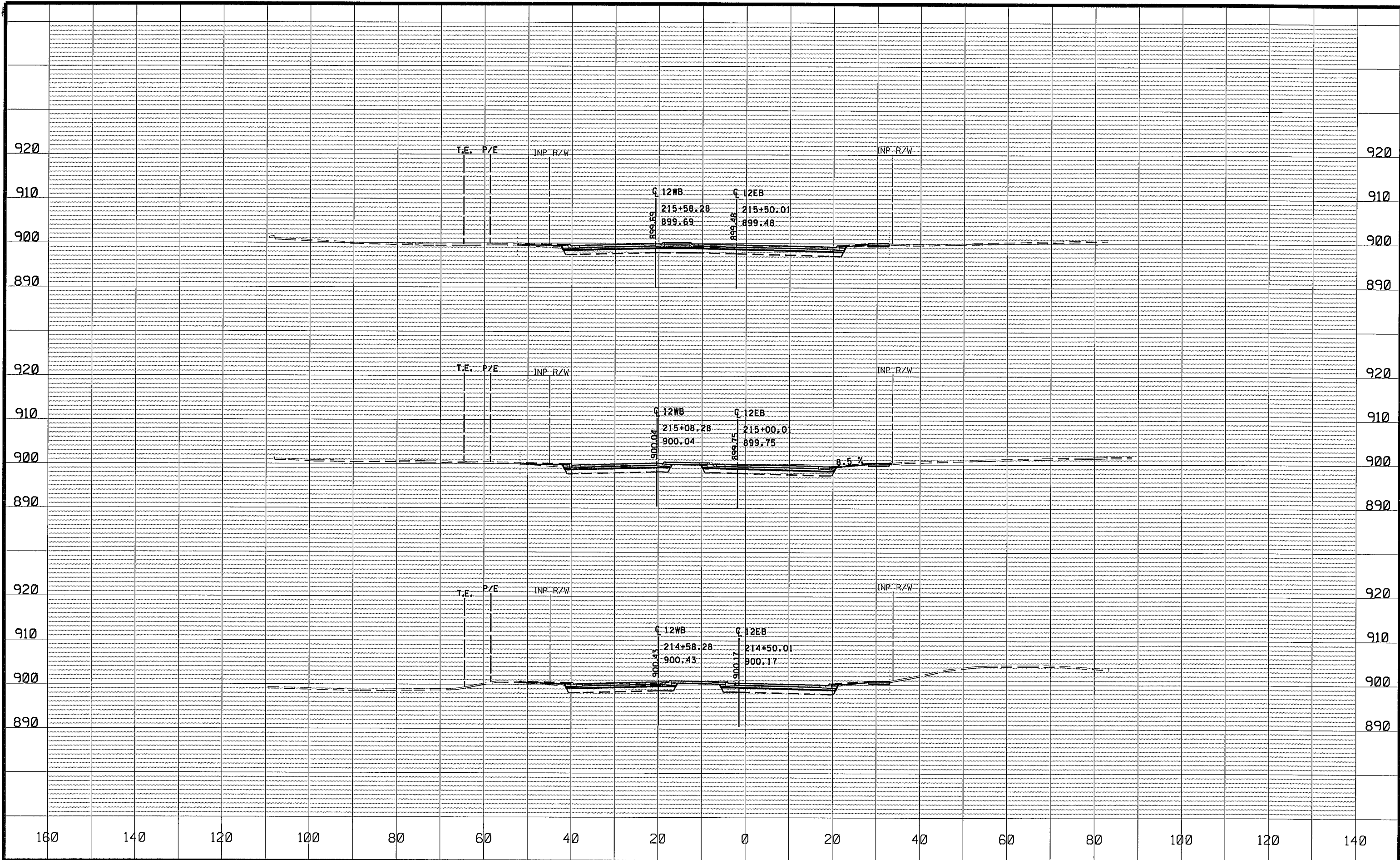
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 CHECKED BY: JEO DATE: 02/27/16



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CROSS SECTIONS
 STA 213+50.00 TO 214+00.00
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DRAWN BY DFF DATE 02/27/16
 DESIGN BY DFF DATE 02/27/16
 CHECKED BY JEO DATE 02/27/16



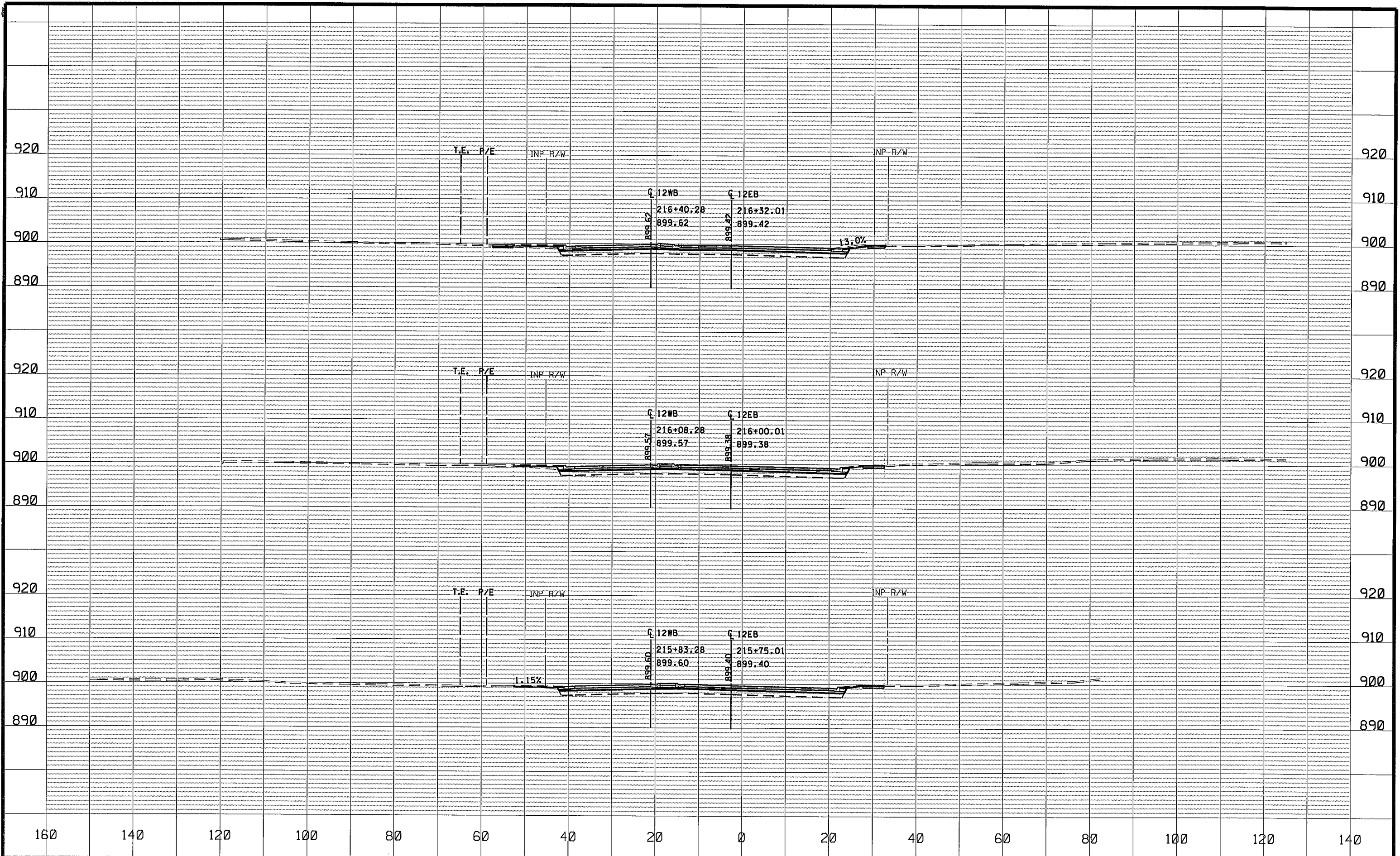
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CROSS SECTIONS
 STA 214+50.01 TO 215+50.01
 Sheet 290 of 298 Sheets

NO	DATE	BY	CHKD	APPR	REVISION

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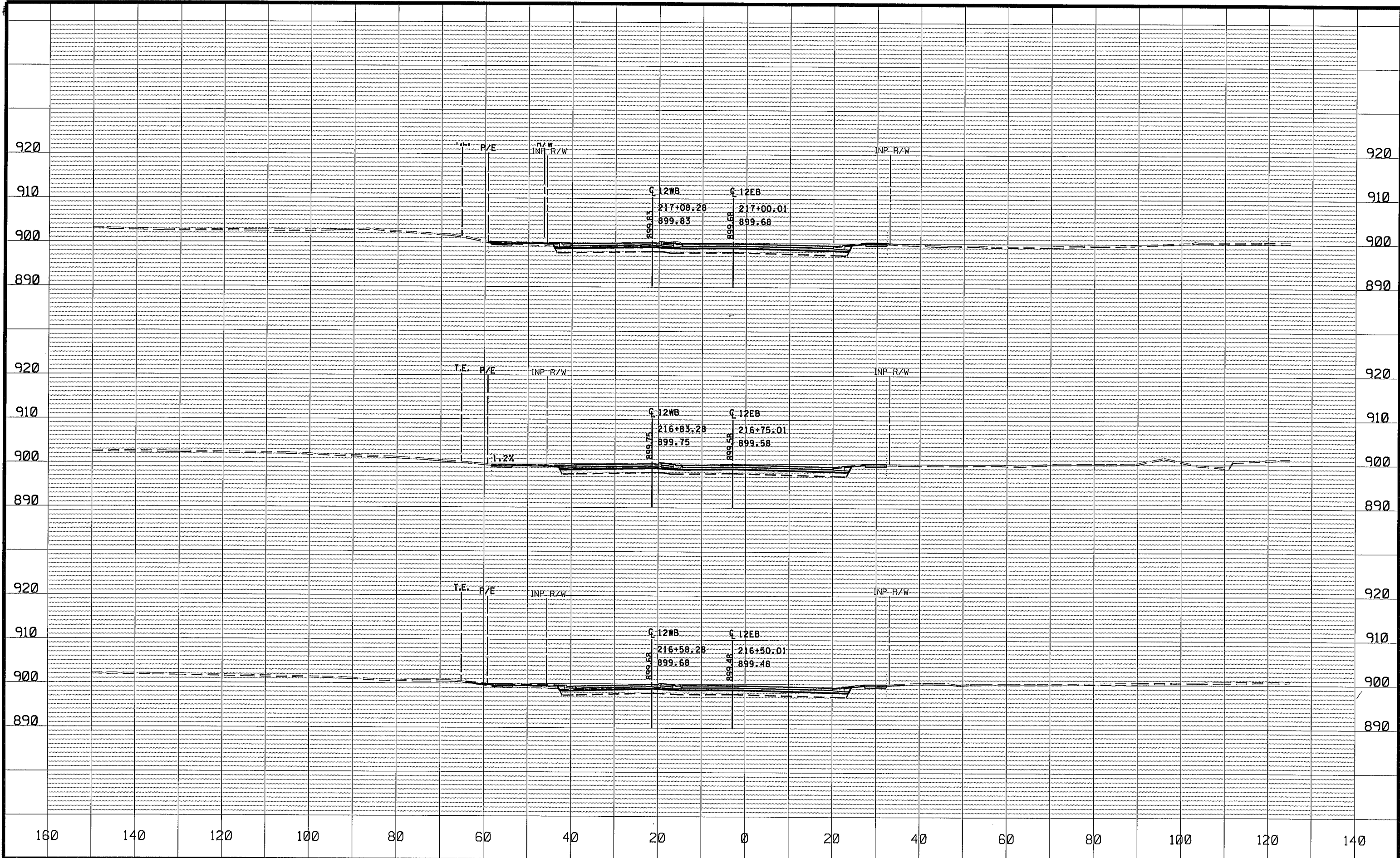
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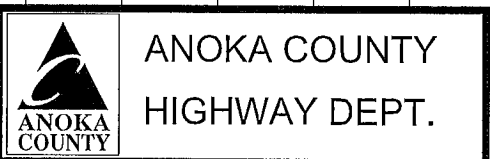
CROSS SECTIONS
 STA 215+75.01 TO 216+32.01
 Sheet 291 of 298 Sheets



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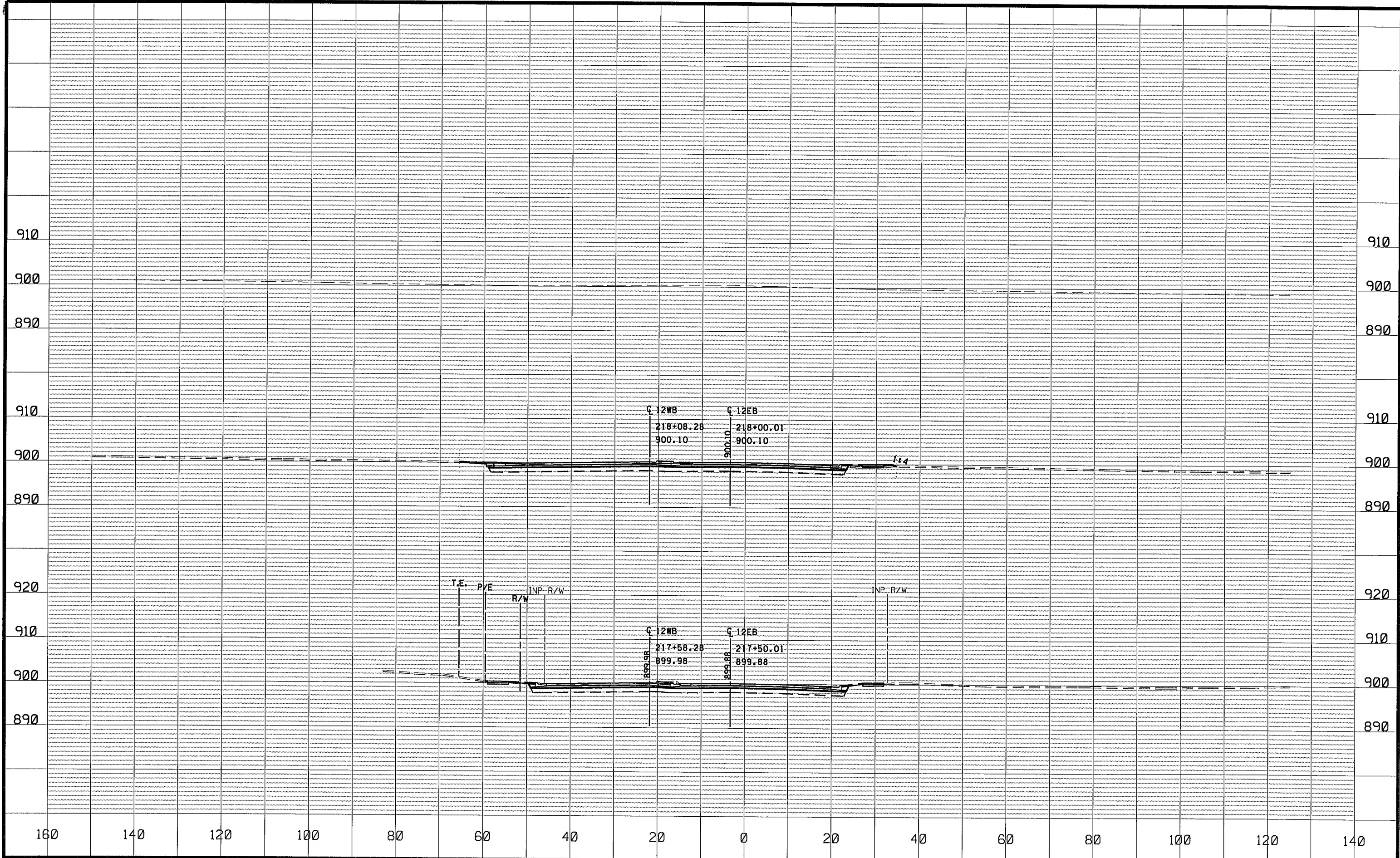
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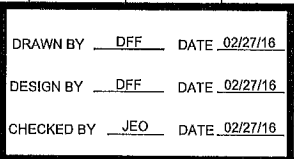
SP 002-611-034
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CROSS SECTIONS
 STA 216+50.01 TO 217+00.01
 Sheet 292 of 298 Sheets



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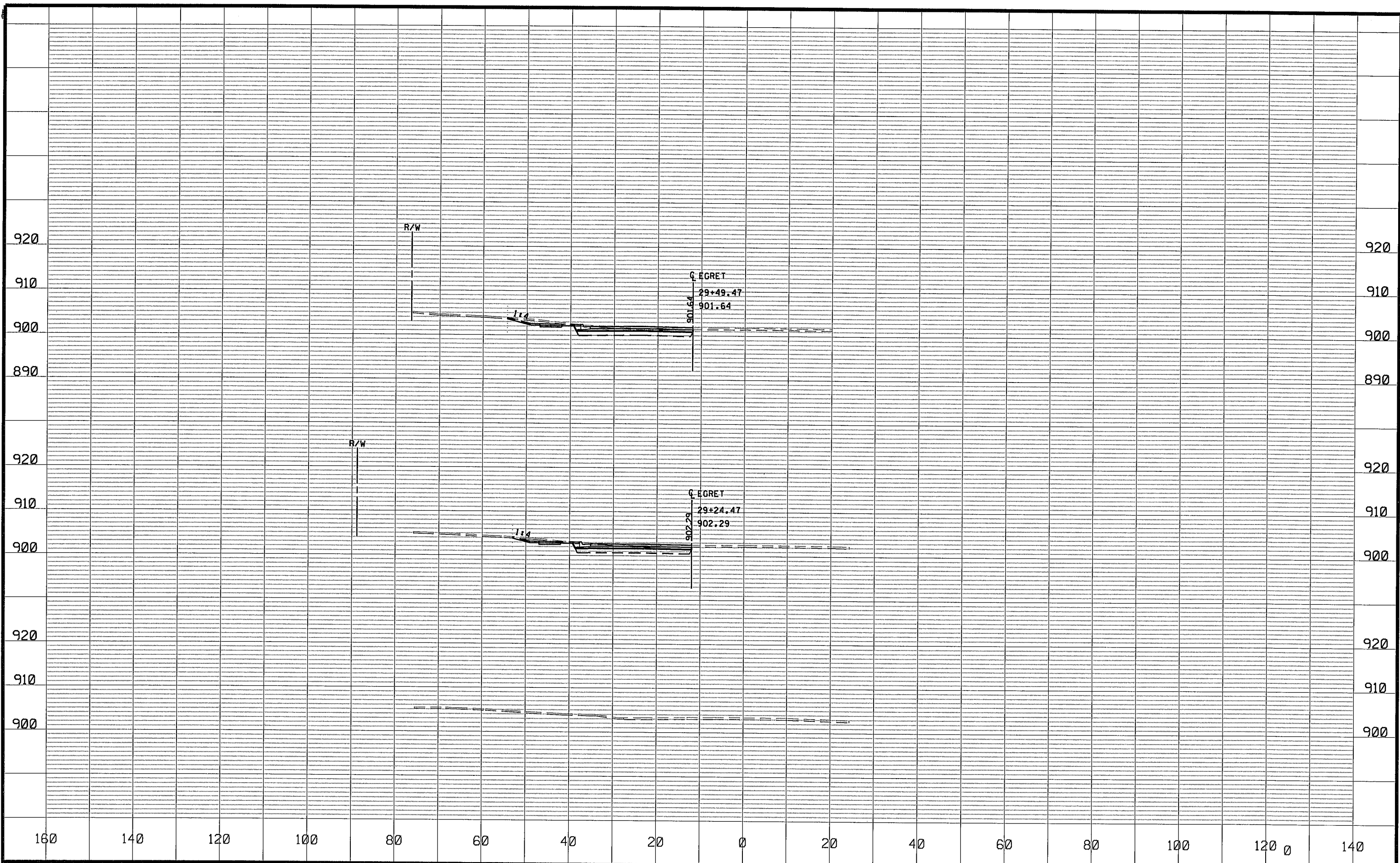
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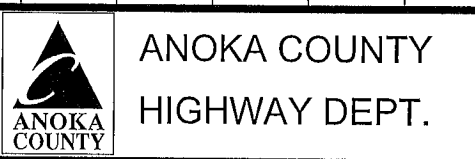
SP 002-611-034
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CROSS SECTIONS
STA 217+50.01 TO 218+50.00
Sheet 293 of 298 Sheets



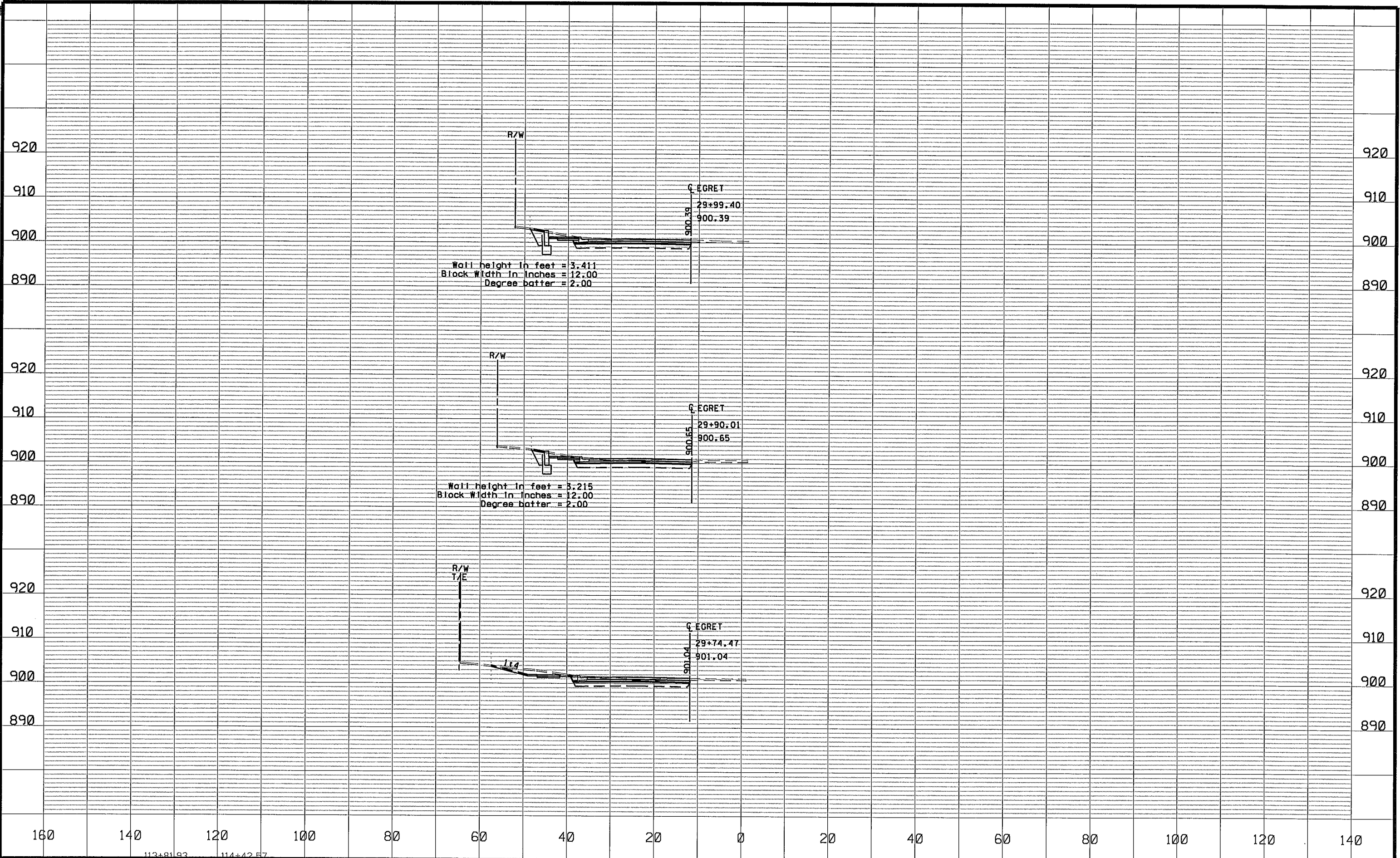
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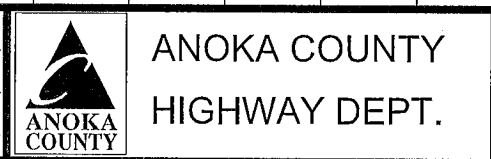
CROSS SECTIONS
 STA 29+00.00 TO 29+49.47
 Sheet 294 of 298 Sheets



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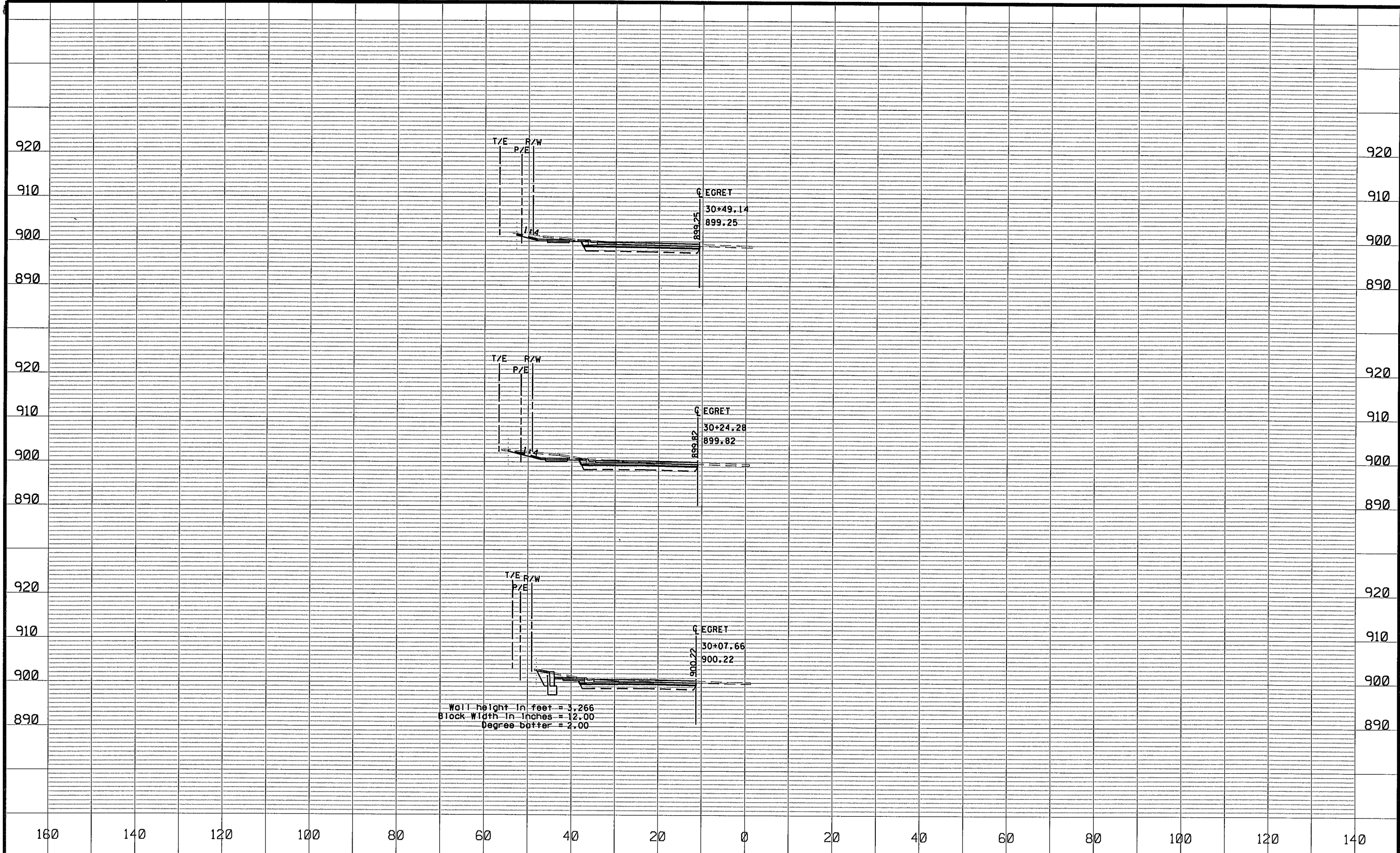
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CROSS SECTIONS
 STA 29+74.47 TO 29+99.40
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NO	DATE	BY	CKD	APPR	REVISION

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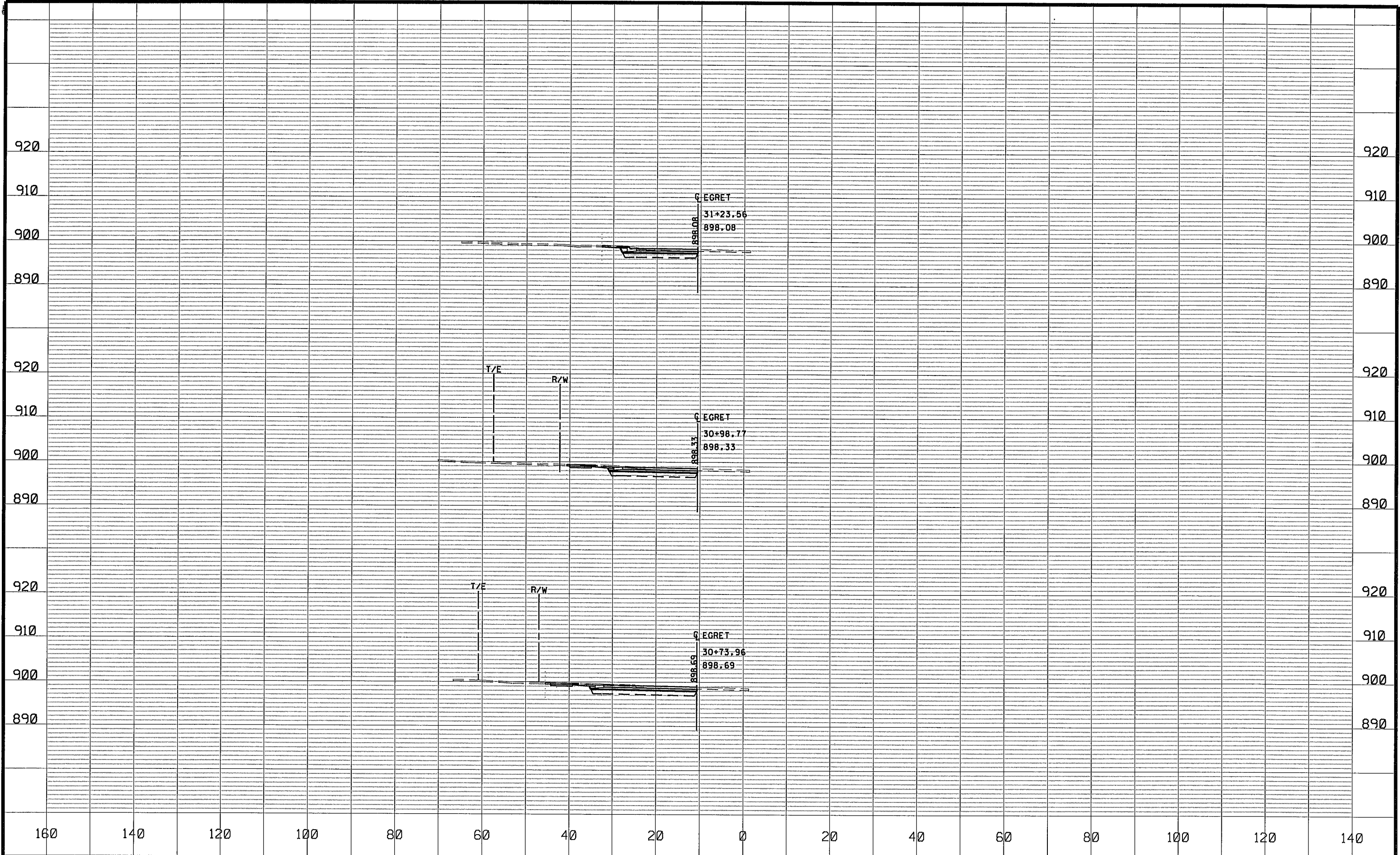
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CROSS SECTIONS
 STA 30+07.66 TO 30+49.14
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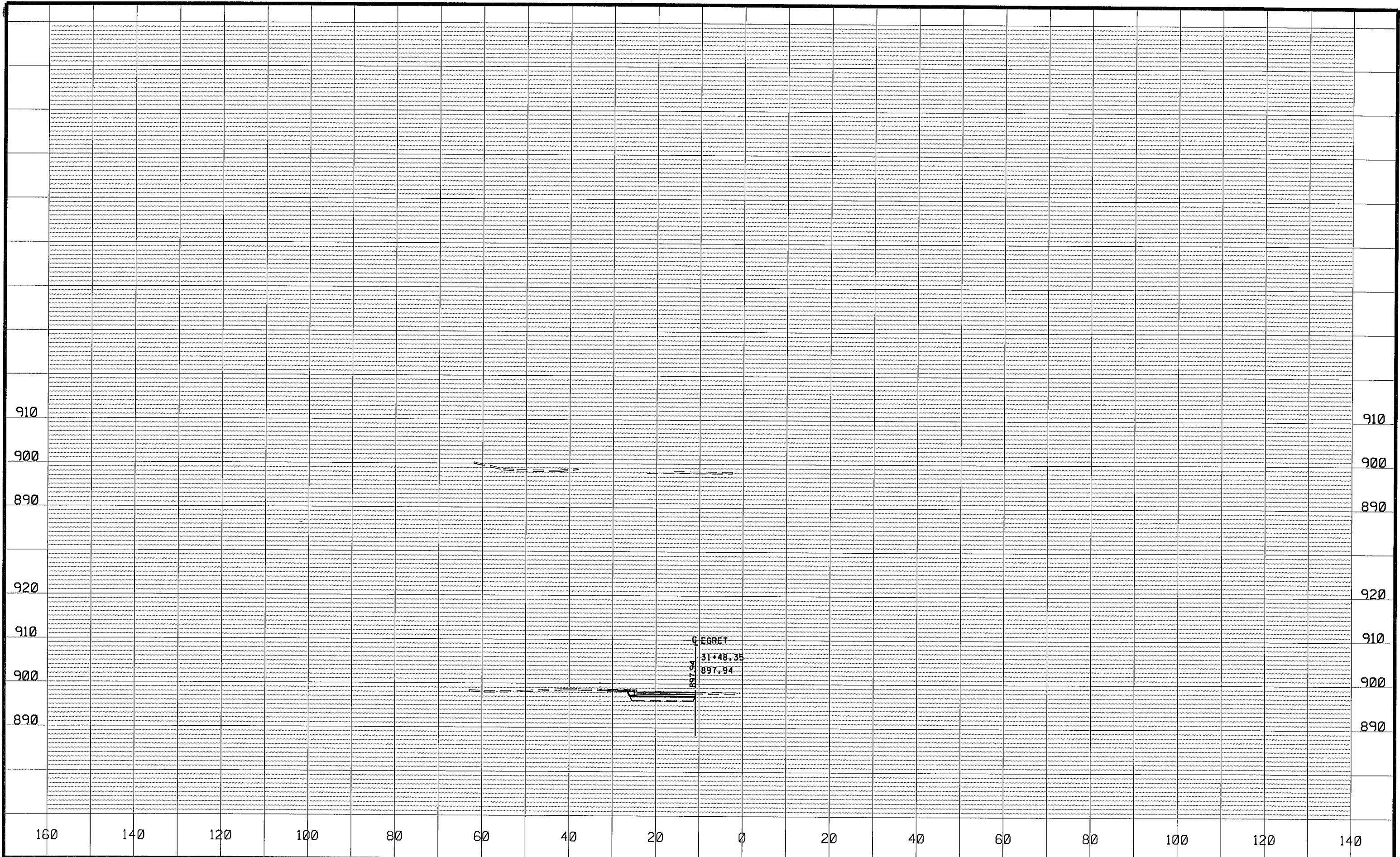
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CROSS SECTIONS
STA 30+73.96 TO 31+23.56
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DRAWN BY DFF DATE 02/27/16
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CROSS SECTIONS
 STA 31+48.35 TO 31+73.99
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