

PLANS SYMBOLS

STATE LINE	[Symbol]
COUNTY LINE	[Symbol]
TOWNSHIP OR RANGE LINE	[Symbol]
SECTION LINE	[Symbol]
QUARTER SECTION LINE	[Symbol]
SIXTEENTH LINE	[Symbol]
RIGHT OF WAY LINE	[Symbol]
RIGHT OF WAY LINE	[Symbol]
PRESENT RIGHT OF WAY LINE	[Symbol]
CENTRE LINE OF ALLEYS	[Symbol]
PROPERTY LINE (UNDEVELOPED)	[Symbol]
VACATED ALLEYS	[Symbol]
CORPORATE OR CITY LIMITS	[Symbol]
RAILROAD CENTER LINE	[Symbol]
RAILROAD WALL	[Symbol]
RAILROAD	[Symbol]
RAILROAD RIGHT OF WAY LINE	[Symbol]
NEVER OR CREEK	[Symbol]
DRY RUN	[Symbol]
DRAINAGE DITCH	[Symbol]
DRAIN TREE	[Symbol]
GRAVEL	[Symbol]
DRAIN TIE	[Symbol]
RAILROAD	[Symbol]
BARGE WARE HOUSE	[Symbol]
WOODEN WARE HOUSE	[Symbol]
QUINN WARE HOUSE	[Symbol]
RAILROAD SIDEWALK	[Symbol]
STONE WALL OR FENCE	[Symbol]
FENCE	[Symbol]
RAILROAD CROSSING DRAIN	[Symbol]
RAILROAD CROSSING HILL	[Symbol]
ELECTRIC WAREHOUSE	[Symbol]
CROSSING GATE	[Symbol]
SEMI-CIRCLE	[Symbol]
SPRING	[Symbol]
MARSH	[Symbol]

TERRAIN	[Symbol]
ORIGINAL	[Symbol]
BURSH	[Symbol]
MURDER	[Symbol]
CATCH BASIN	[Symbol]
TEE HYDRANT	[Symbol]
CATTLE GUARD	[Symbol]
OVERPASS (Highway Over)	[Symbol]
UNDERPASS (Highway Under)	[Symbol]
BRIDGE	[Symbol]
BUILDING (One Story Frame)	[Symbol]
F-FRAME	[Symbol]
S-STORE	[Symbol]
B-BLOCK	[Symbol]
ST-STUCCO	[Symbol]
RIDGE POLE OR IRON	[Symbol]
MOUNTAIN (STONE, CONCRETE, OR METAL)	[Symbol]
WOODEN RUB	[Symbol]
GRAVEL PIT	[Symbol]
SAND PIT	[Symbol]
DIRTY PIT	[Symbol]
ROCK QUARRY	[Symbol]

UTILITIES SYMBOLS

POWER POLE LINE	[Symbol]
TELEPHONE OR TELEGRAPH POLE LINE	[Symbol]
JOINT TELEPHONE AND POWER POLE LINES	[Symbol]
ON TELEPHONE POLES	[Symbol]
ARCHWAY	[Symbol]
STEEL TOWER	[Symbol]
STREET LIGHT	[Symbol]
PEDESTAL (TELEPHONE CABLE TERMINAL)	[Symbol]
GAS MAIN	[Symbol]
WATER MAIN	[Symbol]
CURB CUT	[Symbol]
TELEPHONE CABLE IN CURB CUT	[Symbol]
ELECTRIC CABLE IN CURB CUT	[Symbol]
TELEPHONE MAINLINE	[Symbol]
ELECTRIC MAINLINE	[Symbol]
BURIED TELEPHONE CABLE	[Symbol]
BURIED ELECTRIC CABLE	[Symbol]
AIRIAL TELEPHONE CABLE	[Symbol]
SEWER (SANITARY OR STORM)	[Symbol]
SEWER MAINLINE	[Symbol]

SCALES

PLAN	1" = 50'
PROFILE	1" = 5'
INDEX MAP	1" = 2.4 MI
GENERAL LAYOUT	N/A

MINNESOTA DEPARTMENT OF TRANSPORTATION

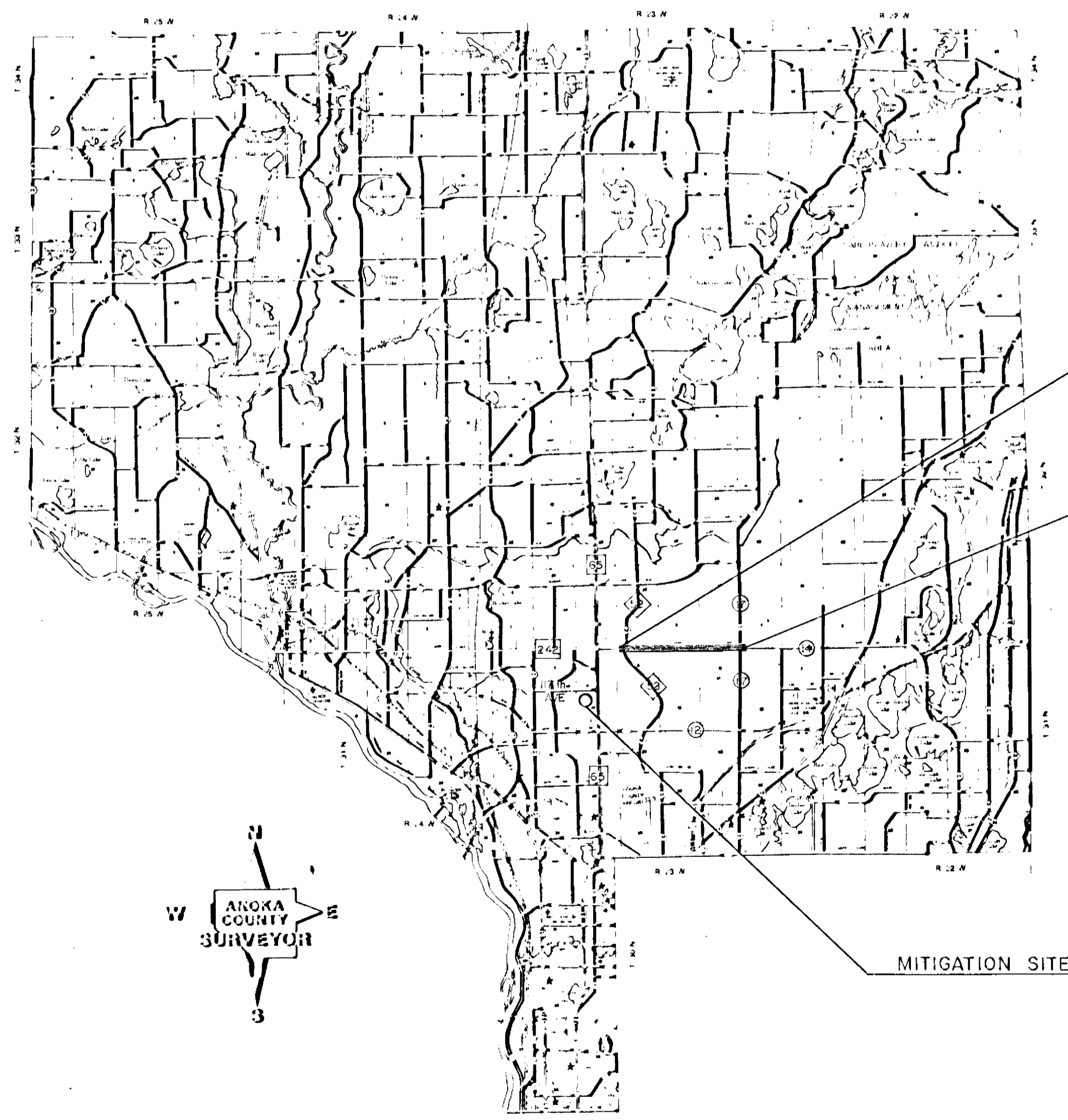
ANOKA COUNTY

CONSTRUCTION PLAN FOR GRADING, AGGREGATE BASE, BITUMINOUS SURFACING AND SIGNAL SYSTEMS

LOCATED ON C.S.A.H. 14 BETWEEN C.R. 52 (RADISSON ROAD) AND C.S.A.H. 17 (LEXINGTON AVENUE) (Geographic Description)

FROM A POINT 174.74' E. OF THE NW CORNER OF SECTION 9, T.31N, R.23W TO A POINT 1031.20' E. OF THE NW CORNER OF SECTION 12, T.31N, R.23W (Legal Description)

STATE AID PROJ. NO. 02-614-14, 106-020-06	STATE AID PROJ. NO.
GROSS LENGTH 16,747.00 FEET 3.172 MILES	GROSS LENGTH FEET MILES
BRIDGES-LENGTH 0.00 FEET 0.000 MILES	BRIDGES-LENGTH FEET MILES
EXCEPTIONS-LENGTH 0.00 FEET 0.000 MILES	EXCEPTIONS-LENGTH FEET MILES
NET LENGTH 16,747.00 FEET 3.172 MILES	NET LENGTH FEET MILES



MINN. PROJ. NO.
MINN. PROJ. NO.

GOVERNING SPECIFICATIONS

THE 1988 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" AS AMENDED BY THE JANUARY 2, 1991 SUPPLEMENTAL SPECIFICATIONS SHALL GOVERN.

INDEX

SHEET NO.	DESCRIPTION
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89	SHORT-TERM LANE CLOSURE
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THIS PLAN CONTAINS 117 SHEETS

DESIGN DESIGNATION

	T.H. 65-CR 52	C.R. 52-C.S.A.H. 17
EN 18 20	2,044,249	1,327,222
R Value	60	50
ADT (1992) =	11,459	7,439
Proj. ADT (201) =	19,480	12,646
Soil Factor	N.A.	
10	on Design	

Functional Classification Low Density Arterial
 No. of Traffic Lanes 2 No. of Parking Lanes 0
 Design Speed 55 MPH
 Based on Stopping Sight Distance
 Height of eye 3.5 Height of object 0.5
 Design Speed not achieved at:
 STA TO STA MPH
 STA TO STA MPH
 STA TO STA MPH

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

DATE 7-10-92 REG. NO. 20235 ENGR. *Greg Anderson*

DESIGN SQUAD GREG ANDERSON

Recommended for Approval: *Michael R. Kelly* July 10, 1992
 Recommended for Approval: *Alan K. Kelleher* July 10, 1992
 Recommended for Approval: *Alan K. Kelleher* July 10, 1992
 Approved: 7/10/92 *Verde K. Kelly*
 Approved: 7/11/92 *Charles Kelleher*
 Recommended for Approval: *Greg Anderson* 8/12/92
 Recommended for Approval: *Mark J. Jurek* 8/11/92
 Approved: 8/11/92 *Quinn E. Carlson*

C.S.A.H. 14, STATION 32+20 TO STATION 199+67

EXCAVATION:

COMMON: 43,092 CU. YDS. — TOPSOIL: 7,704 CU. YDS.
 REGULAR: 11,863 CU. YDS. (1)
 SUBCUT: 33,368 CU. YDS.
 SUBGRADE: 32,837 CU. YDS.
 MUCK: 75,637 CU. YDS.
 CHANNEL: 2,476 CU. YDS.

EMBANKMENT: (CV)

SELECT GRANULAR: 39,104 CU. YDS.
 GRANULAR: 141,902 CU. YDS.
 MUCK: 21,511 CU. YDS.
 TOPSOIL DRESSING: 6,736 CU. YDS.

BALANCE:

SELECT GRANULAR: 36,348 C.Y.(LV) FROM MITIGATION SITE; 22,308 C.Y.(LV) FROM BORROW.
 GRANULAR: 119,861 C.Y.(LV) FROM MITIGATION SITE; 3,016 C.Y.(LV) FROM C.R. 52; 5,055 C.Y.(LV) FROM C.S.A.H. 17; 2,020 C.Y.(EV) FROM COMMON EXCAVATION; 66,205 C.Y.(EV) FROM SUBCUT AND SUBGRADE EXCAVATION.
 TOPSOIL: 7,704 C.Y.(EV) FROM TOPSOIL STRIPPING; 2,476 C.Y.(EV) FROM CHANNEL EXCAVATION; 1,453 C.Y.(EV) TO BE DISPOSED.
 MUCK: 46,632 C.Y.(EV) TO MITIGATION SITE; 25,813 C.Y.(EV) ON SLOPES; 3,990 C.Y.(LV) TO BE DISPOSED.

(1) INCLUDES 9,843 CU. YDS. BITUMINOUS REMOVAL.

EARTHWORK SUMMARY (P)

C.R. 52, STATION 501+00 TO STATION 509+93

EXCAVATION:

COMMON: 2,981 CU. YDS. — TOPSOIL: 403 CU. YDS.
 REGULAR: 1,741 CU. YDS. (1)
 SUBCUT: 1,424 CU. YDS.
 SUBGRADE: 0 CU. YDS.
 MUCK: 0 CU. YDS.
 CHANNEL: 0 CU. YDS.

EMBANKMENT: (CV)

SELECT GRANULAR: 1,733 CU. YDS.
 GRANULAR: 0 CU. YDS.
 MUCK: 0 CU. YDS.
 TOPSOIL DRESSING: 327 CU. YDS.

BALANCE:

SELECT GRANULAR: 2,600 C.Y.(LV) FROM BORROW.
 GRANULAR: 2,578 C.Y.(EV) EXCESS FOR C.S.A.H. 14 CONSTRUCTION.
 TOPSOIL: 403 C.Y.(EV) FROM TOPSOIL STRIPPING; 0 C.Y. EXCESS.
 MUCK: 0 C.Y.

(1) INCLUDES 587 CU. YDS. BITUMINOUS REMOVAL.

PROJECT TOTALS

EXCAVATION:

COMMON: 52,812 CU. YDS.* — TOPSOIL: 9,857 CU. YDS.
 REGULAR: 14,604 CU. YDS.
 SUBCUT: 38,198 CU. YDS.
 SPECIAL: 158,157 CU. YDS.* — TOPSOIL: 12,362 CU. YDS.
 GRANULAR: 145,795 CU. YDS.
 SUBGRADE: 32,837 CU. YDS.*
 MUCK: 75,637 CU. YDS.*
 CHANNEL: 2,476 CU. YDS.*

EMBANKMENT: (CV)

SELECT GRANULAR: 46,965 CU. YDS.
 GRANULAR: 142,714 CU. YDS.
 MUCK: 61,445 CU. YDS.
 TOPSOIL DRESSING: 18,237 CU. YDS.

BORROW: (LV)

SELECT GRANULAR: 34,100 CU. YDS.*

EXCESS:

MUCK: 5,601 CU. YDS. (LV)
 TOPSOIL: 1,859 CU. YDS. (EV)

* SIGNIFIES PAY ITEM.

(1) INCLUDES 11,722 CU. YDS. BITUMINOUS REMOVAL.

C.S.A.H. 17, STA. 689+80 TO 699+00 & STA. 701+00 TO 710+23

EXCAVATION:

COMMON: 6,739 CU. YDS. — TOPSOIL: 1,452 CU. YDS.
 REGULAR: 3,173 CU. YDS. (1)
 SUBCUT: 3,406 CU. YDS.
 SUBGRADE: 0 CU. YDS.
 MUCK: 0 CU. YDS.
 CHANNEL: 0 CU. YDS.

EMBANKMENT: (CV)

SELECT GRANULAR: 6,128 CU. YDS.
 GRANULAR: 812 CU. YDS.
 MUCK: 1,074 CU. YDS.
 TOPSOIL DRESSING: 872 CU. YDS.

BALANCE:

SELECT GRANULAR: 9,192 C.Y.(LV) FROM BORROW.
 GRANULAR: 974 C.Y.(EV) FROM SUBCUT; 4,313 C.Y.(EV) EXCESS FOR C.S.A.H. 14 CONSTRUCTION.
 TOPSOIL: 1,046 C.Y.(EV) FROM TOPSOIL STRIPPING; 406 C.Y.(EV) TO BE DISPOSED.
 MUCK: 1,611 C.Y.(LV) FROM C.S.A.H. 14 TO BE DISPOSED.

(1) INCLUDES 1,292 CU. YDS. BITUMINOUS REMOVAL.

MITIGATION SITE

EXCAVATION:

SPECIAL: 158,157 CU. YDS. — TOPSOIL: 12,362 CU. YDS.
 REGULAR: 145,795 CU. YDS.
 SUBGRADE: 0 CU. YDS.
 MUCK: 0 CU. YDS.
 CHANNEL: 0 CU. YDS.

EMBANKMENT: (CV)

SELECT GRANULAR: 0 CU. YDS.
 GRANULAR: 0 CU. YDS.
 MUCK: 38,860 CU. YDS.
 TOPSOIL DRESSING: 10,302 CU. YDS.

BALANCE:

SELECT GRANULAR: 33,925 C.Y.(EV) FOR C.S.A.H. 14 CONSTRUCTION.
 GRANULAR: 111,870 C.Y.(EV) FOR C.S.A.H. 14 CONSTRUCTION.
 TOPSOIL: 12,362 C.Y.(EV) FROM TOPSOIL STRIPPING; 0 C.Y. EXCESS.
 MUCK: 58,290 C.Y.(LV) FROM C.S.A.H. 14 EXCESS.

SOILS AND CONSTRUCTION NOTES

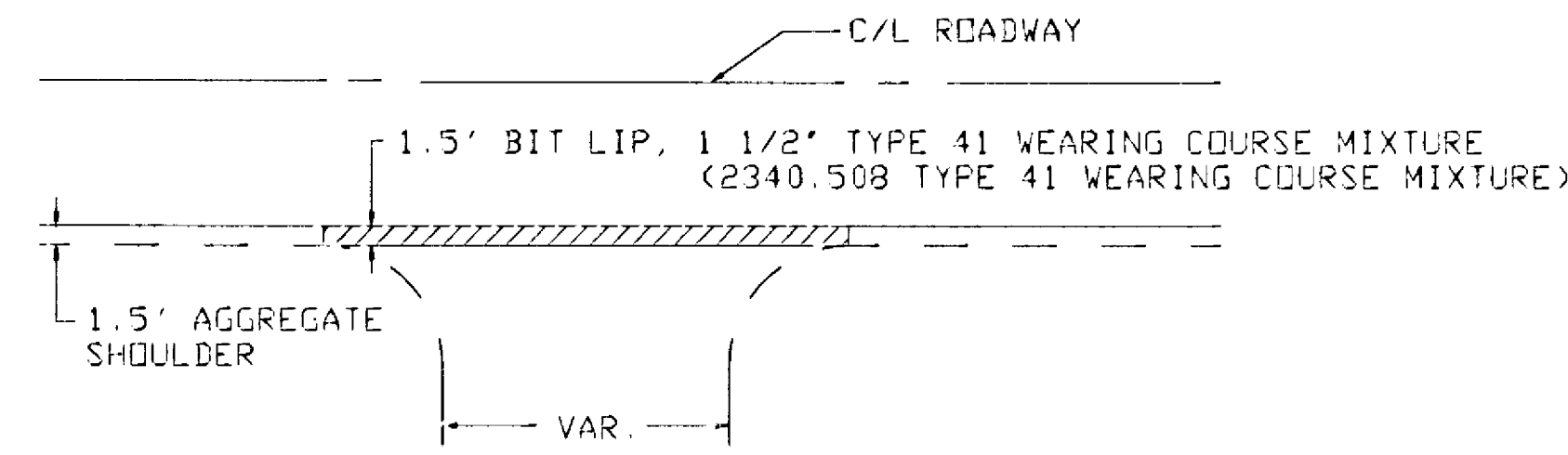
- TOP OF GRADING SUBGRADE IS DEFINED AS THE BOTTOM OF THE AGGREGATE BASE.
- IN FILL AREAS, THE SUBGRADE SHALL BE CONSTRUCTED WITH SELECTED GRADING MATERIAL.
- SELECTED GRADING MATERIALS SHALL CONSIST OF GRANULAR MATERIALS, OF WHICH THE UPPER 1.0 FOOT OF THE SUBGRADE SHALL CONSIST OF SELECT GRANULAR MATERIALS.
- GRANULAR MATERIAL, REGARDLESS OF SOURCE, SHALL MEET THE REQUIREMENTS OF SPEC. 3149.2A.
- SELECT GRANULAR MATERIAL SHALL MEET THE REQUIREMENTS OF SPEC. 3149.2B.
- COMPACTION OF THE GRADING PORTION OF THIS PROJECT SHALL BE BY THE "SPECIFIED DENSITY METHOD".
- TEST ROLLING WILL NOT BE REQUIRED.
- BITUMINOUS AND/OR CONCRETE ITEMS REMOVED BY CONSTRUCTION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL EITHER BE RECYCLED OR DISPOSED OF OFF THE PROJECT LIMITS.
- DISPOSITION OF EXCAVATED MATERIAL SHALL BE IN ACCORDANCE WITH SPEC. 2105.3D WITH NO DIRECT COMPENSATION MADE THEREFORE.

- WHERE MATCHING INTO THE INPLACE ROADWAY AT THE ENDS OF CONSTRUCTION, CUT VERTICALLY TO THE TOP OF THE GRADING SUBGRADE AND THEN AT A 20:1 TAPER TO THE BOTTOM OF THE RECOMMENDED SUBGRADE EXCAVATION.
- WHERE CONNECTING NEW SURFACING TO AN INPLACE PAVEMENT, THE EXCAVATION SHALL BE BACKFILLED PROMPTLY TO AVOID UNDERMINING THE INPLACE PAVEMENT.
- USE TACK COAT BETWEEN ALL BITUMINOUS MIXTURES PRIOR TO PLACING BITUMINOUS MIXTURES AND PRIOR TO PLACING ANY BITUMINOUS MIXTURES ON EXISTING CONCRETE OR BITUMINOUS SURFACES. THE BITUMINOUS TACK COAT MATERIAL SHALL BE APPLIED AT A UNIFORM RATE OF 0.03 TO 0.05 GALLONS PER SQUARE YARD BETWEEN BITUMINOUS LAYERS. THE APPLICATION RATES ARE FOR UNDILUTED EMULSIONS (AS SUPPLIED FROM THE REFINERY); ASPHALT EMULSION MAY BE FURTHER DILUTED IN THE FIELD IN ACCORDANCE WITH SPEC. 2357.
- COMPACTION OF THE BITUMINOUS BASE AND BINDER SHALL BE BY THE "SPECIFIED DENSITY METHOD". COMPACTION OF THE TYPE 41 WEAR SHALL BE BY THE "ORDINARY COMPACTION METHOD".
- COMPACTION OF THE AGGREGATE BASE LAYERS SHALL BE BY THE "SPECIFIED DENSITY METHOD".

- IN AREAS TO BE DISTURBED BY CONSTRUCTION, STRIP AND RE-USE AS SLOPE DRESSING ALL TOPSOIL AND INPLACE SLOPE DRESSING. REFER TO THE CROSS-SECTIONS FOR THE LIMITS OF TOPSOIL STRIPPING. GENERAL DEPTHS OF TOPSOIL LAYER ARE ASSUMED TO BE 0"-3".
- SLOPE DRESSING ON THIS PROJECT IS DEFINED AS THE TOPSOIL OR OTHER SOIL PLACED DURING PRIOR CONSTRUCTION TO PROVIDE A MEDIUM FOR ESTABLISHING TURF.
- PLACE A MINIMUM OF 3 INCHES TOPSOIL OR SLOPE DRESSING ON ALL AREAS DISTURBED BY CONSTRUCTION AND SCHEDULED FOR PERMANENT TURF ESTABLISHMENT. FERTILIZE WITH COMMERCIAL FERTILIZER, ANALYSIS 10-10-10, AT A RATE OF 450 POUNDS PER ACRE, OR EQUIVALENT.
- ON ALL DISTURBED AREAS, USE MIXTURE 700 SEED WITH TYPE 1 MULCH, AND DISK ANCHORING, UNLESS SPECIFIED FOR SOD.
- SOD ALL PERMANENT BOULEVARD AREAS, AND DISTURBED LAWNS.
- ALL SOD UTILIZED WITHIN THE PROJECT LIMITS SHALL MEET THE REQUIREMENTS OF SPEC. 3878.2A (LAWN AND BOULEVARD SOD).
- EXCESS TOPSOIL AND MUCK EXC. MAY BE USED IN EMBANKMENT CONSTRUCTION IN AREAS OUTSIDE OF A 1 1/2:1 SLOPE FROM THE GRADING SHOULDER P.I.
- EXISTING STABILIZED SUBGRADE MUST BE PULVERIZED PRIOR TO USE AS EMBANKMENT MATERIAL.
- GEOTEXTILE FABRIC SHALL BE UTILIZED, WHEN DIRECTED BY THE ENGINEER, IN AREAS WHERE THE SUBCUT AND SUBGRADE EXCAVATION OPERATIONS ENCOUNTER UNSUITABLE UNDERLYING SUBSOILS. THE FABRIC SHALL BE PLACED AND MEET THE REQUIREMENTS AS PROVIDED IN THE SPECIAL PROVISIONS.
- EXCESS MUCK EXCAVATION TO BE DISPOSED OF BY THE CONTRACTOR OUTSIDE OF THE RIGHT-OF-WAY LIMITS AS APPROVED BY THE ENGINEER.
- 120% SHRINKAGE FACTOR FOR SUBCUT AND SUBGRADE COMPACTION (EV TO CV).
- 120% SHRINKAGE FACTOR FOR MUCK FILL AND TOPSOIL COMPACTION (EV TO CV).
- 140% SHRINKAGE FACTOR FOR NORMAL GRADING AND BORROW MATERIALS (EV TO CV).
- 150% SHRINKAGE FACTOR FOR TRUCK HAUL BORROW (LV TO CV).
- BITUMINOUS REMOVAL QUANTITY BASED ON 3" BITUMINOUS SURFACING AND 3 1/2" BITUMINOUS STABILIZED BASE. CONTRACTOR SHALL INVESTIGATE AND MAKE THEIR OWN DETERMINATION OF ACTUAL PAVEMENT DEPTH.

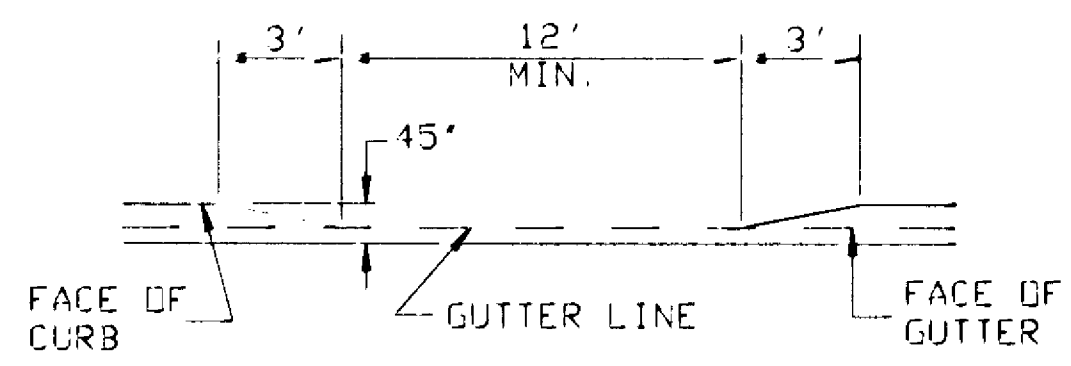
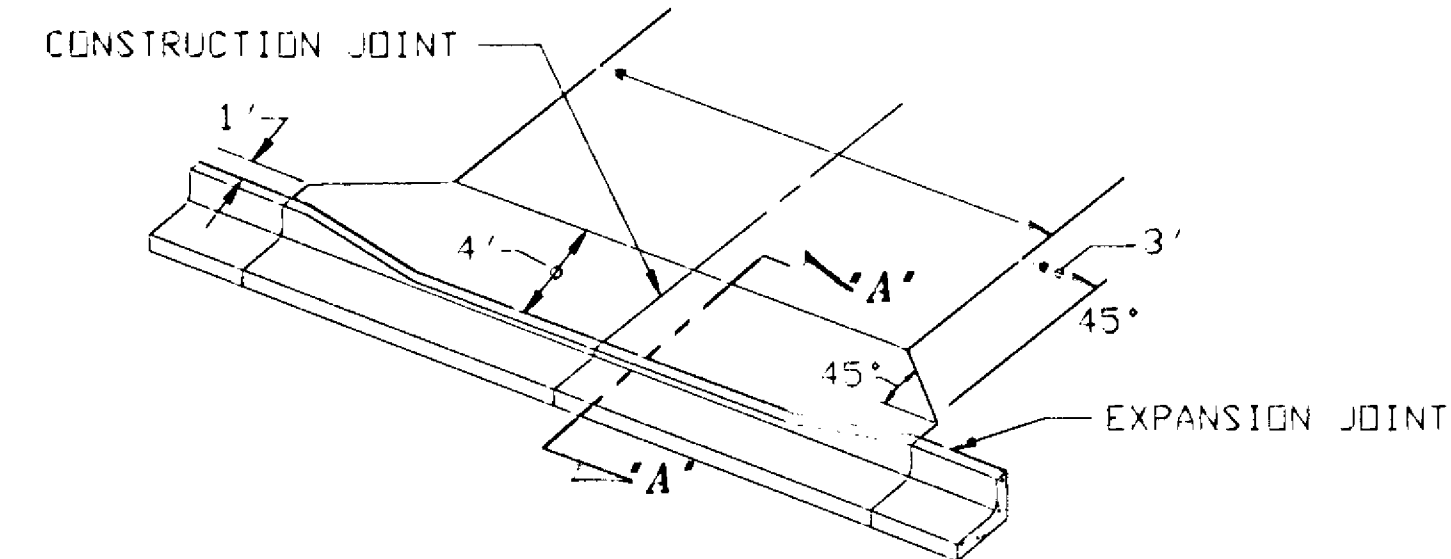
EARTHWORK SUMMARY AND CONSTRUCTION NOTES

UNPAVED RESIDENTIAL AND FIELD ENTRANCES

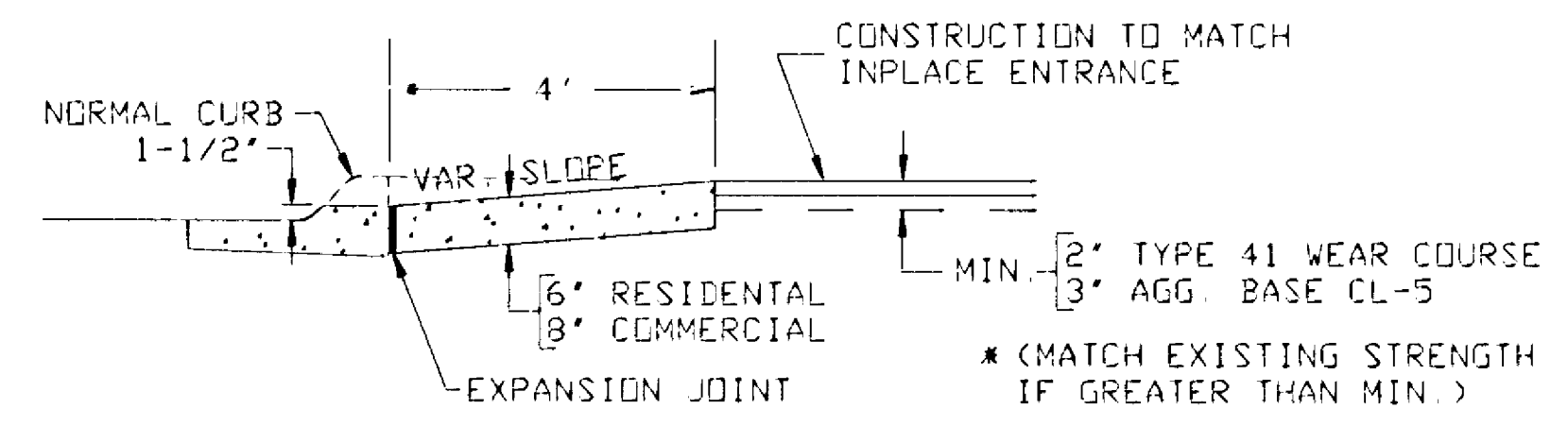


NOTE: COMMERCIAL AND RESIDENTIAL ENTRANCES WITH INPLACE BITUMINOUS SURFACING SHALL BE RESTORED WITH BITUMINOUS SURFACING, ITEM ON. 0331.601 2" THICK WEARING COURSE PLACED.

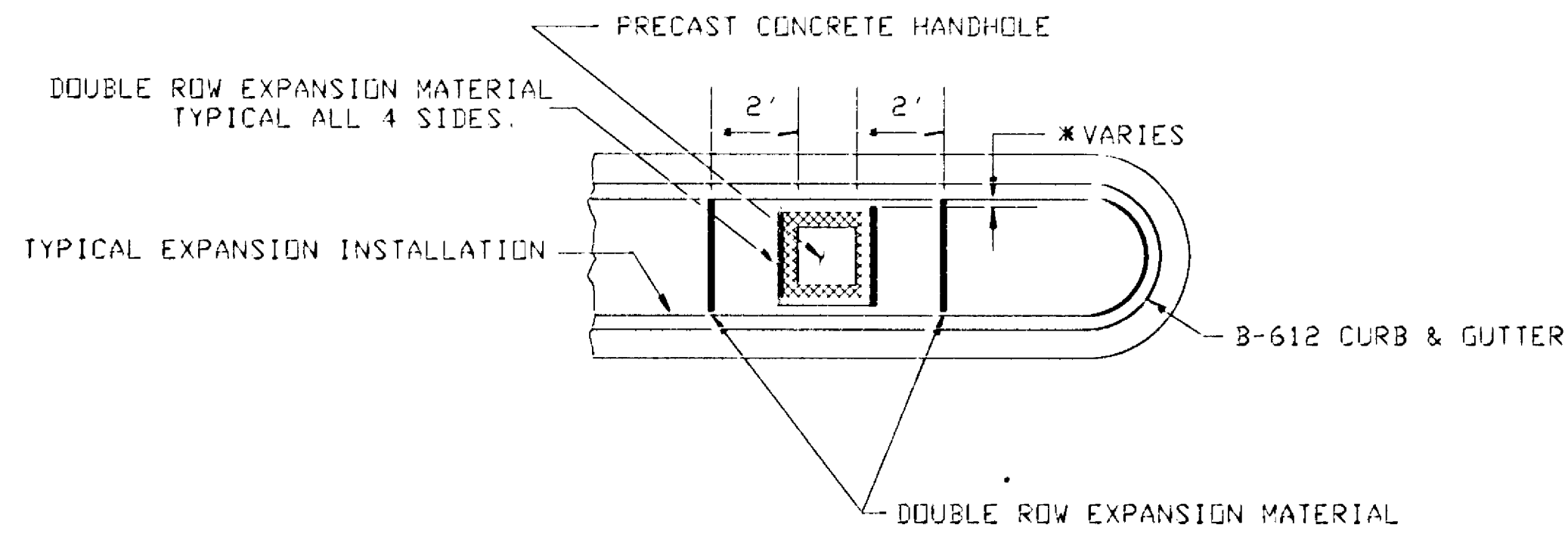
CONCRETE APRON DETAIL



SECTION 'A'-'A'

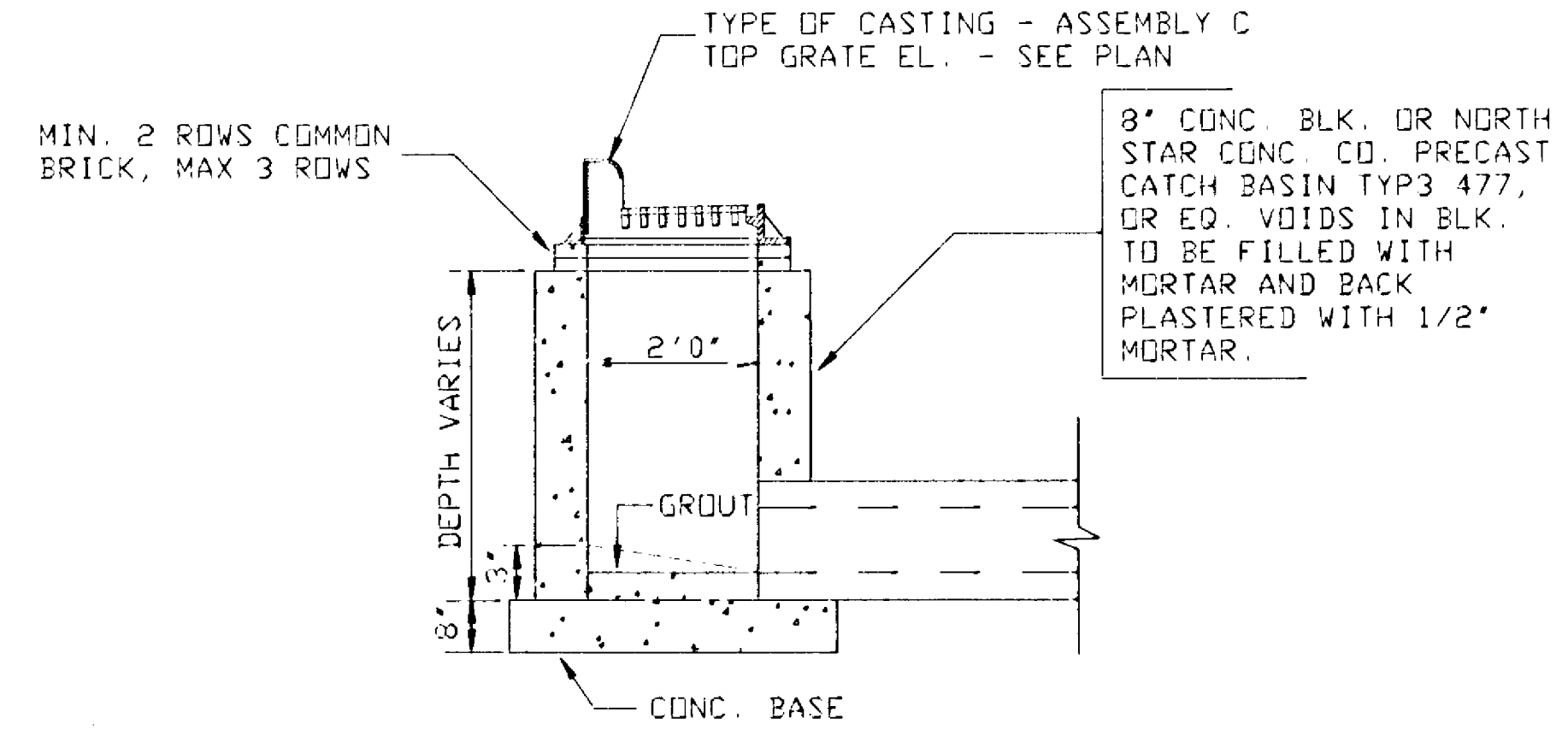
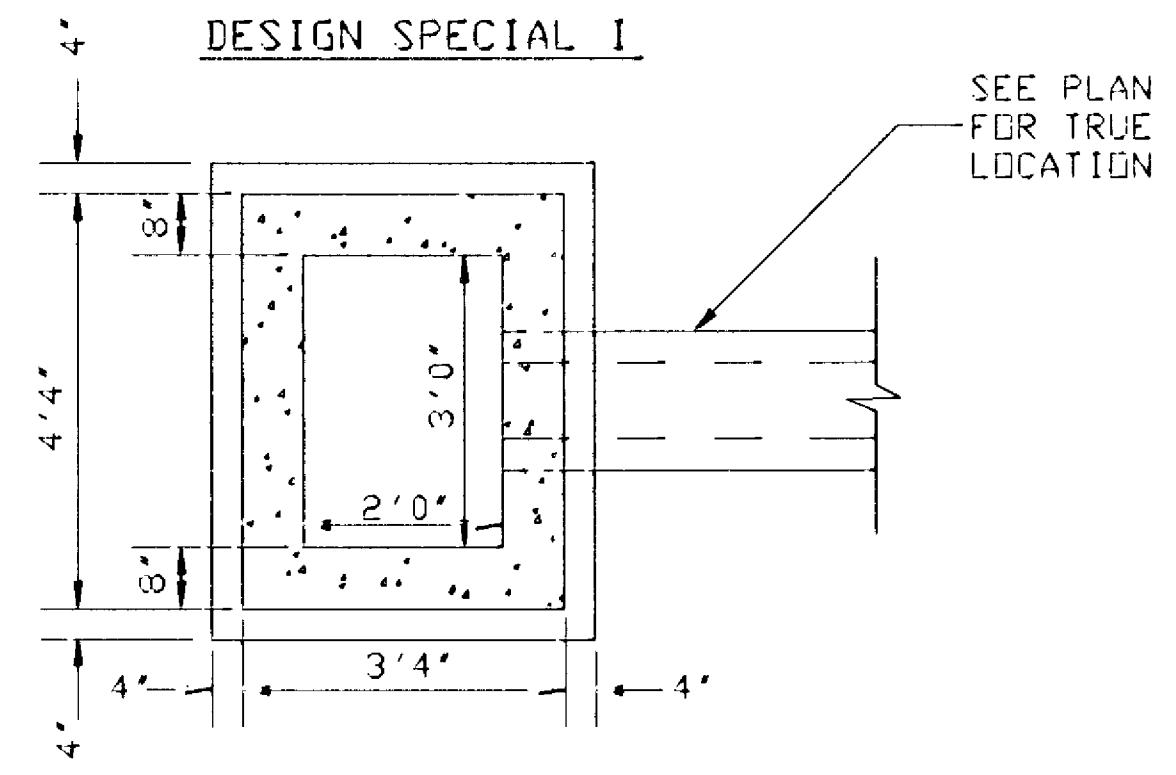


EXPANSION MATERIAL PLACEMENT FOR PRECAST HANDHOLES

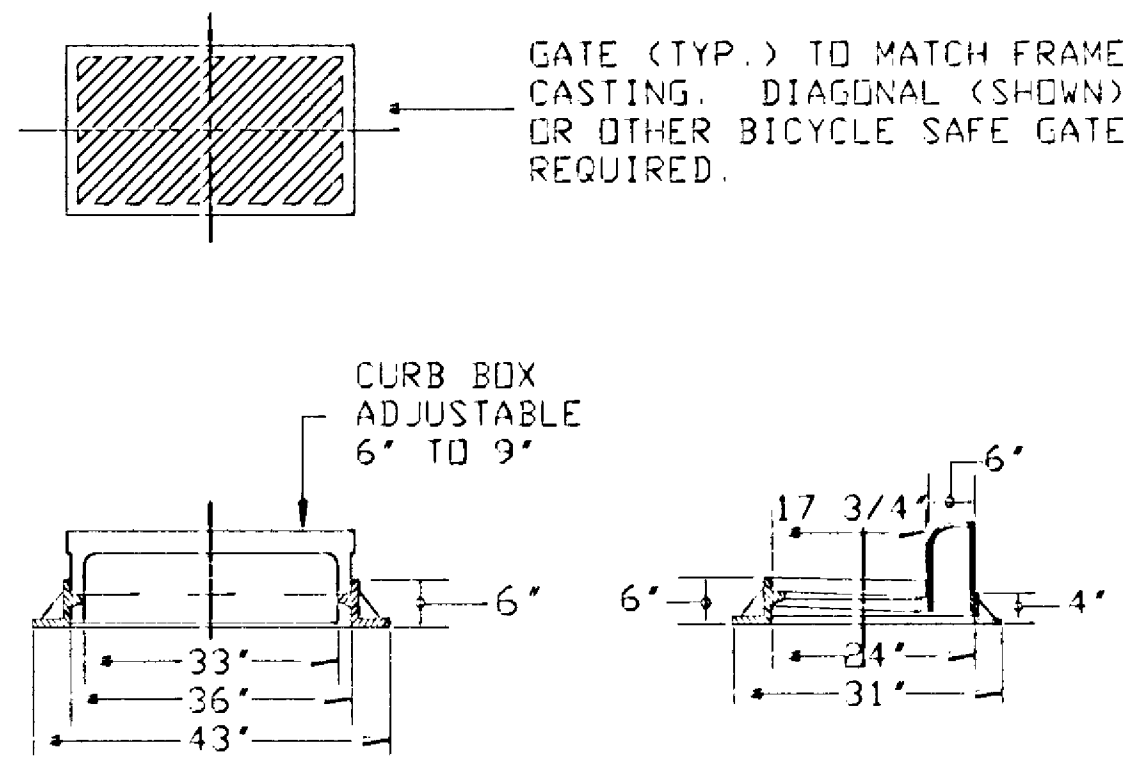


NOTE: * DIMENSION VARIES DEPENDING ON ISLAND WIDTH OR PLACEMENT OF HANDHOLE OTHER THAN IN AN ISLAND.

TYPICAL BOX CATCH BASIN

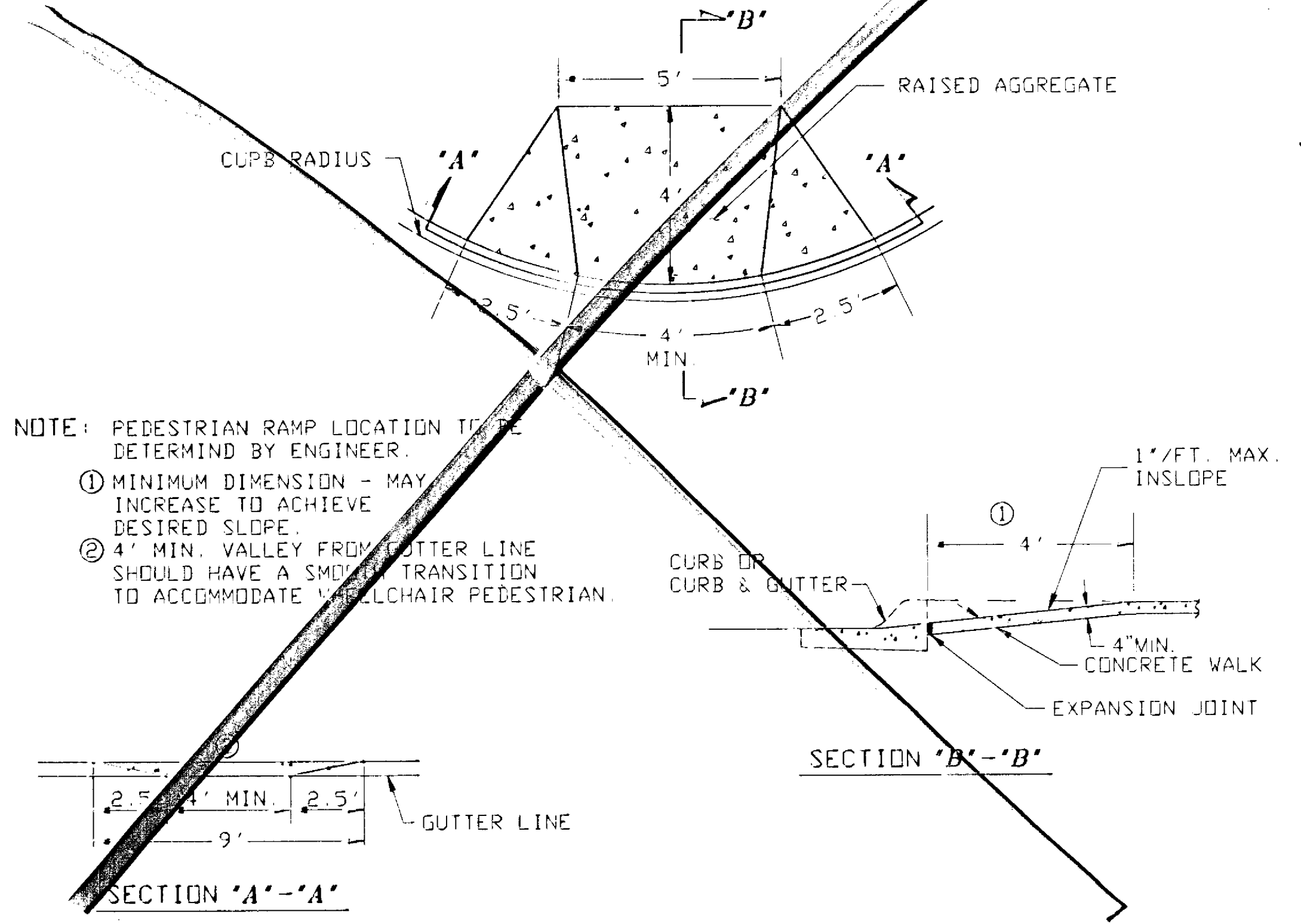


CASTING ASSEMBLY C

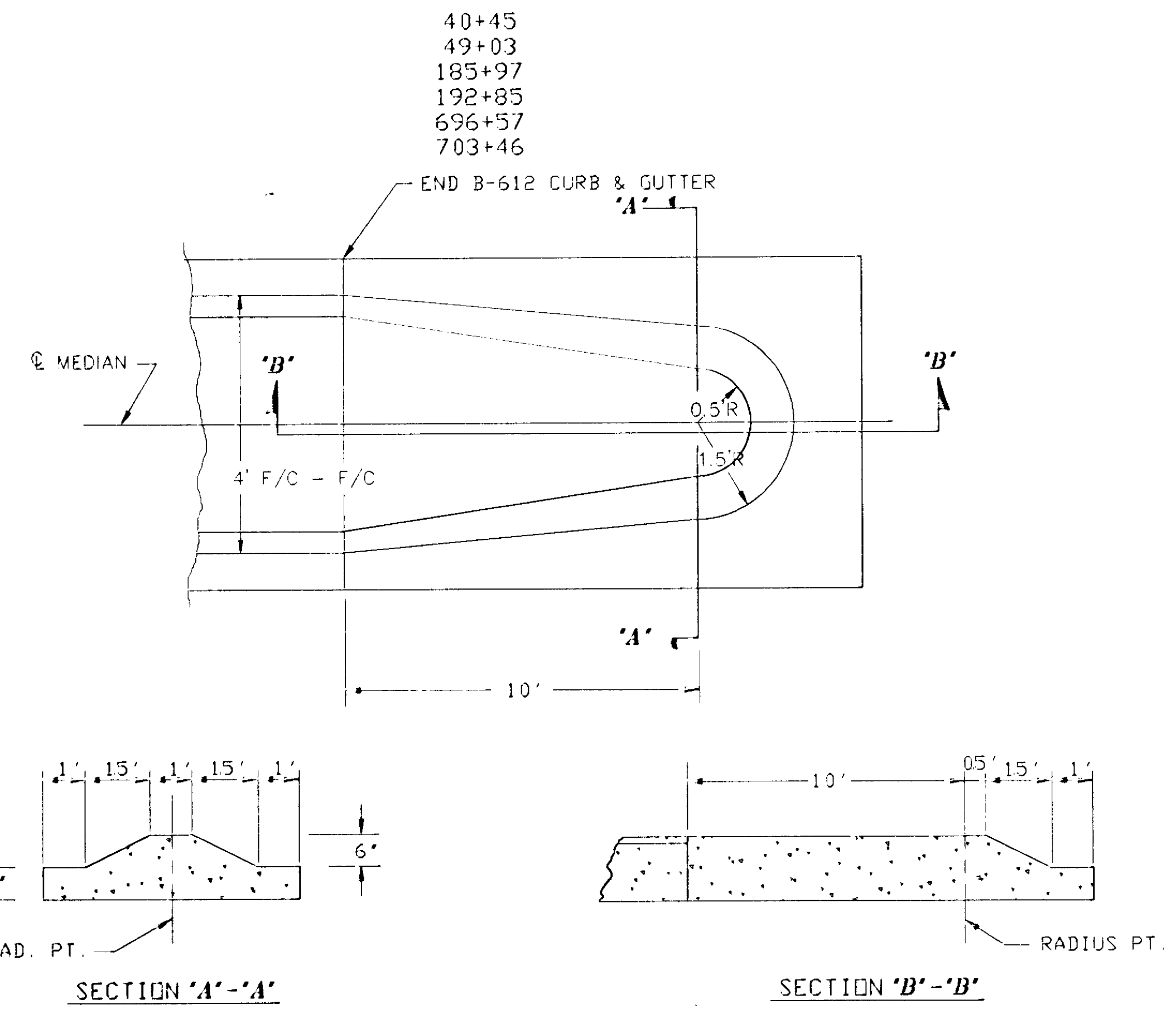


NOTE: TO BE USED WITH C.B. DESIGN SPECIAL I

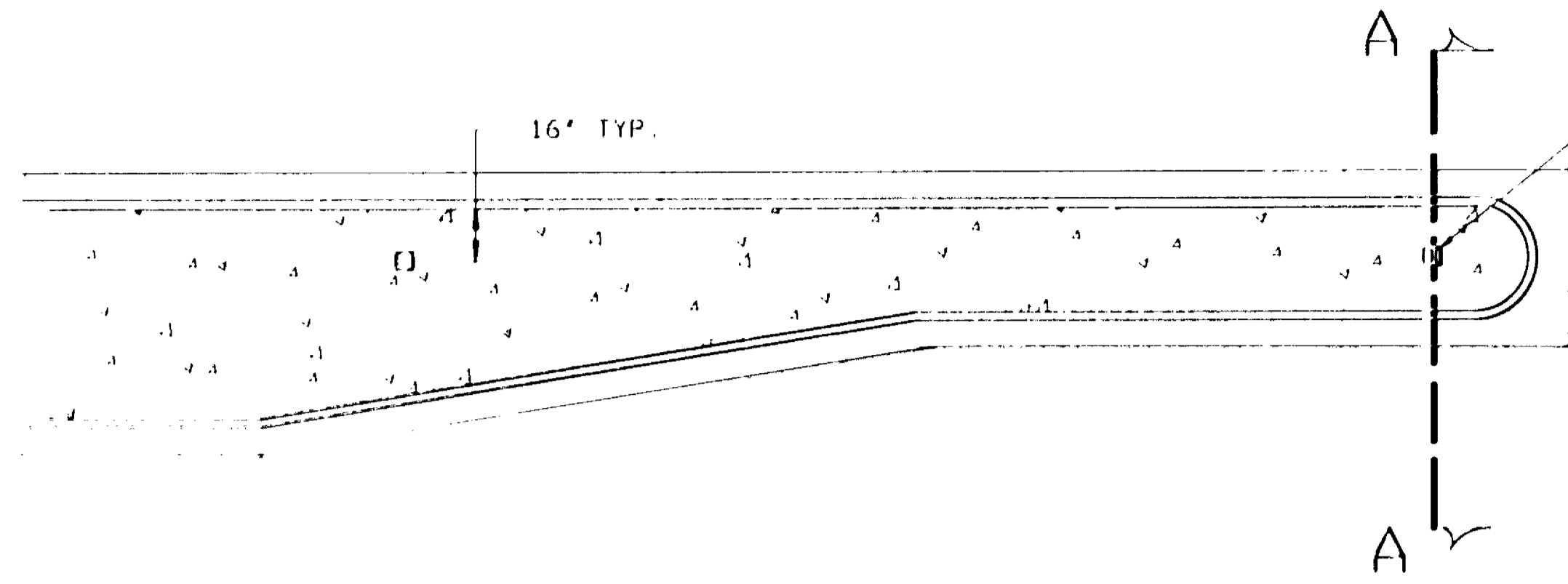
PEDESTRIAN RAMP DETAIL



MEDIAN NOSE DETAIL

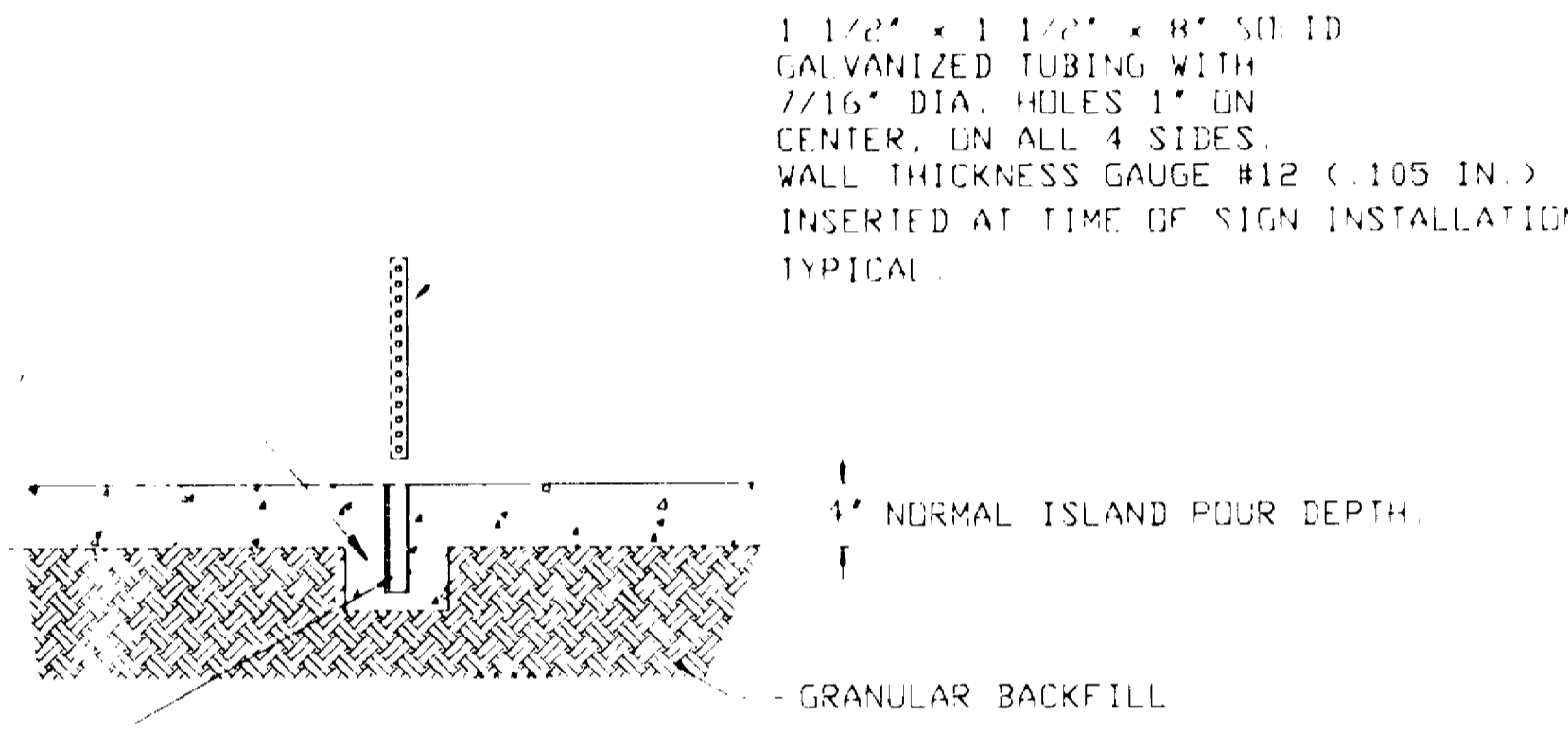


REVISIONS			
DATE	BY	DATE	BY



INSTALL 1 3/4" x 1 3/4" x 8' SOLID GALVANIZED SQUARE TUBING IN ISLAND NOSE, DURING CONCRETE MEDIAN POUR, PLUMB AS REQUIRED. TAPE BOTTOM OF TUBING TO PREVENT CONCRETE FROM ENTERING TUBING.

AT THE CORRECT LOCATION, DIG HOLE A MIN. OF 9" INTO GRANULAR BORROW TO INSURE SQUARE TUBING WILL NOT PROTRUDE ABOVE CONCRETE AT TIME OF MEDIAN CONCRETE POUR. THIS IS TYP. ON ALL SIGN LOCATIONS.

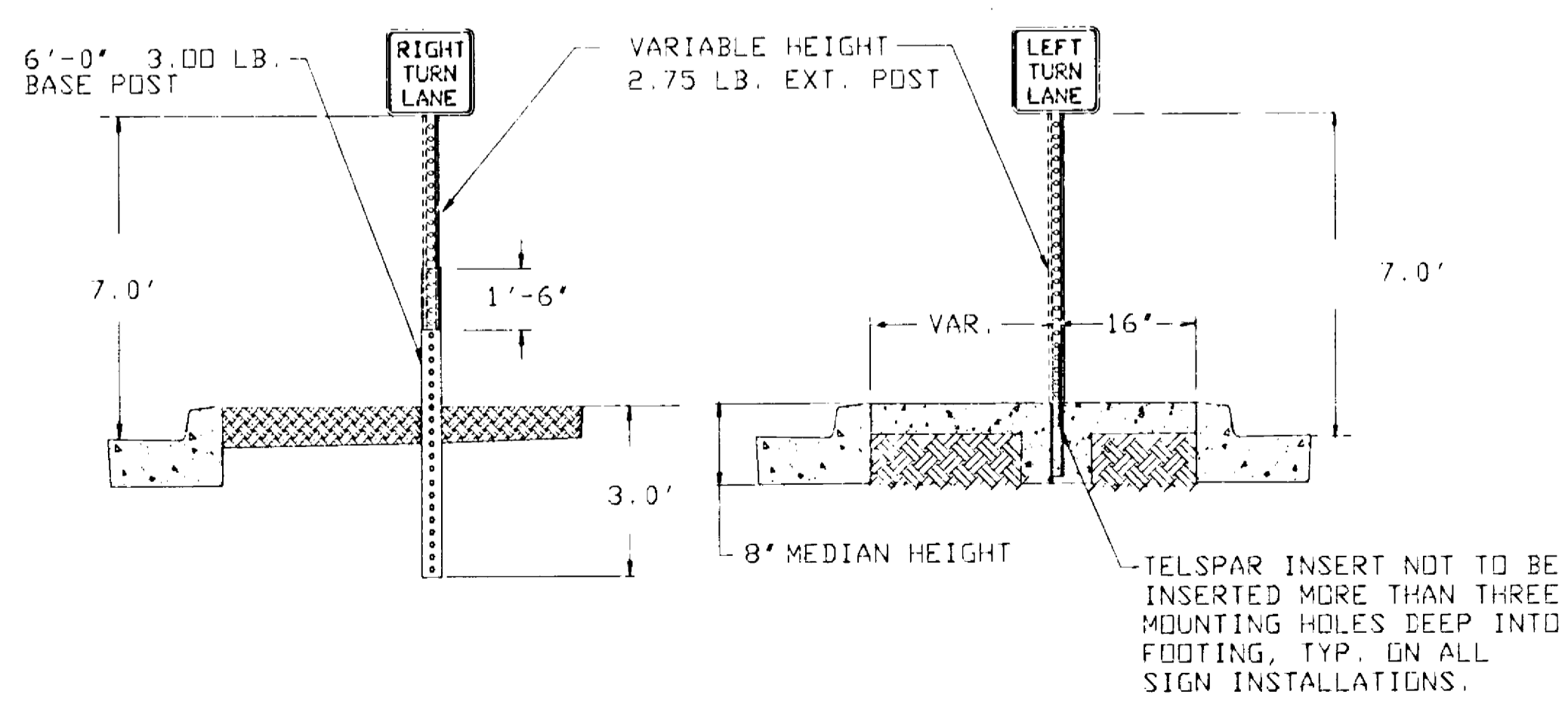


SECTION A-A

INSTALL 1 3/4" x 1 3/4" x 8' SOLID WALL GALVANIZED SQUARE TUBING. TAPE BOTTOM OF TUBING TO PREVENT CONCRETE FROM ENTERING TUBE. PLUMB AND ALIGN AT TIME OF POUR AS REQUIRED. TYPICAL.

GROUND POST MOUNT SIGN INSTALLATION TYPICAL

ISLAND MOUNT BREAK-AWAY SIGN INSTALLATION TYPICAL



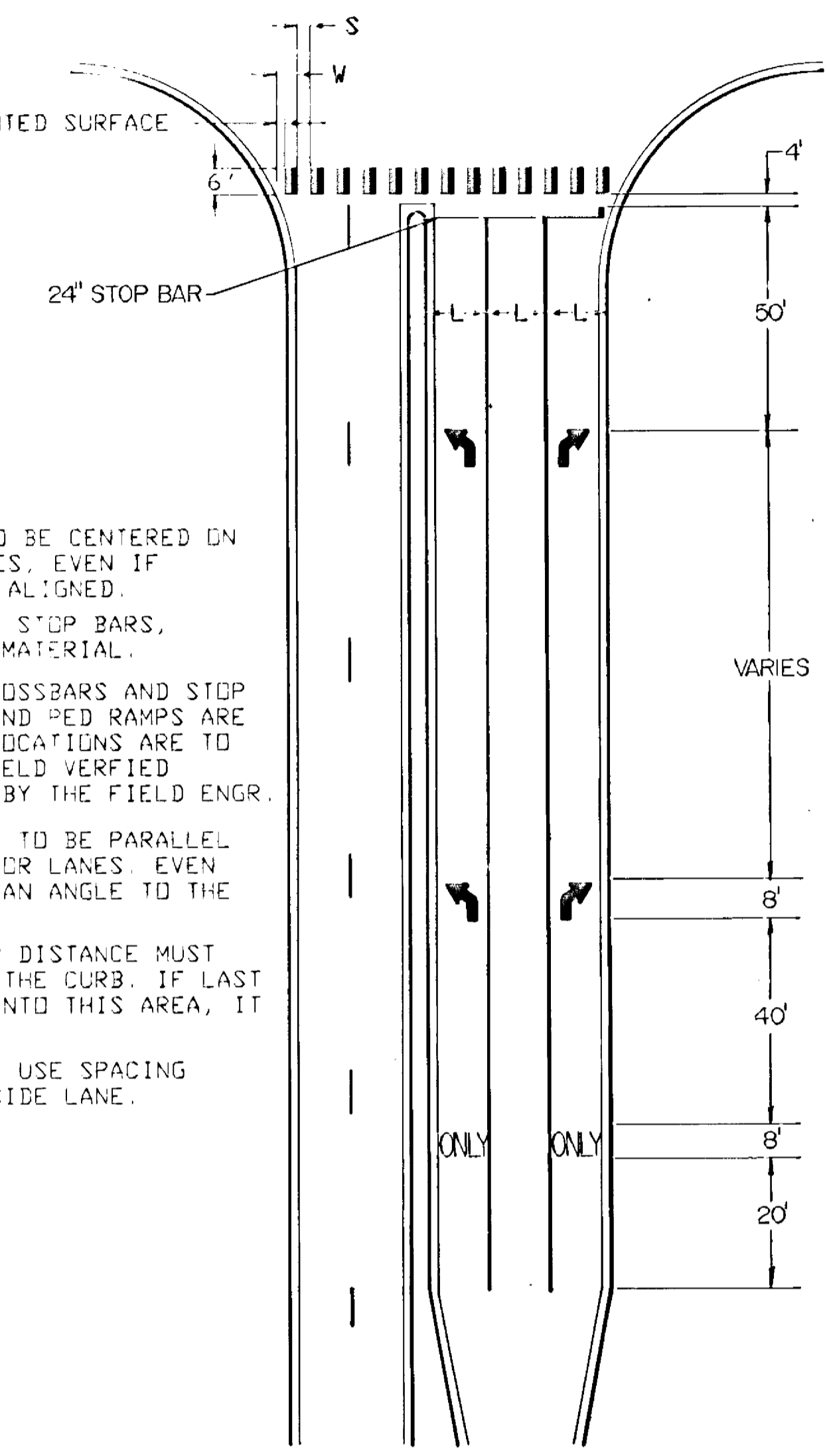
REVISIONS

DATE	BY	DATE	BY

(L)	(W)	(S)
WIDTH OF INSIDE LANE	WIDTH OF PAINTED AREAS	WIDTH OF SPACE
9'	2.0'	2.5'
10'	2.5'	2.5'
11'	2.5'	3.0'
12'	3.0'	3.0'
13'	3.0'	3.5'

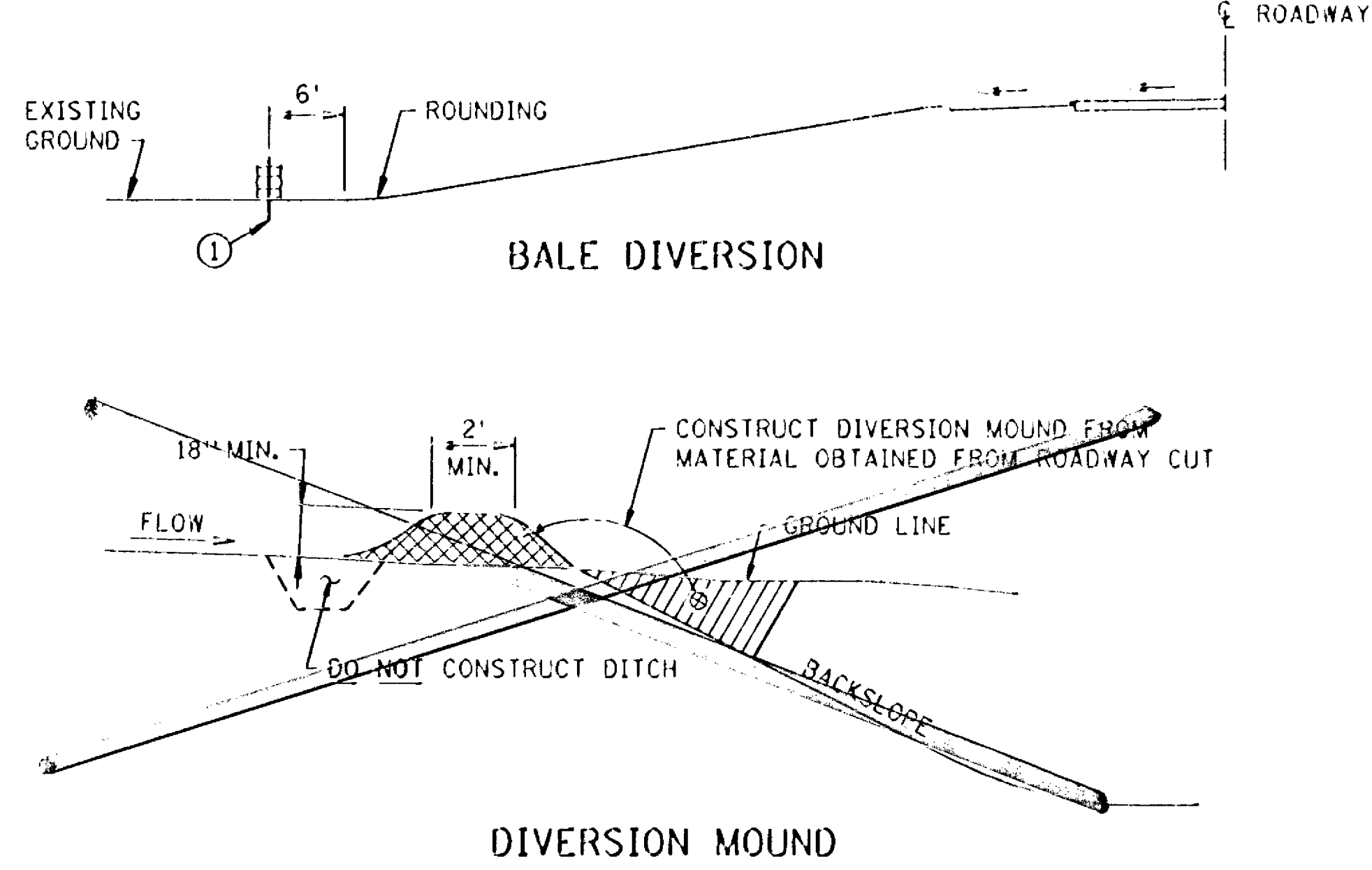
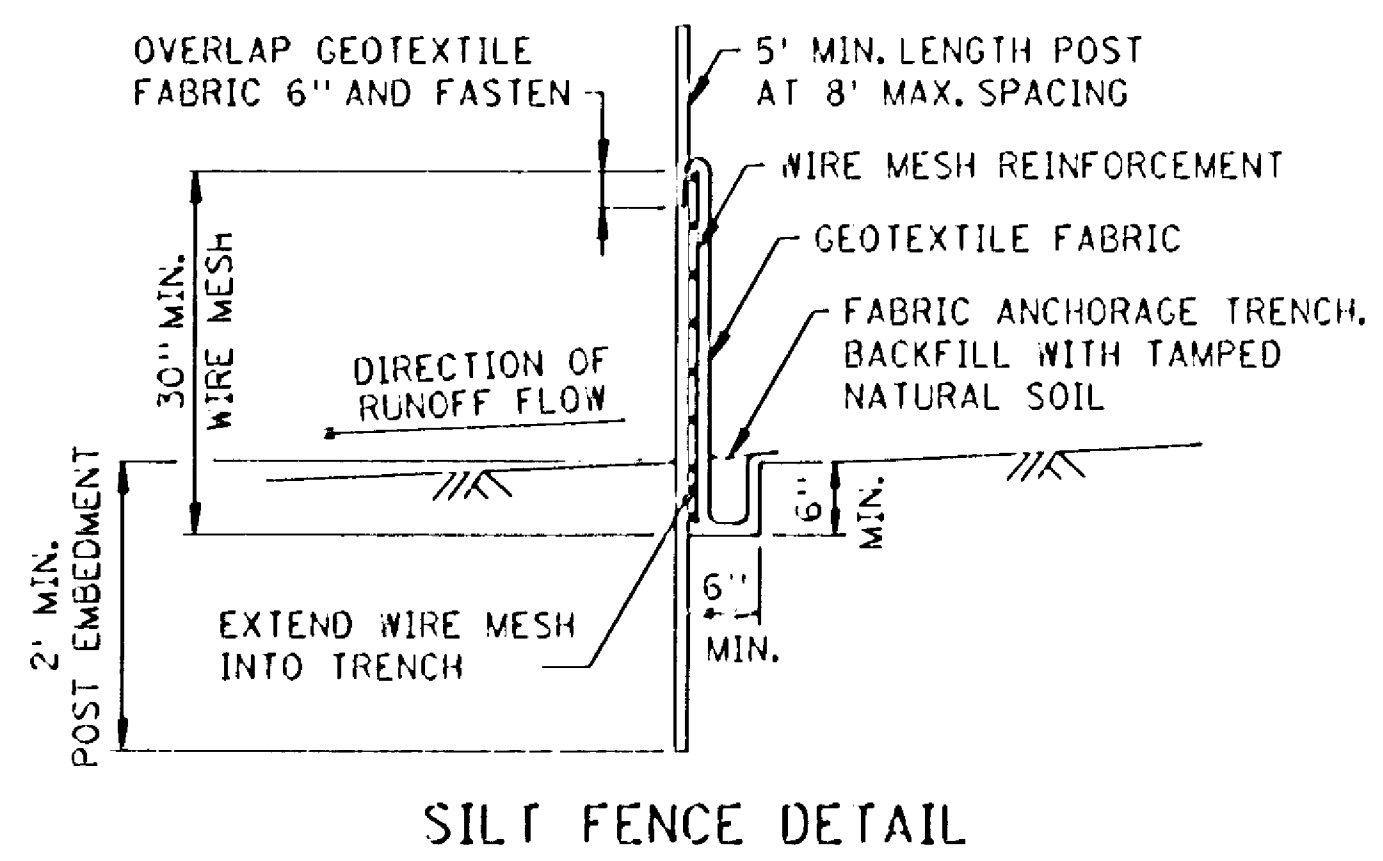
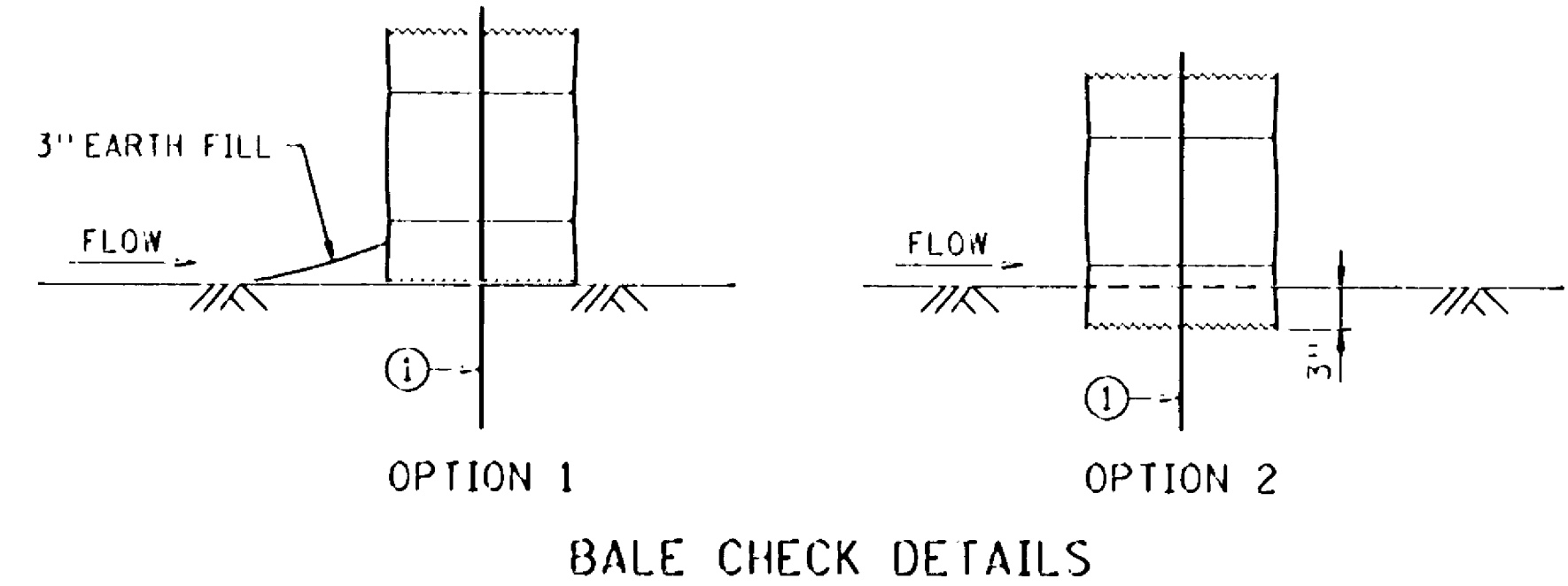
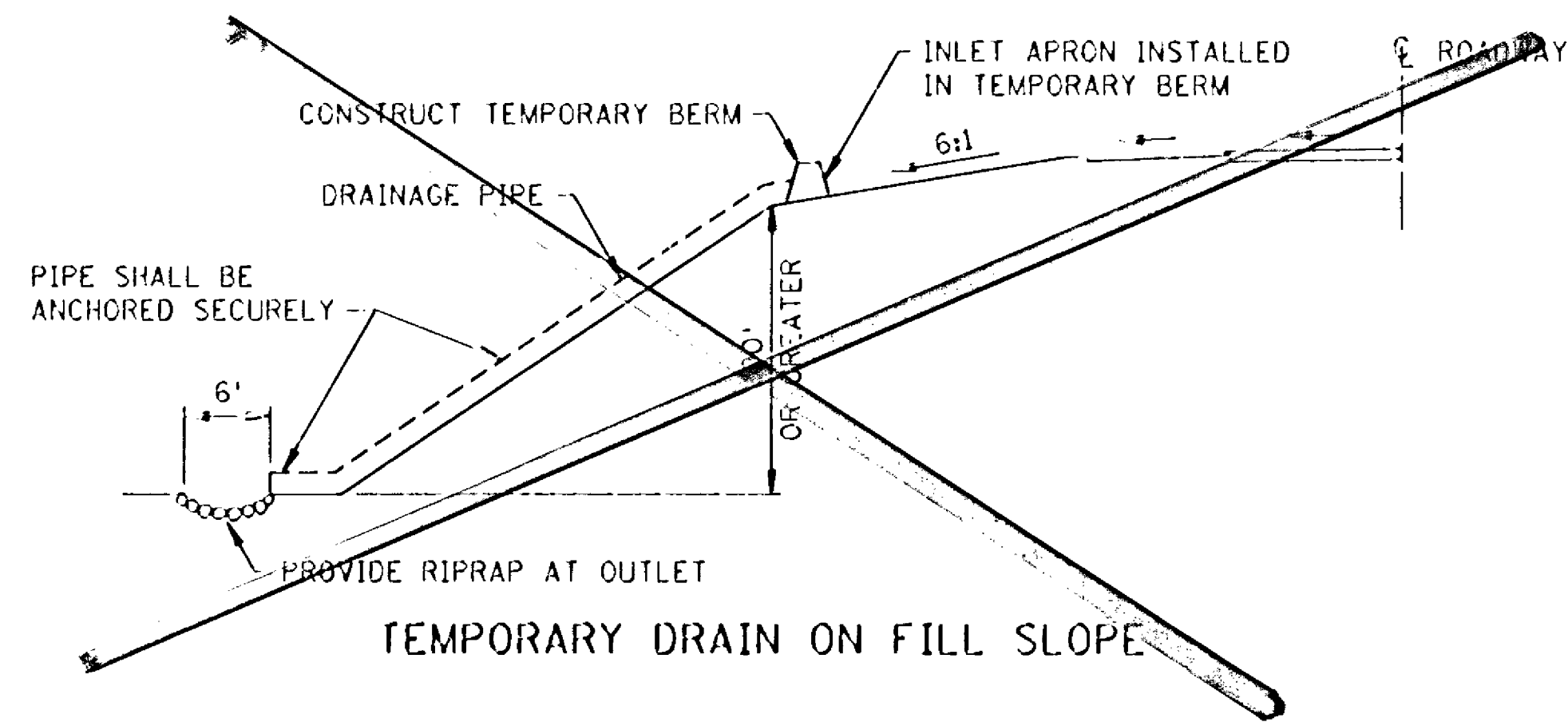
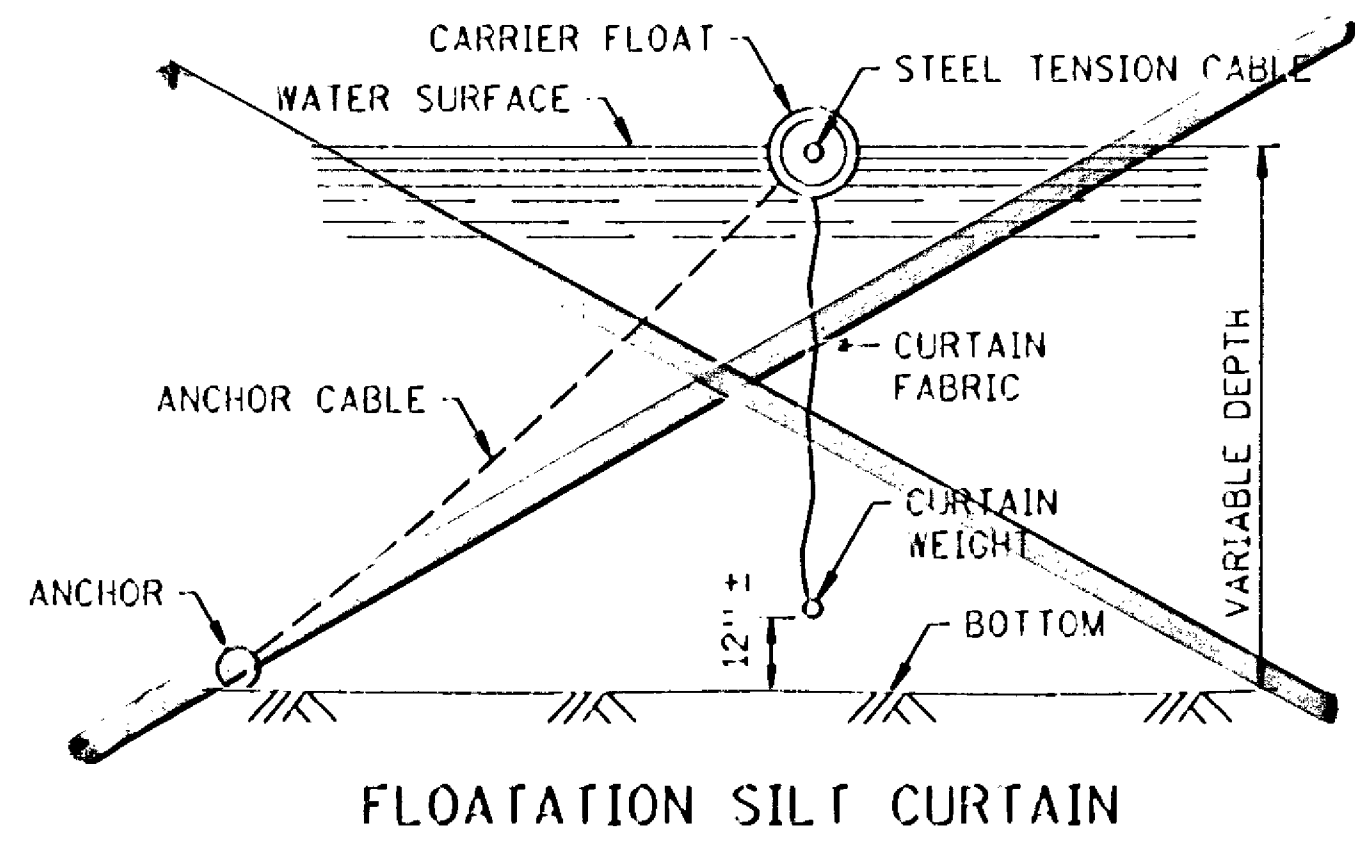
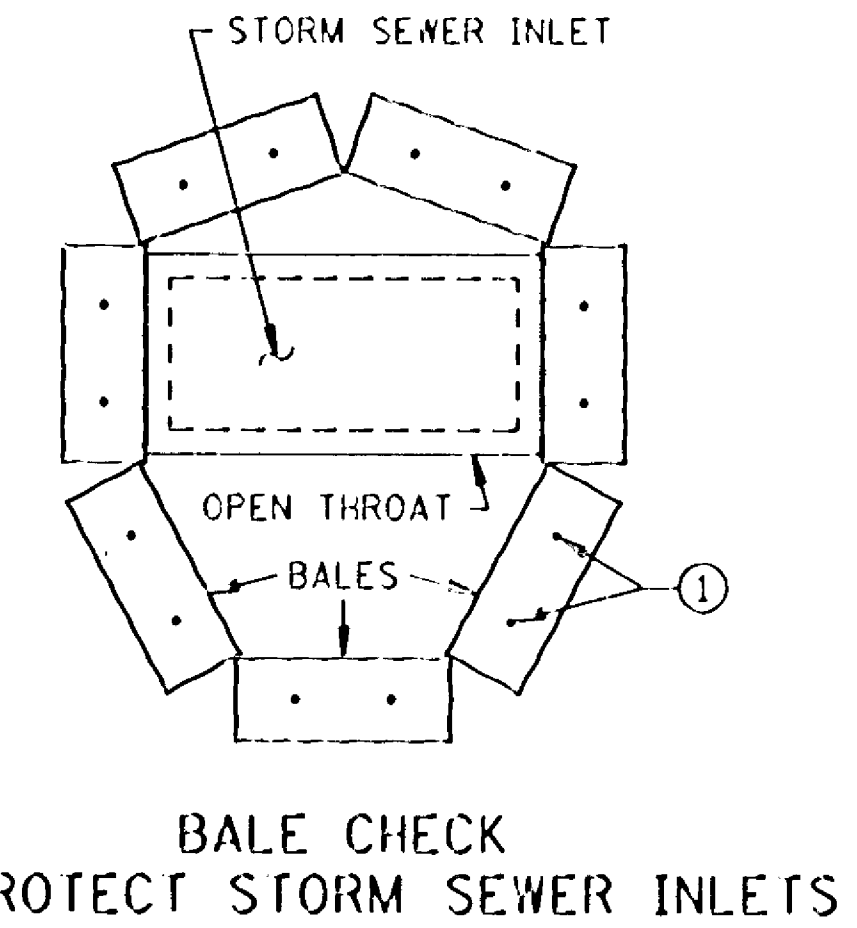
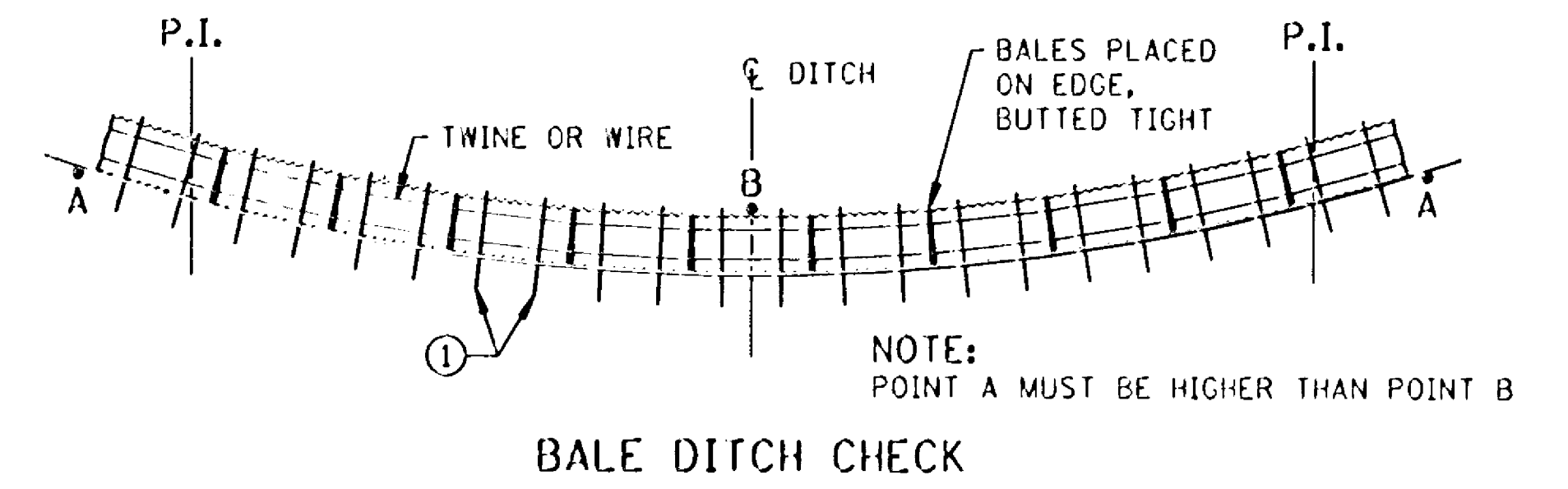
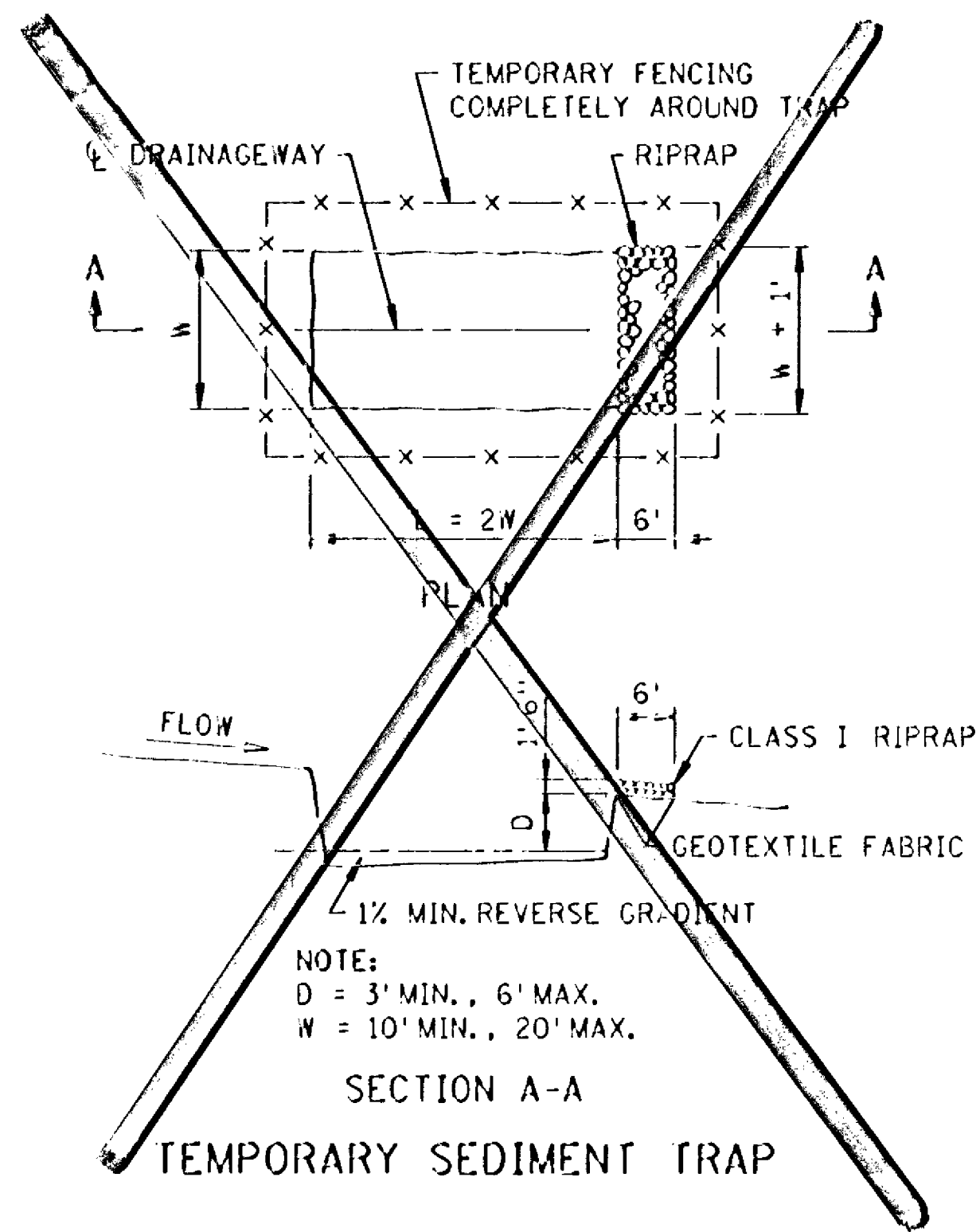
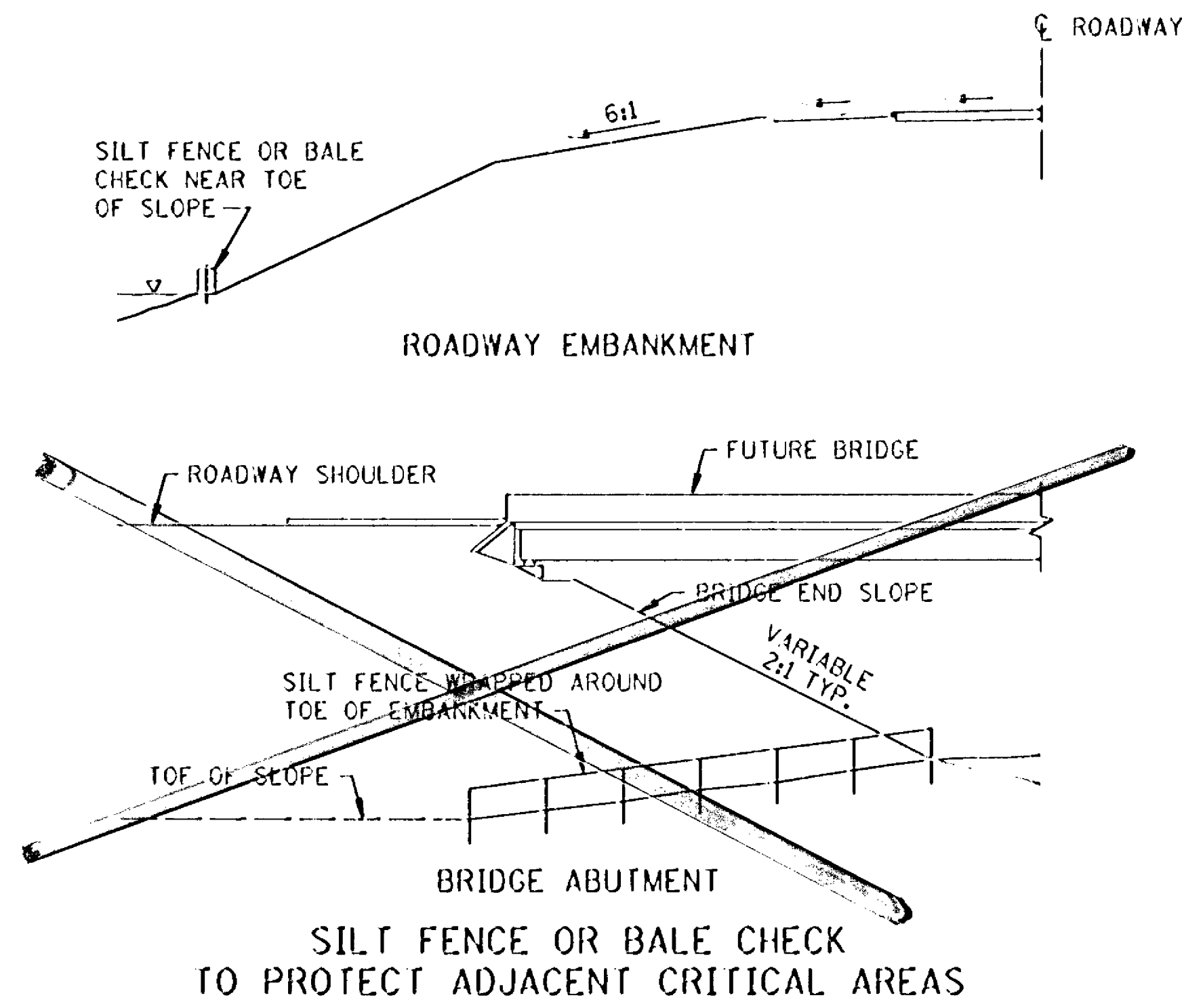
NOTES:

- 1.) PAINTED AREAS ARE TO BE CENTERED ON CENTER AND LANE LINES, EVEN IF INTERSECTION IS NOT ALIGNED.
- 2.) ZEBRA CROSSWALKS AND STOP BARS, REQUIRE REFLECTIVE MATERIAL.
- 3.) LOCATION OF ZEBRA CROSSBARS AND STOP BARS, SIGNAL LEDS AND PED RAMPS ARE APPROXIMATE. FINAL LOCATIONS ARE TO BE DETERMINED AND FIELD VERIFIED DURING CONSTRUCTION BY THE FIELD ENGR.
- 4.) ZEBRA CROSSWALKS ARE TO BE PARALLEL TO THE DRIVING LANE OR LANES, EVEN IF THE STREET IS ON AN ANGLE TO THE INTERSECTION.
- 5.) A MIN. OF 1.5' CLEAR DISTANCE MUST BE LEFT ADJACENT TO THE CURB. IF LAST PAINTED AREA FALLS INTO THIS AREA, IT MUST BE OMITTED.
- 6.) ON TWO-LANE STREETS, USE SPACING SHOWN FOR AN 11' INSIDE LANE.



NOTES:

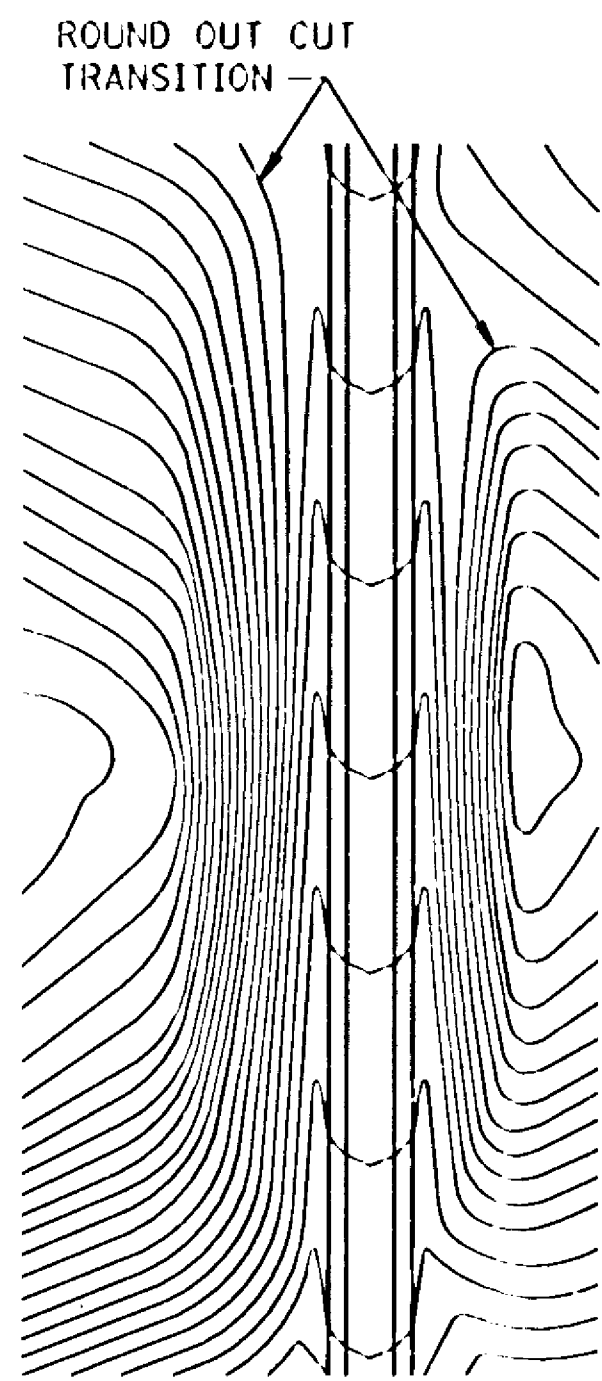
- SECTION B-B THE SOLID GALVANIZED SQUARE TUBING FOR THE 'DO NOT ENTER' SIGNS SHALL BE SET AT THE PROPER ANGLE. REFER TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- * WHEN THE DISTANCE BETWEEN SIGN POSTS VARIES, REFER TO THE STANDARD SIGNS MANUAL FOR THE SPACING CHART THAT RELATES TO THE SIZE AND SHAPE OF THE INSTALLATION.



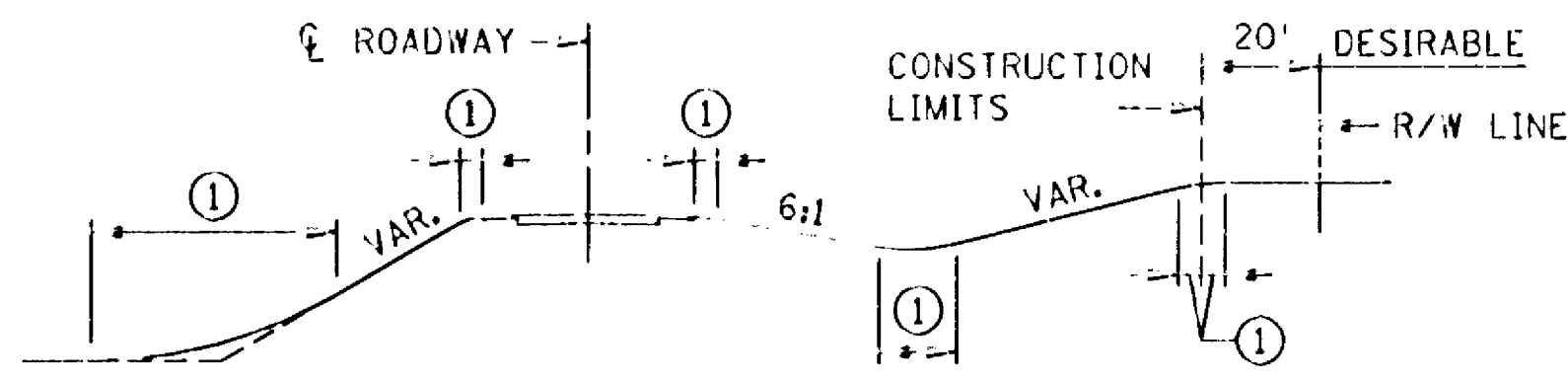
NOTE:
① TWO 2" X 2" WOOD STAKES OR REINFORCING BARS IN EACH BALE AND EMBEDDED IN THE GROUND 10" MINIMUM.

STANDARD SHEET NO. 5-297.405	TITLE: TEMPORARY EROSION CONTROL
STANDARD APPROVED: DECEMBER 19, 1990	
STATE PROJ. NO. 02-614-14	SHEET NO. 12 OF 117 SHEETS

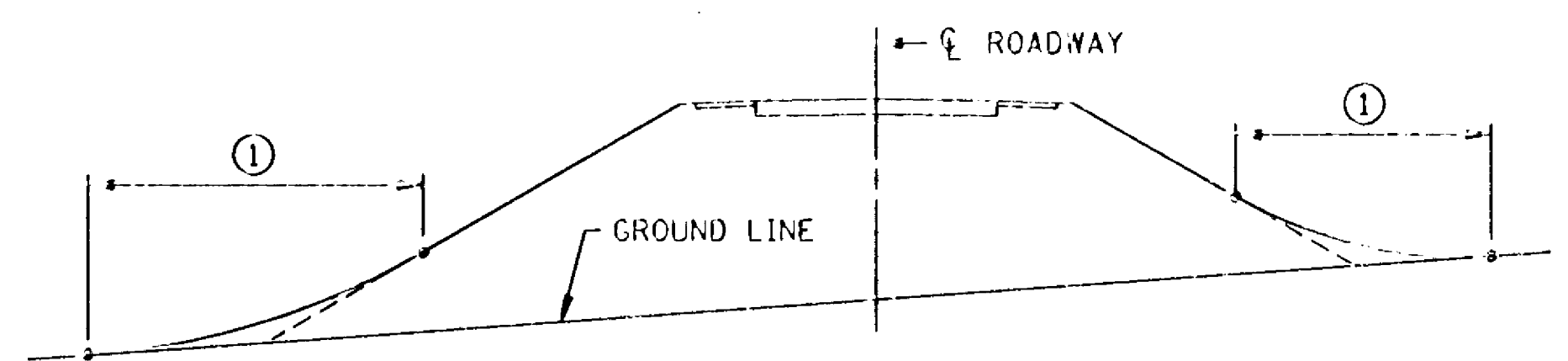
VAX780 CSA3:45:1001 FILE NAME S405L50.SPN



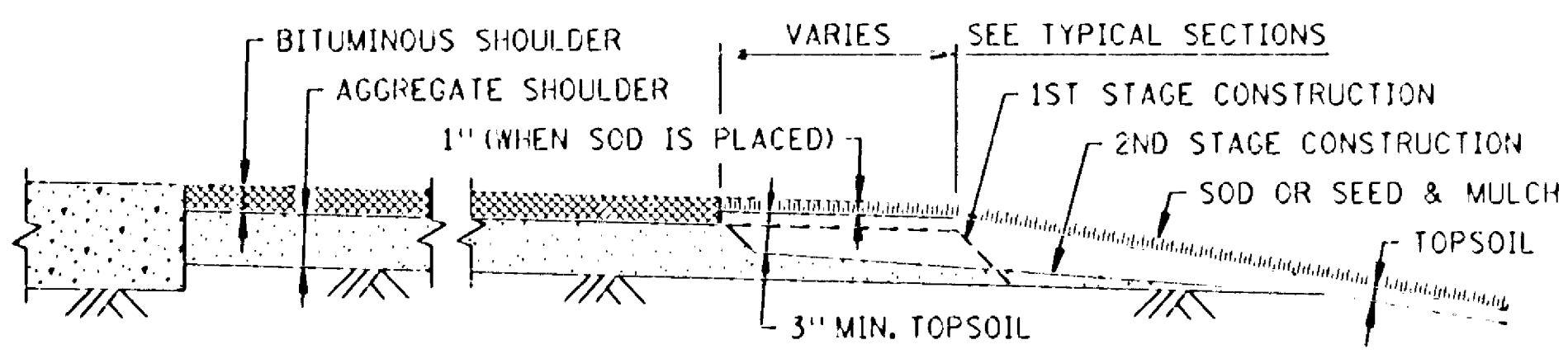
CONTOURING ROAD CUTS



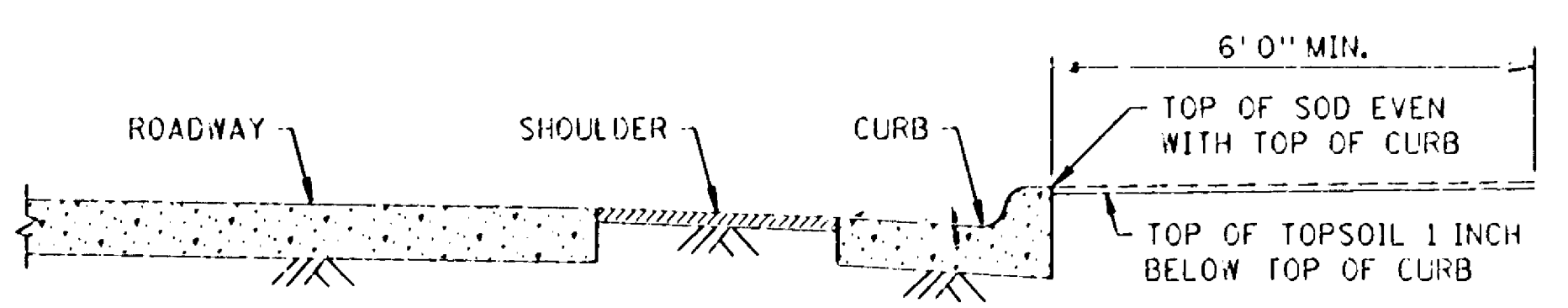
ROUNDING SHOULDERS AND BACKSLOPES



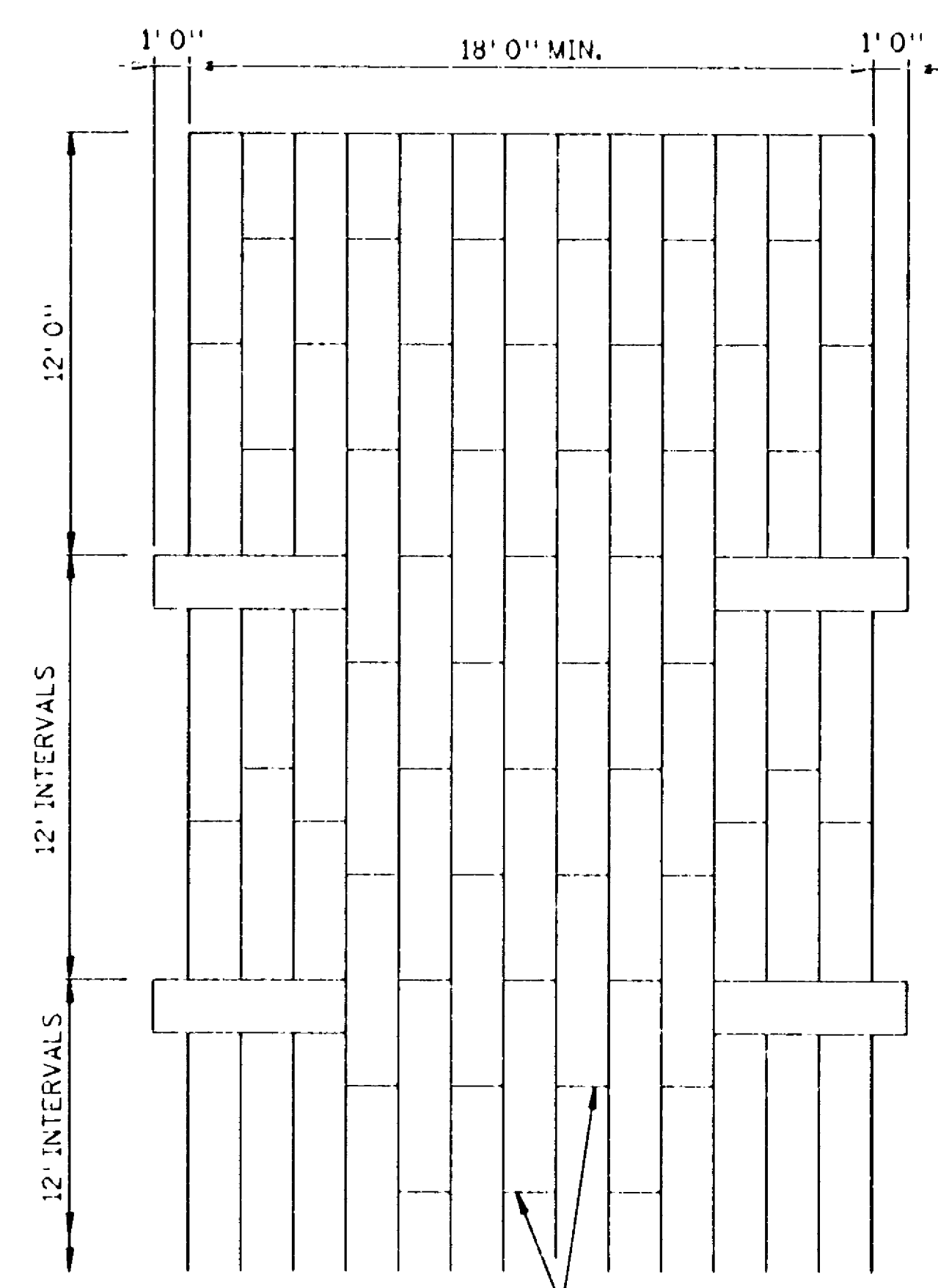
SHAPING FOR DRAINAGE ALONG THE TOE OF FILL SLOPES



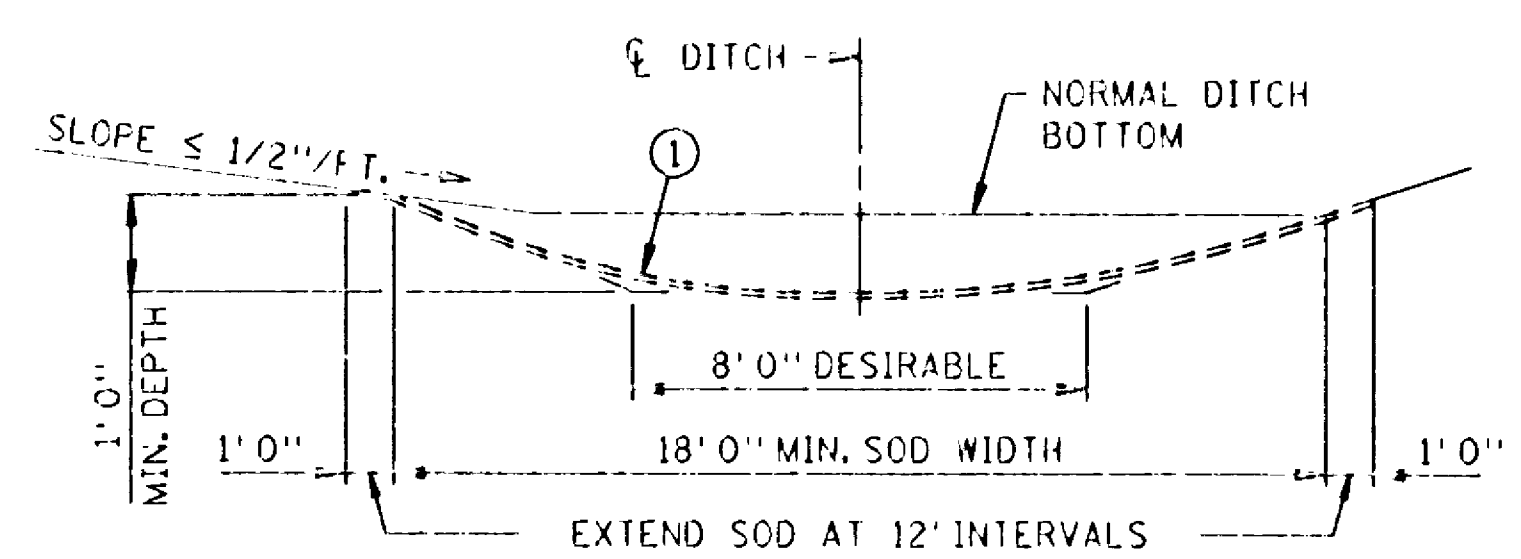
SHAPING AND TOPSOILING INSLOPES



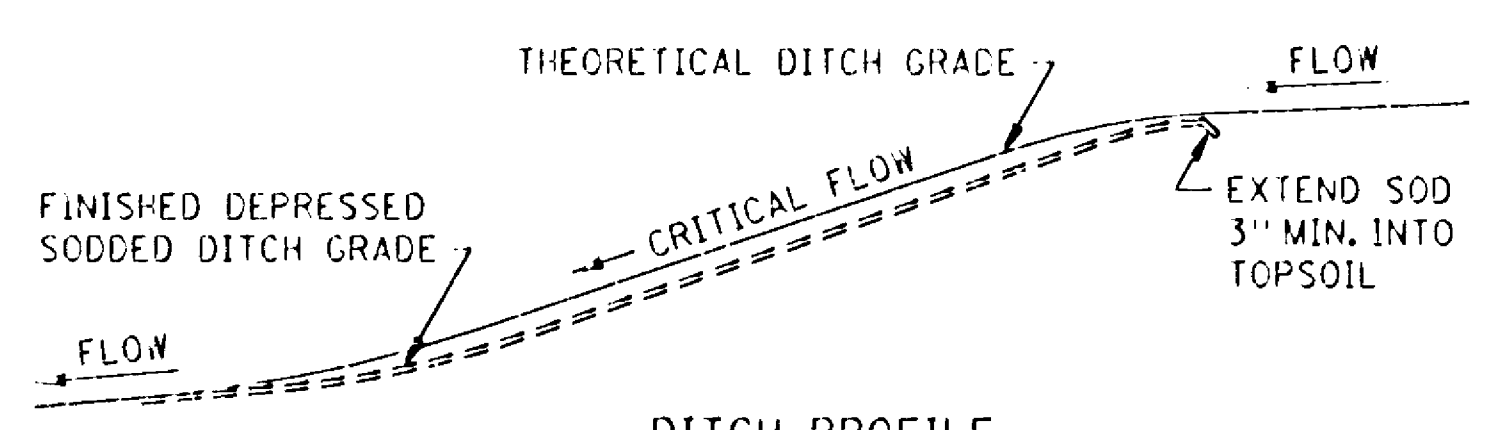
SHAPING ADJACENT TO CURBS WHEN SOD IS PLACED



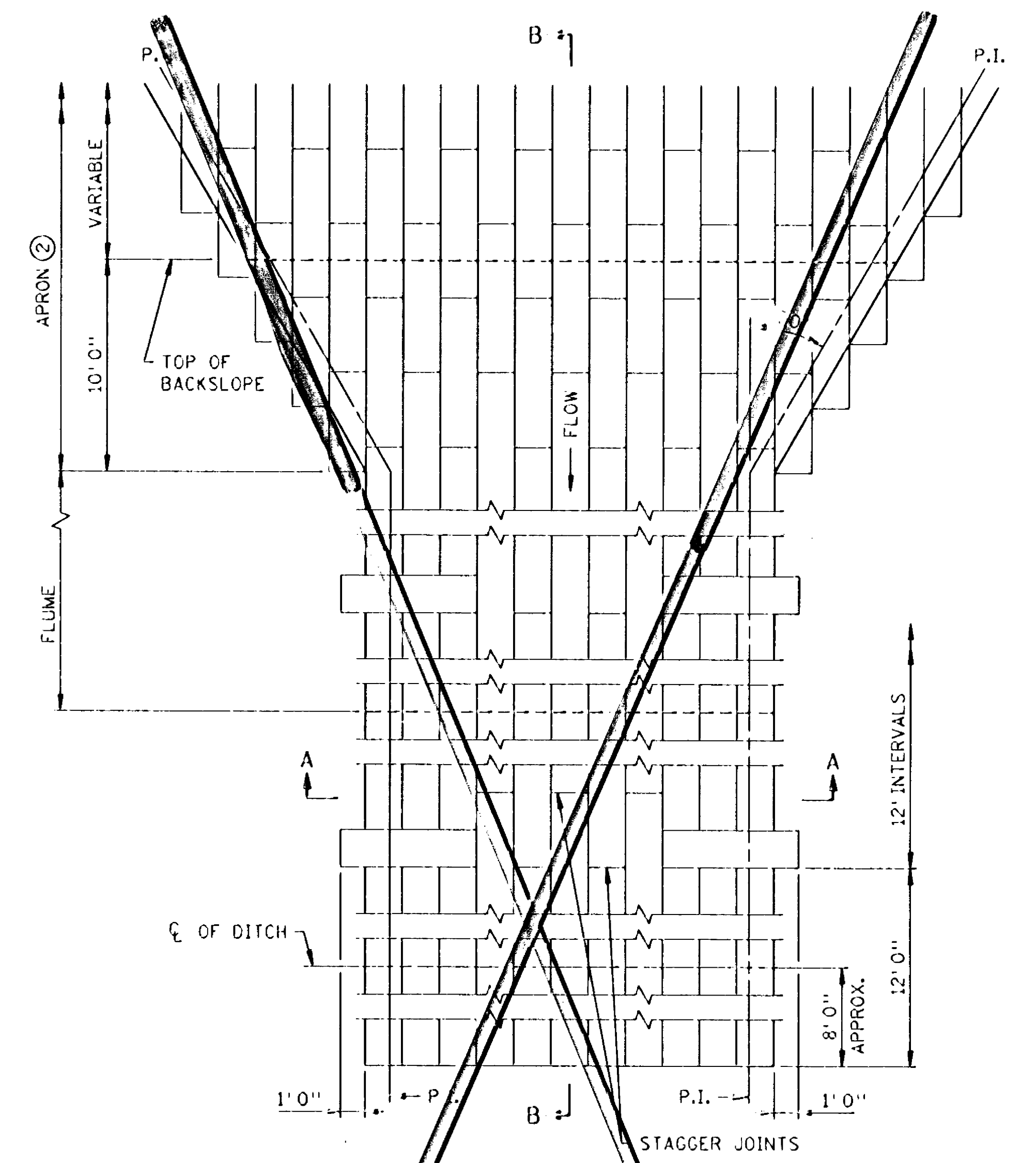
PLAN VIEW



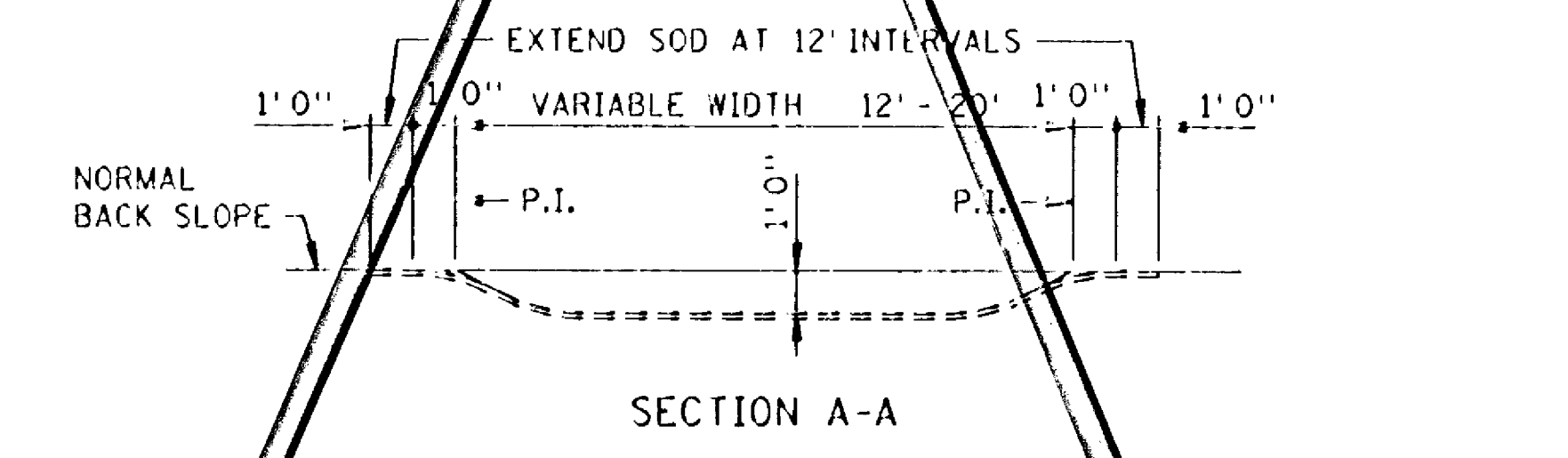
SODDED DITCH CROSS SECTION WHERE FRONT OR BACK SLOPE IS FLAT (LESS THAN 1/2"/FT.), FIRST NOTCH DITCH AND THEN PROVIDE ROUNDING.



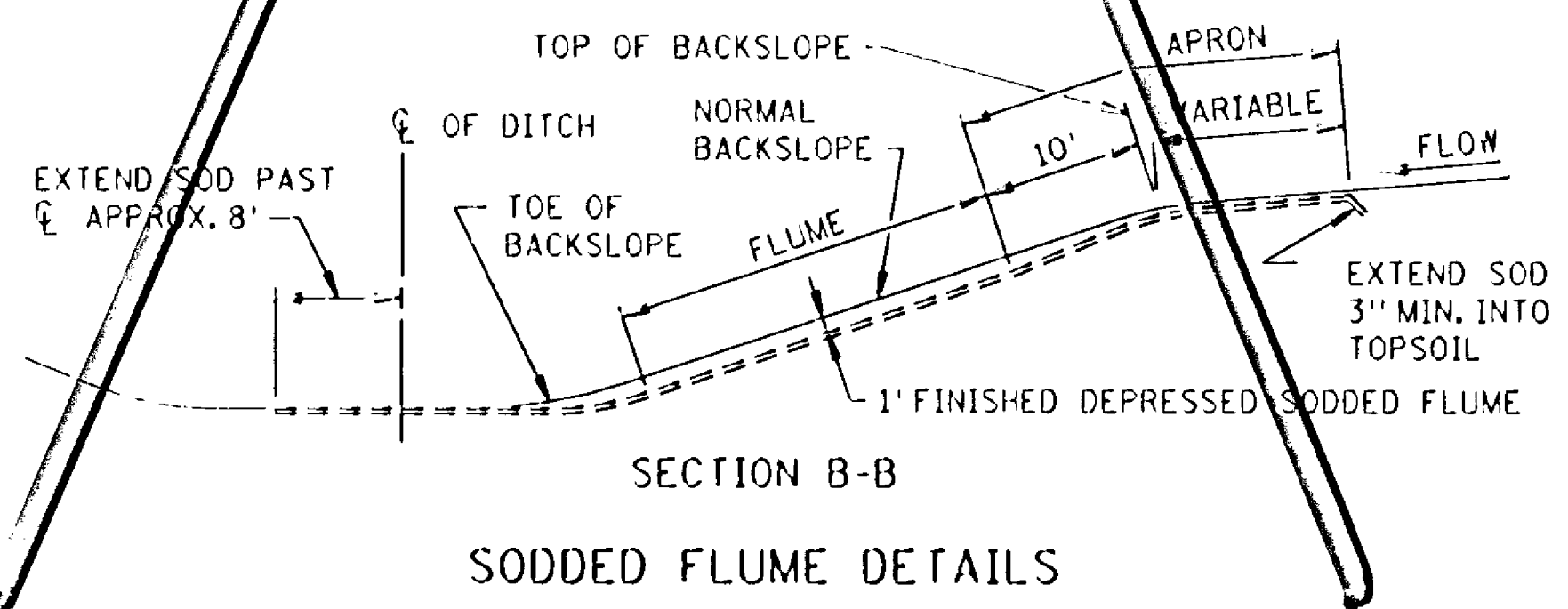
DITCH PROFILE SODDED DITCH DETAILS



PLAN VIEW



SECTION A-A



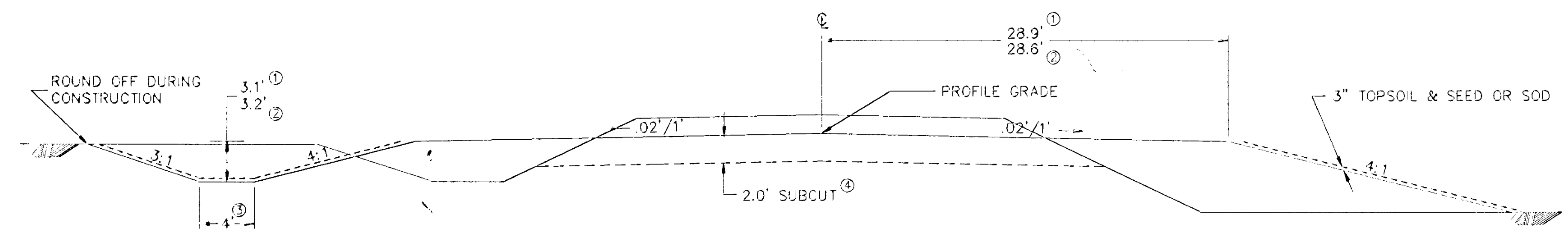
SECTION B-B SODDED FLUME DETAILS

- NOTES:
 SEE SPEC. 2575.3 FOR ADDITIONAL INFORMATION.
 ① FOR ROUNDING, SEE ROAD DESIGN MANUAL.
 ② CONSTRUCT TAPER AS DIRECTED BY THE ENGINEER.

STANDARD SHEET NO. 5-297.404	TITLE: PERMANENT EROSION CONTROL ALONG ROADWAYS, DITCHES AND FLUMES
STANDARD APPROVED: DECEMBER 19, 1930	
STATE PROJ. NO. 02-614-14	SHEET NO. 13 OF 117 SHEETS

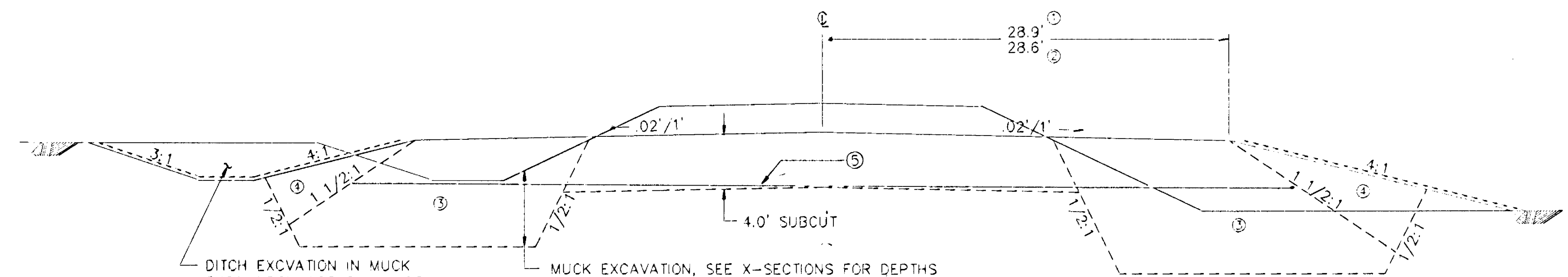
VAX780 C:\3\1\51.100\ FILE NAME S404L90.SPN

NORMAL GRADING



- ① STA. 33+65 - 53+67 (INCLUDING RADISSON RD. APPR.)
- ② STA. 53+67 - 199+67 (INCLUDING LEXINGTON AVE. APPR.)
- ③ FOR SPECIAL DITCHES SEE PROFILE SHEETS & X-SECTIONS
- ④ 1.0' SUBCUT, C.R. 52 STA. 500+85 - 509+93

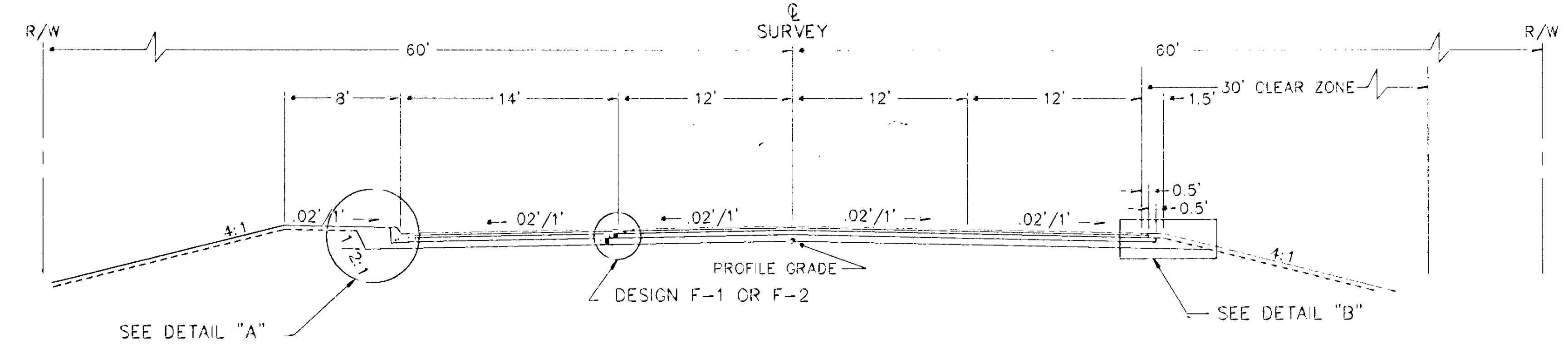
MUCK EXCAVATION



- ① STA. 33+65 - 53+67 (INCLUDING RADISSON RD. APPR.)
- ② STA. 53+67 - 199+67 (INCLUDING LEXINGTON AVE. APPR.)
- ③ FILL WITH GRANULAR MATERIAL
- ④ FILL WITH MUCK EXCAVATION MATERIAL
- ⑤ GEO-FABRIC TO BE USED AS DIRECTED BY THE ENGINEER FOR SEPARATION OF MATERIALS IN AREAS OF UNSUITABLE SUBBASE.

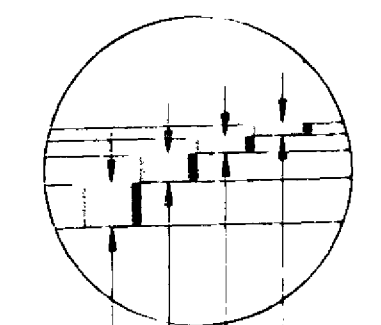
RURAL TYPICAL SECTION

C.S.A.H. 14, 53+67 - 179+20
C.R. 52, 500+85 - 509+93



DESIGN F-1

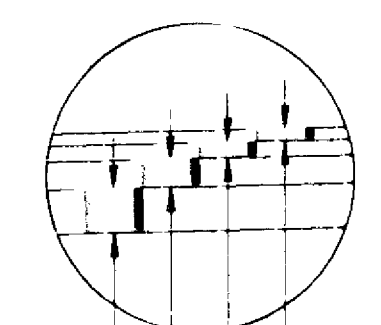
C.S.A.H. 14, STA. 33+65 - 53+67
C.R. 52, STA. 499+00 - 510+00



- 1 1/2" TYPE 41 WEARING COURSE-SPEC. 2340
- 2" TYPE 31 BINDER COURSE-SPEC. 2340
- 3 1/2" TYPE 31 BASE COURSE-SPEC. 2340
- 5 1/2" AGGREGATE BASE CL-5-SPEC. 2211
- TACK COAT-SPEC. 2357, TO BE APPLIED BETWEEN ALL BITUMINOUS LIFTS.

DESIGN F-2

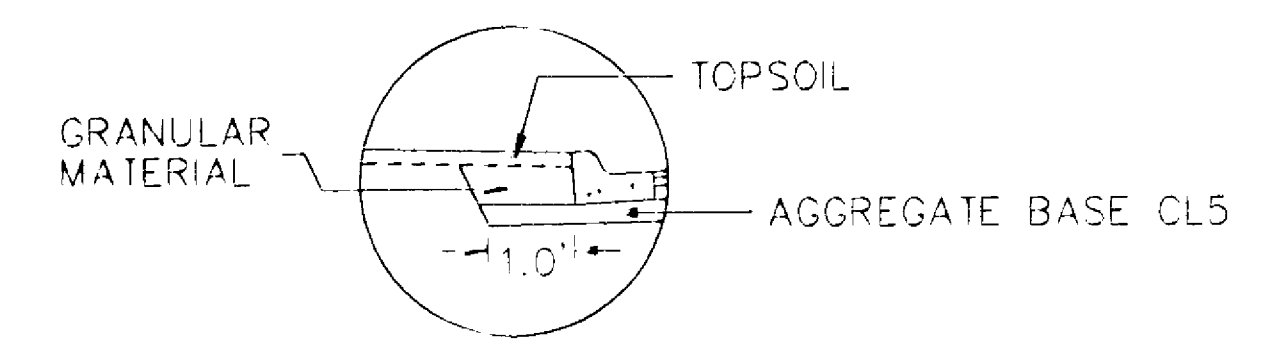
C.S.A.H. 14, STA. 53+67 - 199+67
C.S.A.H. 17, STA. 689+80 - 710+23



- 1 1/2" TYPE 41 WEARING COURSE-SPEC. 2340
- 2" TYPE 31 BINDER COURSE-SPEC. 2340
- 3" TYPE 31 BASE COURSE-SPEC. 2340
- 5" AGGREGATE BASE CL-5-SPEC. 2211
- TACK COAT-SPEC. 2357, TO BE APPLIED BETWEEN ALL BITUMINOUS LIFTS.

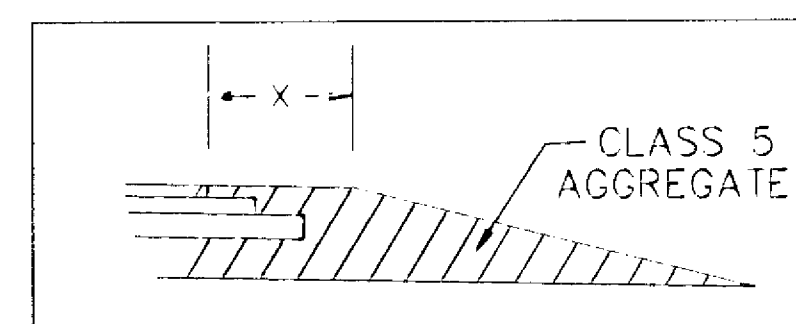
DETAIL "A"

C.S.A.H. 14, STA. 32+20 - 43+40 LT.
C.R. 52, STA. 500+85 - 504+77 LT.



DETAIL "B"

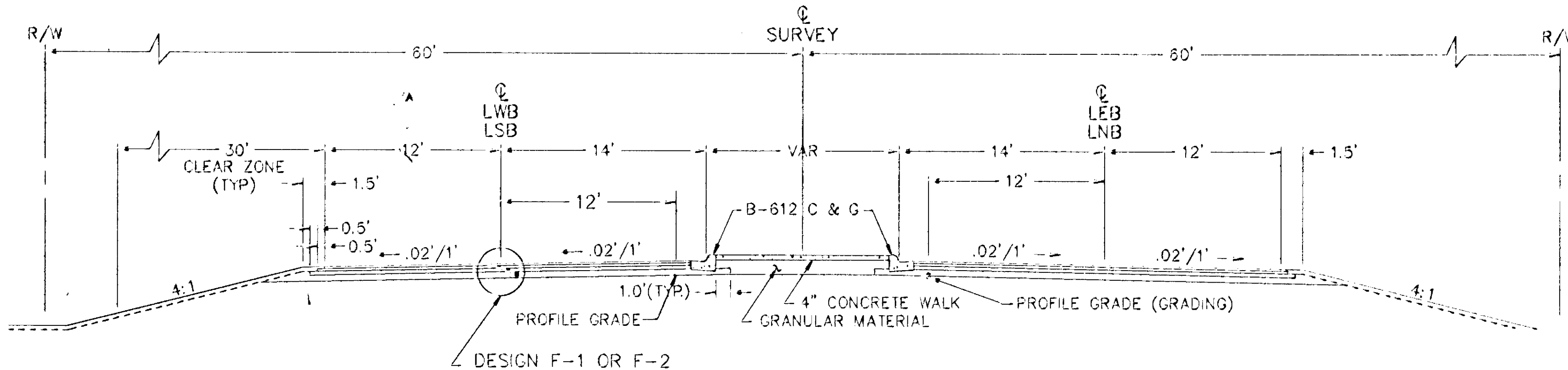
C.R. 52: STA. 500+85 - 503+33 RT. X = 8' BITUMINOUS SHOULDER
STA. 503+33 - 503+93 RT. X = VAR. AGGREGATE SHOULDER
STA. 503+93 - 509+93 RT. X = 4' AGGREGATE SHOULDER
STA. 505+43 - 509+93 LT. X = 4' AGGREGATE SHOULDER



REVISIONS			
DATE	BY	DATE	BY

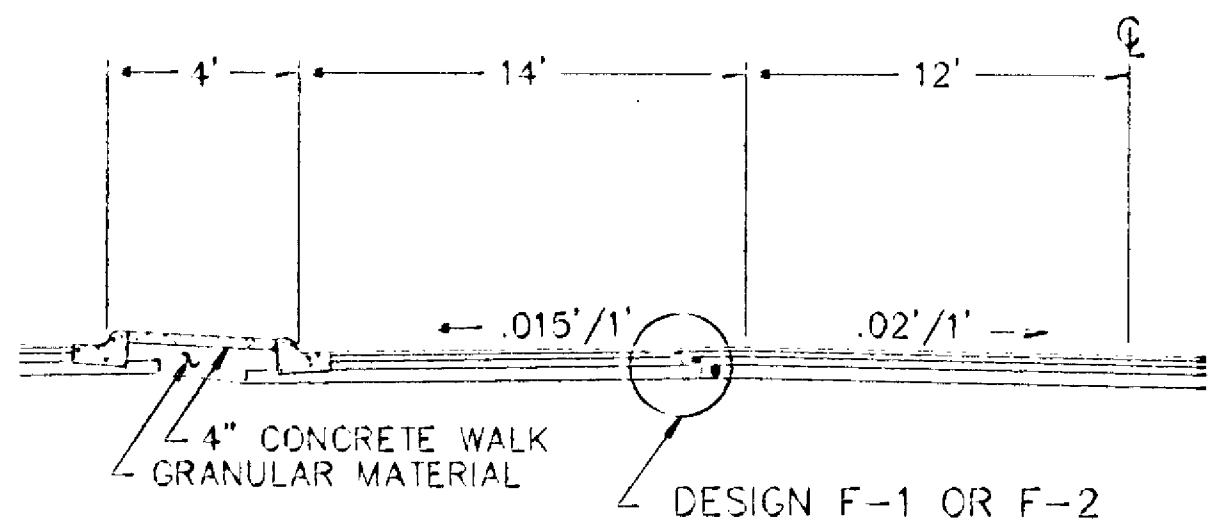
TYPICAL SECTION

C.S.A.H. 14, 33+65 - 53+67
 C.S.A.H. 14, 179+20 - 199+67
 C.S.A.H. 17, 689+80 - 710+23



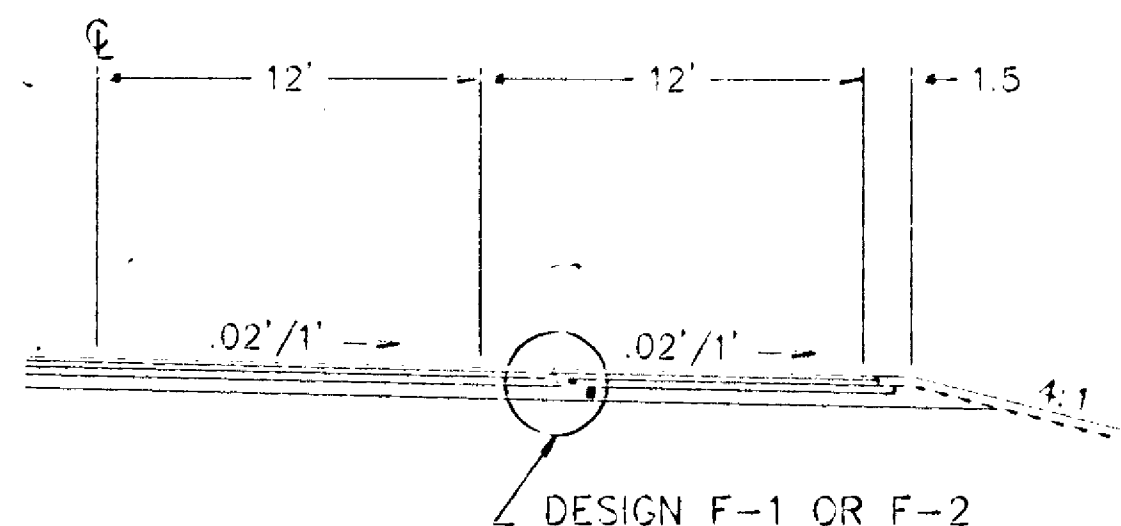
LEFT TURN LANES

C.S.A.H. 14, LEB 40+43 - 43+34
 C.S.A.H. 14, LWB 44+10 - 46+90
 C.S.A.H. 14, LEB 185+97 - 188+91
 C.S.A.H. 14, LWB 189+98 - 192+90
 C.S.A.H. 17, LNB 696+57 - 699+49
 C.S.A.H. 17, LSB 700+56 - 703+46

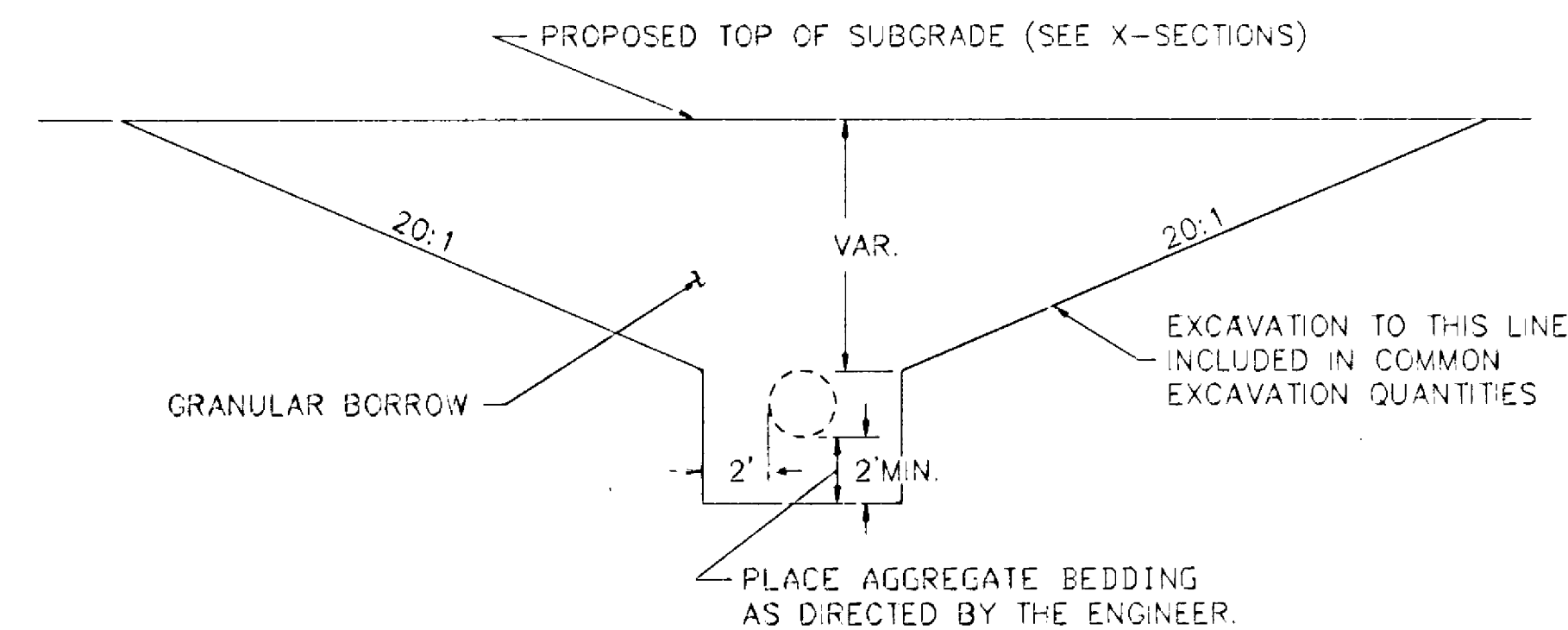


RIGHT TURN LANES

C.S.A.H. 14, LEB 40+42 - 43+04
 C.S.A.H. 14, LWB 44+40 - 46+90
 C.S.A.H. 14, LEB 185+97 - 188+64
 C.S.A.H. 14, LWB 190+26 - 192+90
 C.S.A.H. 17, LNB 696+57 - 699+12
 C.S.A.H. 17, LSB 700+86 - 703+46

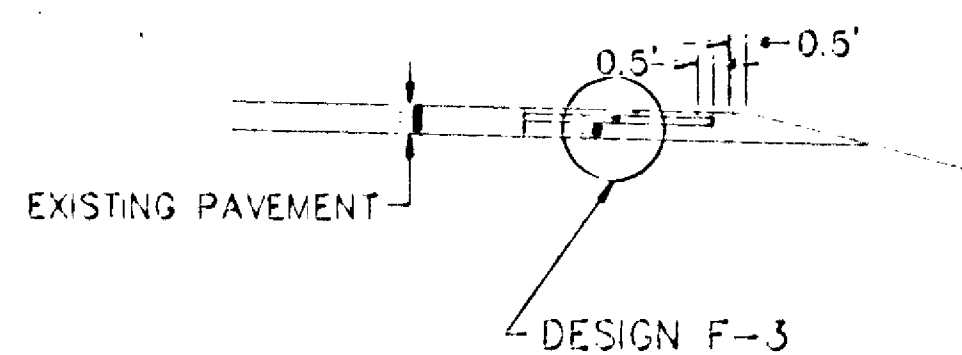


CENTERLINE CULVERT INSTALLATION



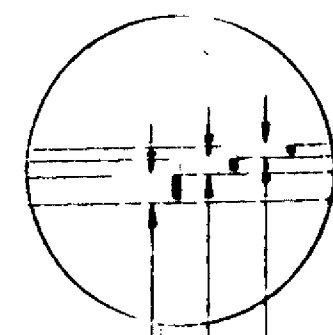
- NOTES:
- HAULING OPERATIONS TO BE COMPLETED PRIOR TO CENTERLINE CULVERT INSTALLATION.
 - FILL AND COMPACT GRANULAR TO 2" ABOVE FLOWLINE BEFORE PLACING CULVERT
 - AT CENTERLINE CULVERT LOCATIONS CULVERT EXC. TO EXTEND 3' BEYOND END OF APRON
 - SEE CROSS SECTION AND PROFILE SHEETS FOR DEPTH AND LIMITS OF CULVERT EXC.

TEMPORARY PAVEMENT



DESIGN F-3

TEMPORARY PAVEMENT SECTION



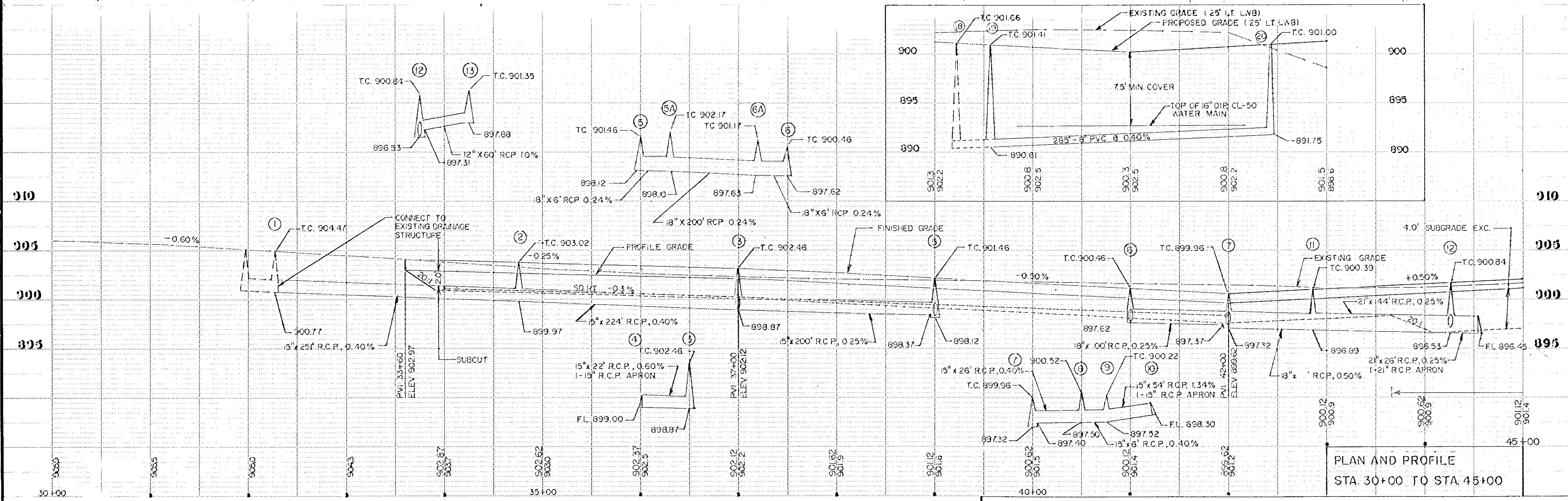
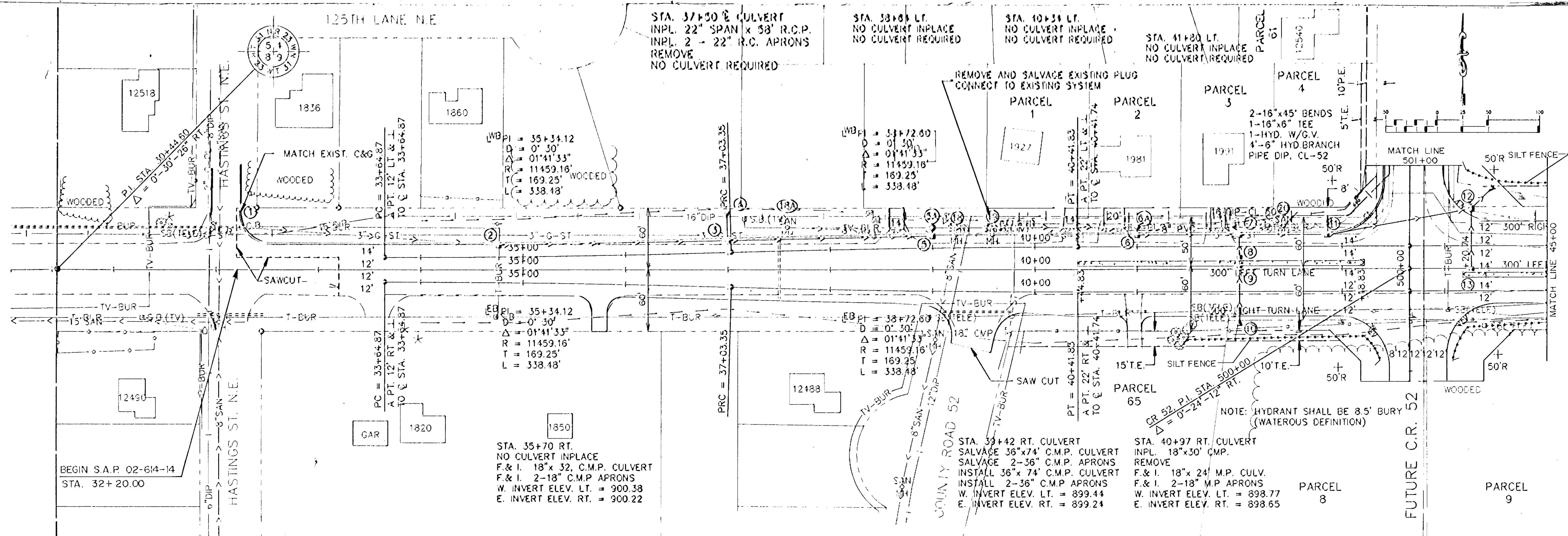
1 1/2" TYPE 31 WEARING COURSE-SPEC. 2331
 2" TYPE 31 BASE COURSE-SPEC. 2331
 4" AGGREGATE BASE, CL. 5-SPEC. 2211
 TACK COAT-SPEC. 2357, TO BE APPLIED BETWEEN ALL BITUMINOUS LIFTS.

REVISIONS			
DATE	BY	DATE	BY

S.A.P. 02-614-14 S.P. _____ C.P. _____

TYPICAL SECTIONS

Sheet No. 15 of 17 Sheets



STA. 47+57.4 CULVERT
 INPL. 36" x 44' C.M.P.
 REMOVE
 F. & I. 24" x 112' R.C. PIPE CULVERT
 F. & I. 2 - 24" R.C. PIPE APRONS
 INVERT ELEV. LT. = 896.75
 INVERT ELEV. RT. = 896.75

STA. 51+14 LT.
 INPL. 18" x 30' C.M.P.
 REMOVE
 F. & I. 18" x 50' C.M.P. CULV.
 F. & I. 2-18" C.M.P. APRONS
 W. INVERT ELEV. = 897.74
 E. INVERT ELEV. = 898.06

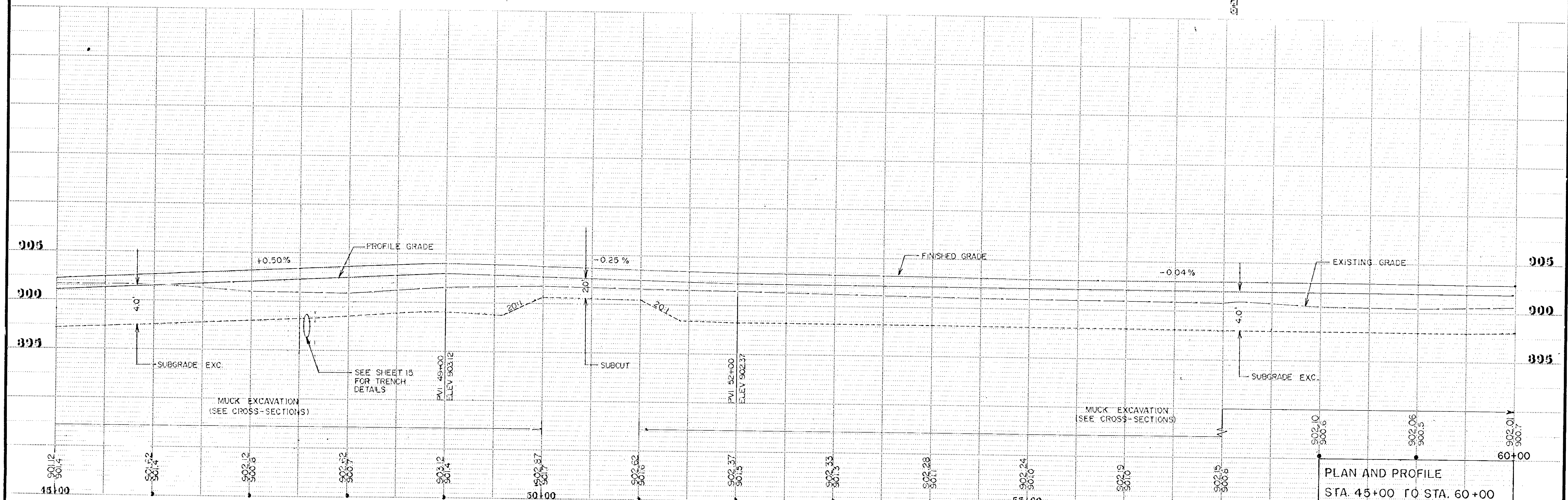
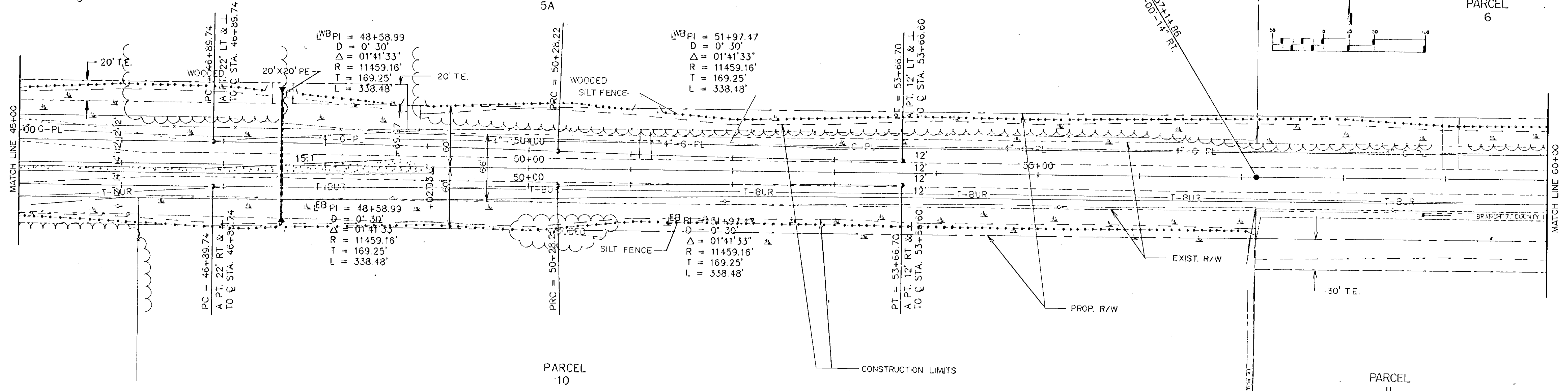
Fed. Proj. No.

STA. 59+04 LT.
 NO CULVERT INPLACE
 NO CULVERT REQUIRED

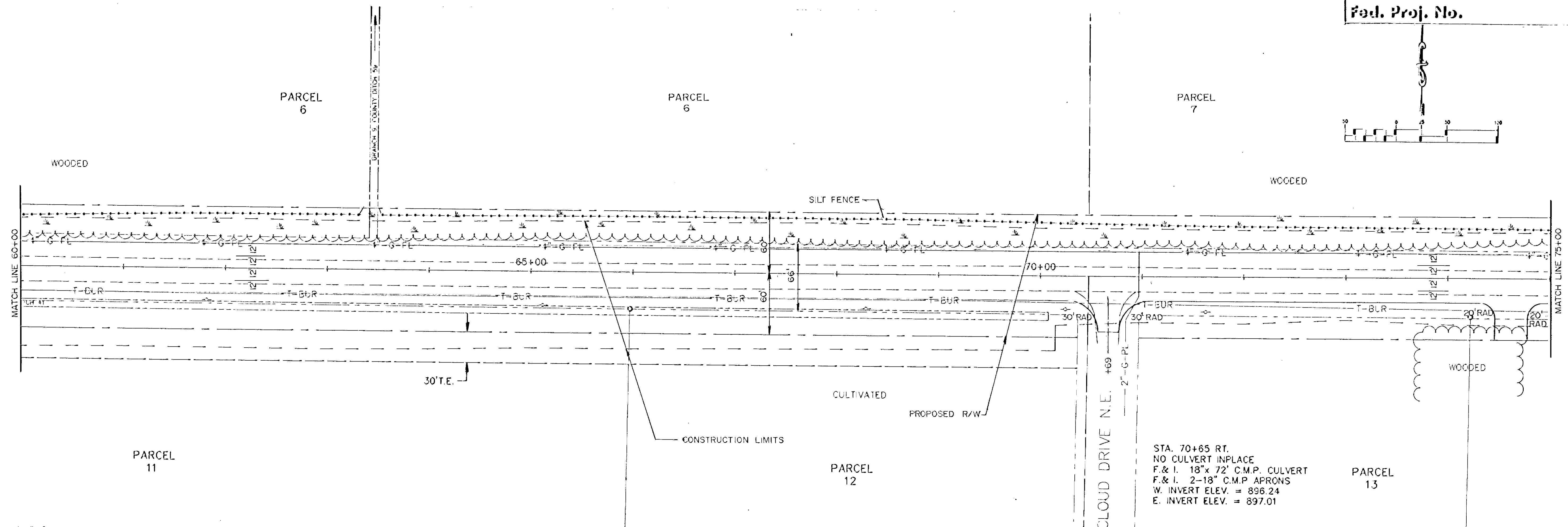
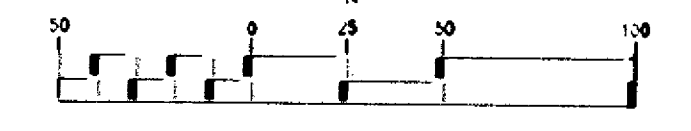
PARCEL 6

PARCEL 5

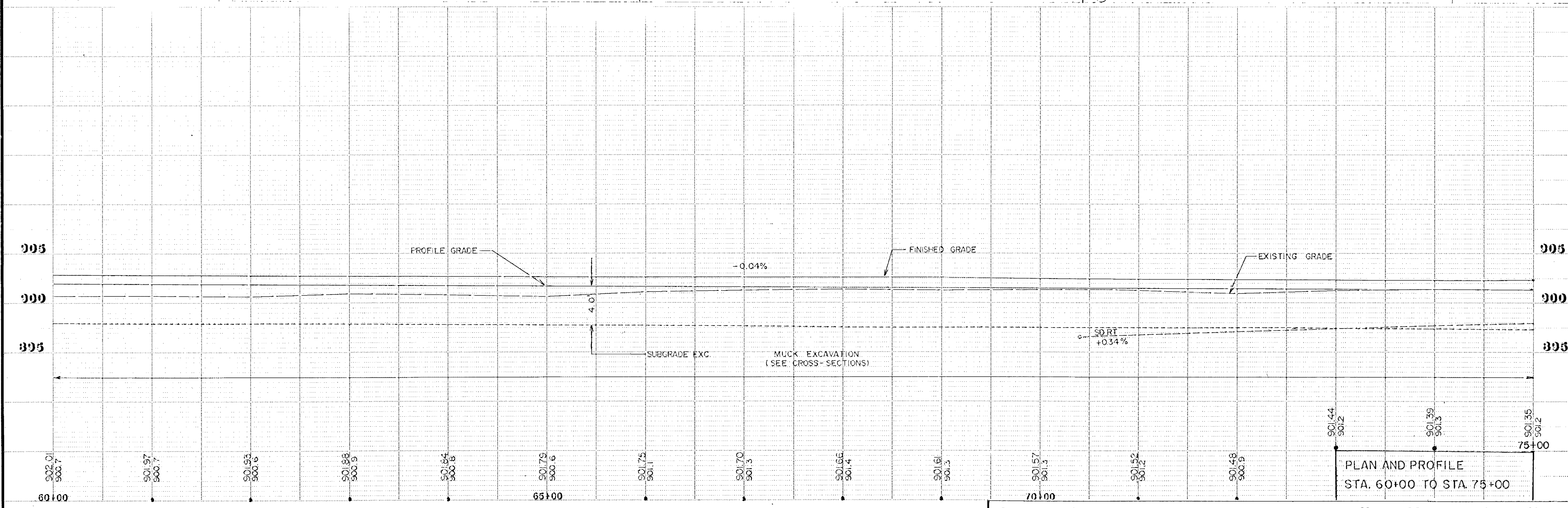
PARCEL 5A



PLAN AND PROFILE
 STA. 45+00 TO STA. 60+00



STA. 70+65 RT.
 NO CULVERT IN PLACE
 F. & I. 18" x 72" C.M.P. CULVERT
 F. & I. 2-18" C.M.P. APRONS
 W. INVERT ELEV. = 896.24
 E. INVERT ELEV. = 897.01



PLAN AND PROFILE
 STA. 60+00 TO STA. 75+00

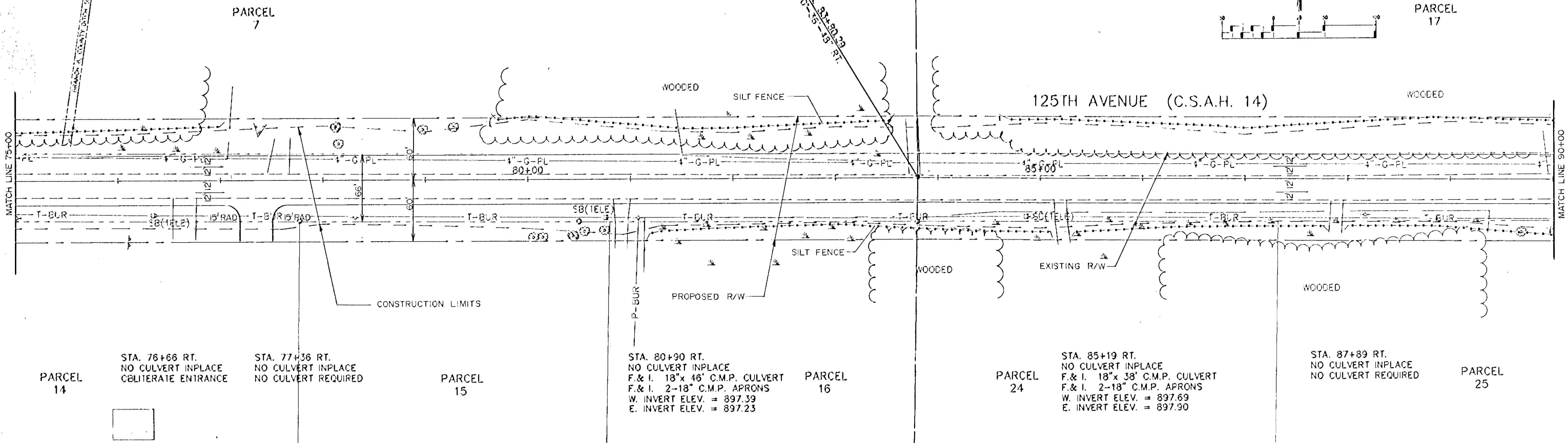
COPY Equipment Form #1

STA. 77+58 LT.
NO CULVERT INPLACE
F.&I. 18" x 18" C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 897.45
E. INVERT ELEV. = 897.71

STA. 83+76 LT. CULVERT
INPL. NONE
F.&I. 18" x 44" C.M.P.
F.&I. 2-18" M.P. APRONS
W. INVERT ELEV. = 897.26
E. INVERT ELEV. = 897.40



PARCEL 17



PARCEL 14

STA. 76+66 RT.
NO CULVERT INPLACE
CELTERRATE ENTRANCE

STA. 77+36 RT.
NO CULVERT INPLACE
NO CULVERT REQUIRED

PARCEL 15

STA. 80+90 RT.
NO CULVERT INPLACE
F.&I. 18" x 46" C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 897.39
E. INVERT ELEV. = 897.23

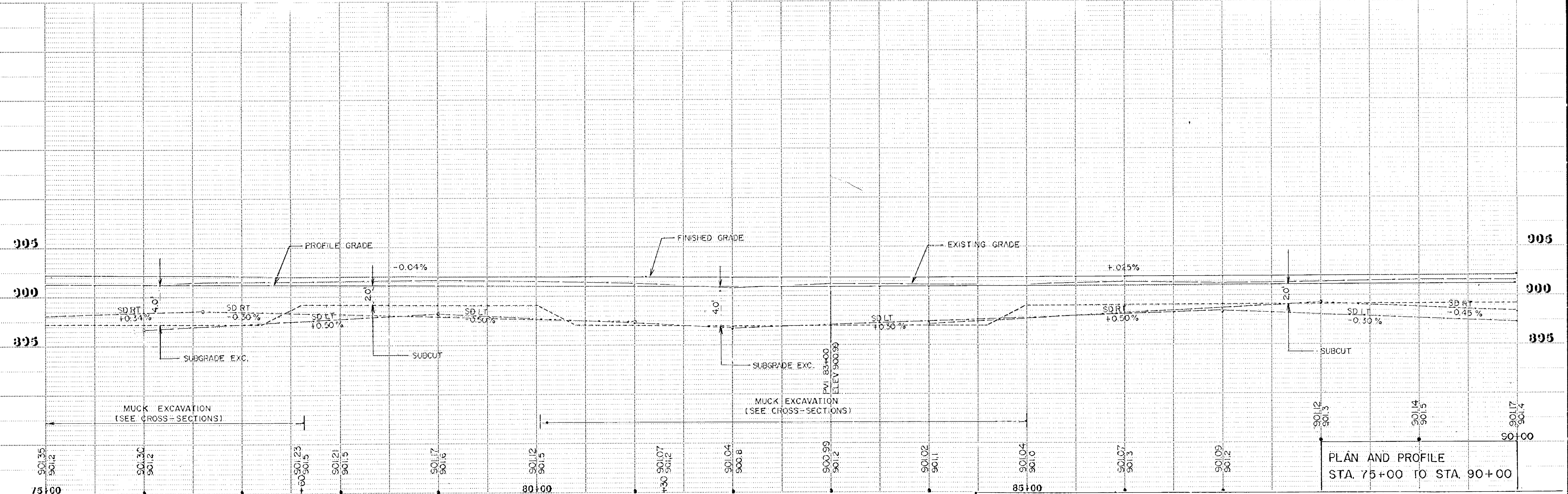
PARCEL 16

PARCEL 24

STA. 85+19 RT.
NO CULVERT INPLACE
F.&I. 18" x 38" C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 897.69
E. INVERT ELEV. = 897.90

STA. 87+89 RT.
NO CULVERT INPLACE
NO CULVERT REQUIRED

PARCEL 25



PLAN AND PROFILE
STA. 75+00 TO STA. 90+00

STA. 90+02 LT.
 INPL. 19" x 26' C.M.P.
 REMOVE
 F. & I. 18" x 56' C.M.P. CULVERT
 F. & I. 2-18" C.M.P. APRONS
 W. INVERT ELEV. = 896.89
 E. INVERT ELEV. = 896.71

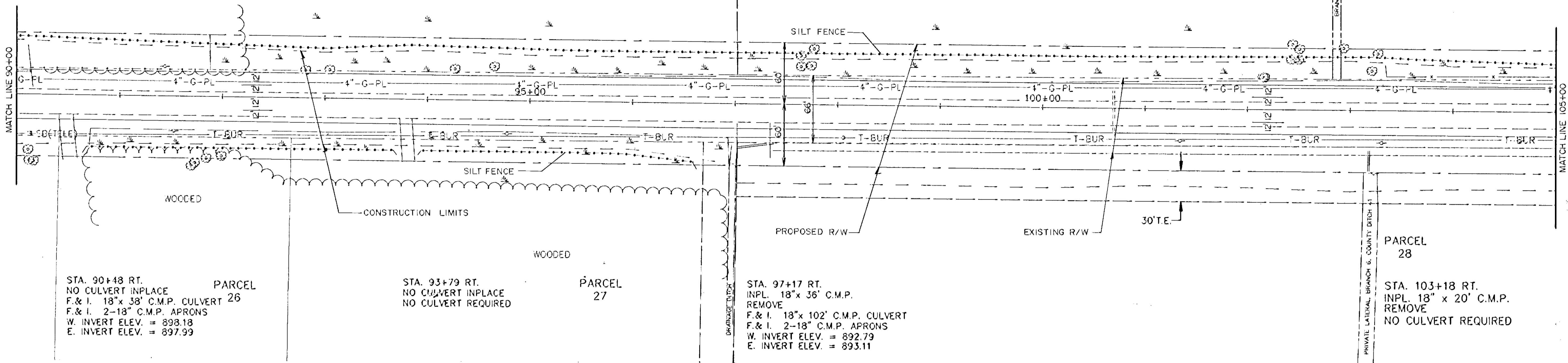
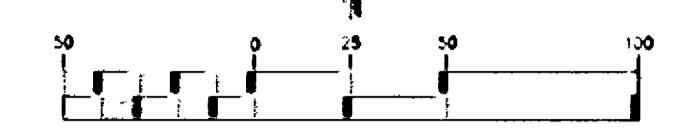
STA. 100+69 C CULVERT
 INPL. 36" x 44' C.M.P.
 REMOVE
 NO CULVERT REQUIRED

Fed. Proj. No.

PARCEL 17

PARCEL 18

PARCEL 18



STA. 90+48 RT.
 NO CULVERT INPLACE
 F. & I. 18" x 38' C.M.P. CULVERT 26
 F. & I. 2-18" C.M.P. APRONS
 W. INVERT ELEV. = 898.18
 E. INVERT ELEV. = 897.99

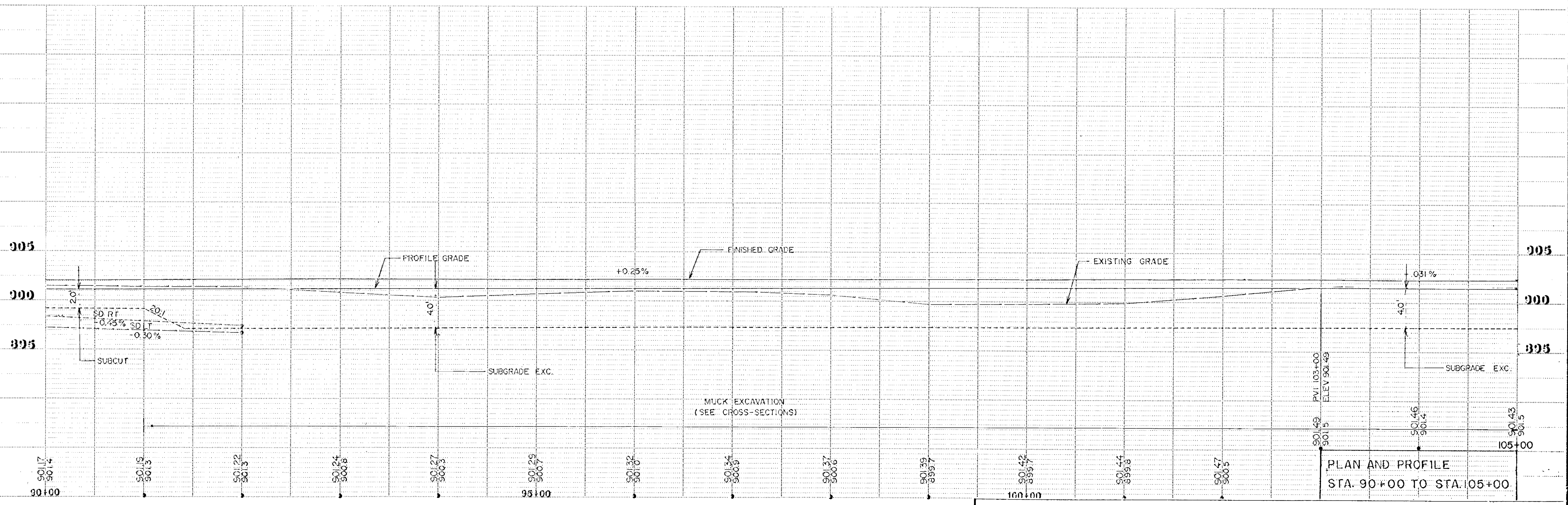
STA. 93+79 RT.
 NO CULVERT INPLACE
 NO CULVERT REQUIRED

PARCEL 27

STA. 97+17 RT.
 INPL. 18" x 36' C.M.P.
 REMOVE
 F. & I. 18" x 102' C.M.P. CULVERT
 F. & I. 2-18" C.M.P. APRONS
 W. INVERT ELEV. = 892.79
 E. INVERT ELEV. = 893.11

PARCEL 28

STA. 103+18 RT.
 INPL. 18" x 20' C.M.P.
 REMOVE
 NO CULVERT REQUIRED



PLAN AND PROFILE
 STA. 90+00 TO STA. 105+00

STA. 108+40 LT.
NO CULVERT INPLACE
F.& I. 18"x 44" C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 897.55
E. INVERT ELEV. = 897.67

STA. 111+56 LT.
NO CULVERT INPLACE
F.& I. 18"x 42" C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 898.15
E. INVERT ELEV. = 898.27

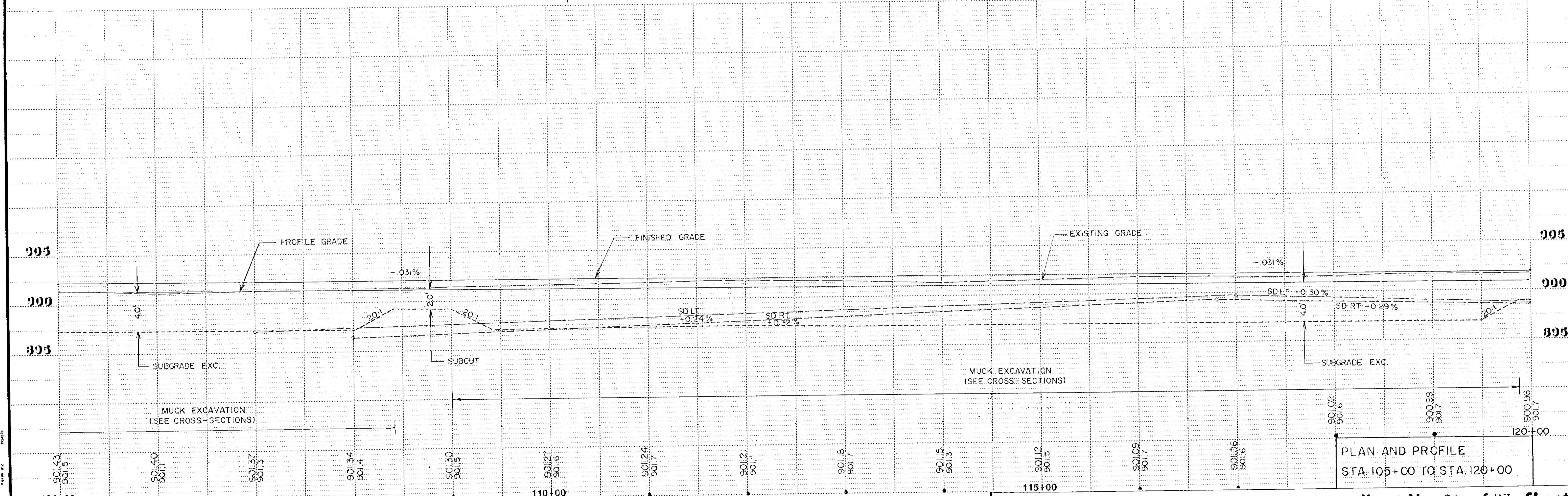
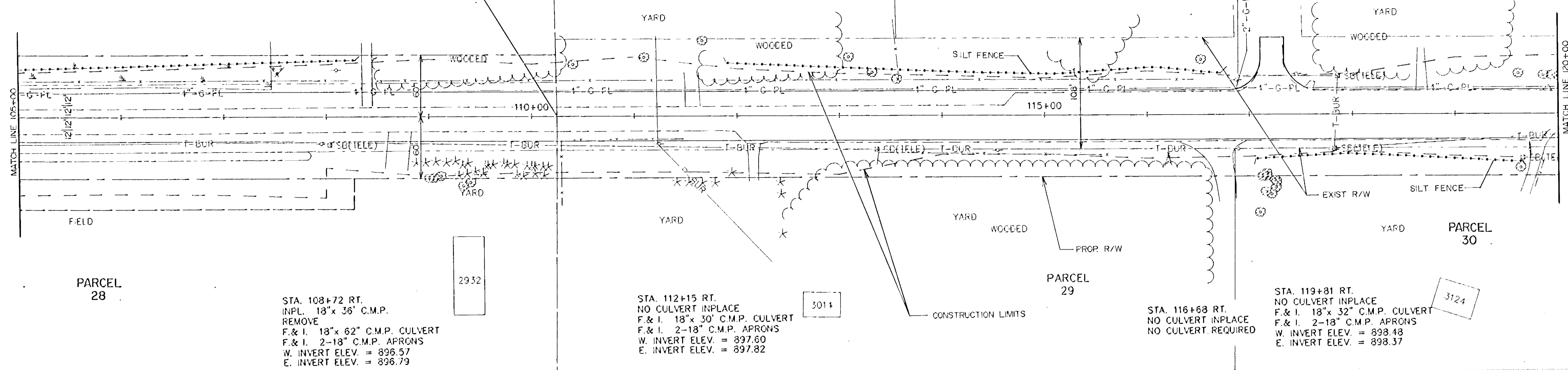
STA. 118+67 LT.
NO CULVERT INPLACE
F.& I. 18"x 28" C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 899.05
E. INVERT ELEV. = 899.15

STA. 108+72 RT.
INPL. 18"x 36" C.M.P.
REMOVE
F.& I. 18"x 62" C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 896.57
E. INVERT ELEV. = 896.79

STA. 112+15 RT.
NO CULVERT INPLACE
F.& I. 18"x 30" C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 897.60
E. INVERT ELEV. = 897.82

STA. 116+68 RT.
NO CULVERT INPLACE
NO CULVERT REQUIRED

STA. 119+81 RT.
NO CULVERT INPLACE
F.& I. 18"x 32" C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 898.48
E. INVERT ELEV. = 898.37



3327

STA. 120+49 LT.
NO CULVERT INPLACE
F.&I. 18"x 38' C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 898.62
E. INVERT ELEV. = 898.48

STA. 124+77 C/L CULVERT
INPL. 24" x 48' C.M.P.
REMOVE
F.&I. 24" x 68' R.C.P. CULVERT
F.&I. 2 - 24" R.C.P. APRONS
INVERT ELEV. LT. = 897.30
INVERT ELEV. RT. = 897.00

STA. 126+71 LT.
NO CULVERT INPLACE
F.&I. 18" x 40' C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 898.13
E. INVERT ELEV. = 898.35

STA. 129+94 LT.
NO CULVERT INPLACE
F.&I. 18" x 32' C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 899.72
E. INVERT ELEV. = 899.90

STA. 132+11 LT.
NO CULVERT INPLACE
F.&I. 18" x 28' C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 900.79
E. INVERT ELEV. = 900.95

PARCEL 18

PARCEL 19

PARCEL 20

PARCEL 22

PARCEL 19B

3151

3259

PARCEL 21

PARCEL 19A

YARD

STA. 123+97 RT.
INPL. 18"x 68' C.M.P. CULVERT
REMOVE
F.&I. 18"x 58' C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS

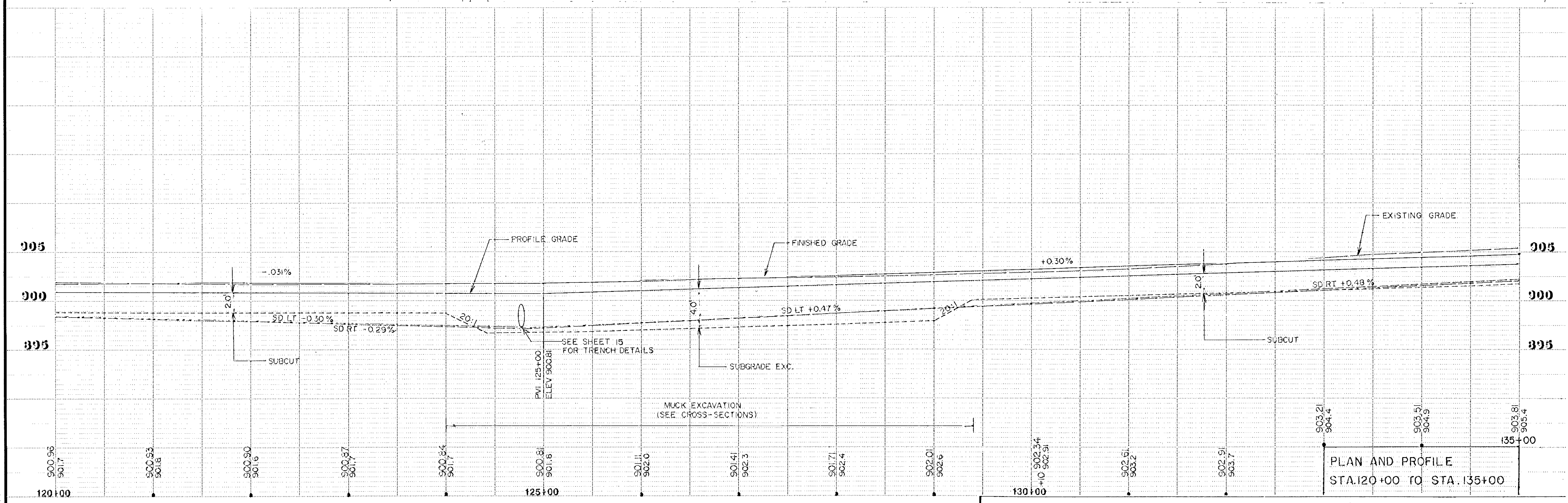
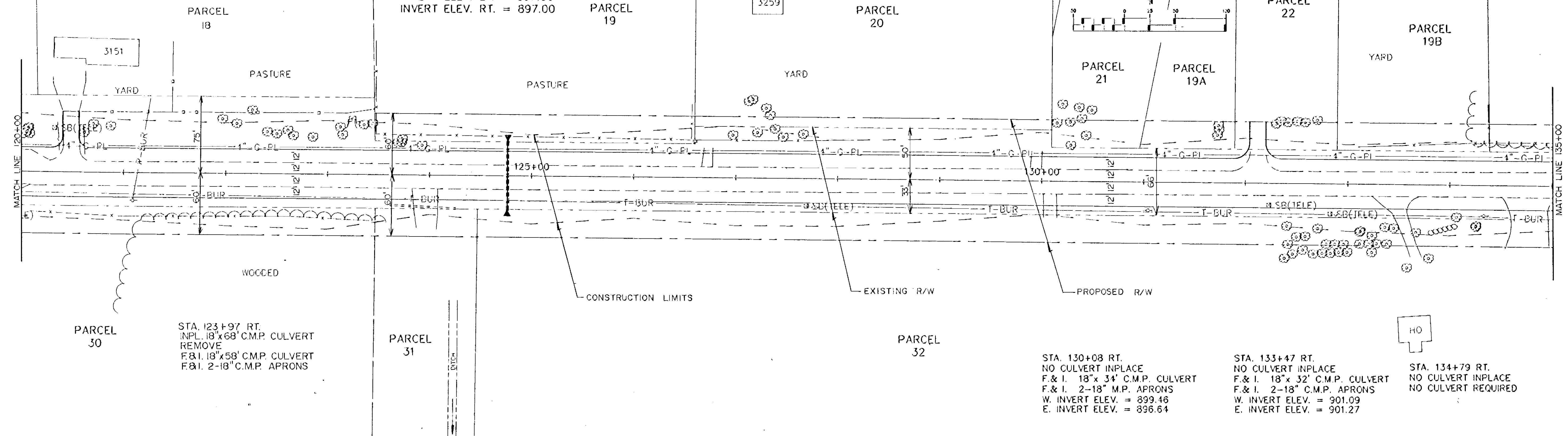
PARCEL 30

PARCEL 32

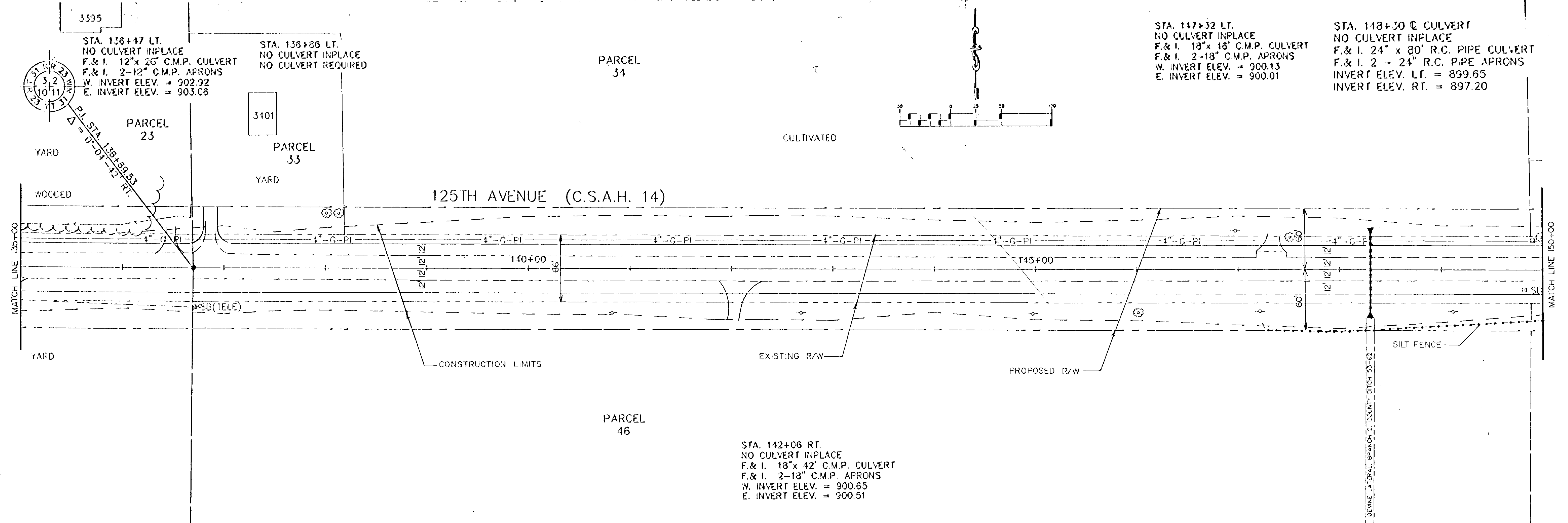
STA. 130+08 RT.
NO CULVERT INPLACE
F.&I. 18" x 34' C.M.P. CULVERT
F.&I. 2-18" M.P. APRONS
W. INVERT ELEV. = 899.46
E. INVERT ELEV. = 896.64

STA. 133+47 RT.
NO CULVERT INPLACE
F.&I. 18" x 32' C.M.P. CULVERT
F.&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 901.09
E. INVERT ELEV. = 901.27

STA. 134+79 RT.
NO CULVERT INPLACE
NO CULVERT REQUIRED

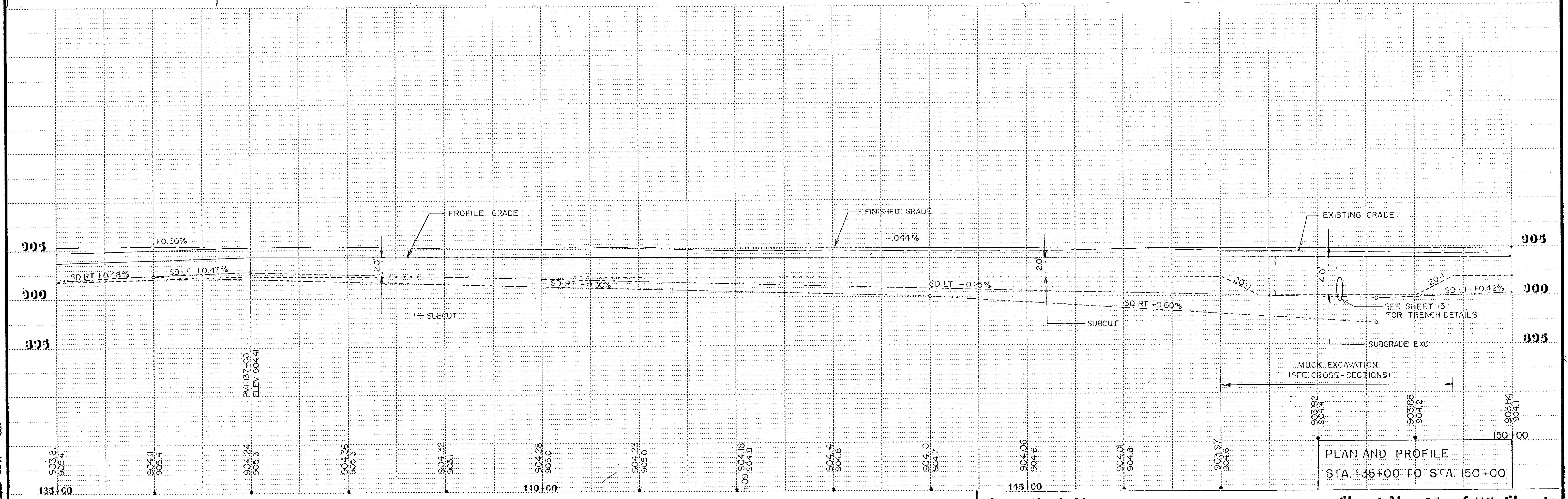


PLAN AND PROFILE
STA. 120+00 TO STA. 135+00



STA. 147+32 LT.
NO CULVERT INPLACE
F&I. 18" x 48" C.M.P. CULVERT
F&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 900.13
E. INVERT ELEV. = 900.01

STA. 148+50 C CULVERT
NO CULVERT INPLACE
F&I. 24" x 80" R.C. PIPE CULVERT
F&I. 2-24" R.C. PIPE APRONS
INVERT ELEV. LT. = 899.65
INVERT ELEV. RT. = 897.20



PLAN AND PROFILE
STA. 135+00 TO STA. 150+00

STA. 150+57 LT.
INPL. 18" x 44' C.M.P.
REMOVE
F&I. 18" x 38' C.M.P. CULVERT
F&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 900.38
E. INVERT ELEV. = 900.56

STA. 152+24 LT.
INPL. 18" x 30' C.M.P.
REMOVE
F&I. 18" x 34' C.M.P. CULVERT
F&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 901.09
E. INVERT ELEV. = 901.25

STA. 153+94 LT.
INPL. 18" x 30' C.M.P.
REMOVE
NO CULVERT REQUIRED

STA. 154+50 LT.
INPL. 15" x 36' C.M.P.
REMOVE
F&I. 12" x 38' C.M.P. CULVERT
F&I. 2-12" C.M.P. APRONS

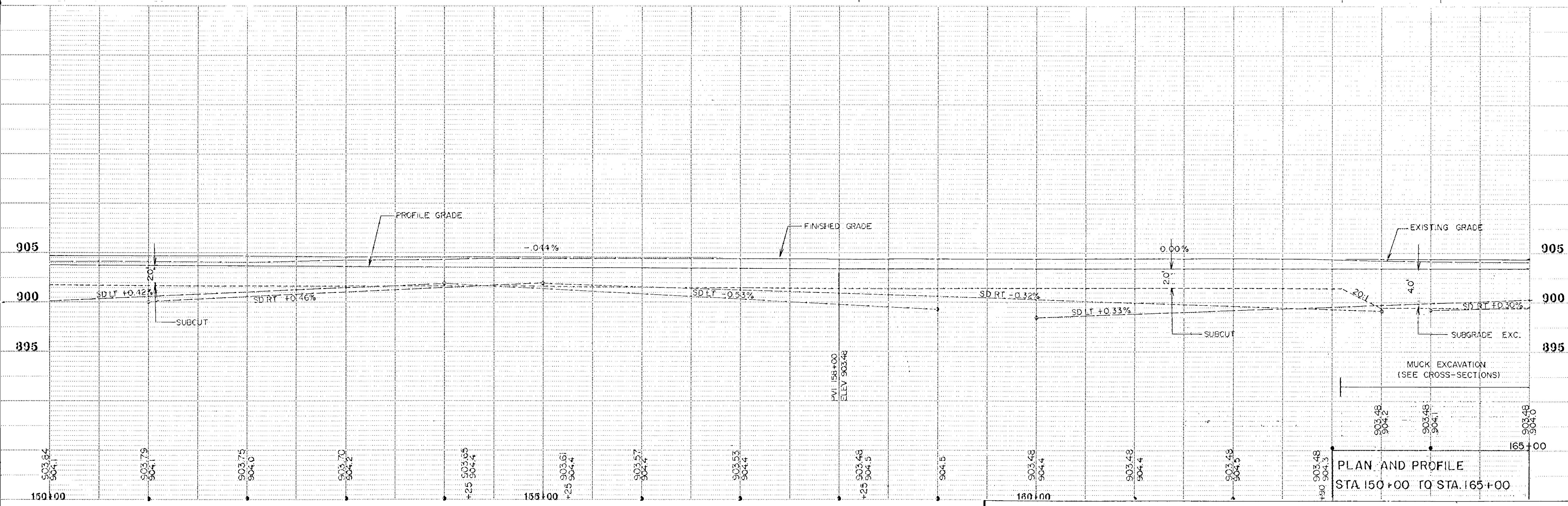
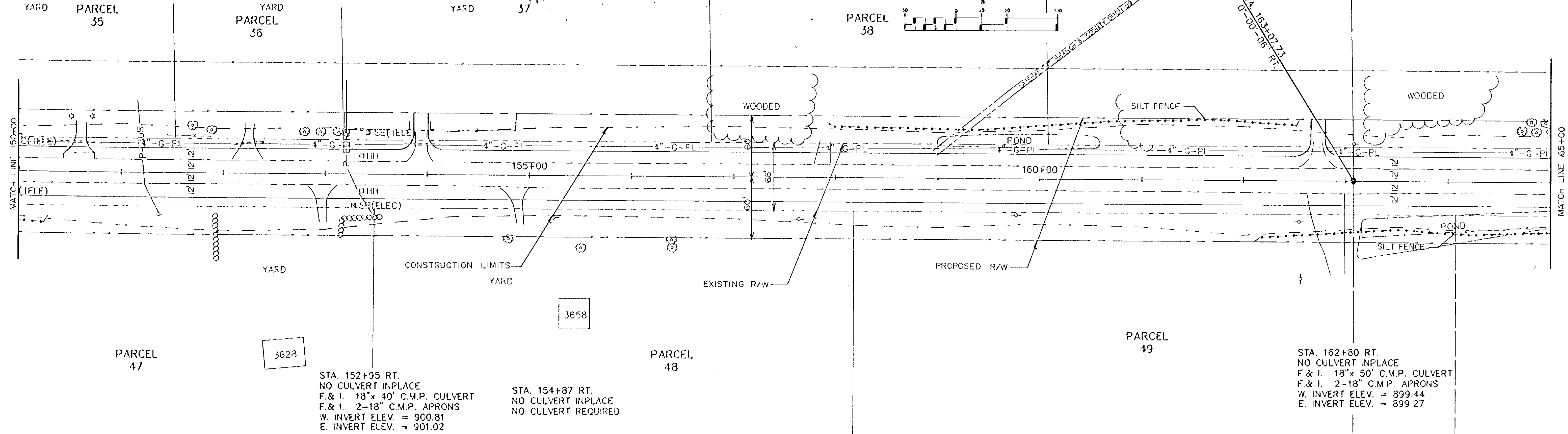
STA. 157+87 LT.
NO CULVERT INPLACE
F&I. 18" x 52' C.M.P. CULVERT
F&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 898.90
E. INVERT ELEV. = 899.20

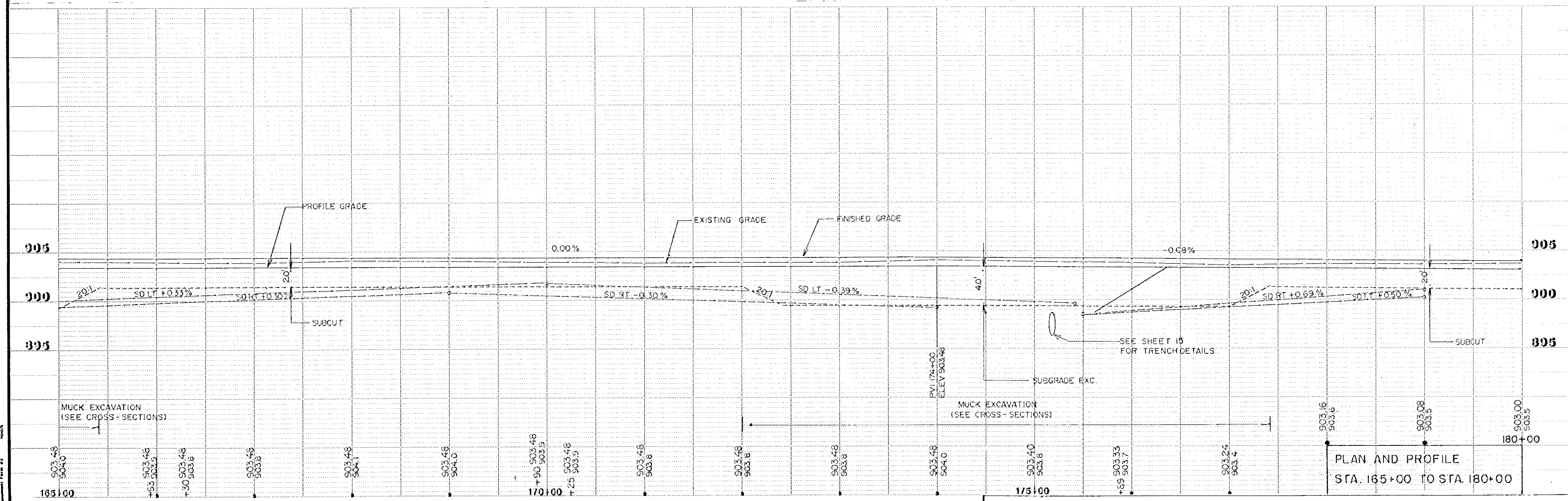
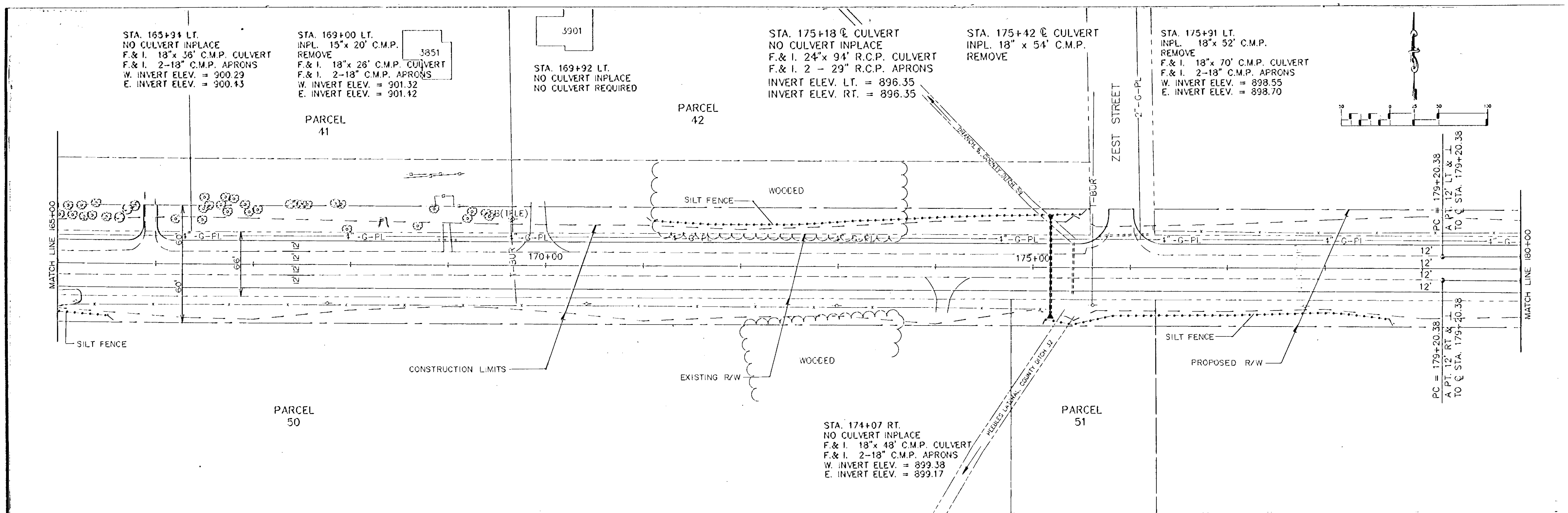
STA. 162+72 LT.
NO CULVERT INPLACE
F&I. 18" x 52' C.M.P. CULVERT
F&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 899.20
E. INVERT ELEV. = 899.39

STA. 152+95 RT.
NO CULVERT INPLACE
F&I. 18" x 40' C.M.P. CULVERT
F&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 900.81
E. INVERT ELEV. = 901.02

STA. 154+87 RT.
NO CULVERT INPLACE
NO CULVERT REQUIRED

STA. 162+80 RT.
NO CULVERT INPLACE
F&I. 18" x 50' C.M.P. CULVERT
F&I. 2-18" C.M.P. APRONS
W. INVERT ELEV. = 899.44
E. INVERT ELEV. = 899.27





PLAN AND PROFILE
STA. 165+00 TO STA. 180+00

STA. 183+09 \odot CULVERT
 INPL. 24" x 42' R.C.P.
 REMOVE
 F. & I. 28" x 82' R.C.P. ARCH CULVERT
 F. & I. 2 - 28" R.C.P. ARCH APRONS
 INVERT ELEV. LT. = 900.80
 INVERT ELEV. RT. = 898.90

PARCEL
 43

PARCEL
 44

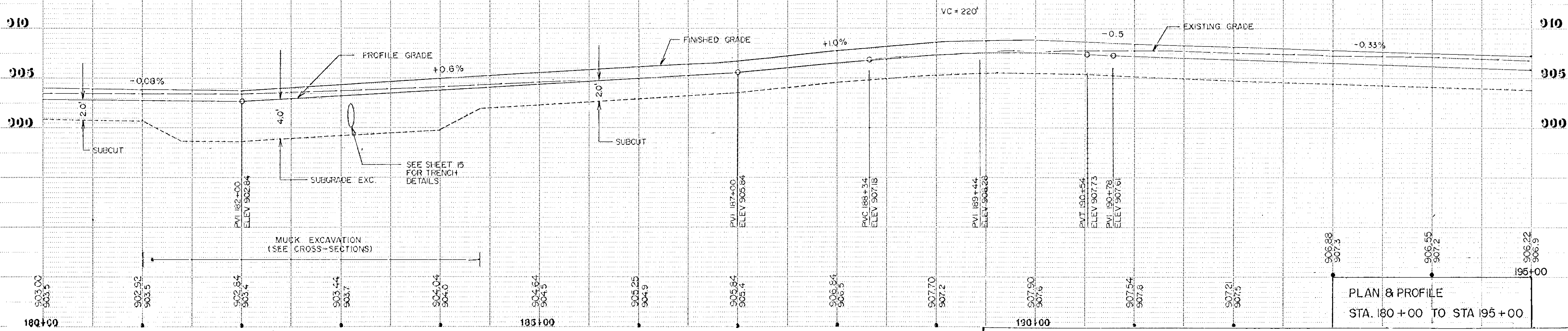
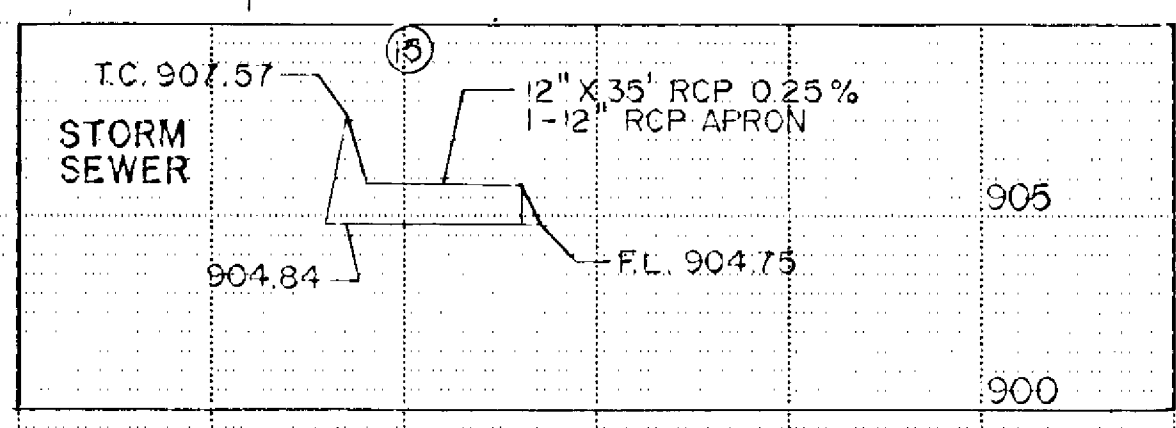
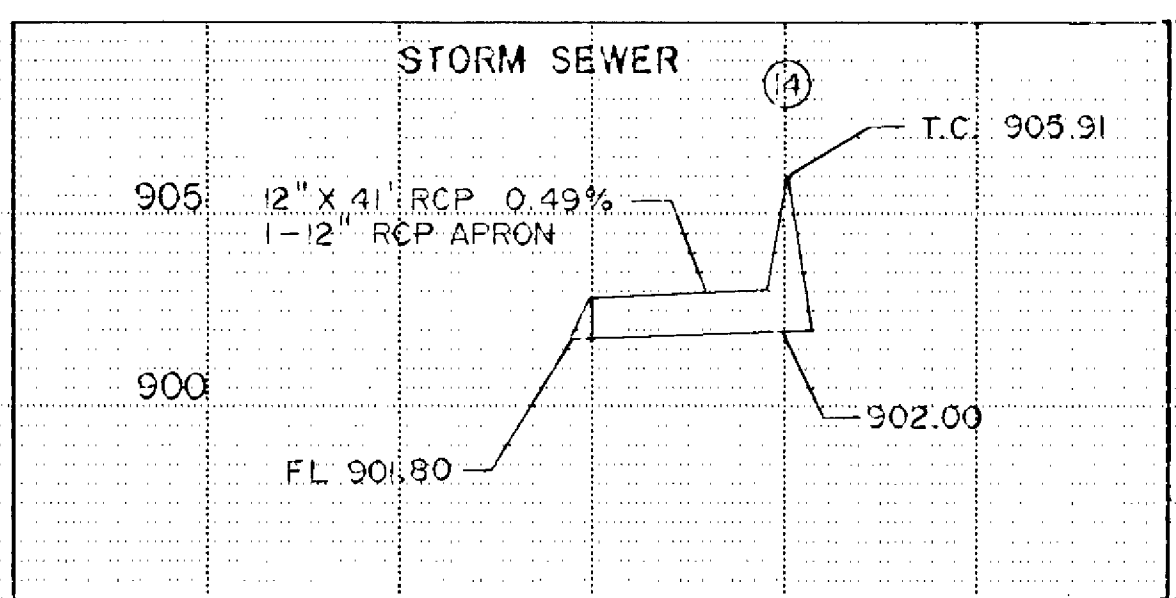
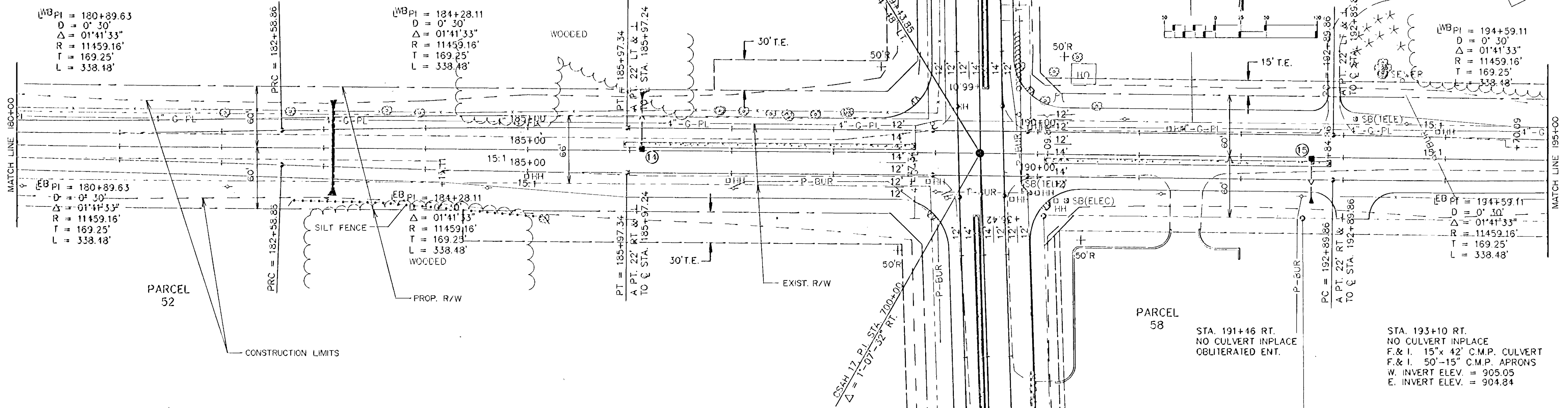
PARCEL
 53

STA. 192+91 LT.
 NO CULVERT INPLACE
 F. & I. 15" x 28" C.M.P. CULVERT
 F. & I. 2-15" C.M.P. APRONS
 W. INVERT ELEV. = 904.41
 E. INVERT ELEV. = 904.25

WB PI = 180+89.63
 D = 0' 30"
 Δ = 01'41"33"
 R = 11459.16'
 T = 169.25'
 L = 338.48'

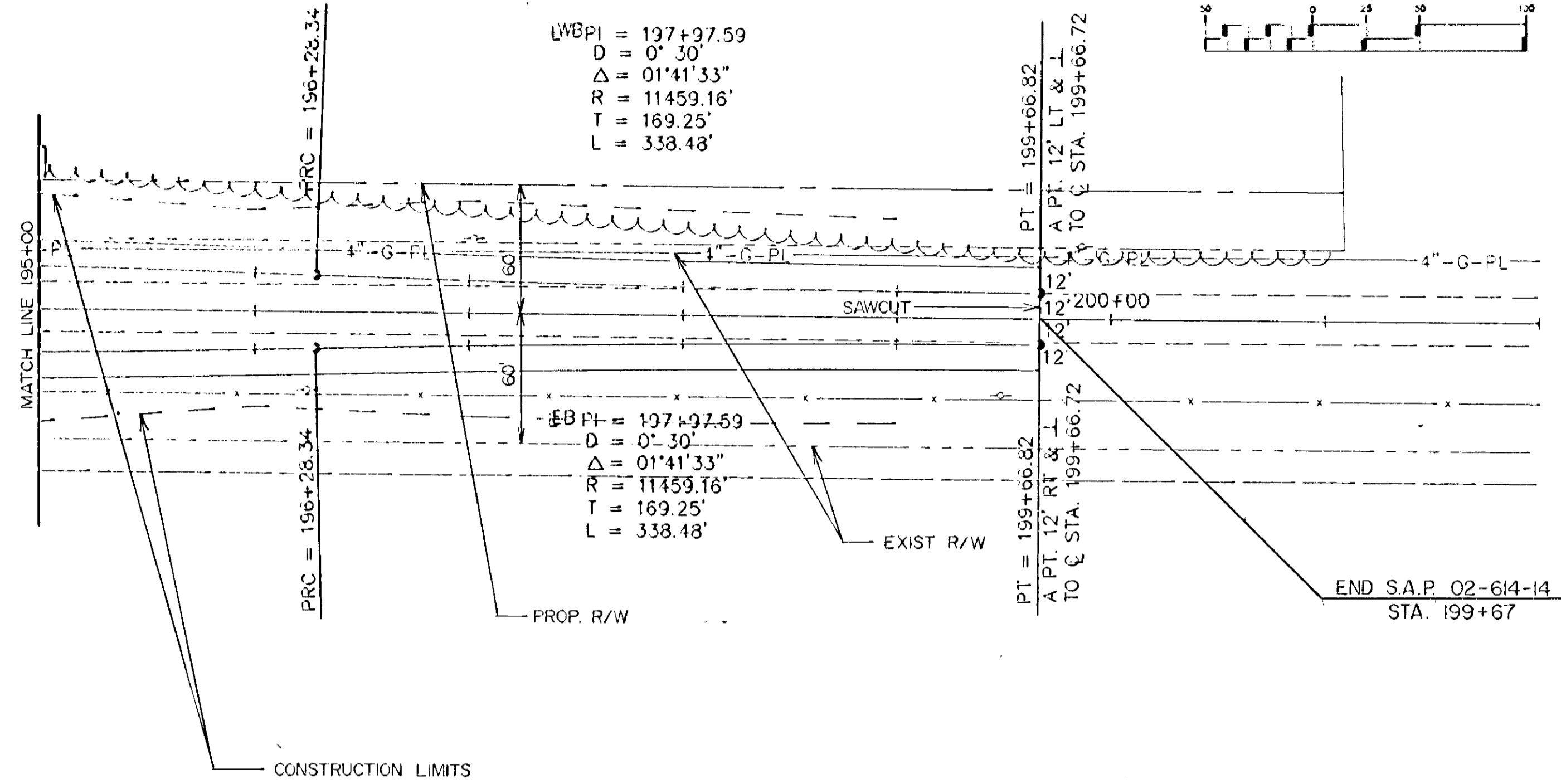
WB PI = 184+28.11
 D = 0' 30"
 Δ = 01'41"33"
 R = 11459.16'
 T = 169.25'
 L = 338.48'

WB PI = 194+59.11
 D = 0' 30"
 Δ = 01'41"33"
 R = 11459.16'
 T = 169.25'
 L = 338.48'



PLAN & PROFILE
 STA. 180+00 TO STA 195+00

PARCEL
54

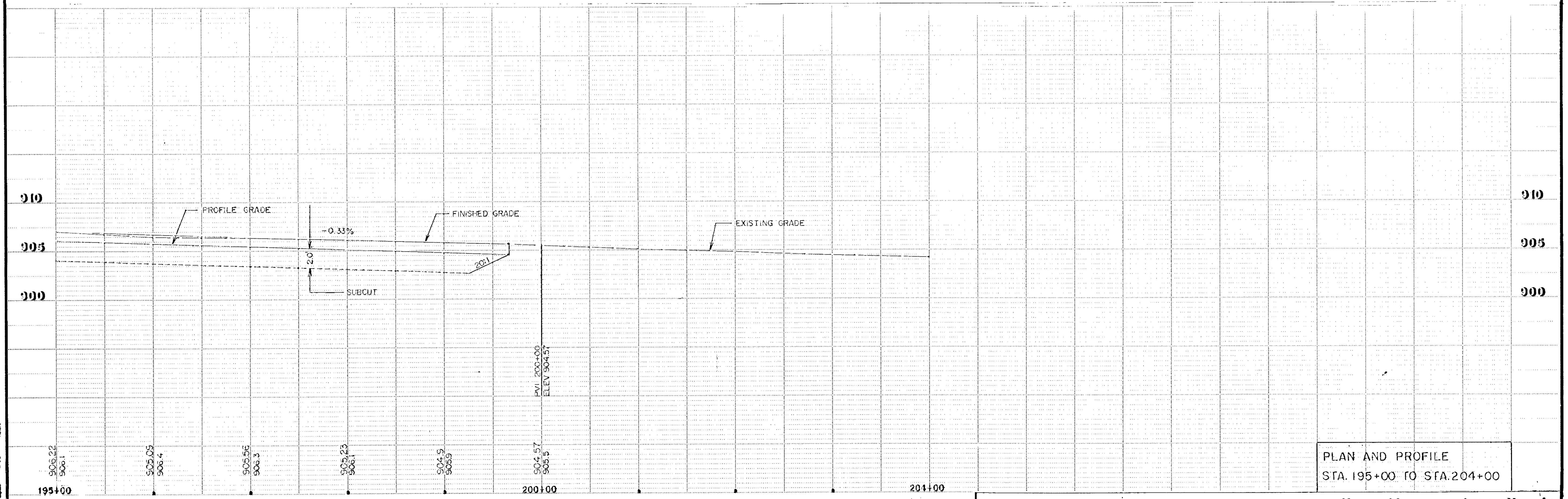
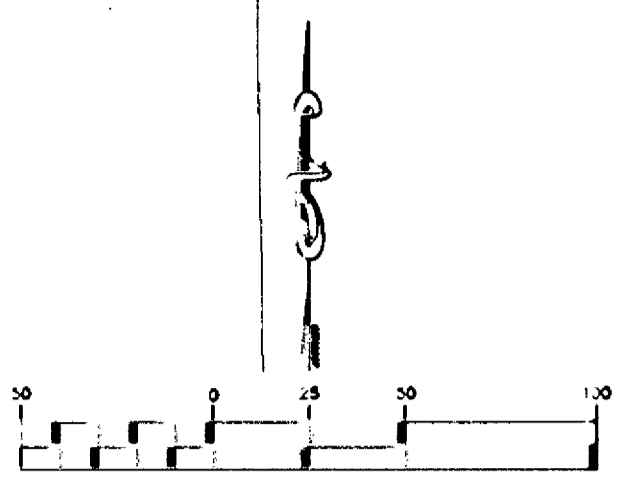


LWBPI = 197+97.59
D = 0° 30'
Δ = 01°41'33"
R = 11459.16'
T = 169.25'
L = 338.48'

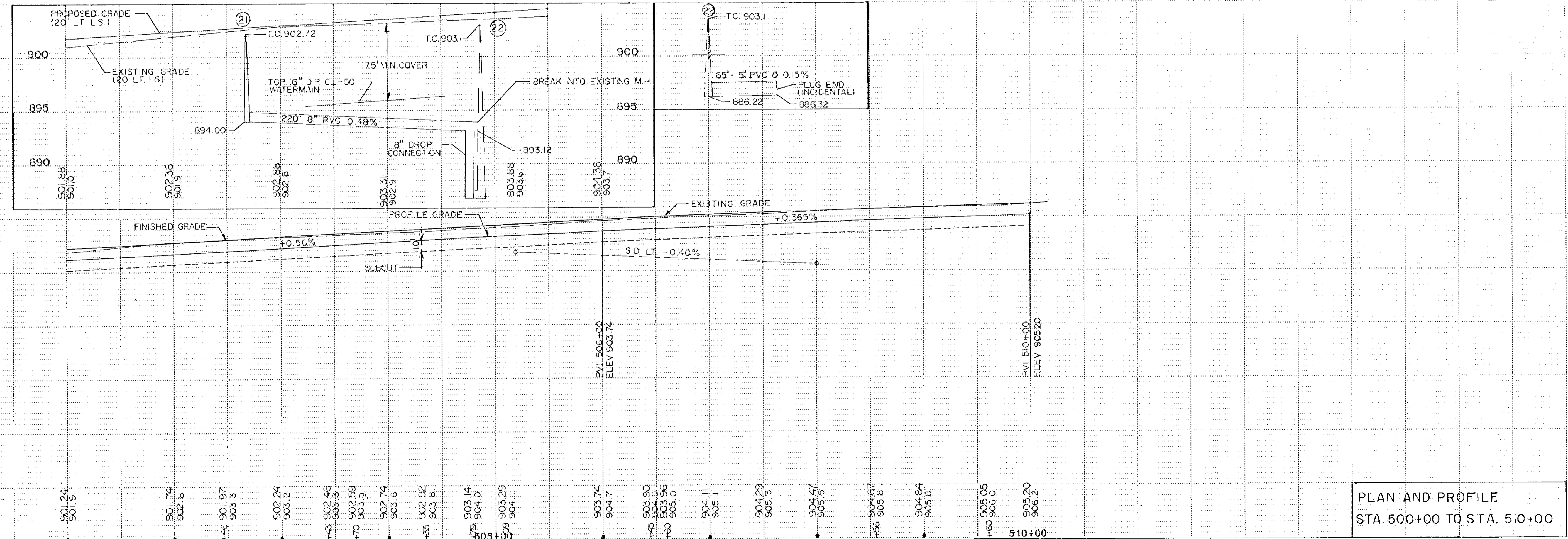
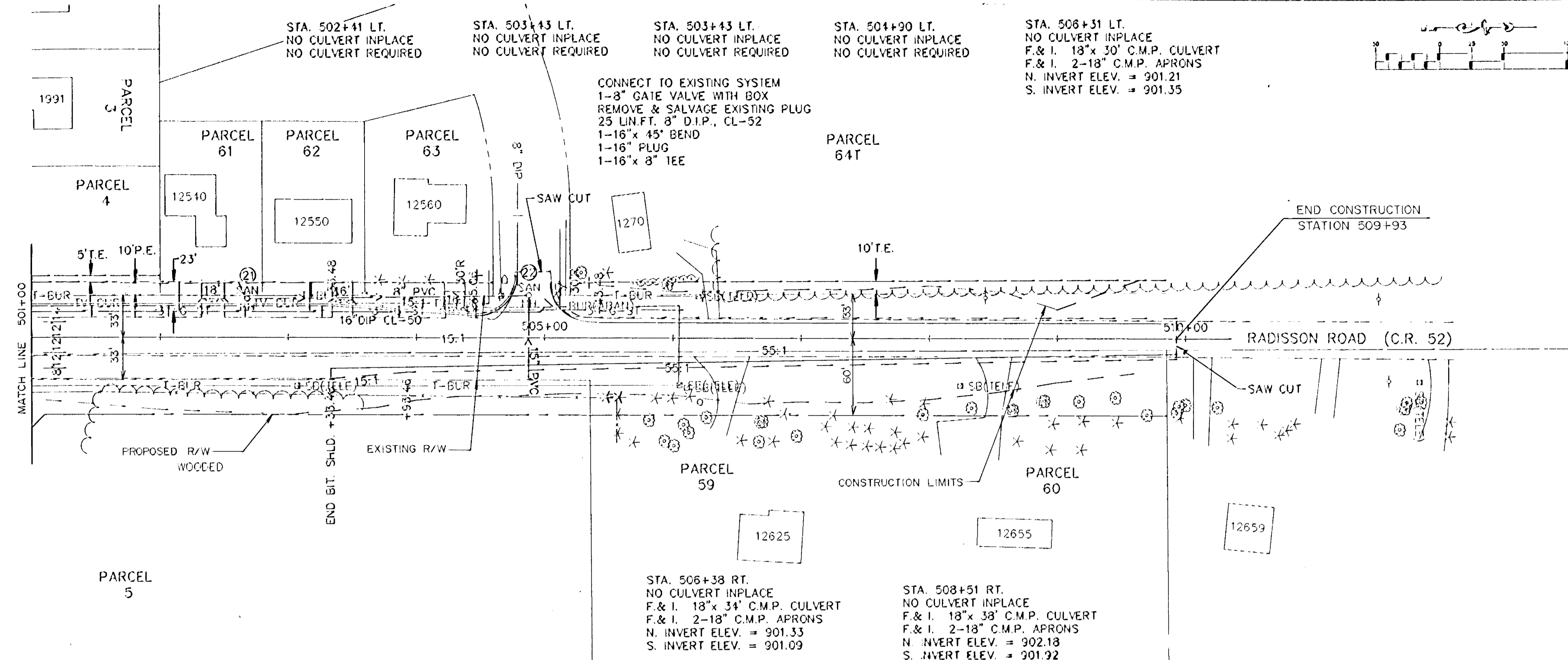
LWBPI = 197+97.59
D = 0° 30'
Δ = 01°41'33"
R = 11459.16'
T = 169.25'
L = 338.48'

PT = 199+66.82
A PT. 12' LT & L
TO C STA. 199+66.72

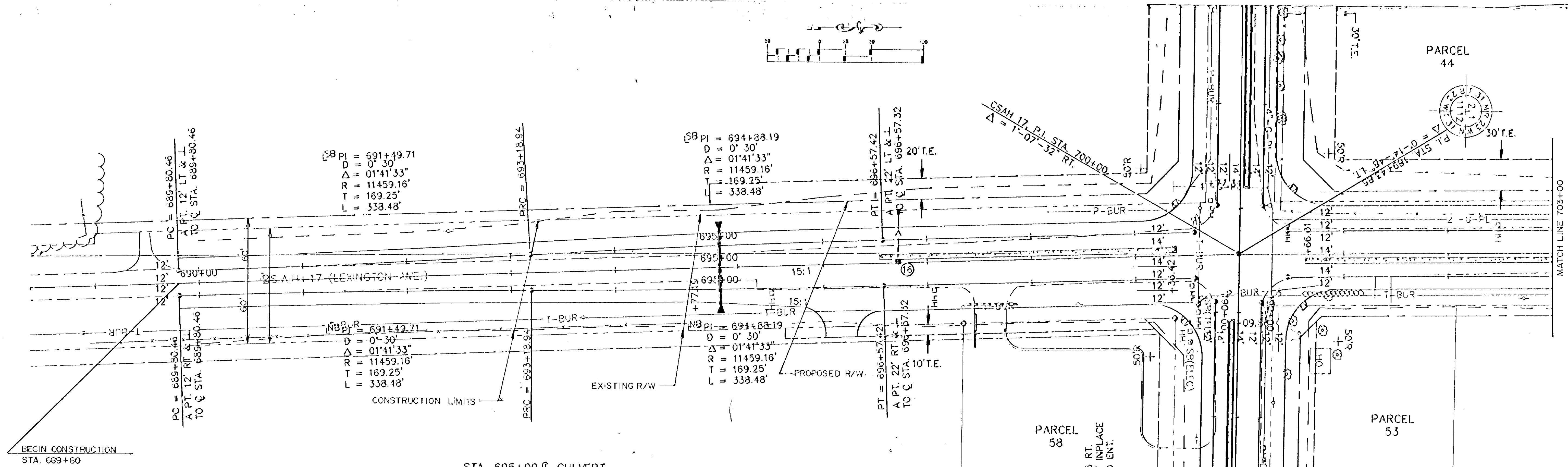
END S.A.P. 02-614-14
STA. 199+67



PLAN AND PROFILE
STA. 195+00 TO STA. 204+00



PLAN AND PROFILE
 STA. 500+00 TO STA. 510+00

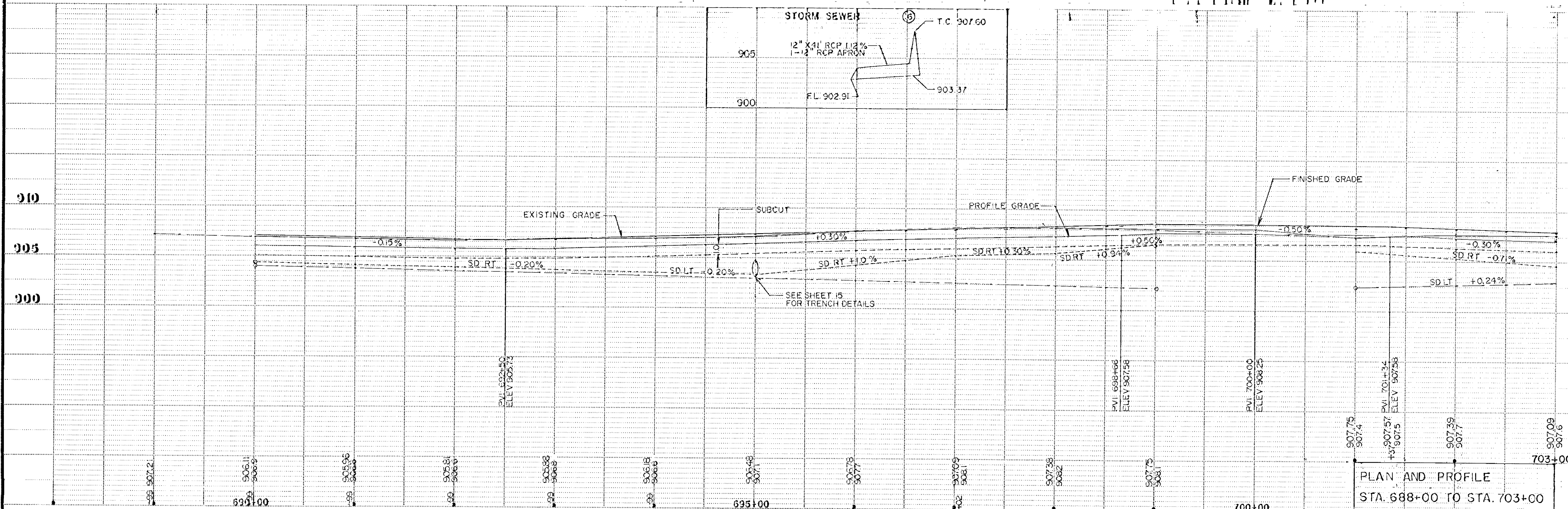


STA. 695+00 @ CULVERT
 NO CULVERT INPLACE
 F. & I. 28" SPAN x 86' R.C.P. ARCH CULV.
 F. & I. 2-28" SPAN R.C.P. ARCH APRONS
 INVERT ELEV. LT. = 903.00
 INVERT ELEV. RT. = 903.50

STA. 696+16 RT.
 NO CULVERT INPLACE
 F. & I. 15' x 52' C.M.P. CULVERT
 F. & I. 46'-15" C.M.P. APRONS
 N. INVERT ELEV. = 904.77
 S. INVERT ELEV. = 903.53

STA. 697+61 RT.
 INPL. 22' x 48' C.M.P. + APRONS
 REMOVE
 OBLITERATE ENT.

STA. 701+37 RT.
 NO CULVERT INPLACE
 OBLITERATED ENT.

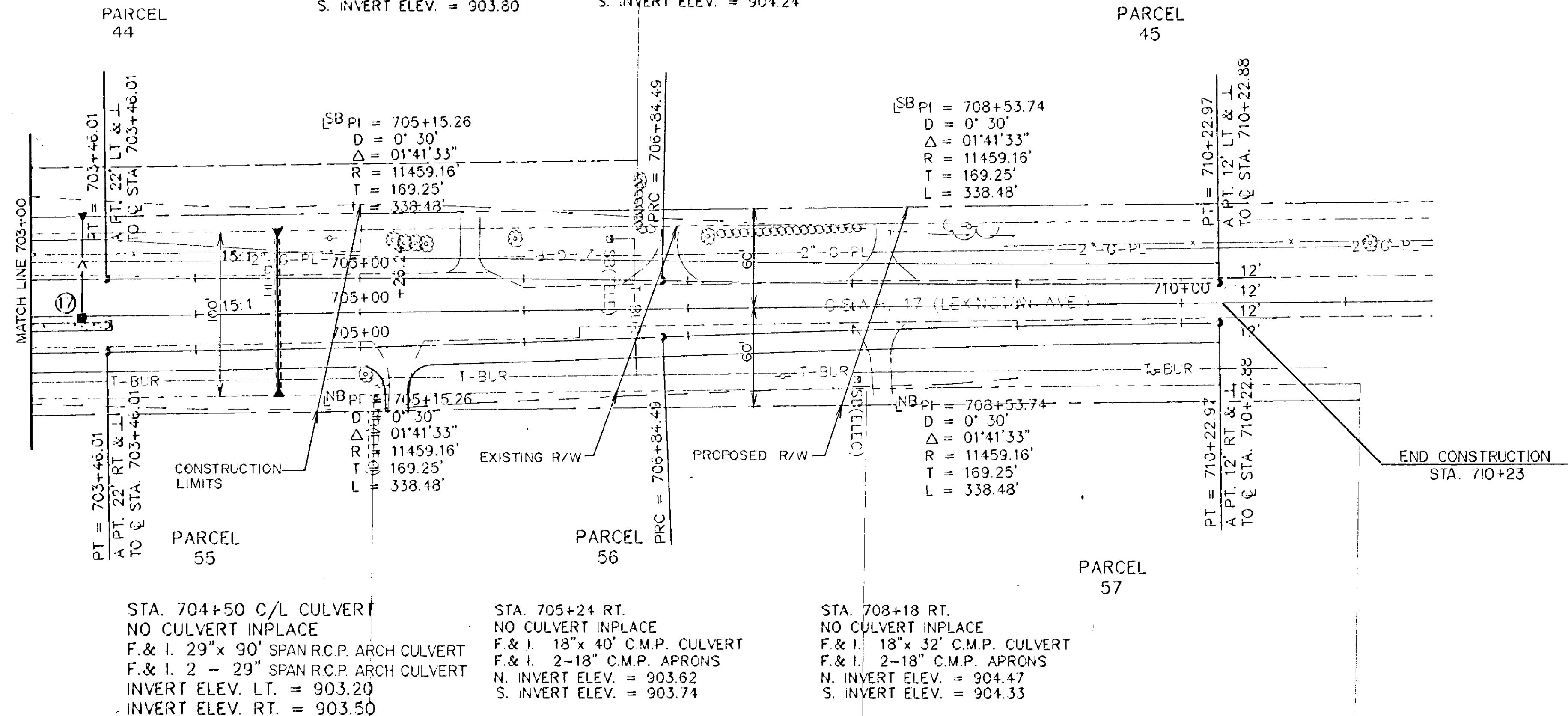
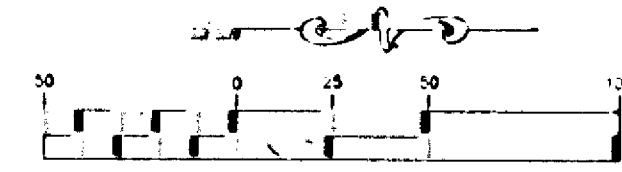


PLAN AND PROFILE
 STA. 688+00 TO STA. 703+00

STA. 705+67 LT.
NO CULVERT INPLACE
F.& I. 18" x 34' C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
N. INVERT ELEV. = 903.56
S. INVERT ELEV. = 903.80

STA. 706+88 LT.
NO CULVERT INPLACE
F.& I. 18" x 34' C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
N. INVERT ELEV. = 904.08
S. INVERT ELEV. = 904.24

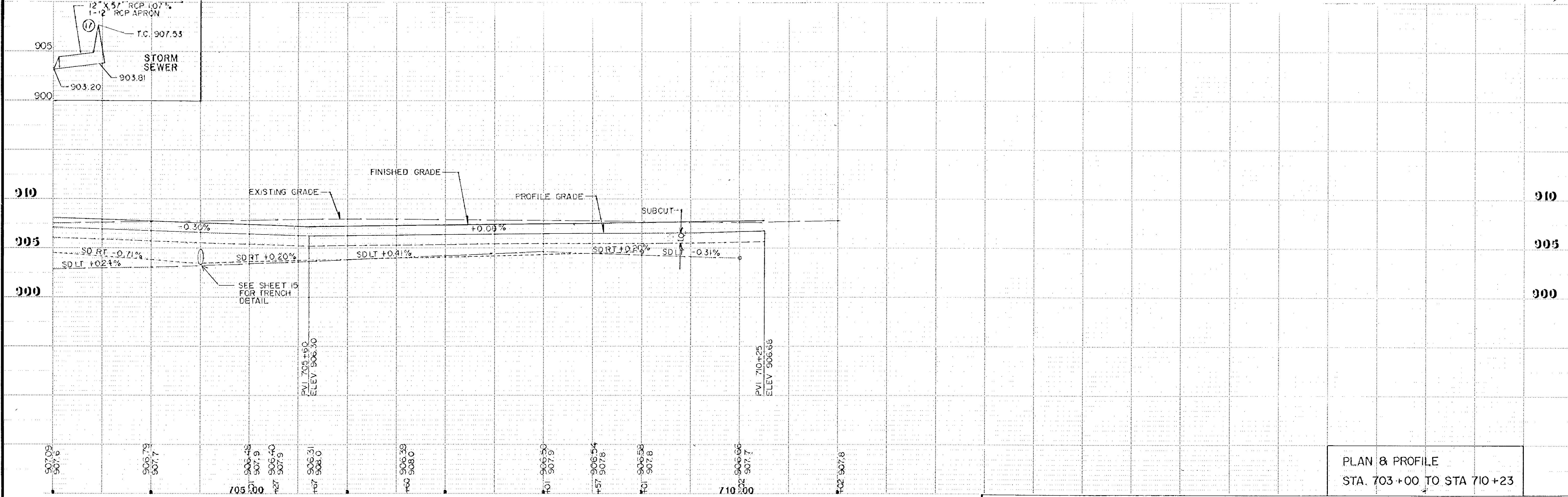
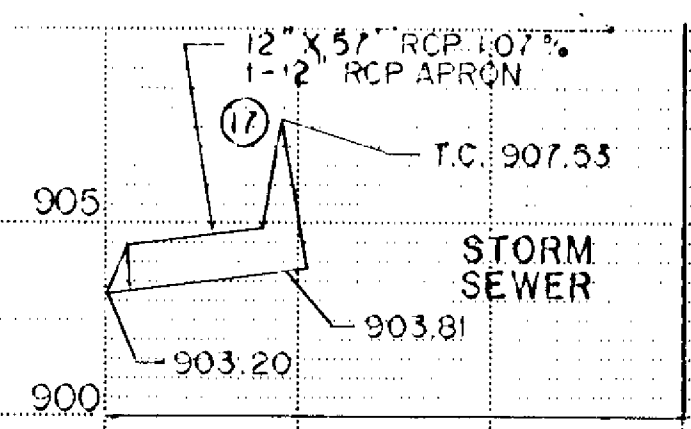
STA. 708+18 LT.
NO CULVERT INPLACE
NO CULVERT REQUIRED



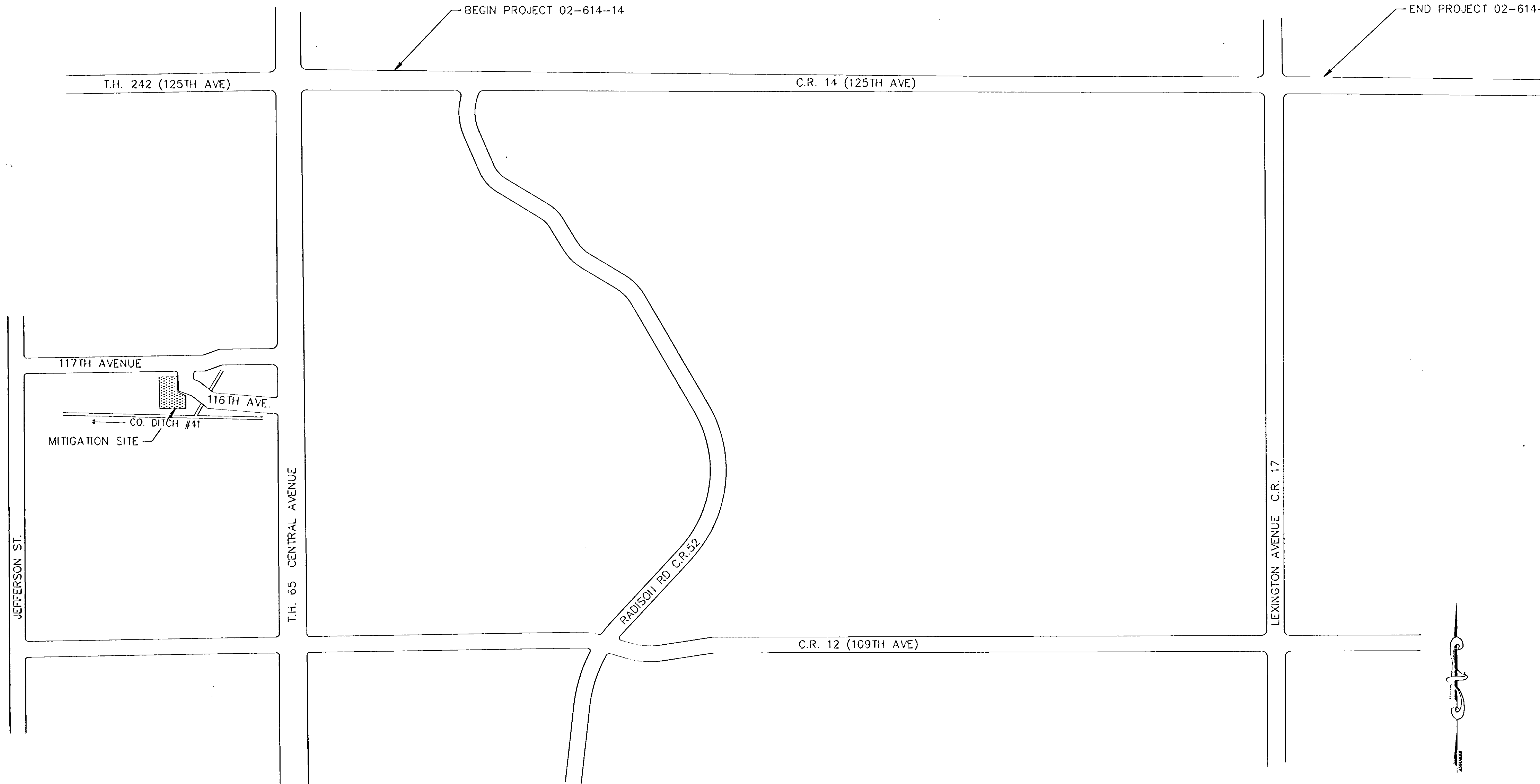
STA. 704+50 C/L CULVERT
NO CULVERT INPLACE
F.& I. 29" x 90' SPAN R.C.P. ARCH CULVERT
F.& I. 2 - 29" SPAN R.C.P. ARCH CULVERT
INVERT ELEV. LT. = 903.20
INVERT ELEV. RT. = 903.50

STA. 705+24 RT.
NO CULVERT INPLACE
F.& I. 18" x 40' C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
N. INVERT ELEV. = 903.62
S. INVERT ELEV. = 903.74

STA. 708+18 RT.
NO CULVERT INPLACE
F.& I. 18" x 32' C.M.P. CULVERT
F.& I. 2-18" C.M.P. APRONS
N. INVERT ELEV. = 904.47
S. INVERT ELEV. = 904.33



PLAN & PROFILE
STA. 703+00 TO STA 710+23



BEGIN PROJECT 02-614-14

END PROJECT 02-614-14

T.H. 242 (125TH AVE)

C.R. 14 (125TH AVE)

117TH AVENUE

116TH AVE.

CO. DITCH #41

MITIGATION SITE

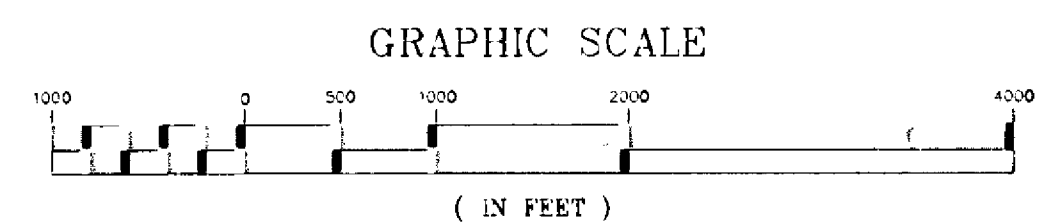
JEFFERSON ST.

T.H. 65 CENTRAL AVENUE

RADISON RD C.R. 52

C.R. 12 (109TH AVE)

LEXINGTON AVENUE C.R. 17



117TH AVENUE MITIGATION SITE
GENERAL LOCATION MAP

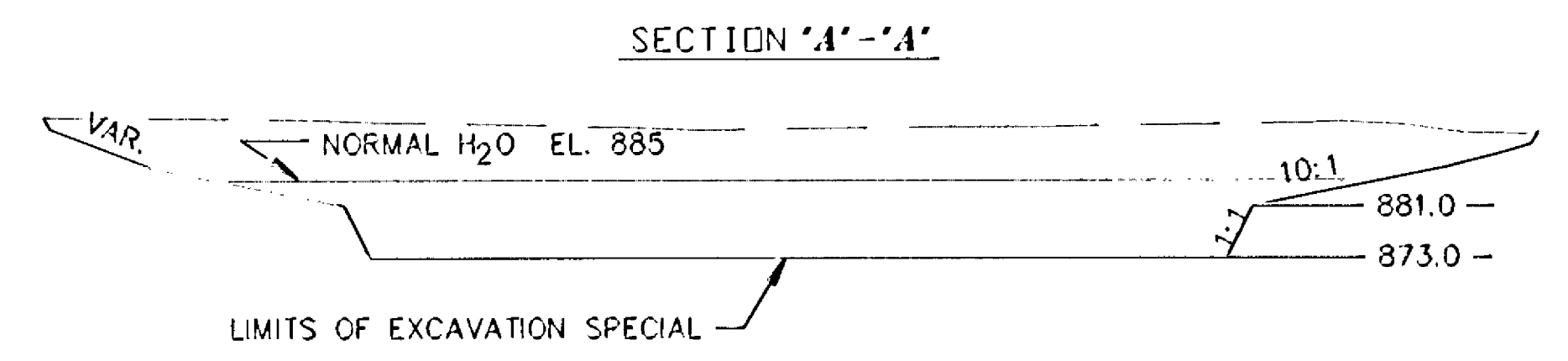
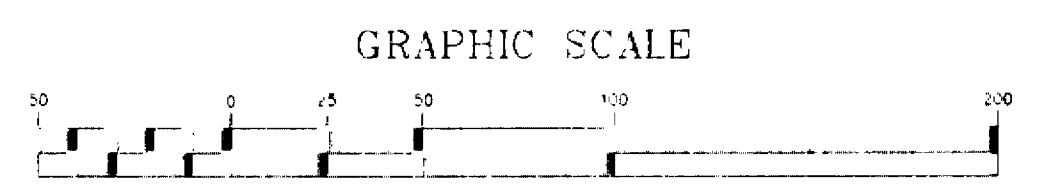
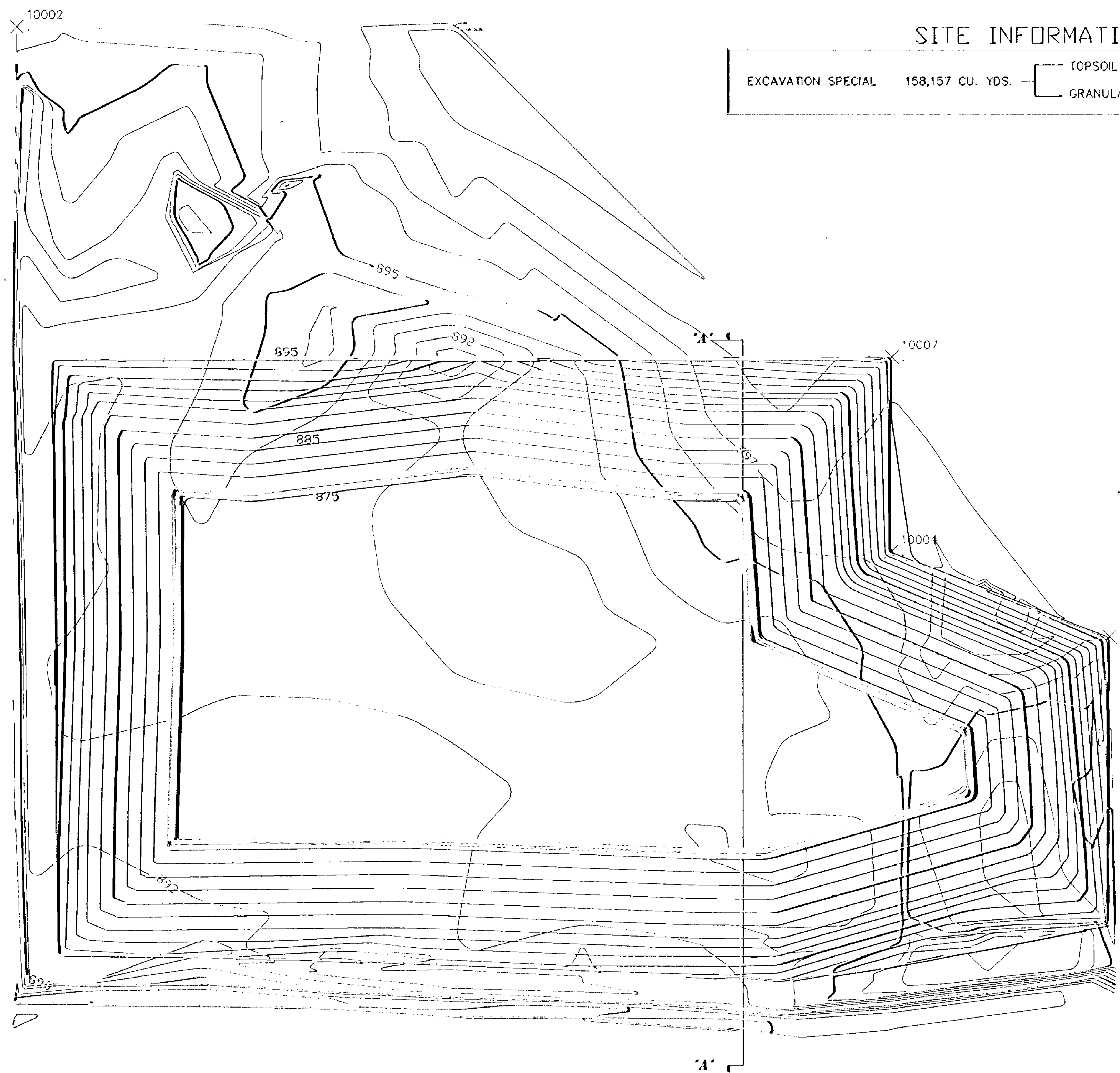
REVISIONS			
DATE	BY	DATE	BY

S.A.P. 02-614-14 S.P. _____ C.P. _____

Sheet No. 31 of 117 Sheets

SITE INFORMATION

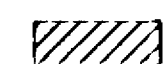
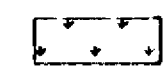



EXCAVATION SPECIAL	158,157 CU. YDS.	TOPSOIL	12,362 CU. YDS.
		GRANULAR	145,795 CU. YDS.

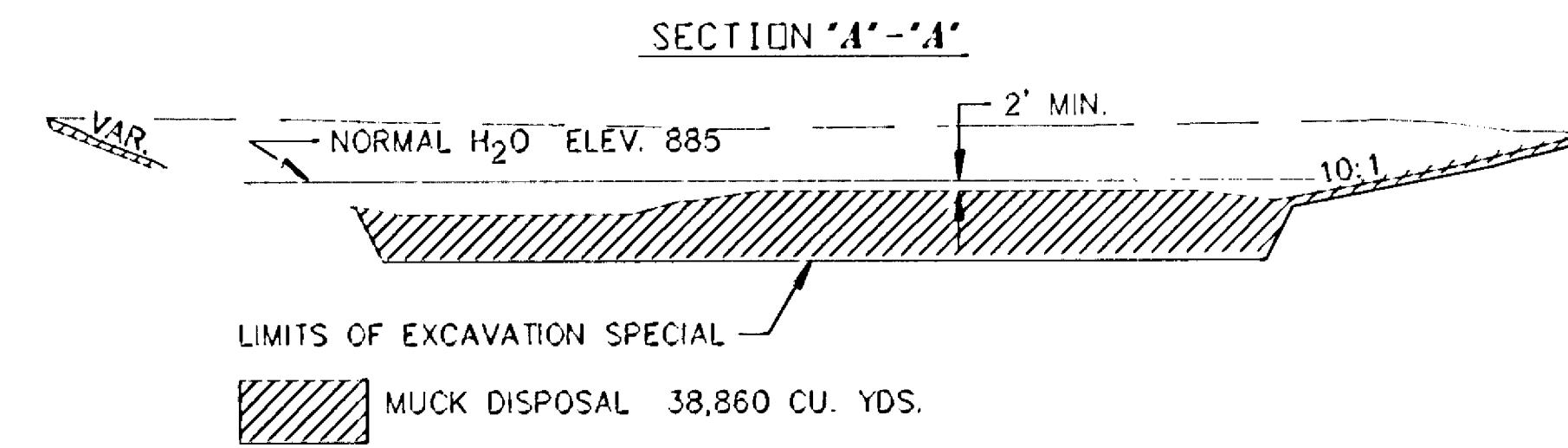
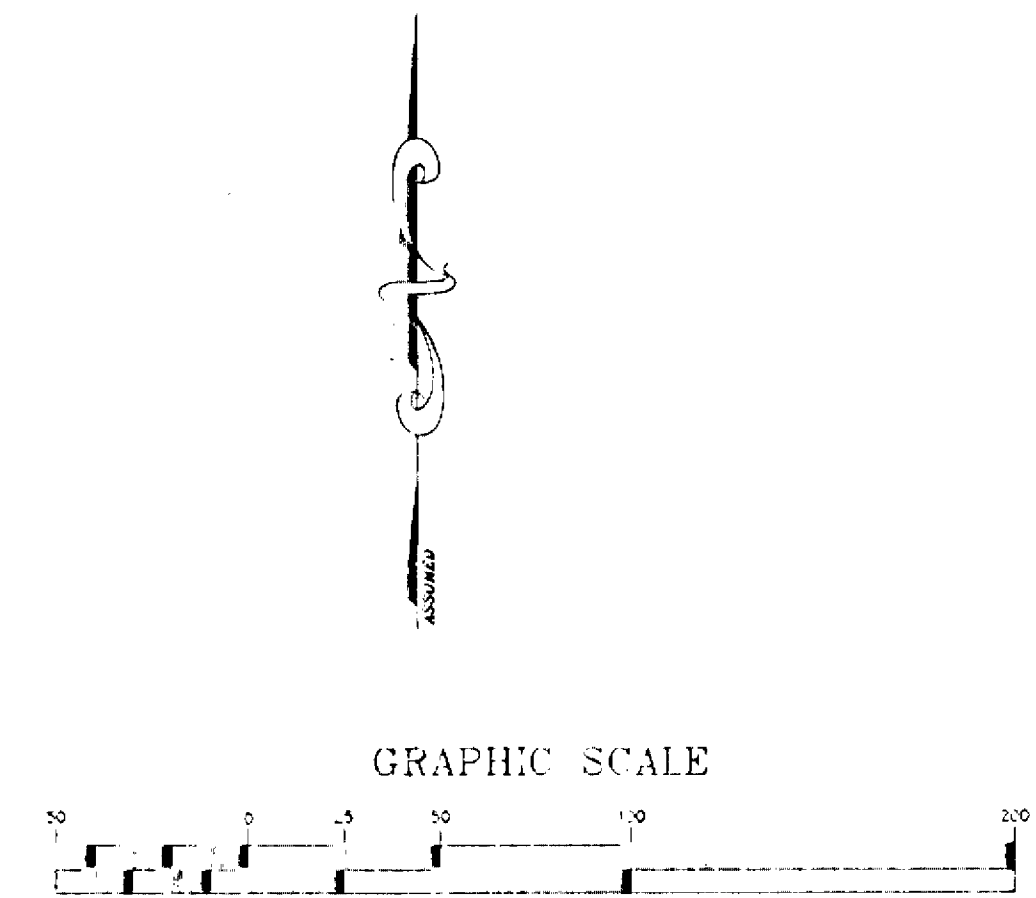
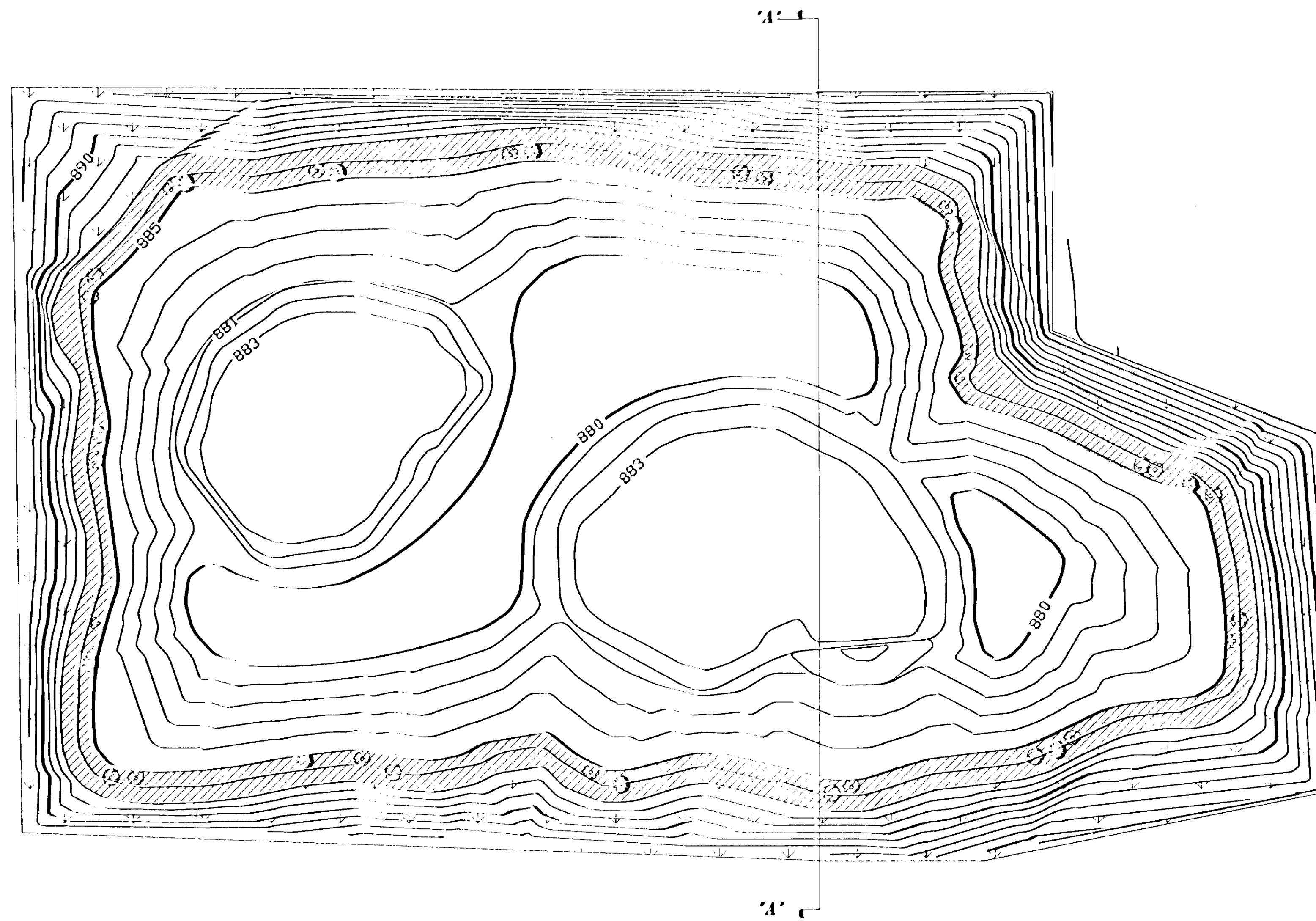


117TH AVENUE MITIGATION SITE
OVER-EXCAVATED
PLAN AND TYPICAL SECTION

REVISIONS			
DATE	BY	DATE	BY

LEGEND:

-  SEED MIXTURE, SPECIAL 1
-  SEED MIXTURE, SPECIAL 2
-  PUSSY WILLOW (24")
-  RED-OSIER DOGWOOD (24")
-  SPECKLED ALDER OR WINTERBERRY (24")

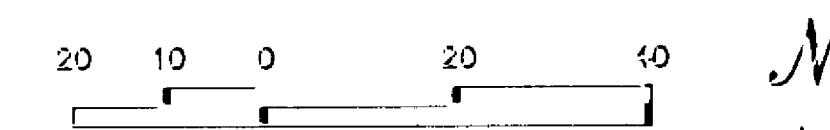


117TH AVENUE MITIGATION SITE
FINISHED LANDSCAPING
PLAN AND TYPICAL SECTION

REVISIONS			
DATE	BY	DATE	BY

S.A.P. 02-614-14 S.P. _____ C.P. _____

Sheet No. 33 of 117 Sheets



NOTES:

- 1) LOCATION OF CONTROLLER CABINET, SERVICE CABINET LOOP DETECTORS, POLE BASES AND HANDHOLES SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) EACH SIGNAL FACE SHALL BE 12"-3 SECTION R-Y-G, EXCEPT THAT SIGNAL FACES (5-1) AND (5-2) SHALL BE 12"-3 SECTION RLTA-YLTA-GLTA.
- 3) SIGNAL SYSTEM FLASH MODE SHALL BE ALL RED.
- 4) EACH SIGNAL FACE SHALL HAVE BACKGROUND SHIELD.
- 5) EACH PEDESTRIAN INDICATION SHALL BE 12"x12".
- 6) EACH LUMINAIRE SHALL INCLUDE PHOTOELECTRIC CELL AND STREET LIGHT CHECK SWITCH.
- 7) SEE SPECIAL PROVISIONS AND DETAILS FOR ANOKA COUNTY SERVICE CABINET INFORMATION.
- 8) SEE SPECIAL PROVISIONS FOR COUNTY FURNISHED MATERIALS.
- 9) LOOP DETECTOR WIRES SHALL BE CROSS-LINKED POLYETHYLENE (XLP) IN 1"N.M.C. SEE SPECIAL PROVISIONS AND DETAILS.
- 10) EACH HANDHOLE SHALL BE CONCRETE HANDHOLE WITH TYPE "C" COVER PER Mn/DOT STANDARD PLATE NO. 8117F.
- 11) SEE SPECIAL PROVISIONS AND DETAILS REGARDING TYPE "D" SIGN PANELS TO BE FURNISHED AND INSTALLED BY CONTRACTOR (INCIDENTAL TO ITEM NO. 2565.511).
- 12) Δ = LOOP DETECTORS TO BE FURNISHED AND INSTALLED, BUT NOT MADE OPERATIONAL. SEE SPECIAL PROVISIONS.
- 13) ----- = FUTURE ROADWAY CONSTRUCTION/RESTRIPIPING. COORDINATE PLACEMENT OF ALL SIGNAL SYSTEM MATERIALS IN ORDER TO ACCOMMODATE FUTURE CONSTRUCTION.
- 14) SEE SPECIAL PROVISIONS REGARDING ADDITIONAL SIGNAL MATERIALS TO BE FURNISHED BY THE CONTRACTOR (FOR OTHERS TO INSTALL).
- 15) SEE SPECIAL PROVISIONS REGARDING ADDITIONAL TRAFFIC SIGNAL CABLES AND CONDUCTORS TO BE FURNISHED AND INSTALLED BY CONTRACTOR.

BASE	OVERLAY	DRG. NO.

MATCH LINE - STATION 40+75 - SEE SHEET 35

MATCH LINE - STATION 47+00 - SEE SHEET 36

LOOP DETECTORS			
NUMBER	SIZE (FT.)	LOCATION	FUNCTION
Δ D1-1	MULTIPLE	5'	1
D2-1	6x6	405'	1
Δ D3-1	2-6x6	5'	1
D4-1	6x6	300'	3,8
D4-2	2-6x6	5'	7
D4-3	2-6x6	5'	1
D5-1	MULTIPLE	5'	1
D6-1	2-6x6	405'	1
Δ D7-1	MULTIPLE	5'	1
Δ D8-1	-	FUTURE	3,8
Δ D8-2	2-6x6	5'	7
Δ D8-3	2-6x6	5'	1

FUNCTIONS:

- 1) CALL AND EXTEND
- 2) CALL ONLY
- 3) EXTEND ONLY
- 4) CALL ONLY DENSITY
- 5) DELAYED CALL ONLY
- 6) DELAYED CALL ONLY DENSITY
- 7) DELAYED CALL-IMMEDIATE EXTEND
- 8) CARRY OVER (STRETCH)
- 9) ADVISORY DETECTOR
- 10) SAMPLING DETECTOR
- 11) SPECIAL DETECTOR

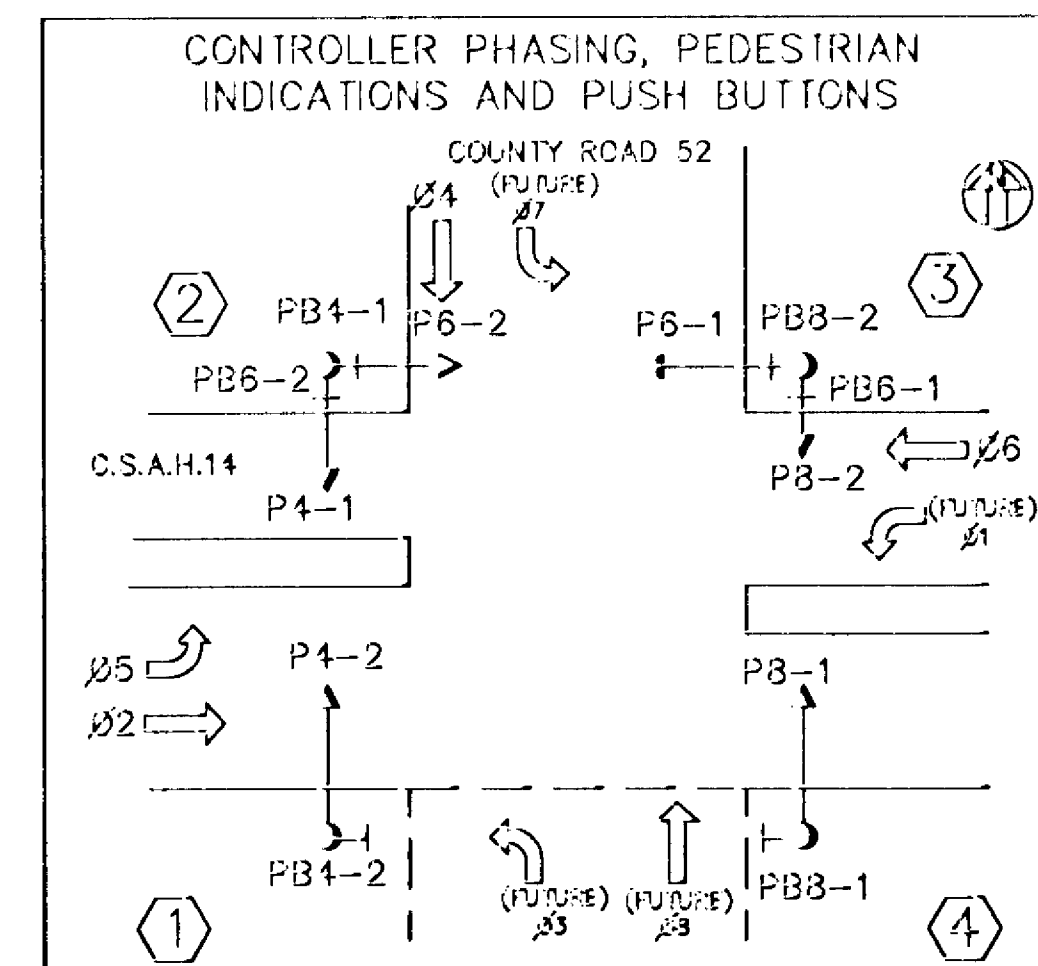
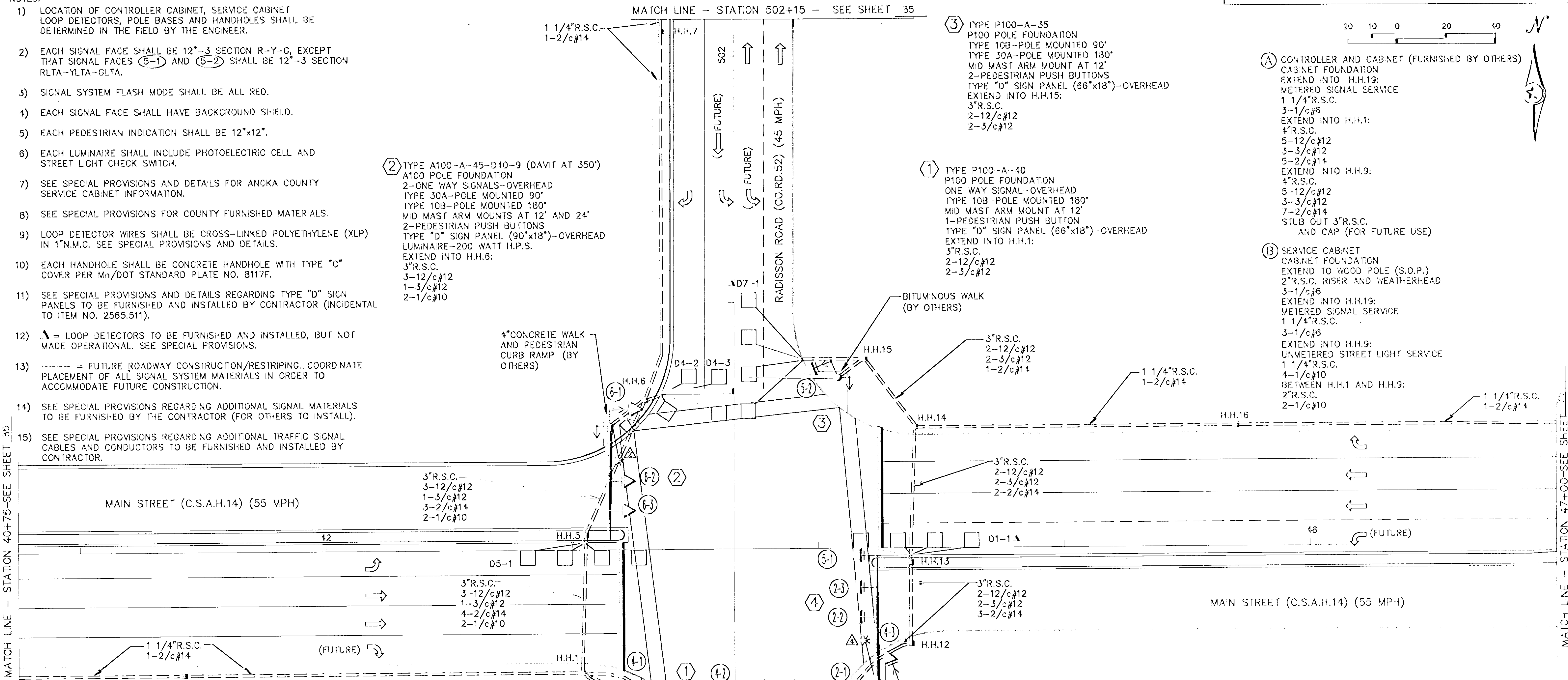
NOTE: LOCATION = DISTANCE FROM STOP BAR TO LOOP DETECTOR.

② TYPE A100-A-45-D40-9 (DAVIT AT 350')
 A100 POLE FOUNDATION
 2-ONE WAY SIGNALS-OVERHEAD
 TYPE 30A-POLE MOUNTED 90°
 TYPE 10B-POLE MOUNTED 180°
 MID MAST ARM MOUNTS AT 12' AND 24'
 2-PEDESTRIAN PUSH BUTTONS
 TYPE "D" SIGN PANEL (90"x18")-OVERHEAD
 LUMINAIRE-200 WATT H.P.S.
 EXTEND INTO H.H.6:
 3"R.S.C.
 3-12/c#12
 1-3/c#12
 2-1/c#10

③ TYPE P100-A-35
 P100 POLE FOUNDATION
 TYPE 10B-POLE MOUNTED 90°
 TYPE 30A-POLE MOUNTED 180°
 MID MAST ARM MOUNT AT 12'
 2-PEDESTRIAN PUSH BUTTONS
 TYPE "D" SIGN PANEL (66"x18")-OVERHEAD
 EXTEND INTO H.H.15:
 3"R.S.C.
 2-12/c#12
 2-3/c#12

① TYPE P100-A-40
 P100 POLE FOUNDATION
 ONE WAY SIGNAL-OVERHEAD
 TYPE 10B-POLE MOUNTED 180°
 MID MAST ARM MOUNT AT 12'
 1-PEDESTRIAN PUSH BUTTON
 TYPE "D" SIGN PANEL (66"x18")-OVERHEAD
 EXTEND INTO H.H.1:
 3"R.S.C.
 2-12/c#12
 2-3/c#12

④ TYPE A100-A-50-D40-9 (DAVIT AT 350')
 A100 POLE FOUNDATION
 3-ONE WAY SIGNALS-OVERHEAD
 TYPE 10B-POLE MOUNTED 90°
 TYPE 10A-POLE MOUNTED 180°
 MID MAST ARM MOUNTS AT 13' AND 25'
 1-PEDESTRIAN PUSH BUTTON
 TYPE "D" SIGN PANEL (90"x18")-OVERHEAD
 LUMINAIRE-200 WATT H.P.S.
 EXTEND INTO H.H.11:
 3"R.S.C.
 3-12/c#12
 1-3/c#12
 2-1/c#10



NO.	BY	DATE	REVISIONS

"ELECTRICAL ENGINEER CERTIFICATION"
 I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
Robert A. Elber
 Date: 7-2-92 Reg. No. 5859

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
 Date: 7-2-92 Reg. No. 20943

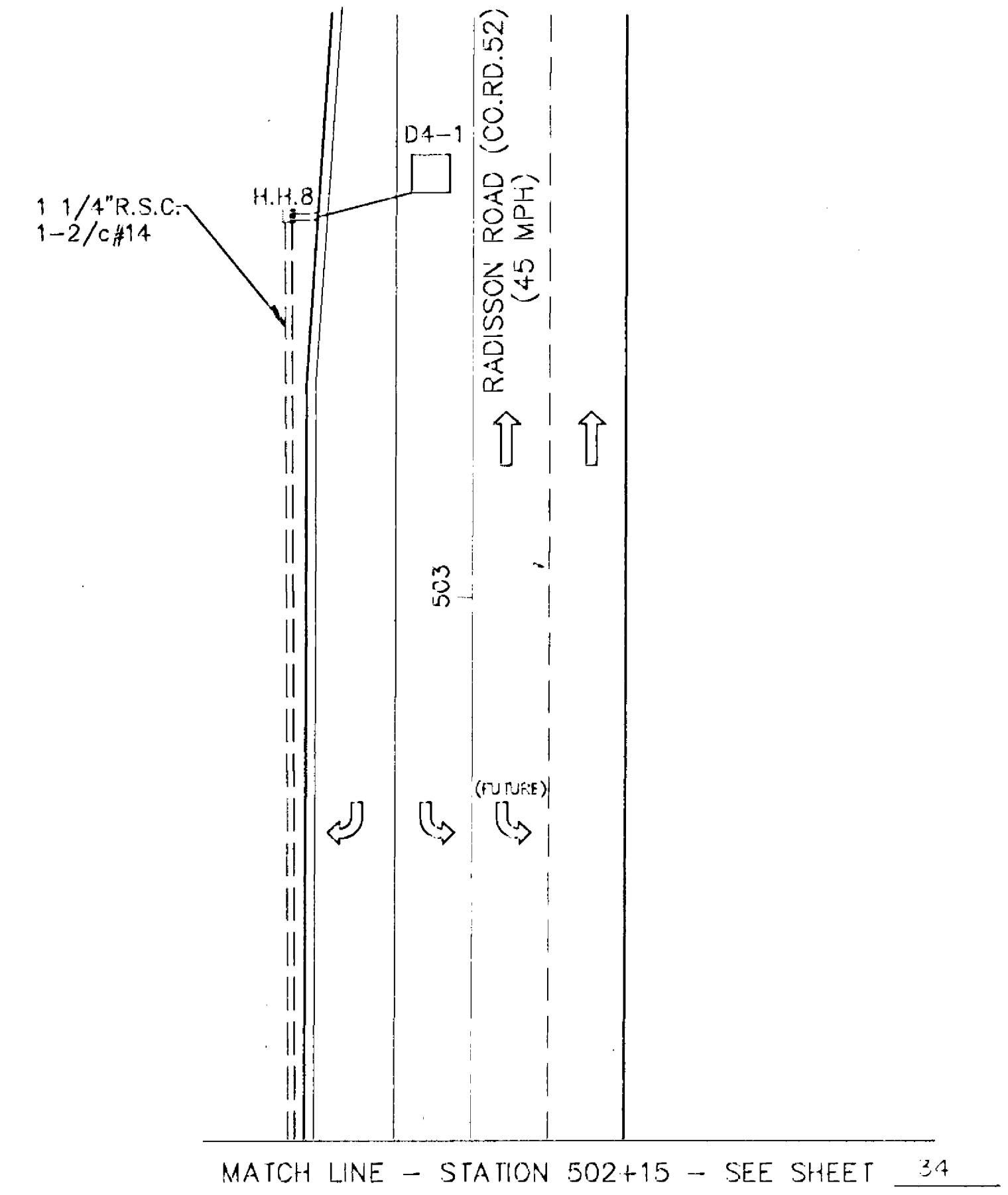
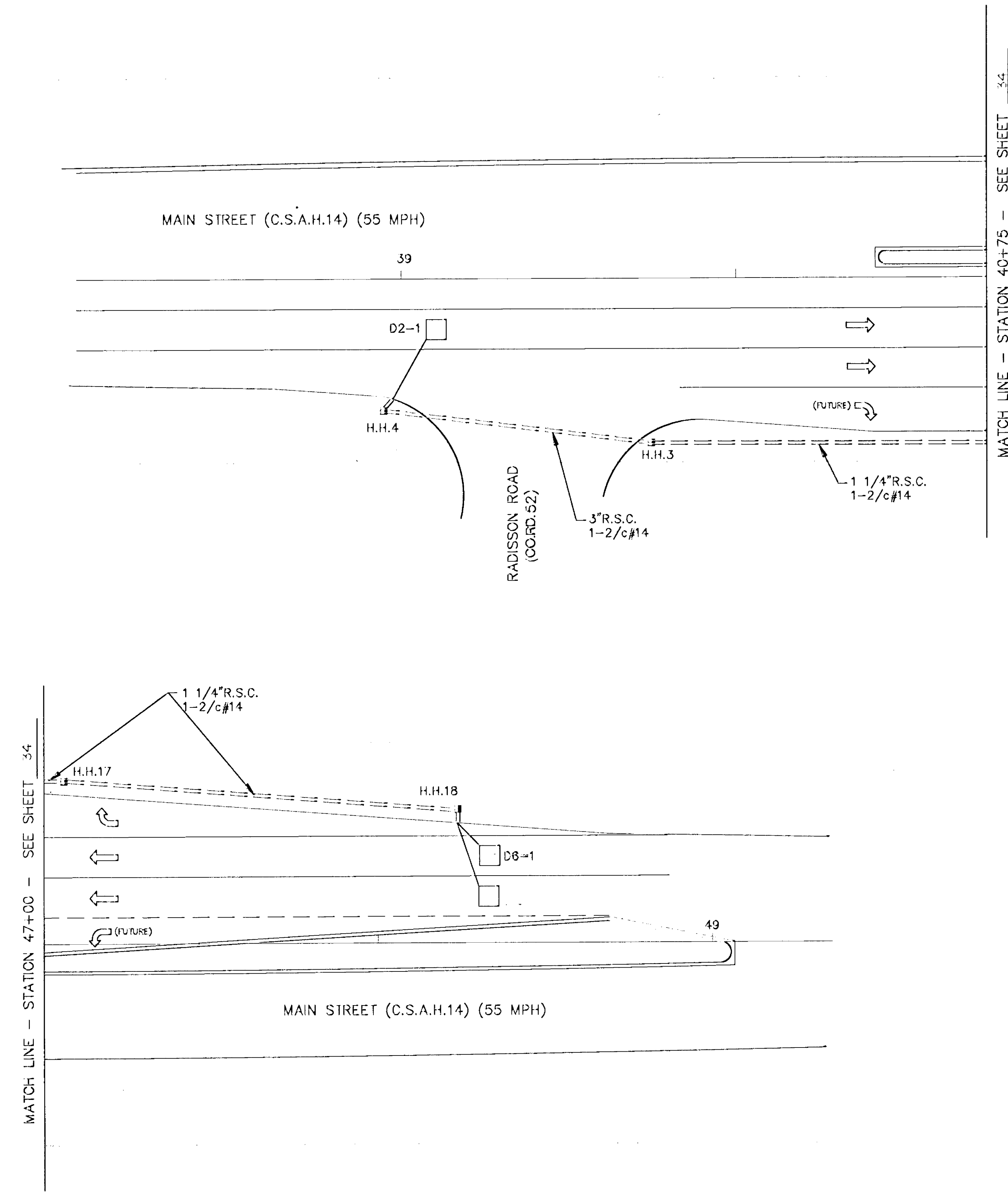
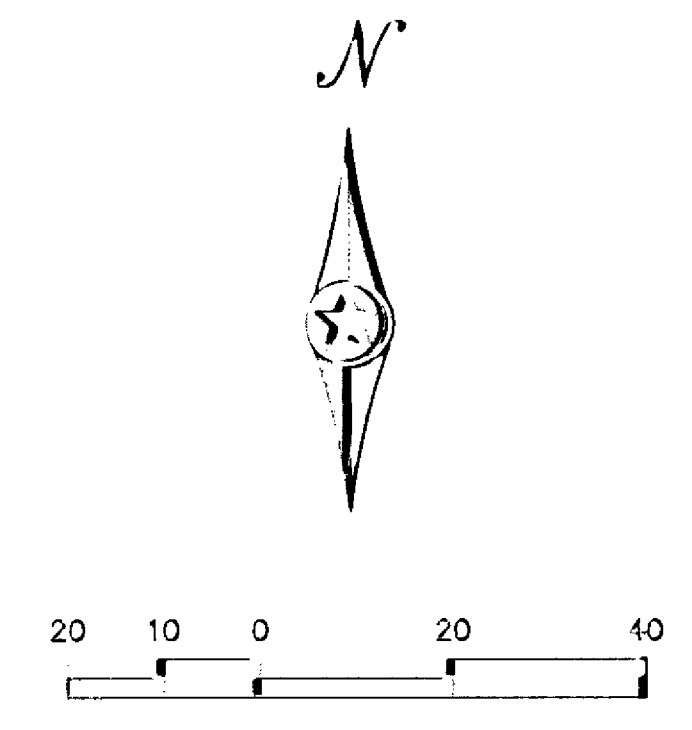
SEA
 ENGINEERS ARCHITECTS P.L.L.C.
 S.A.P. 02-614-14

ANOKA COUNTY, MINNESOTA
 CITY OF BLAINE
 S.P. _____ C.P. _____

TRAFFIC SIGNAL SYSTEM 'A'
 INTERSECTION LAYOUT
 MAIN STREET (CSAH 14) AT RADISSON ROAD (CO.RD.52)
 Sheet No. 34 of 117 Sheets
 FILE NO. 92219
 DATE _____

BASE OVERLAY/DWG. NO.

Fed. Project No. _____



NO.	BY	DATE	REVISIONS

"ELECTRICAL ENGINEER CERTIFICATION"
 I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
Robert A. ...
 Date: 7-2-72 Reg. No. 20943

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
...
 Date: 2-12-1972 Reg. No. 20943

ES&P
 ENGINEERS ARCHITECTS PLANNERS

ANOKA COUNTY, MINNESOTA
 CITY OF BLAINE

TRAFFIC SIGNAL SYSTEM 'A'
 INTERSECTION LAYOUT
 MAIN STREET (CSAH 14) AT RADISSON ROAD (CO.RD.52)

FILE NO. 92219
DATE

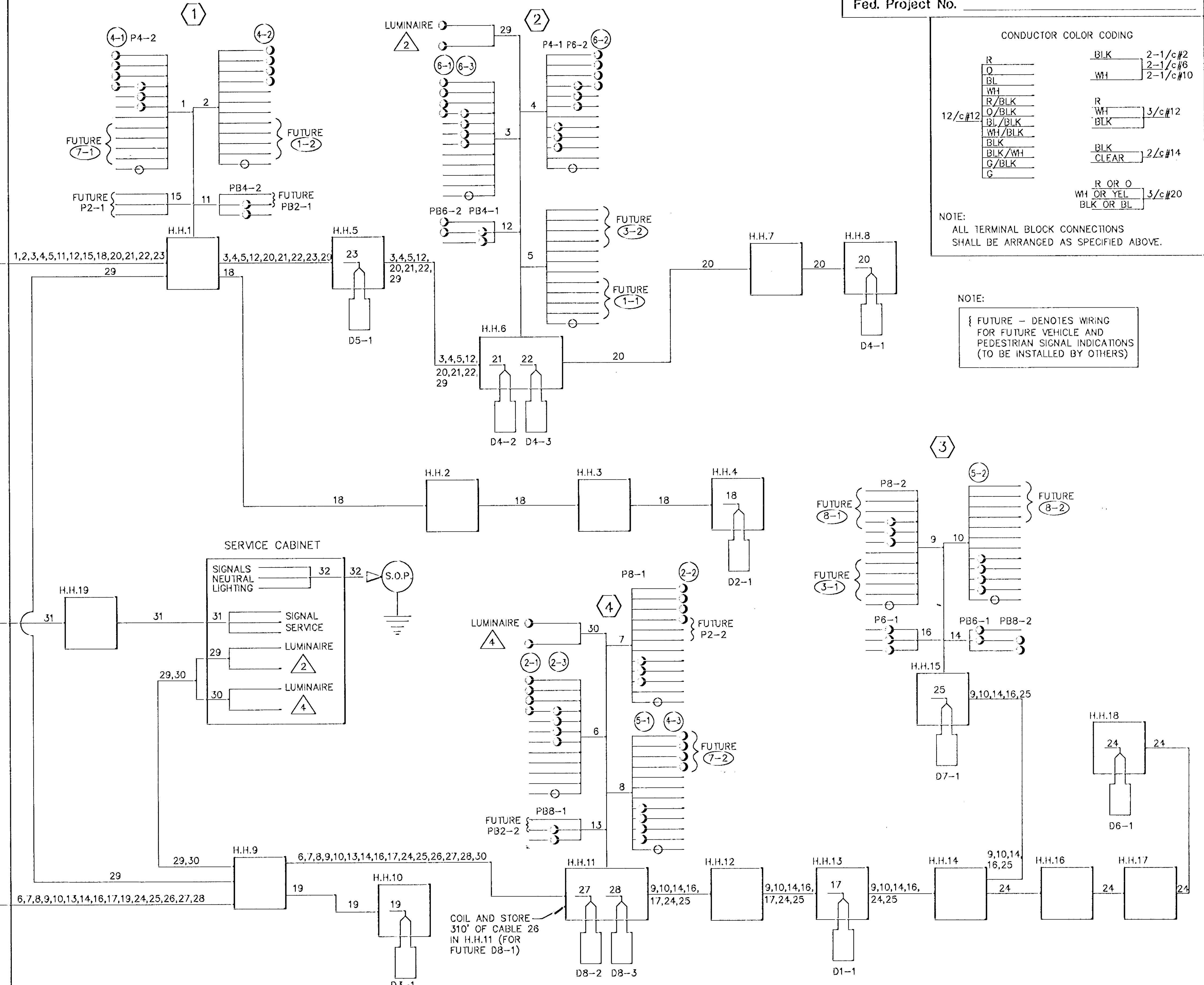
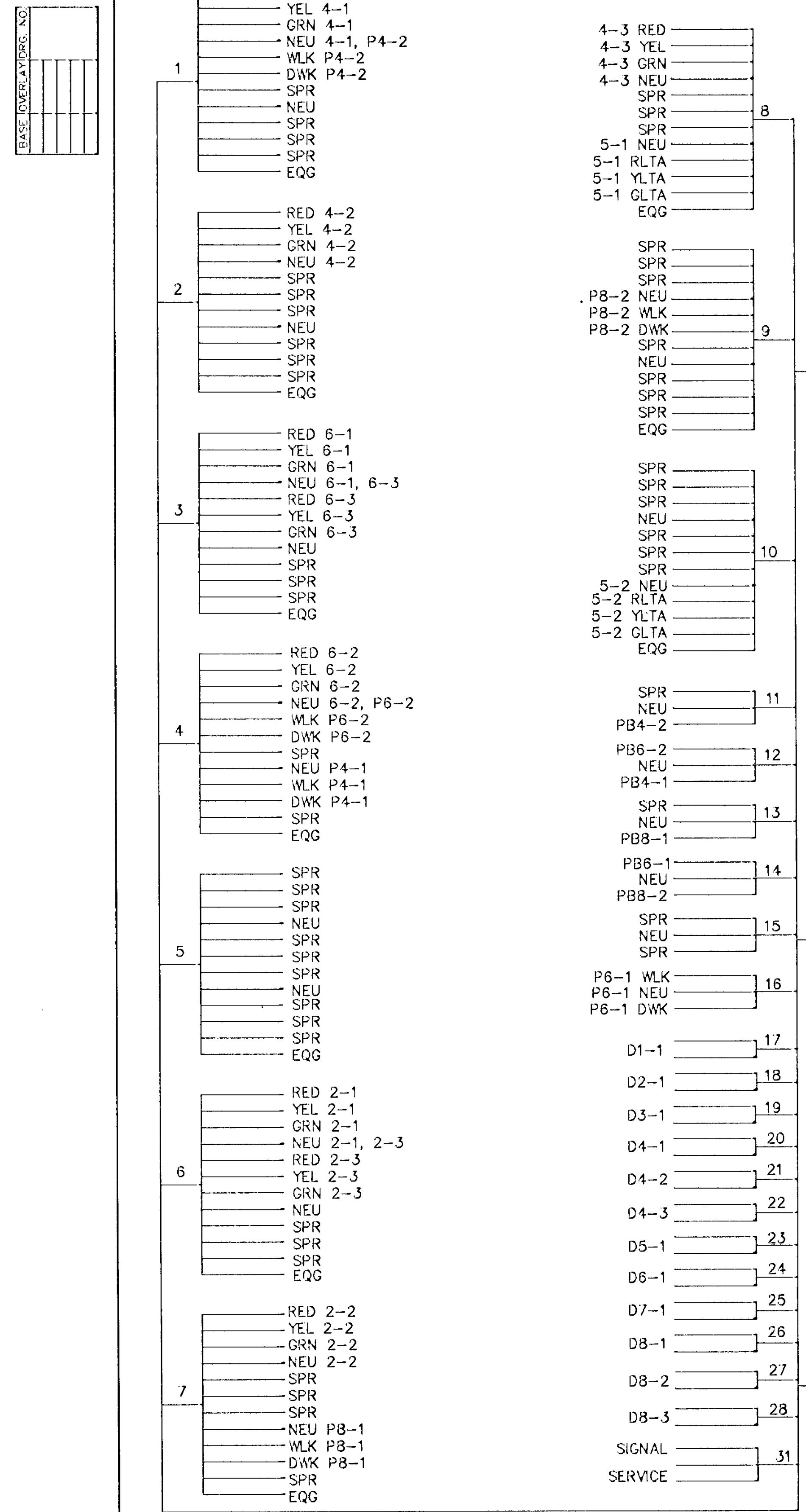
CONDUCTOR COLOR CODING

R	BLK	2-1/c#2
O	WH	2-1/c#6
BL	WH	2-1/c#10
WH	R	3/c#12
R/BLK	WH	3/c#12
O/BLK	BLK	3/c#12
BL/BLK	BLK	3/c#12
WH/BLK	BLK	3/c#12
BLK	BLK	2/c#14
BLK/WH	CLEAR	2/c#14
G/BLK	R OR O	3/c#20
G	WH OR YEL	3/c#20
	BLK OR BL	3/c#20

NOTE:
ALL TERMINAL BLOCK CONNECTIONS SHALL BE ARRANGED AS SPECIFIED ABOVE.

NOTE:
{ FUTURE - DENOTES WIRING FOR FUTURE VEHICLE AND PEDESTRIAN SIGNAL INDICATIONS (TO BE INSTALLED BY OTHERS)

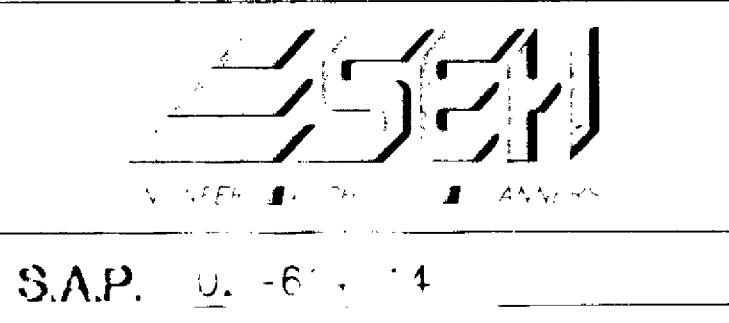
CONTROLLER AND CABINET



NO.	BY	DATE	REVISIONS

"ELECTRICAL ENGINEER CERTIFICATION"
I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
Date: 7-2-12 Re: 5859

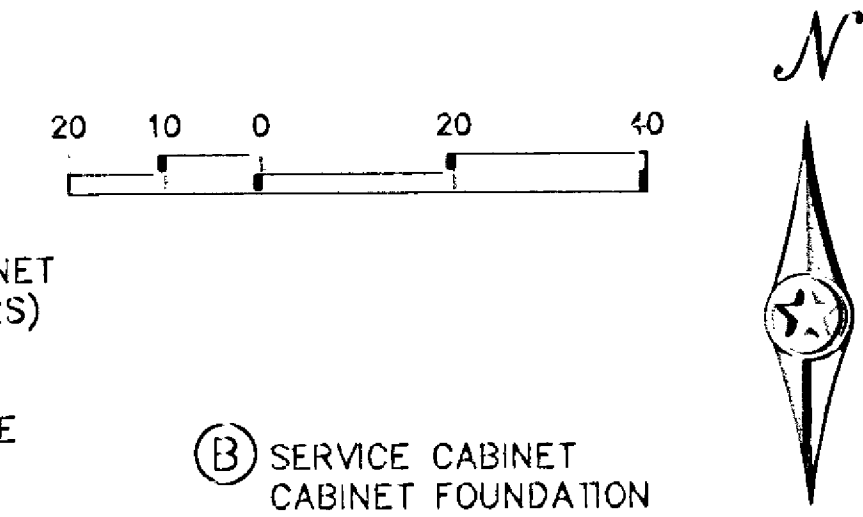
I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
Date: 7-2-12 Reg. No. 20943



ANOKA COUNTY, MINNESOTA
CITY OF BLAINE

TRAFFIC SIGNAL SYSTEM "A"
FIELD WIRING DIAGRAM
MAIN STREET (CSAH 14) AT RADISSON ROAD (CO. RD. 52)

FILE NO. 92219
DATE



NOTES:

- 1) LOCATION OF CONTROLLER CABINET, SERVICE CABINET, LOOP DETECTORS, POLE BASES AND HANDHOLES SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) EACH SIGNAL FACE SHALL BE 12"-3 SECTION R-Y-G, EXCEPT THAT SIGNAL FACES (1-1), (1-2), (3-1), (3-2), (5-1), (5-2), (7-1) AND (7-2) SHALL BE 12"-3 SECTION RLTA-YLTA-GLTA.
- 3) SIGNAL SYSTEM FLASH MODE SHALL BE ALL RED.
- 4) EACH SIGNAL FACE SHALL HAVE BACKGROUND SHIELD.
- 5) EACH PEDESTRIAN INDICATION SHALL BE 12"x12".
- 6) EACH LUMINAIRE SHALL INCLUDE PHOTOELECTRIC CELL AND STREET LIGHT CHECK SWITCH.
- 7) SEE SPECIAL PROVISIONS AND DETAILS FOR ANOKA COUNTY SERVICE CABINET INFORMATION.
- 8) SEE SPECIAL PROVISIONS FOR COUNTY FURNISHED MATERIALS.
- 9) LOOP DETECTOR WIRES SHALL BE CROSS-LINKED POLYETHYLENE (XLP) IN 1"N.M.C. SEE SPECIAL PROVISIONS AND DETAILS.
- 10) EACH HANDHOLE SHALL BE CONCRETE HANDHOLE WITH TYPE "C" COVER PER Mn/DOT STANDARD PLATE NO.8117F.
- 11) SEE SPECIAL PROVISIONS AND DETAILS REGARDING TYPE "D" SIGN PANELS TO BE FURNISHED AND INSTALLED BY CONTRACTOR (INCIDENTAL TO ITEM NO.2565.511).
- 12) SEE SPECIAL PROVISIONS REGARDING REMOVAL AND SALVAGING OF INPLACE SIGNAL SYSTEM (INCIDENTAL TO ITEM NO.2565.511).
- 13) SEE SPECIAL PROVISIONS REGARDING ADDITIONAL SIGNAL MATERIALS TO BE FURNISHED BY THE CONTRACTOR (FOR OTHERS TO INSTALL).
- 14) SEE SPECIAL PROVISIONS REGARDING ADDITIONAL TRAFFIC SIGNAL CABLES AND CONDUCTORS TO BE FURNISHED AND INSTALLED BY CONTRACTOR.

③ TYPE A100-A-45-D30-9 (DAVIT AT 0°)
 A100 POLE FOUNDATION
 3-ONE WAY SIGNALS-OVERHEAD
 2-TYPE 10B-POLE MOUNTED 90° AND 180°
 MID MAST ARM MOUNTS AT 12' AND 25'
 2-PEDESTRIAN PUSH BUTTONS
 TYPE "D" SIGN PANEL (108"x18")-OVERHEAD
 LUMINAIRE-200 WATT H.P.S.
 EXTEND INTO H.H.18:
 3"R.S.C.
 3-12/c#12
 1-3/c#12
 2-1/c#10

④ TYPE A100-A-45-D30-9 (DAVIT AT 0°)
 A100-POLE FOUNDATION
 2-ONE WAY SIGNALS-OVERHEAD
 2-TYPE 10B-POLE MOUNTED 90° AND 180°
 MID MAST ARM MOUNTS AT 12' AND 25'
 2-PEDESTRIAN PUSH BUTTONS
 TYPE "D" SIGN PANEL (66"x18")-OVERHEAD
 LUMINAIRE-200 WATT H.P.S.
 EXTEND INTO H.H.13:
 3"R.S.C.
 3-12/c#12
 1-3/c#12
 2-1/c#10

Ⓐ CONTROLLER AND CABINET
 (FURNISHED BY OTHERS)
 CABINET FOUNDATION
 EXTEND INTO H.H.22:
 METERED SIGNAL SERVICE
 1 1/4"R.S.C.
 3-1/c#6
 EXTEND INTO H.H.1:
 4"R.S.C.
 6-12/c#12
 2-3/c#12
 10-2/c#14
 EXTEND INTO H.H.11:
 4"R.S.C.
 6-12/c#12
 2-3/c#12
 10-2/c#14
 STUB OUT 3"R.S.C. AND
 CAP (FOR FUTURE USE)

Ⓑ SERVICE CABINET
 CABINET FOUNDATION
 STUB OUT 2"R.S.C. (FOR SERVICE
 BY OTHERS)
 EXTEND INTO H.H.22:
 METERED SIGNAL SERVICE
 1 1/4"R.S.C.
 3-1/c#6
 EXTEND INTO H.H.11:
 UNMETERED STREET LIGHT SERVICE
 2"R.S.C.
 8-1/c#10
 BETWEEN H.H.1 AND H.H.11:
 2"R.S.C.
 4-1/c#10

BASE	OVERLAY	DRG.	NO.

MATCH LINE - STATION 186+42 - SEE SHEET 38

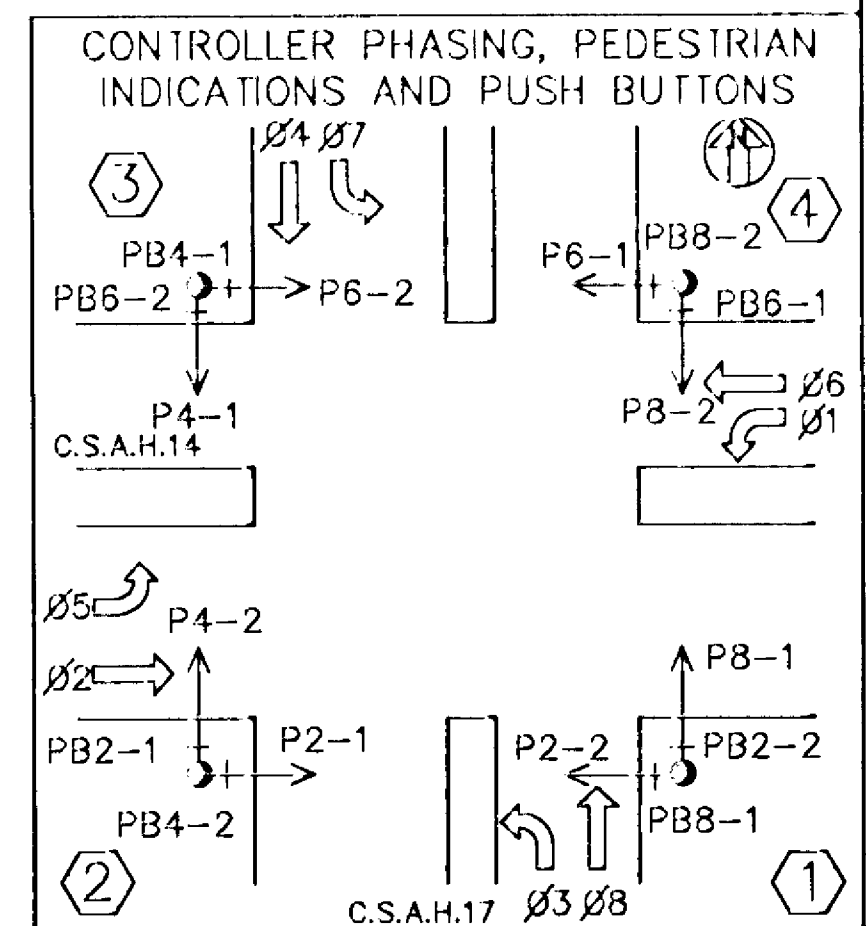
MATCH LINE - STATION 192+42 - SEE SHEET 38

LOOP DETECTORS			
NUMBER	SIZE(FT.)	LOCATION	FUNCTION
D1-1	MULTIPLE	5'	1
D2-1	2-6x6	405'	1
D2-2	6x6	5'	6
D2-3	6x6	5'	4
D2-4	6x6	5'	4
D3-1	MULTIPLE	5'	1
D4-1	2-6x6	405'	1
D4-2	6x6	5'	6
D4-3	6x6	5'	6
D4-4	6x6	5'	4
D5-1	MULTIPLE	5'	1
D6-1	2-6x6	405'	1
D6-2	6x6	5'	6
D6-3	6x6	5'	4
D6-4	6x6	5'	4
D7-1	MULTIPLE	5'	1
D8-1	2-6x6	405'	1
D8-2	6x6	5'	6
D8-3	6x6	5'	6
D8-4	6x6	5'	4

- FUNCTIONS:
 1) CALL AND EXTEND
 2) CALL ONLY
 3) EXTEND ONLY
 4) CALL ONLY DENSITY
 5) DELAYED CALL ONLY
 6) DELAYED CALL ONLY DENSITY
 7) DELAYED CALL-IMMEDIATE
 8) CARRY OVER (STRETCH)
 9) ADVISORY DETECTOR
 10) SAMPLING DETECTOR
 11) SPECIAL DETECTOR

② TYPE A100-A-45-D30-9 (DAVIT AT 0°)
 A100 POLE FOUNDATION
 2-ONE WAY SIGNALS-OVERHEAD
 2-TYPE 10B-POLE MOUNTED 90° AND 180°
 MID MAST ARM MOUNTS AT 12' AND 25'
 2-PEDESTRIAN PUSH BUTTONS
 TYPE "D" SIGN PANEL (66"x18")-OVERHEAD
 LUMINAIRE-200 WATT H.P.S.
 EXTEND INTO H.H.6:
 3"R.S.C.
 3-12/c#12
 1-3/c#12
 2-1/c#10

① TYPE A100-A-45-D30-9 (DAVIT AT 0°)
 A100 POLE FOUNDATION
 3-ONE WAY SIGNALS-OVERHEAD
 2-TYPE 10B-POLE MOUNTED 90° AND 180°
 MID MAST ARM MOUNTS AT 12' AND 25'
 2-PEDESTRIAN PUSH BUTTONS
 TYPE "D" SIGN PANEL (108"x18")-OVERHEAD
 LUMINAIRE-200 WATT H.P.S.
 EXTEND INTO H.H.1:
 3"R.S.C.
 3-12/c#12
 1-3/c#12
 2-1/c#10



NOTE: LOCATION = DISTANCE FROM STOP BAR TO LOOP DETECTOR.

MATCH LINE - STATION 702+00 - SEE SHEET 38

MATCH LINE - STATION 698+25 - SEE SHEET 38

"ELECTRICAL ENGINEER CERTIFICATION"
 I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
Robert A. Eiler
 Date: 7-2-92 Reg. No. 5859

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
James J. Johnson
 Date: 7-2-92 Reg. No. 20343

ES&J
 ENGINEERS ARCHITECTS PLANNERS
 S.A.P. 02-614-14

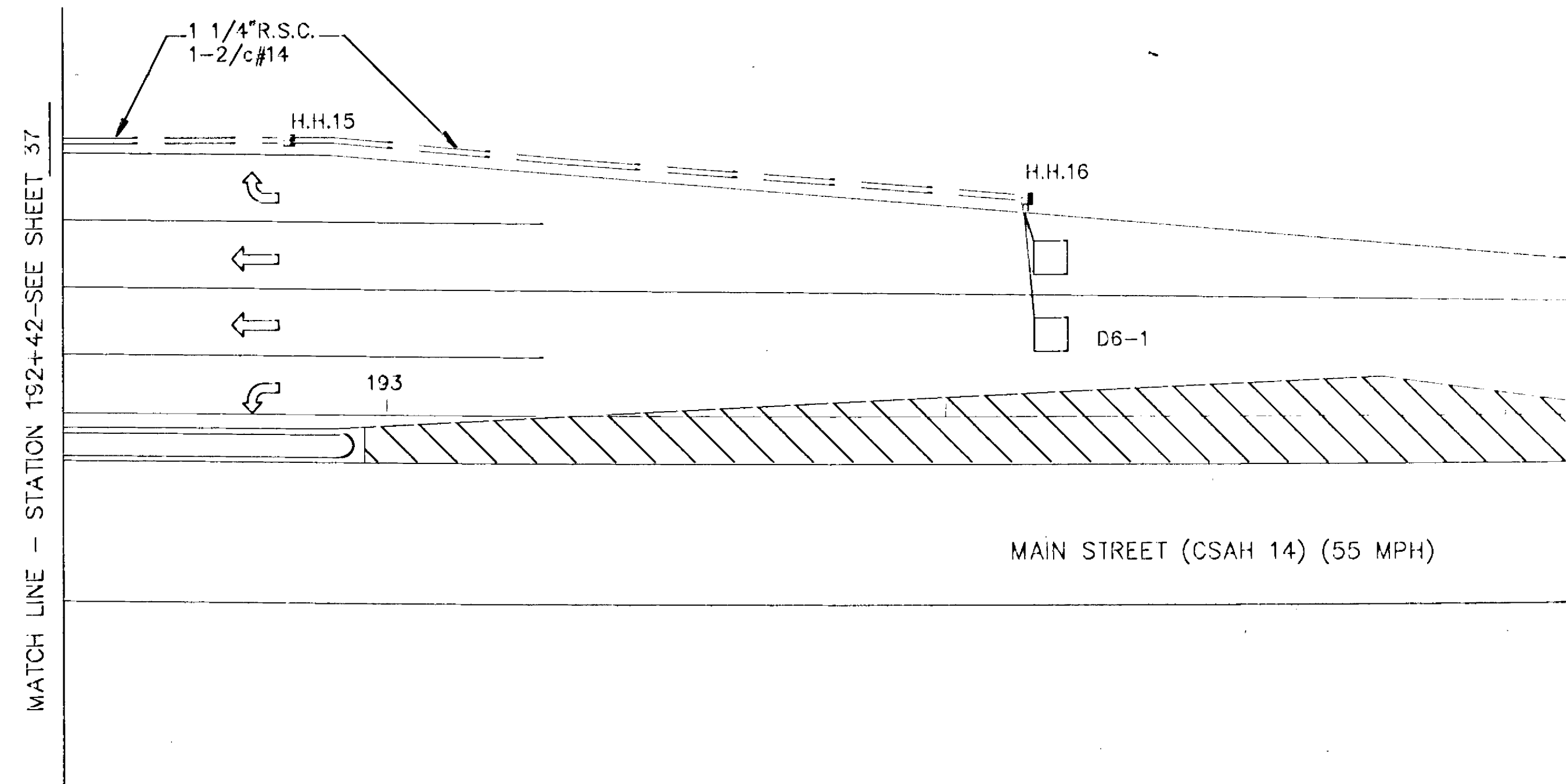
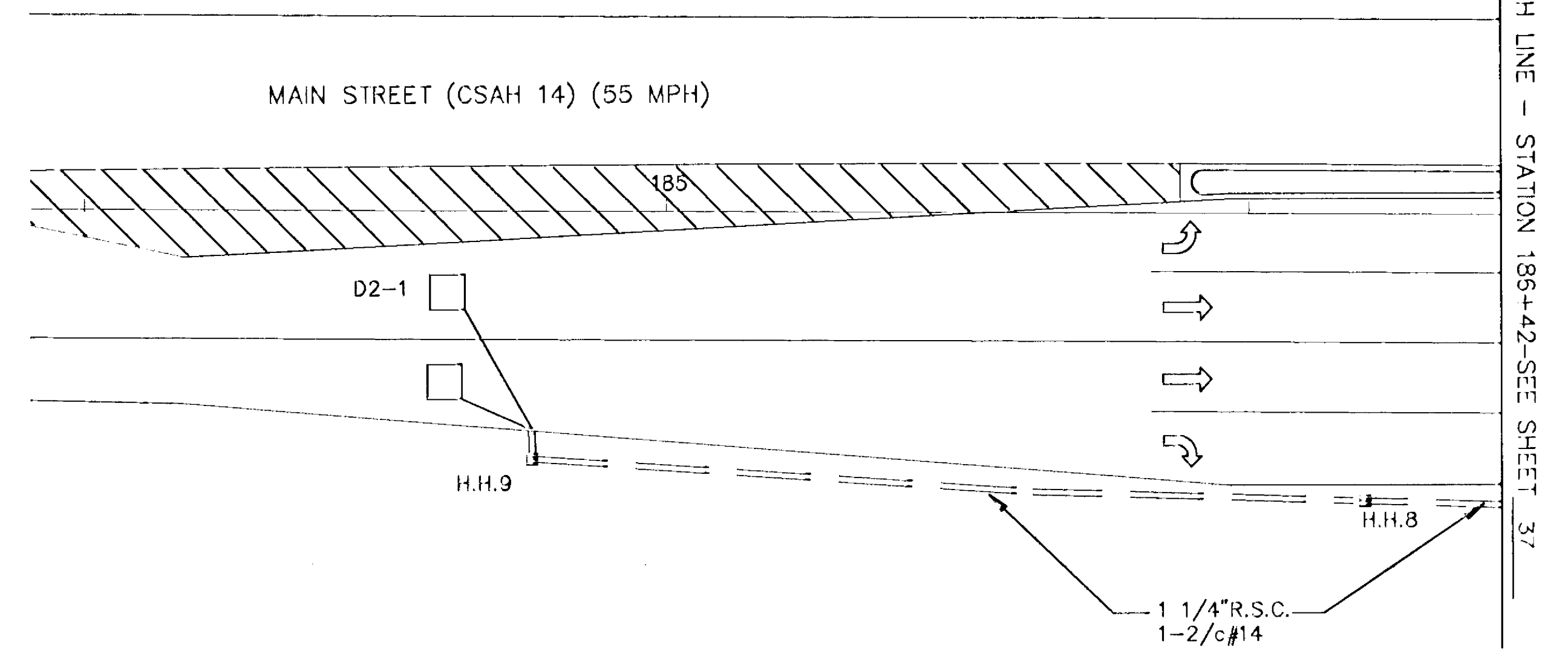
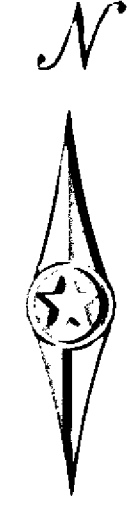
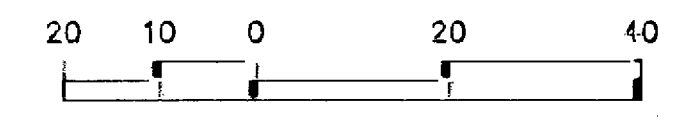
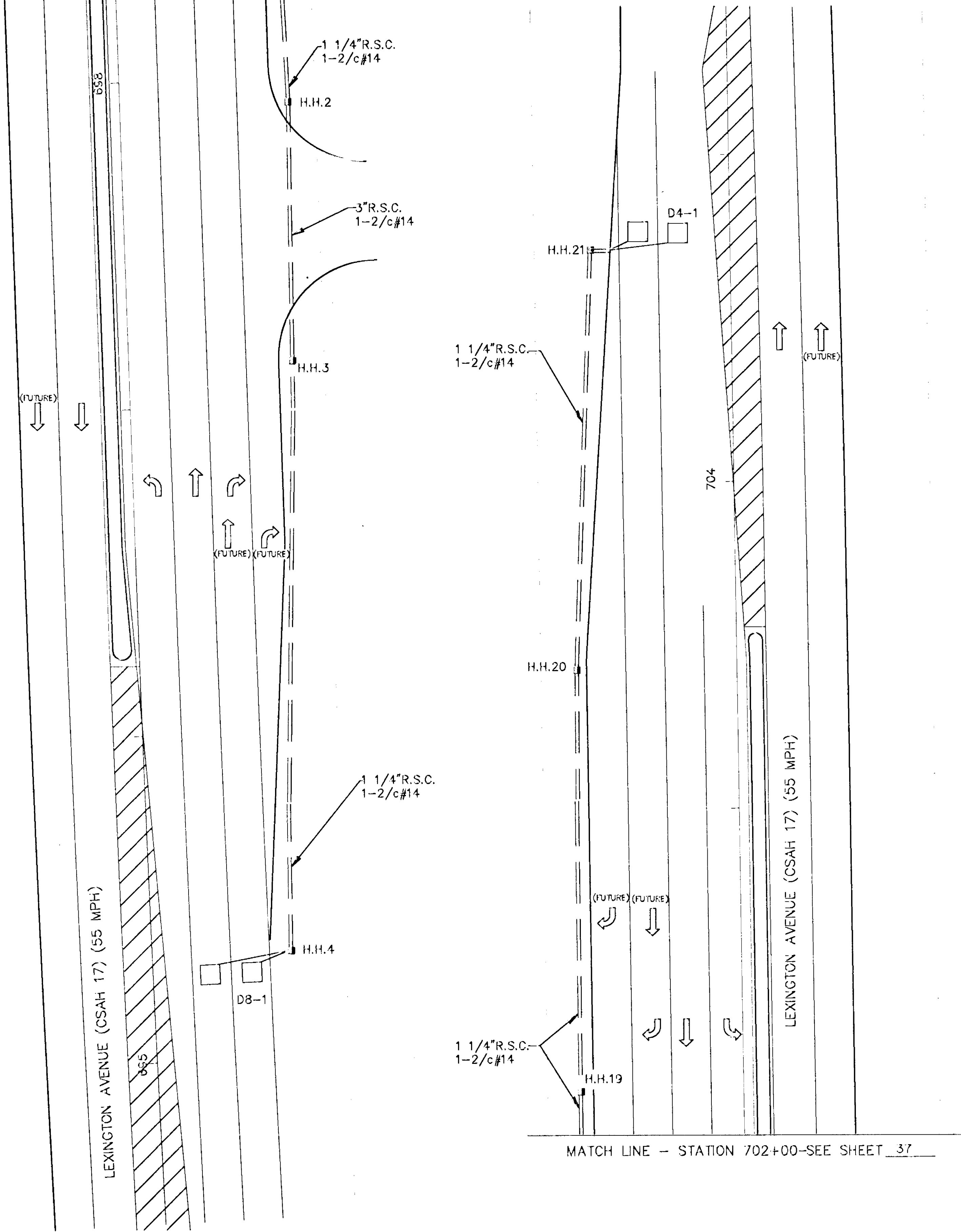
ANOKA COUNTY, MINNESOTA
 CITY OF BLAINE
 S.P. _____ C.P. _____

TRAFFIC SIGNAL SYSTEM "B"
 INTERSECTION LAYOUT
 MAIN STREET (CSAH 14) AT LEXINGTON AVENUE (CSAH 17)
 Sheet No. 37 of 117 Sheets

FILE NO.
 92219
 DATE

NO.	BY	DATE	REVISIONS

BASE OVERLAYING NO.	



NO.	BY	DATE	REVISIONS

"ELECTRICAL ENGINEER CERTIFICATION"
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[Signature]
 Date: 7-2-92 Reg. No. 20943

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
[Signature]
 Date: 7-2-92 Reg. No. 20943



ANOKA COUNTY, MINNESOTA
 CITY OF BLAINE

TRAFFIC SIGNAL SYSTEM 'B'
 INTERSECTION LAYOUT
 MAIN STREET (CSAH 14) AT LEXINGTON AVENUE (CSAH 17)

FILE NO.
 92219
 DATE

CONDUCTOR COLOR CODING

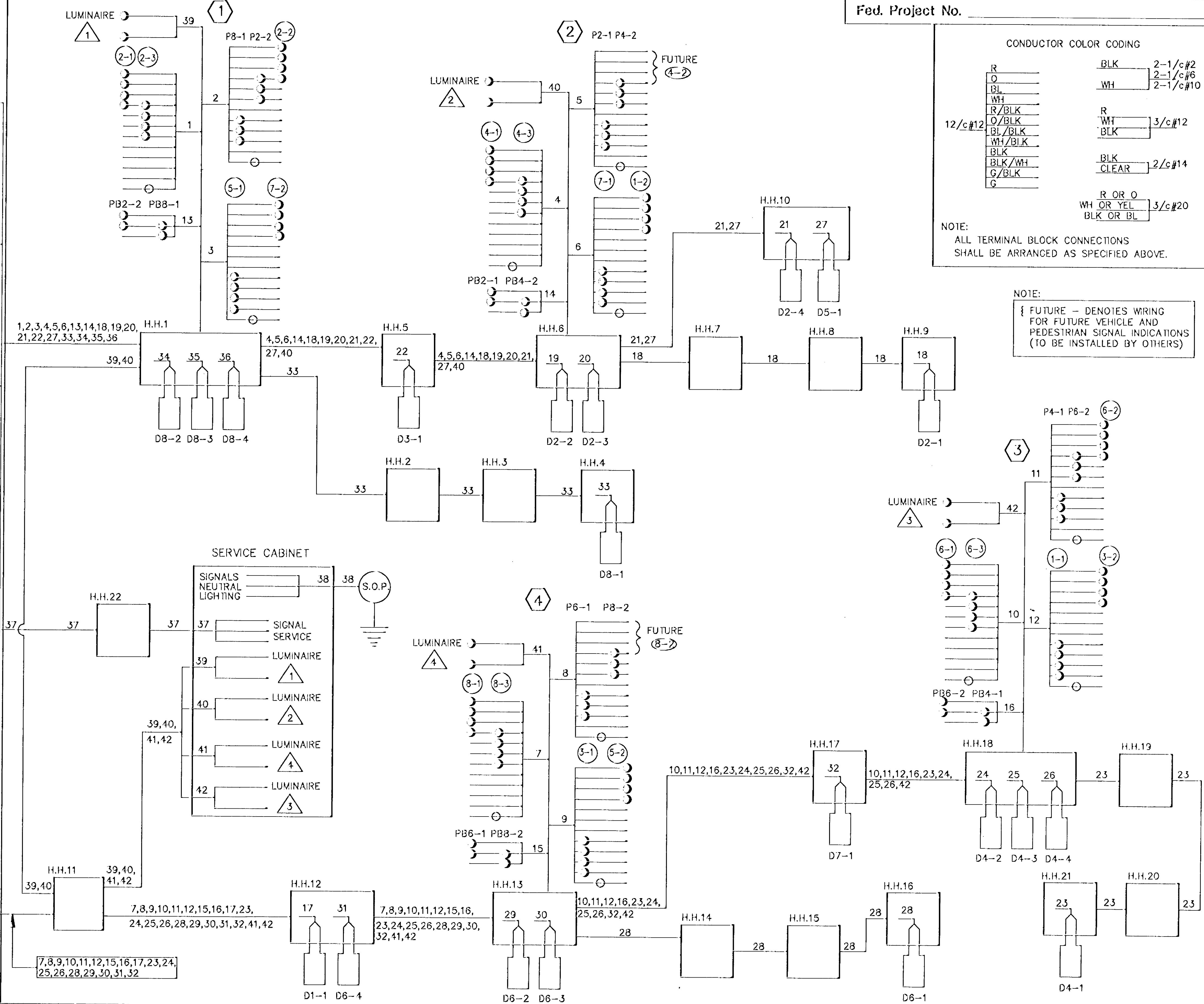
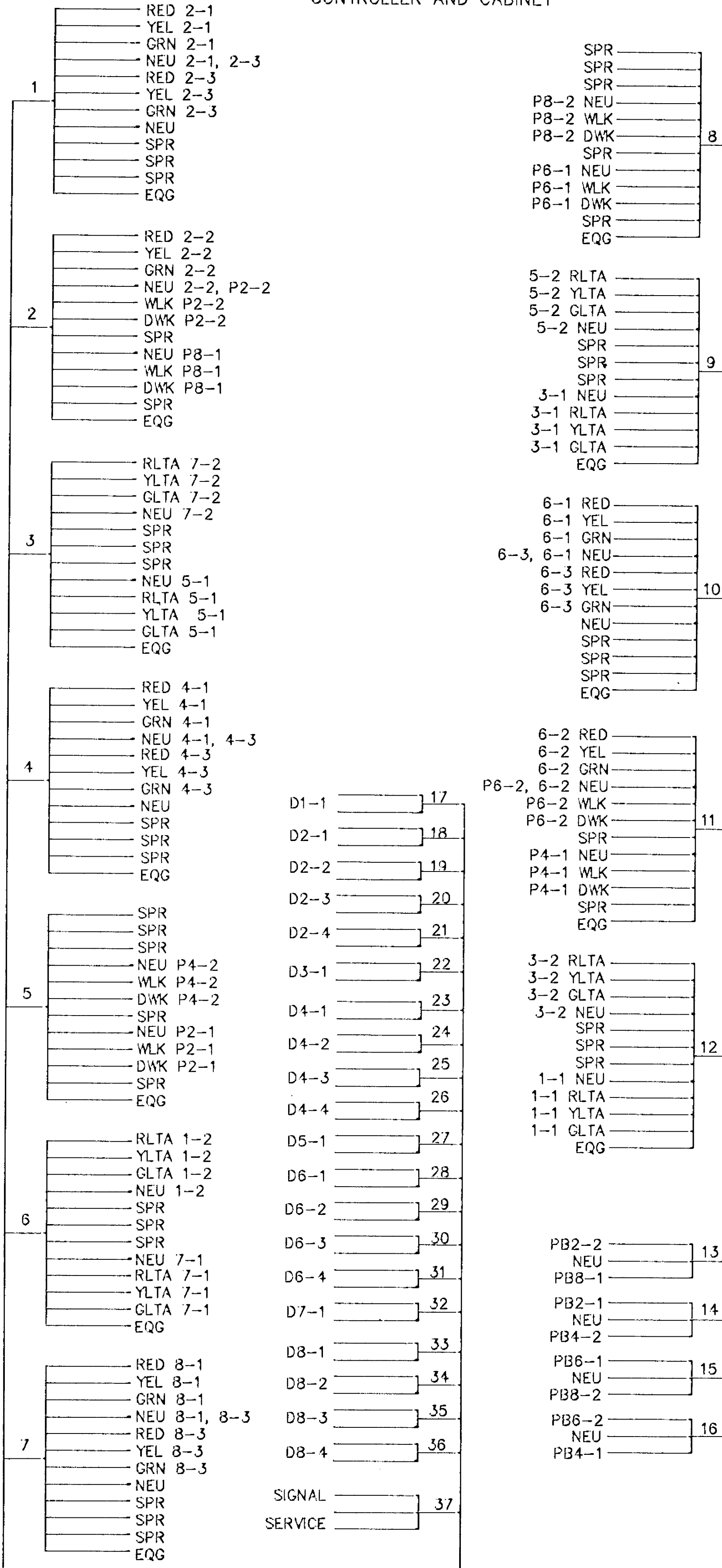
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O	WH	2-1/c#6
BL	WH	2-1/c#10
WH	R	
R/BLK	WH	3/c#12
O/BLK	BLK	
BL/BLK	BLK	2/c#14
WH/BLK	CLEAR	
BLK		
BLK/WH		
G/BLK		
G		
	R OR O	3/c#20
	WH OR YEL	
	BLK OR BL	

NOTE:
ALL TERMINAL BLOCK CONNECTIONS SHALL BE ARRANGED AS SPECIFIED ABOVE.

NOTE:
{ FUTURE - DENOTES WIRING FOR FUTURE VEHICLE AND PEDESTRIAN SIGNAL INDICATIONS (TO BE INSTALLED BY OTHERS)

BASE OVERLAY/DRG. NO.

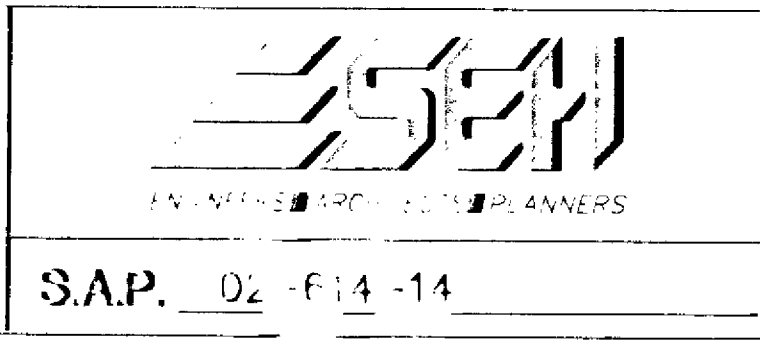
CONTROLLER AND CABINET



NO.	BY	DATE	REVISIONS

"ELECTRICAL ENGINEER CERTIFICATION"
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Robert A. Ellen
Date: 7-2-92 Reg. No. 5859

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
James J. Anderson
Date: 6-6-92 Reg. No. 20943

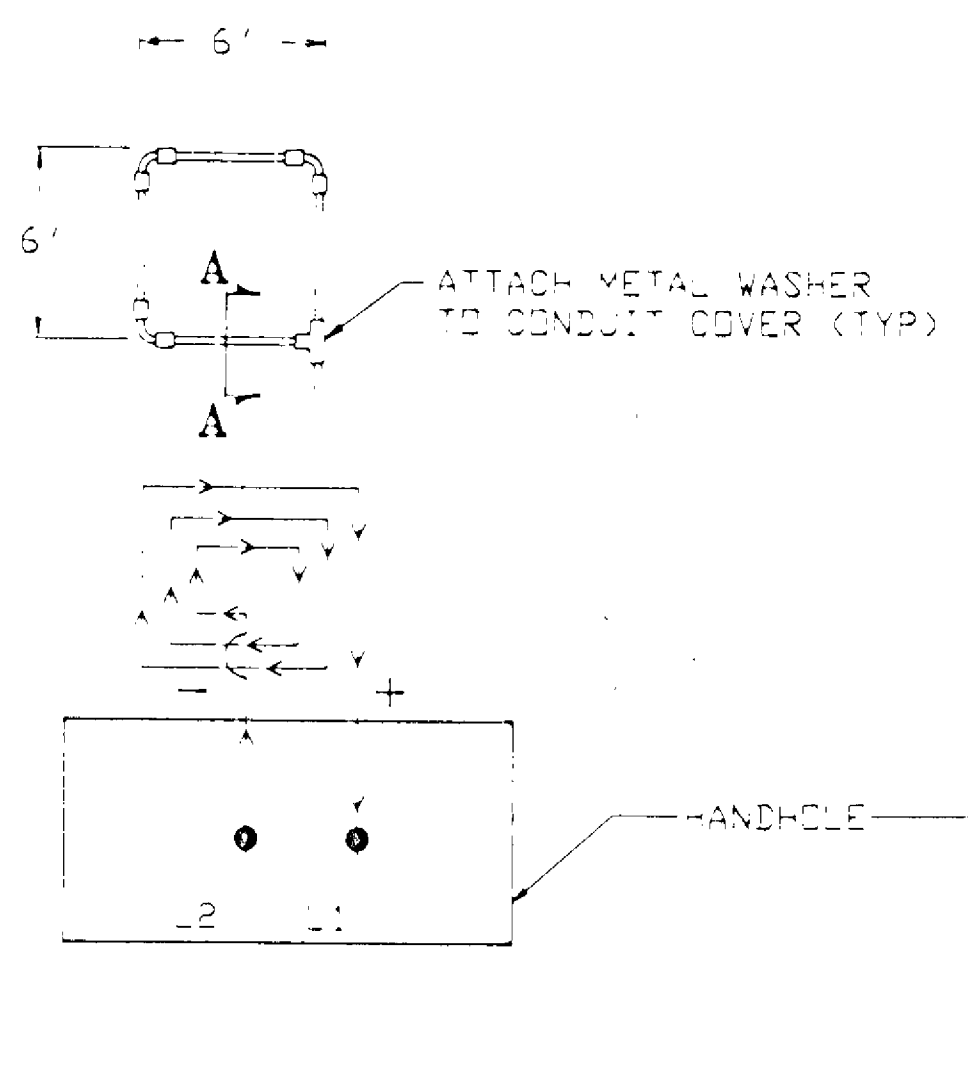


ANOKA COUNTY, MINNESOTA
CITY OF BLAINE
S.A.P. D2-F14-14 SP. _____ C.P. _____

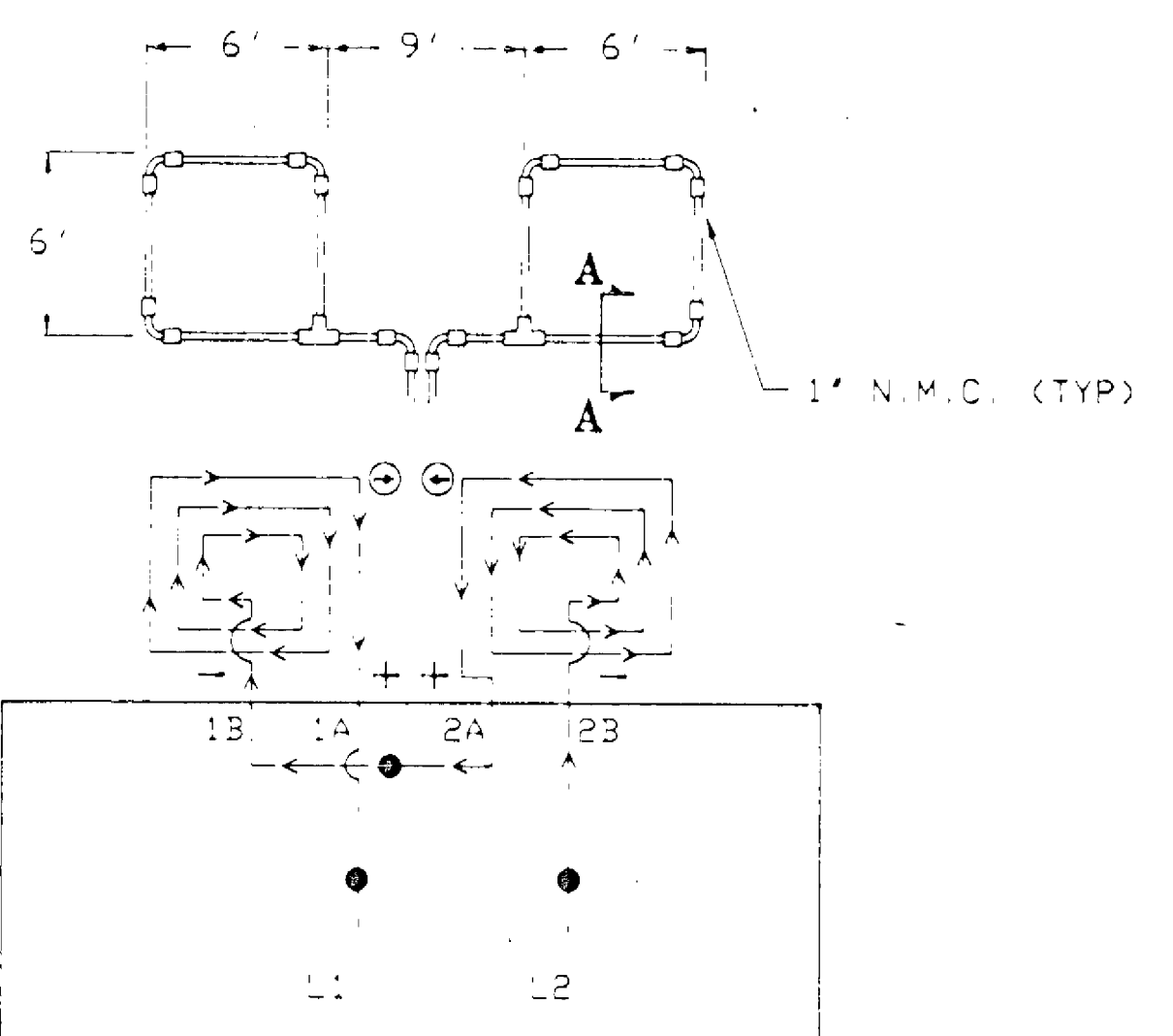
TRAFFIC SIGNAL SYSTEM "B"
FIELD WIRING DIAGRAM
MAIN STREET (CSAH 14) AT LEXINGTON AVENUE (CSAH 17)
Sheet No. 39 of 117 Sheets

FILE NO. 92219
DATE _____

LOOP DETECTOR DETAIL 'A'
 PLAN VIEW (NOT TO SCALE)
 (LOOP PHASING FOR SINGLE CONNECTION)

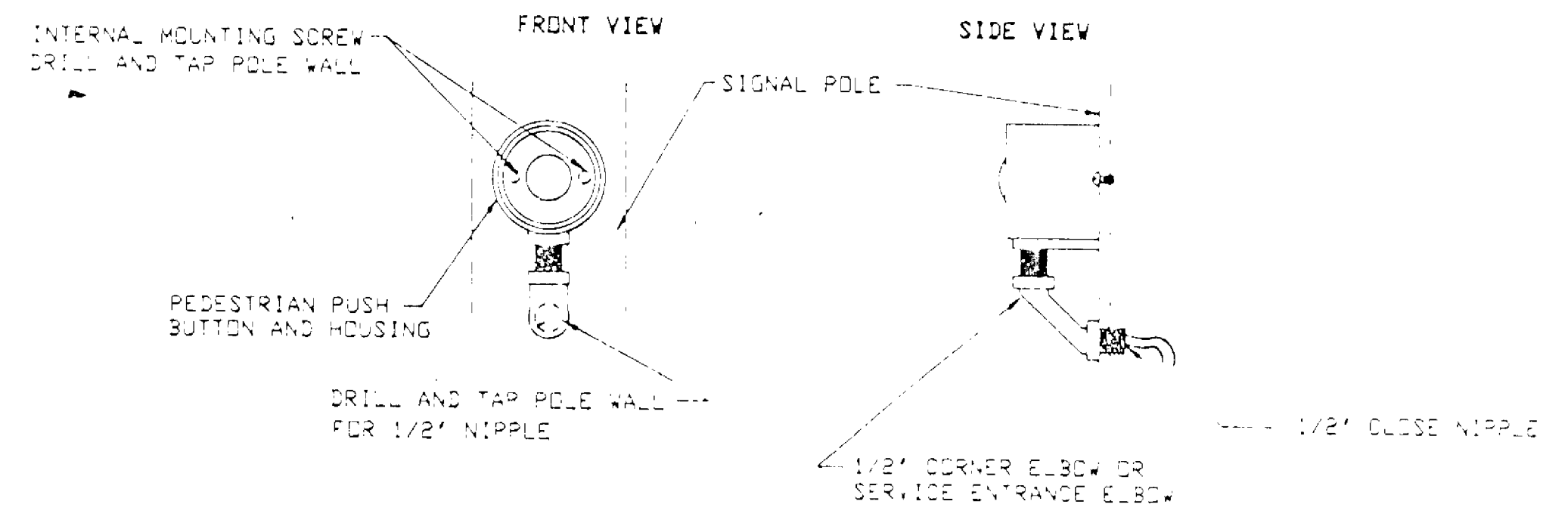


LOOP DETECTOR DETAIL 'B'
 PLAN VIEW (NOT TO SCALE)
 (LOOP PHASING FOR SERIES CONNECTION)



LOOP CONNECTIONS SHALL BE LABELED AND SPLICED IN THE HAND-HOLE AS FOLLOWS:
 L1 TO 1A, 1B TO 2A, AND 2B TO L2.

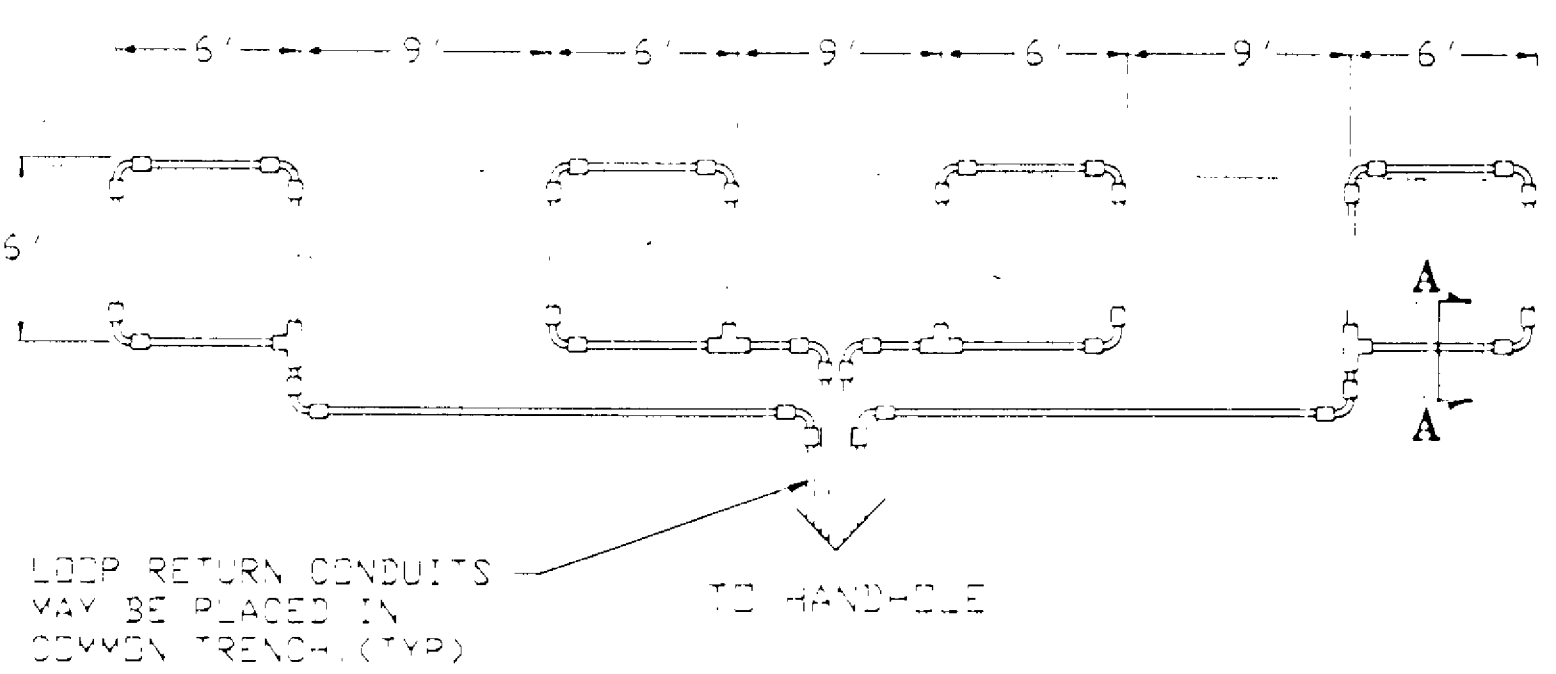
PEDESTRIAN PUSH BUTTON DETAIL



LEGEND OF SYMBOLS

CONTROLLER AND SERVICE EQPT NDS.	_____
SIGNAL BASE NO.	_____
SIGNAL FACE NO.	_____
LUMINAIRE NO.	_____
CONTROLLER AND CABINET	_____
CONTROLLER AND CABINET IN PLACE	_____
HANDHOLE	_____
HANDHOLE IN PLACE	_____
RIGID STEEL CONDUIT (R.S.C.)	_____
RIGID STEEL CONDUIT (R.S.C.) IN PLACE	_____
SIGNAL FACE WITH BACKGROUND SHIELD	_____
SIGNAL FACE W/D BACKGROUND SHIELD	_____
SIGNAL FACE IN PLACE	_____
PEDESTRIAN INDICATORS	_____
PEDESTRIAN INDICATORS IN PLACE	_____
PEDESTRIAN PUSH BUTTONS ON PEDESTAL OR POLE	_____
PEDESTRIAN PUSH BUTTON STATION	_____
TRAFFIC SIGNAL PEDESTAL	_____
TRAFFIC SIGNAL PEDESTAL IN PLACE	_____
TRAFFIC SIGNAL POLE AND MAST ARM	_____
TRAFFIC SIGNAL POLE AND MAST ARM IN PLACE	_____
STREET LIGHT POLE AND LUMINAIRE	_____
STREET LIGHT POLE AND LUMINAIRE IN PLACE	_____
MAST ARM AND LUMINAIRE	_____
MAST ARM AND LUMINAIRE IN PLACE	_____
WOOD POLE	_____
WOOD POLE IN PLACE	_____
SOURCE OF POWER	_____
RAILROAD SIGNAL IN PLACE	_____
SHOULDERLINE	_____
CURB LINE	_____
STOP BAR	_____

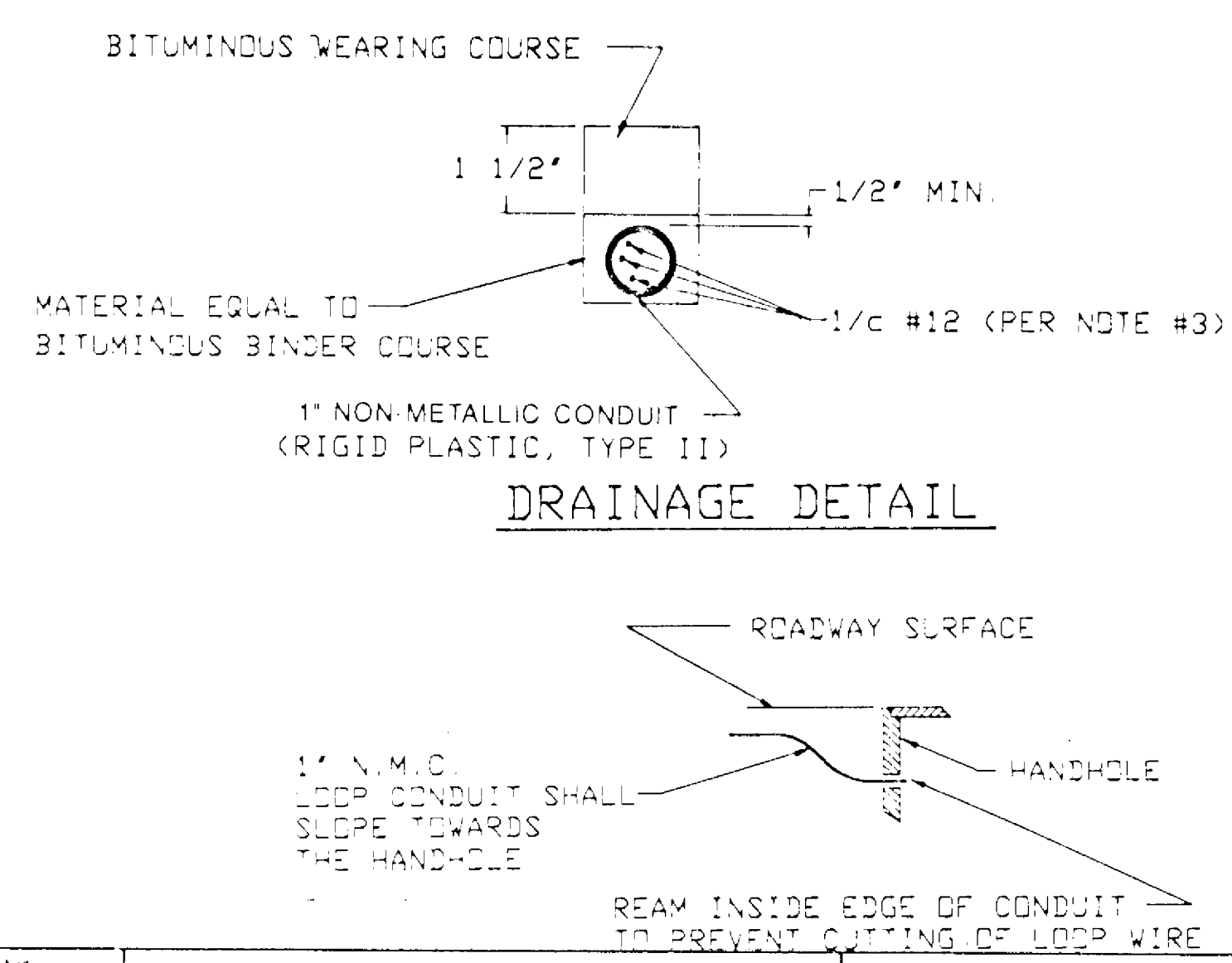
LOOP DETECTOR DETAIL 'C'
 PLAN VIEW (NOT TO SCALE)
 (LOOP PHASING FOR SERIES CONNECTION)



LOOP DETECTOR WIRING

- NOTES:
- 1) ALL CORNERS SHALL BE 90° CONDUIT BENDS
 - 2) CONNECT WIRES IN HANDHOLES USING SPLICE KIT METHOD DESCRIBED IN THE SPECIAL PROVISIONS.
 - 3) LOOP DETECTOR WIRES SHALL BE #12 AWG CROSSED LINKED POLYETHYLENE (XLP). SEE SPECIAL PROVISIONS.
 - 4) LOOP LEAD IN WIRES SHALL BE TWISTED A MIN. OF (5) TURNS PER FOOT THROUGH THE CONDUIT TO THE HANDHOLE.
 - 5) N.M.C. DESIGNATES NON-METALLIC CONDUIT (SPEC. 3803)
 - 6) LOOPS 6'x6' THRU 6'x14' SHALL HAVE (4) TURNS.
 - 7) LOOPS 6'x15' AND LARGER SHALL HAVE TWO (2) TURNS.
 - 8) A CLOSED CELL FOAM BACKER ROD SHALL BE FURNISHED AND INSTALLED WITH THE LAST TURN OF WIRE IN THE 1" N.M.C. LOOP ASSEMBLY.

CROSS SECTION A-A



ABBREVIATIONS
 EQUIPMENT AND INDICATIONS

- RED-RED
- YEL-YELLOW
- GRN-GREEN
- WLK-WALK
- NEU-NEUTRAL
- DWK-DON'T WALK
- LUM-LUMINAIRE
- DNL-DOWNLIGHT
- H-H-HANDHOLE
- EGG-EQUIPMENT GROUND
- R S C-RIGID STEEL CONDUIT
- GLTA-GREEN LEFT TURN ARROW
- YRTA-YELLOW RIGHT TURN ARROW
- D2-1(eg)-DETECTOR-PHASE '2'
- GR-R-GROUND ROD
- SER-SERVICE
- P2-2 PEDESTRIAN INDICATIONS
- 2-1(eg)-SIGNAL HEADS-PHASE '2'
- SPR-SPARE CONDUCTORS
- N.M.C.-NON METALLIC CONDUIT
- E.V.P.-EMERGENCY VEHICLE PRE-EMPTION
- J.B.-JUNCTION BOX
- W.P.-WOOD POLE
- P.E.C.-PHOTOELECTRIC CELL
- S.O.P.-SOURCE OF POWER
- H.P.S.-HIGH PRESSURE SODIUM
- F & I-FURNISH AND INSTALL
- R & S-REMOVE AND SALVAGE
- B.G.-BARE GROUND

CONDUCTOR COLOR CODE

- R - RED
- D - DRANGE
- BL - BLUE
- WH - WHITE
- R/BLK - RED WITH BLACK TRACER
- D/BLK - DRANGE WITH BLACK TRACER
- BL/BLK - BLUE WITH BLACK TRACER
- WH/BLK - WHITE WITH BLACK TRACER
- BLK - BLACK
- BLK/WH - BLACK WITH WHITE TRACER
- G/BLK - GREEN WITH BLACK TRACER
- G - GREEN

STANDARD PLATES

PLATE NO.	DESCRIPTION
* 8110 C	TRAFFIC SIGNAL BRACKETING - POLE MOUNTED
8111 B	TRAFFIC SIGNAL BRACKETING - PEDESTAL MOUNTED
8112 C	PEDESTAL FOUNDATION
8113 C	MAGNETIC VEHICLE DETECTOR INSTALLATION
8115 C	PEDESTRIAN PUSH BUTTON INSTALLATION
* 8117 F	PRECAST CONCRETE HAND HOLE
8118 C	SERVICE EQUIPMENT AND POLE
* 8119 C	GROUND MOUNTED CABINET FOUNDATION
8120 H	P-80 AND P-90 POLE FOUNDATION
* 8121 B	TRANSFORMER BASE WITH POLE BASE PLATE
8122 C	PEDESTAL AND PEDESTAL BASE
* 8123 B	POLE AND MAST ARM
* 8124 D	SIGNAL HEAD MOUNTS
* 8126 C	P-100 POLE FOUNDATION
8130 D	SAW CUT LOOP DETECTORS
* 0005 A	SPECIFICATION REFERENCE
3124 B	METAL APRON CONNECTION
3225 C	CORRUGATED STEEL PIPE COUPLING BAND
7035 J	CONCRETE WALK AND CURB RETURNS AT ENTRANCES
7100 F	CONCRETE CURB AND GUTTERS

* APPLIES TO THIS PROJECT

LOOP CONNECTIONS SHALL BE LABELED AND SPLICED IN THE HAND-HOLE AS FOLLOWS:
 L1 TO 1A, 1B TO 2A, 2B TO 3A, 3B TO 4A AND 4B TO L2.
 SPLICE CONTROL CABLE TO L1 & L2 IN HANDHOLE. ALL CONDUCTORS SHALL BE TAGGED IN HANDHOLE (1A, 1B, ETC.)

	<p>"ELECTRICAL ENGINEER CERTIFICATION" I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota. <i>Robert A. Ellen</i> Date: 7-2-92 Reg. No. 5859</p>	<p>I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota. <i>Thomas J. Schmeider</i> Date: 6-16-92 Reg. No. 20943</p>		<p>ANOKA COUNTY, MINNESOTA CITY OF BLAINE</p>	<p>SIGNALS "A" & "B" DETAILS</p> <p>MAIN STREET (CSAH 14) AT RADISSON ROAD (CO.PD.52) MAIN STREET (CSAH 14) AT LEXINGTON AVENUE (CSAH 17)</p>	<p>FILE NO. 92219</p> <p>DATE</p>	
NO.	BY	DATE	REVISIONS	S.A.P. 02-614-14	S.P.	C.P.	Sheet No. 40 of 117 Sheets

SIGN DETAILS

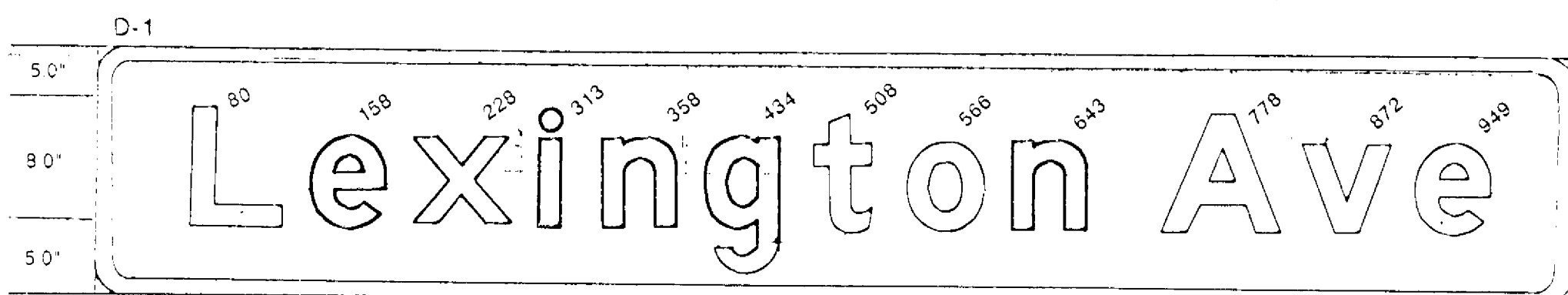
TYPE "D" SIGNS

SIGN PANEL	SIZE	NO. REQ.	NO. BRACKETS PER SIGN	BRACKET SPACING	SQ. FT. PER SIGN	POLE NO.	a
D-1	108"x18"	2	3*	45'	13.50	1	28'
						3	28'
						4	28'
D-2	90"x18"	2	2*	54'	11.25	2	28'
						4	28'
						2	28'
D-3	66"x18"	2	2*	42'	8.25	1	16'
						3	16'

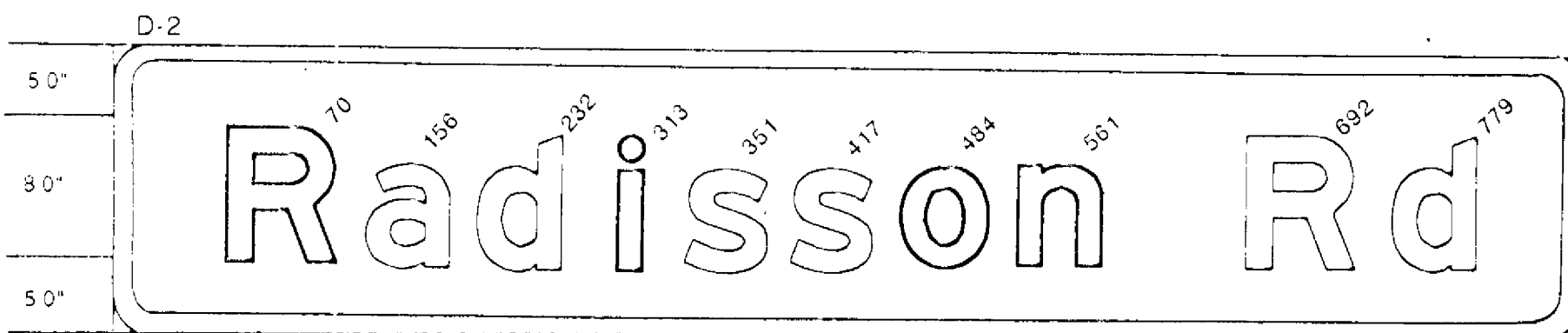
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NOTES

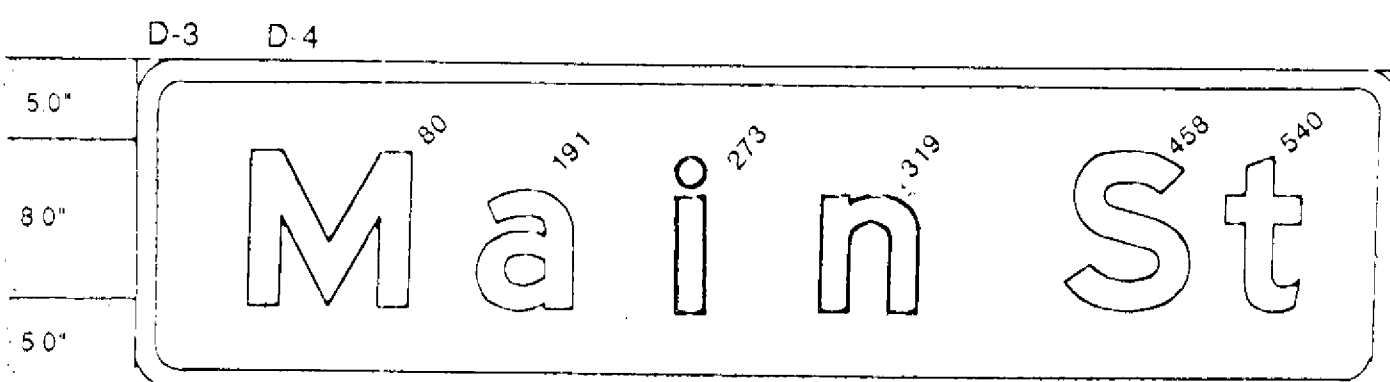
- COLOR: WHITE LEGEND AND BORDER ON GREEN BACKGROUND, FULLY REFLECTORIZED
- CORNERS EXTENDING BEYOND THE BORDER SHALL NOT BE TRIMMED
- FOR STRUCTURAL DETAILS, TYPE D SIGNS SEE STANDARD SIGNS MANUAL, PAGE 105A AND B
- FOR TYPE D STRINGER AND PANEL JOINT DETAIL SEE STANDARD MANUAL
- SIGN PANELS TO BE FURNISHED AND INSTALLED INCIDENTAL TO ITEM NO. 2565 511
- SIGNS D-1 AND D-3 TO BE INSTALLED AT SYSTEM "B"; SIGNS D-2 AND D-4 TO BE INSTALLED AT SYSTEM "A".



108" x 18", 3"R, 1.0"B
LINE 1 916: 8" 6" E MOD



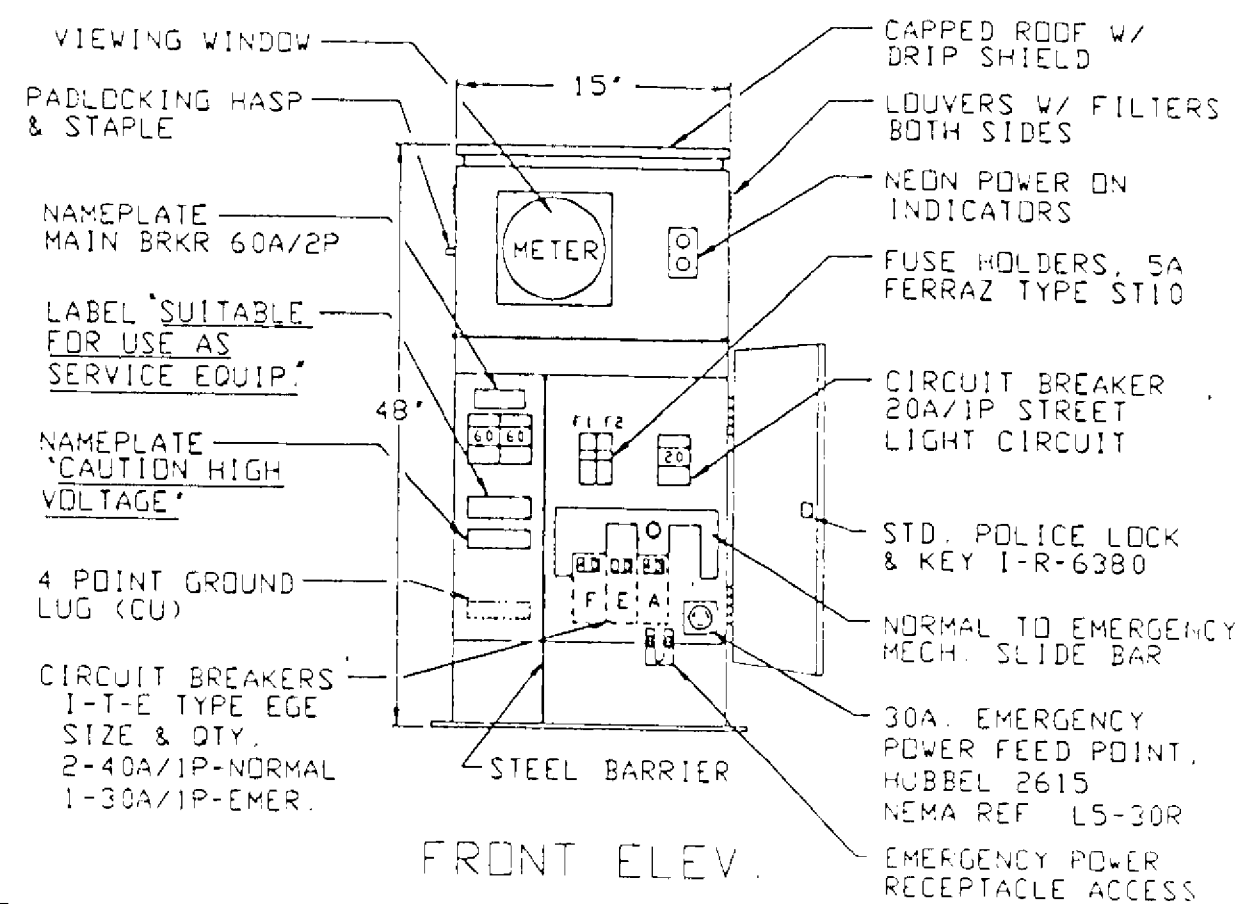
90" x 18", 3"R, 1.0"B
LINE 1 759: 8" 6" E MOD



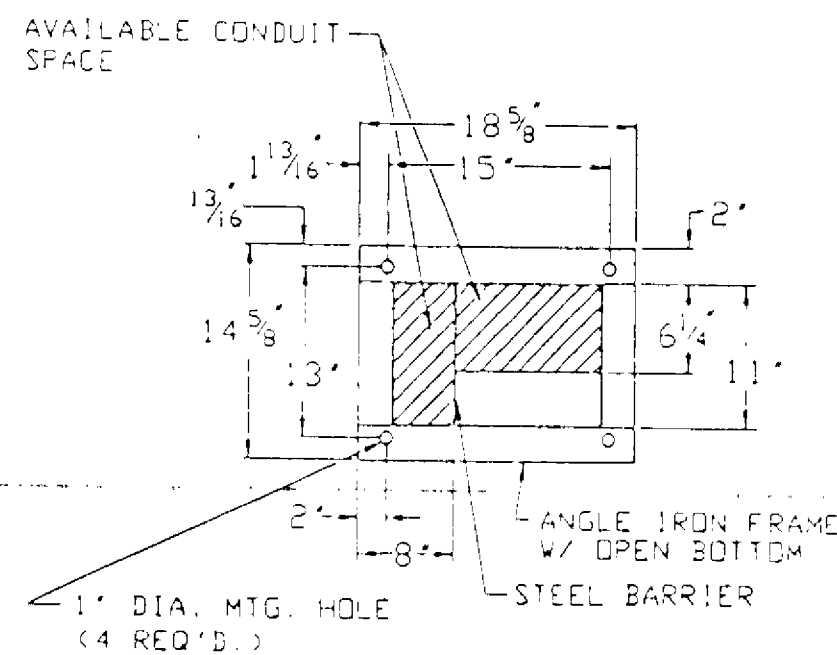
66" x 18", 3"R, 1.0"B
LINE 1 492: 8" 6" E MOD

ANOKA COUNTY HIGHWAY DEPARTMENT

SIGNAL SERVICE CABINET DETAIL



FRONT ELEV.



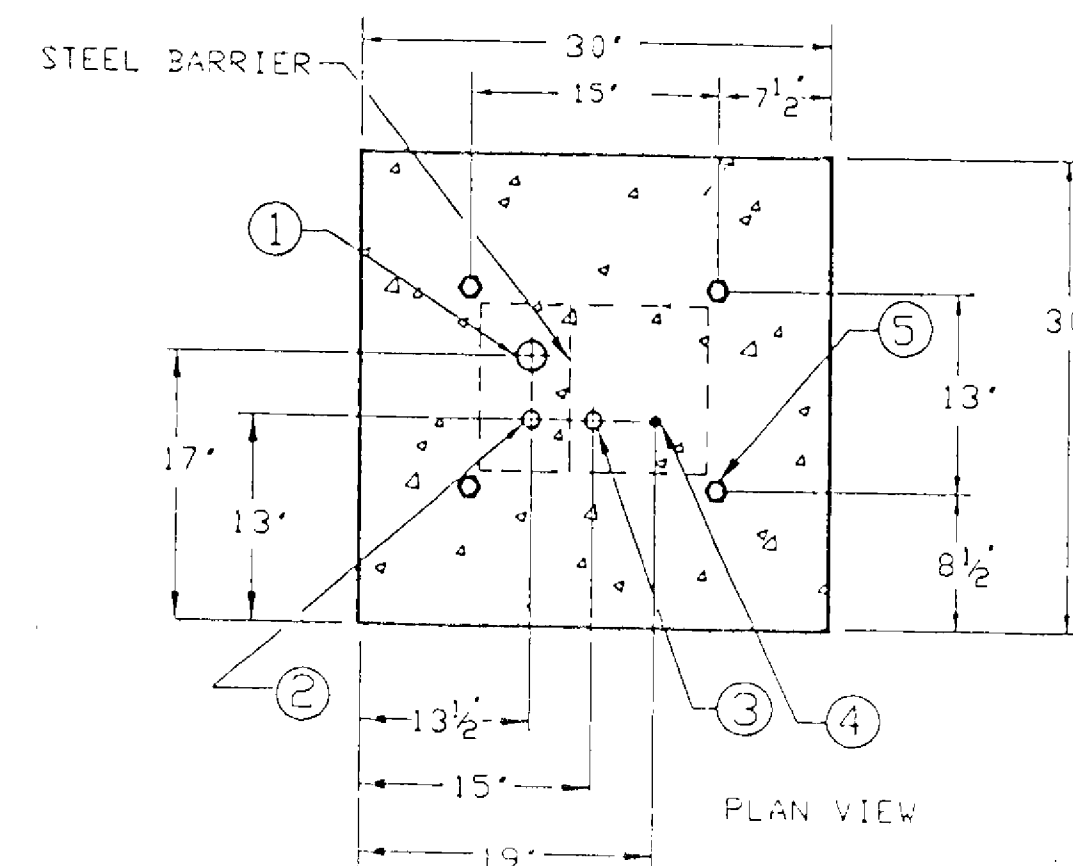
BASE LAYOUT.

NO SCALE

CONSTRUCTION NOTES

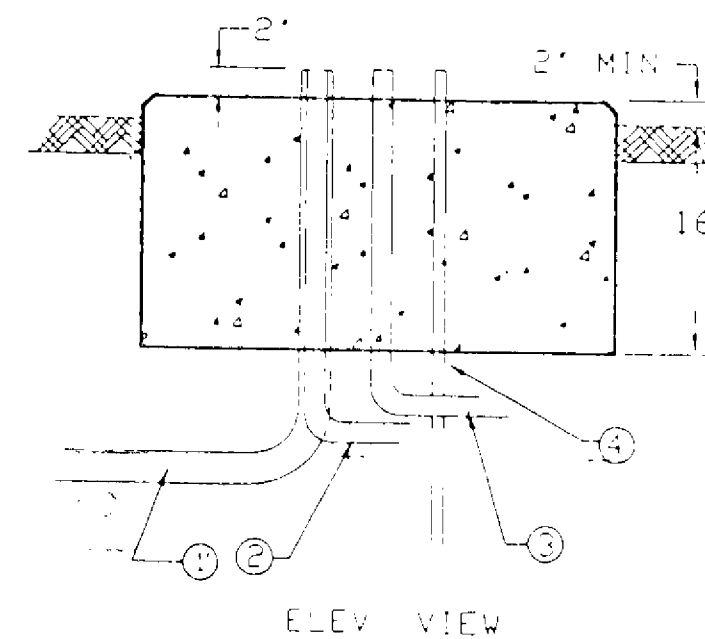
ENCLOSURE SHALL BE FABRICATED FROM #12 GA. ALL WELDED COLD ROLLED STEEL FOR OUTDOOR WEATHER PROOF SERVICE. DOORS TO BE GASKETED, ALL HINGES, PINS AND LOCKS TO BE OF NON-CORRODING CONSTRUCTION. CABINET TO BE PRIMED INSIDE AND OUT WITH RUST INHIBITING PRIMER. FINISH PER MN/DOT #3527

SIGNAL SERVICE CABINET FOUNDATION DETAIL



PLAN VIEW

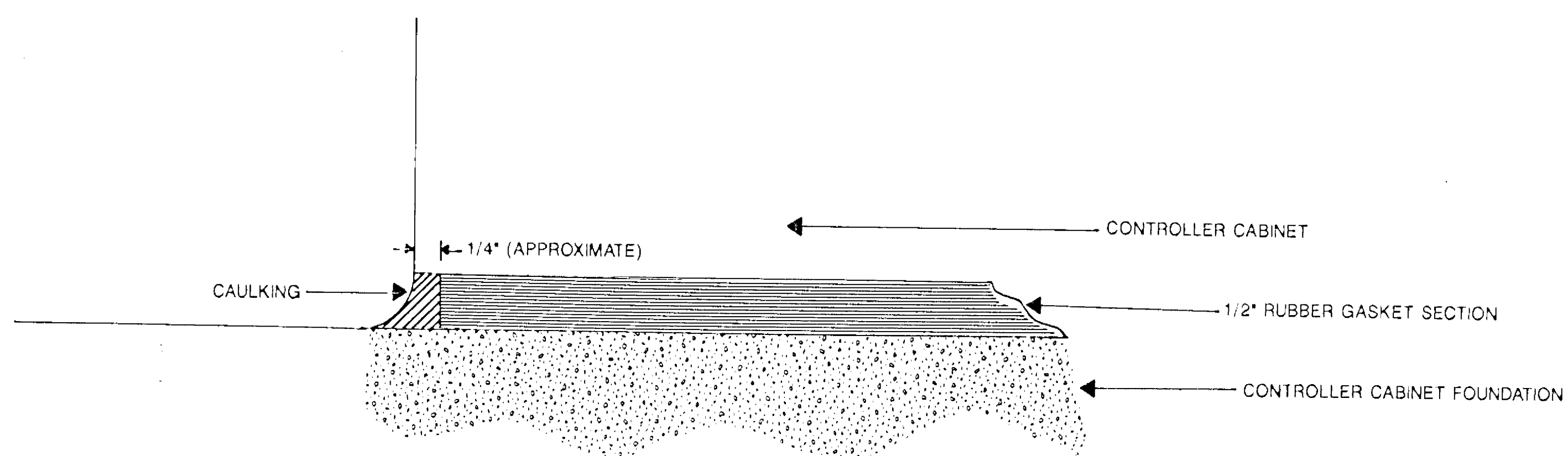
- LEGEND**
- 2" RSC FROM SOURCE OF POWER (VIA HANDHOLE)
 - 1/4" RSC TO HANDHOLE (STREET LIGHTING)
 - 1/4" RSC TO CONTROLLER CABINET (VIA HANDHOLE) SEE PLANS FOR SPECIFIC ROUTING
 - GROUNDING ROD
 - ANCHOR BOLTS (4)



ELEV. VIEW

NO SCALE

CONTROLLER CABINET CAULKING DETAIL

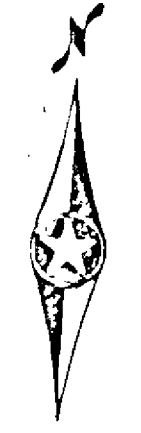


NOTE: CAULK TOP AND BOTTOM OF RUBBER GASKET SECTION BEFORE INSTALLING.

NO.	BY	DATE	REVISIONS	<p>"ELECTRICAL ENGINEER CERTIFICATION"</p> <p>I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.</p> <p><i>Robert A. Ellen</i></p> <p>Date: 7-2-92 Reg. No. 5859</p>	<p>I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.</p> <p><i>Thomas J. Schwach</i></p> <p>Date: 6/22/92 Reg. No. 20943</p>	<p>ASA ENGINEERS ARCHITECTS PLANNERS</p>	<p>ANOKA COUNTY, MINNESOTA</p> <p>CITY OF BLAINE</p>	<p>SIGNALS "A" & "B"</p> <p>DETAILS</p> <p>MAIN STREET (CSAH 14) AT RADISSON ROAD (CO. RD. 52)</p> <p>MAIN STREET (CSAH 14) AT LEXINGTON AVENUE (CSAH 17)</p>	FILE NO.	
									92219	
S.A.P. 02-614-14								S.P.	C.P.	DATE
Sheet No. 41 of 117 Sheets										

BASE OVERLAYING NO.

Fed. Project No. _____

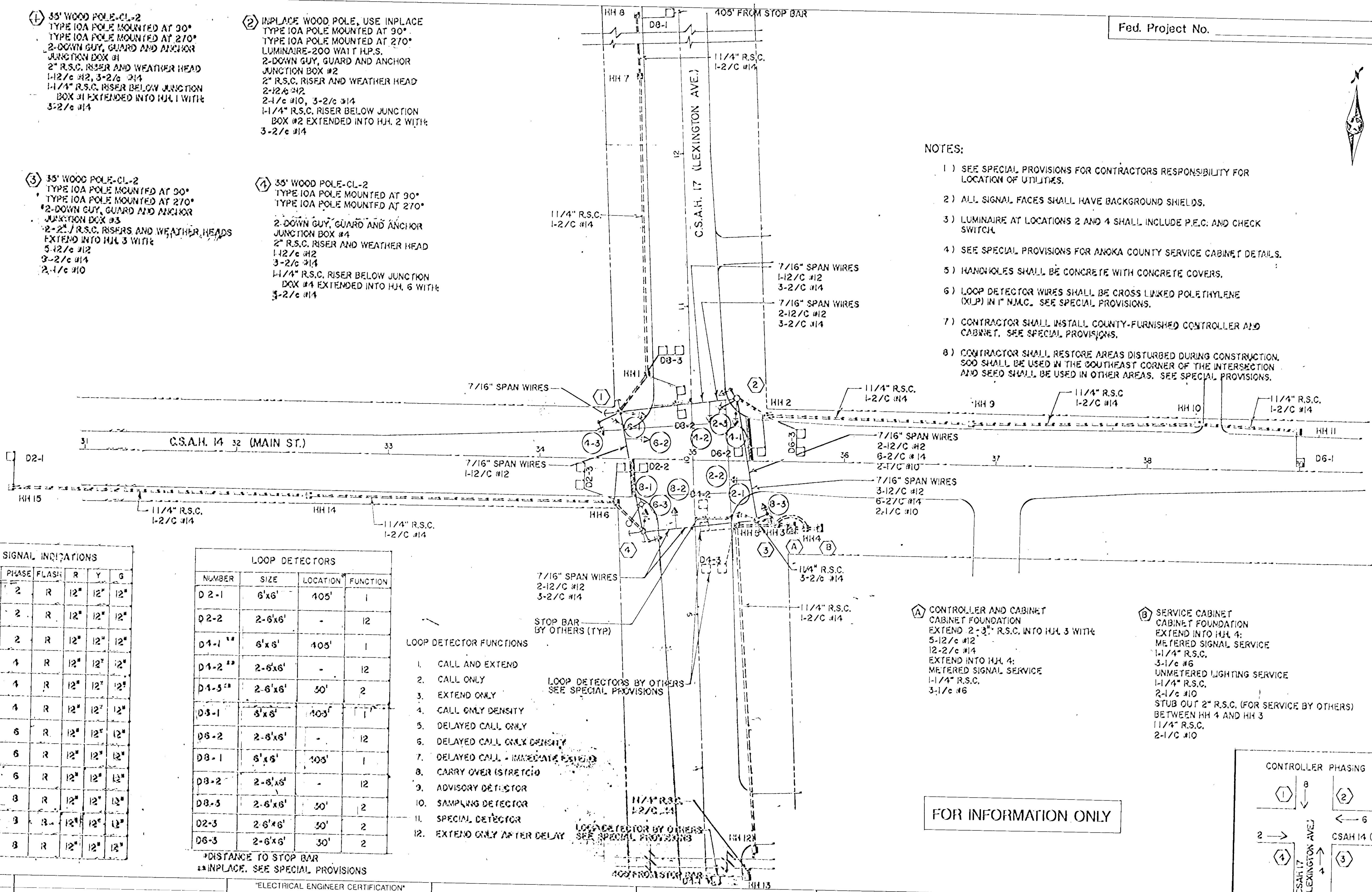


- ① 35' WOOD POLE-CL-2
TYPE IOA POLE MOUNTED AT 90°
TYPE IOA POLE MOUNTED AT 270°
2-DOWN GUY, GUARD AND ANCHOR
JUNCTION BOX #1
2" R.S.C. RISER AND WEATHER HEAD
1-1/2" R.S.C. #12, 3-2/C #14
1-1/4" R.S.C. RISER BELOW JUNCTION
BOX #1 EXTENDED INTO HJ. 1 WITH:
3-2/C #14
- ② INPLACE WOOD POLE, USE INPLACE
TYPE IOA POLE MOUNTED AT 90°
TYPE IOA POLE MOUNTED AT 270°
LUMINAIRE-200 WATT H.P.S.
2-DOWN GUY, GUARD AND ANCHOR
JUNCTION BOX #2
2" R.S.C. RISER AND WEATHER HEAD
2-1/2" R.S.C. #12
2-1/C #10, 3-2/C #14
1-1/4" R.S.C. RISER BELOW JUNCTION
BOX #2 EXTENDED INTO HJ. 2 WITH:
3-2/C #14

- ③ 35' WOOD POLE-CL-2
TYPE IOA POLE MOUNTED AT 90°
TYPE IOA POLE MOUNTED AT 270°
2-DOWN GUY, GUARD AND ANCHOR
JUNCTION BOX #3
2-2" R.S.C. RISERS AND WEATHER HEADS
EXTEND INTO HJ. 3 WITH:
5-1/2" R.S.C. #12
3-2/C #14
2-1/C #10
- ④ 35' WOOD POLE-CL-2
TYPE IOA POLE MOUNTED AT 90°
TYPE IOA POLE MOUNTED AT 270°
2-DOWN GUY, GUARD AND ANCHOR
JUNCTION BOX #4
2" R.S.C. RISER AND WEATHER HEAD
1-1/2" R.S.C. #12
3-2/C #14
1-1/4" R.S.C. RISER BELOW JUNCTION
BOX #4 EXTENDED INTO HJ. 6 WITH:
3-2/C #14

NOTES:

- 1) SEE SPECIAL PROVISIONS FOR CONTRACTORS RESPONSIBILITY FOR LOCATION OF UTILITIES.
- 2) ALL SIGNAL FACES SHALL HAVE BACKGROUND SHIELDS.
- 3) LUMINAIRE AT LOCATIONS 2 AND 4 SHALL INCLUDE P.E.C. AND CHECK SWITCH.
- 4) SEE SPECIAL PROVISIONS FOR ANOKA COUNTY SERVICE CABINET DETAILS.
- 5) HANDHOLES SHALL BE CONCRETE WITH CONCRETE COVERS.
- 6) LOOP DETECTOR WIRES SHALL BE CROSS LINKED POLYETHYLENE (XLPE) IN 1" N.M.C. SEE SPECIAL PROVISIONS.
- 7) CONTRACTOR SHALL INSTALL COUNTY-FURNISHED CONTROLLER AND CABINET. SEE SPECIAL PROVISIONS.
- 8) CONTRACTOR SHALL RESTORE AREAS DISTURBED DURING CONSTRUCTION. SOG SHALL BE USED IN THE SOUTHEAST CORNER OF THE INTERSECTION AND SEED SHALL BE USED IN OTHER AREAS. SEE SPECIAL PROVISIONS.



SIGNAL INDICATIONS						
FACE	PHASE	FLASH	R	Y	G	
2-1	2	R	12"	12"	12"	
2-2	2	R	12"	12"	12"	
2-3	2	R	12"	12"	12"	
4-1	4	R	12"	12"	12"	
4-2	4	R	12"	12"	12"	
4-3	4	R	12"	12"	12"	
6-1	6	R	12"	12"	12"	
6-2	6	R	12"	12"	12"	
6-3	6	R	12"	12"	12"	
8-1	8	R	12"	12"	12"	
8-2	8	R	12"	12"	12"	
8-3	8	R	12"	12"	12"	

LOOP DETECTORS			
NUMBER	SIZE	LOCATION	FUNCTION
D 2-1	6'x6'	405'	1
D 2-2	2-6'x6'	-	12
D 4-1	6'x6'	405'	1
D 4-2	2-6'x6'	-	12
D 4-3	2-6'x6'	50'	2
D 6-1	6'x6'	405'	1
D 6-2	2-6'x6'	-	12
D 8-1	6'x6'	405'	1
D 8-2	2-6'x6'	-	12
D 8-3	2-6'x6'	50'	2
D 2-3	2-6'x6'	50'	2
D 6-3	2-6'x6'	50'	2

LOOP DETECTOR FUNCTIONS

1. CALL AND EXTEND
2. CALL ONLY
3. EXTEND ONLY
4. CALL ONLY DENSITY
5. DELAYED CALL ONLY
6. DELAYED CALL ONLY DENSITY
7. DELAYED CALL - IMMEDIATE EXTEND
8. CARRY OVER (STRETCH)
9. ADVISORY DETECTOR
10. SAMPLING DETECTOR
11. SPECIAL DETECTOR
12. EXTEND ONLY AFTER DELAY

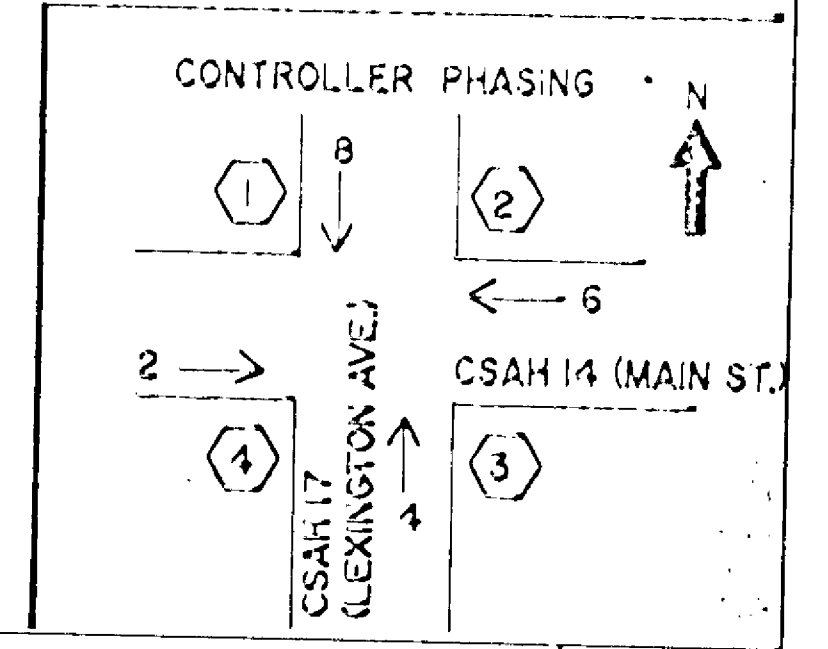
LOOP DETECTORS BY OTHERS
SEE SPECIAL PROVISIONS

LOOP DETECTOR BY OTHERS
SEE SPECIAL PROVISIONS

A) CONTROLLER AND CABINET
CABINET FOUNDATION
EXTEND 2-3" R.S.C. INTO HJ. 3 WITH:
5-1/2" R.S.C. #12
12-2/C #14
EXTEND INTO HJ. 4:
METERED SIGNAL SERVICE
1-1/4" R.S.C.
3-1/C #6

B) SERVICE CABINET
CABINET FOUNDATION
EXTEND INTO HJ. 4:
METERED SIGNAL SERVICE
1-1/4" R.S.C.
3-1/C #6
UNMETERED LIGHTING SERVICE
1-1/4" R.S.C.
2-1/C #10
STUB OUT 2" R.S.C. (FOR SERVICE BY OTHERS)
BETWEEN HJ. 4 AND HJ. 3
1-1/4" R.S.C.
2-1/C #10

FOR INFORMATION ONLY



"ELECTRICAL ENGINEER CERTIFICATION"
I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
Robert A. Ellis
Date: 7-2-72 Reg. No. 5859

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
[Signature]
Date: 7-2-72 Reg. No. 20943



ANOKA COUNTY, MINNESOTA
CITY OF BLAINE

INPLACE SIGNAL SYSTEM "B"
INTERSECTION LAYOUT
MAIN STREET (CSAH 14) AT LEXINGTON AVENUE (CSAH 17)

FILE NO. 92219
DATE

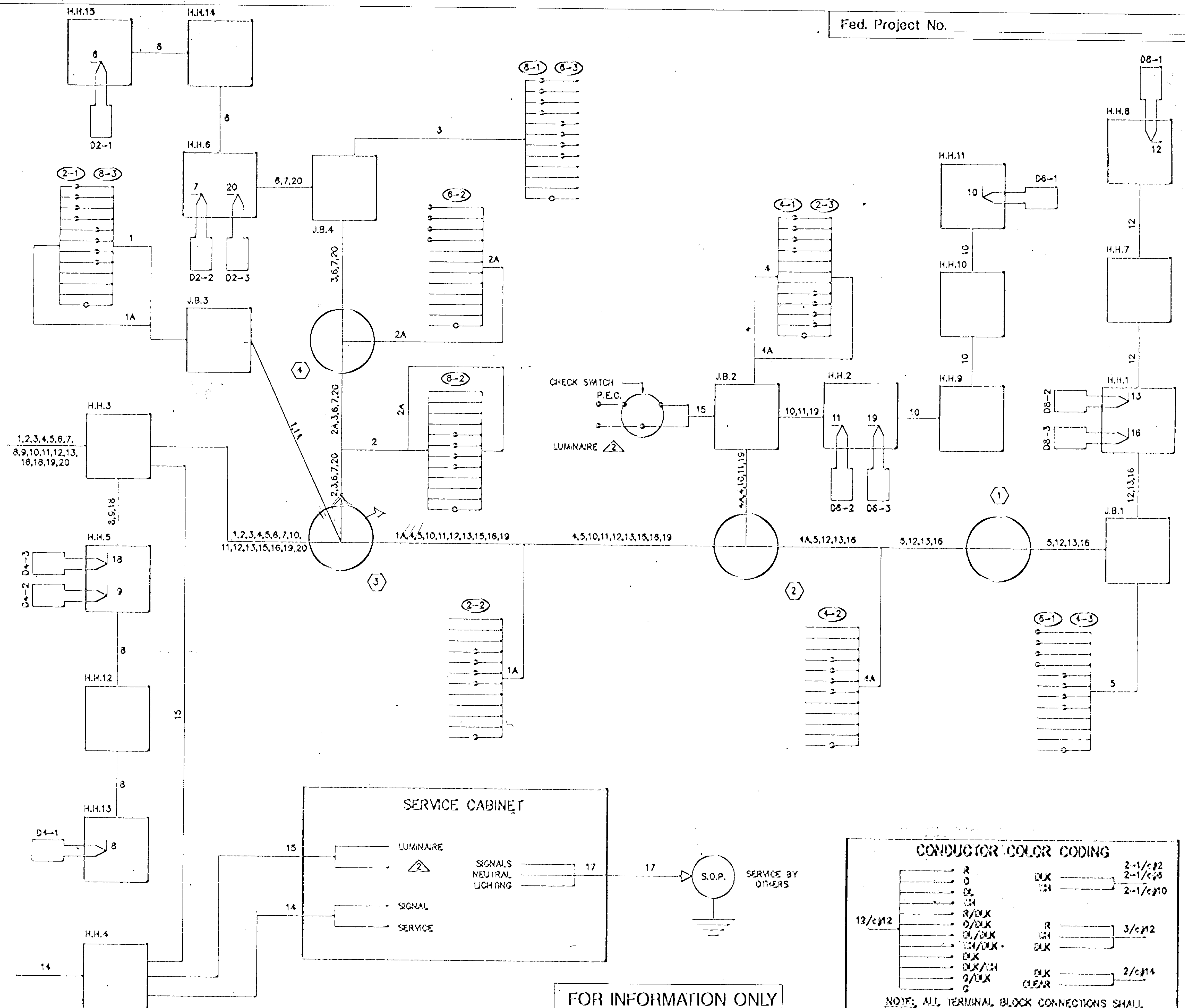
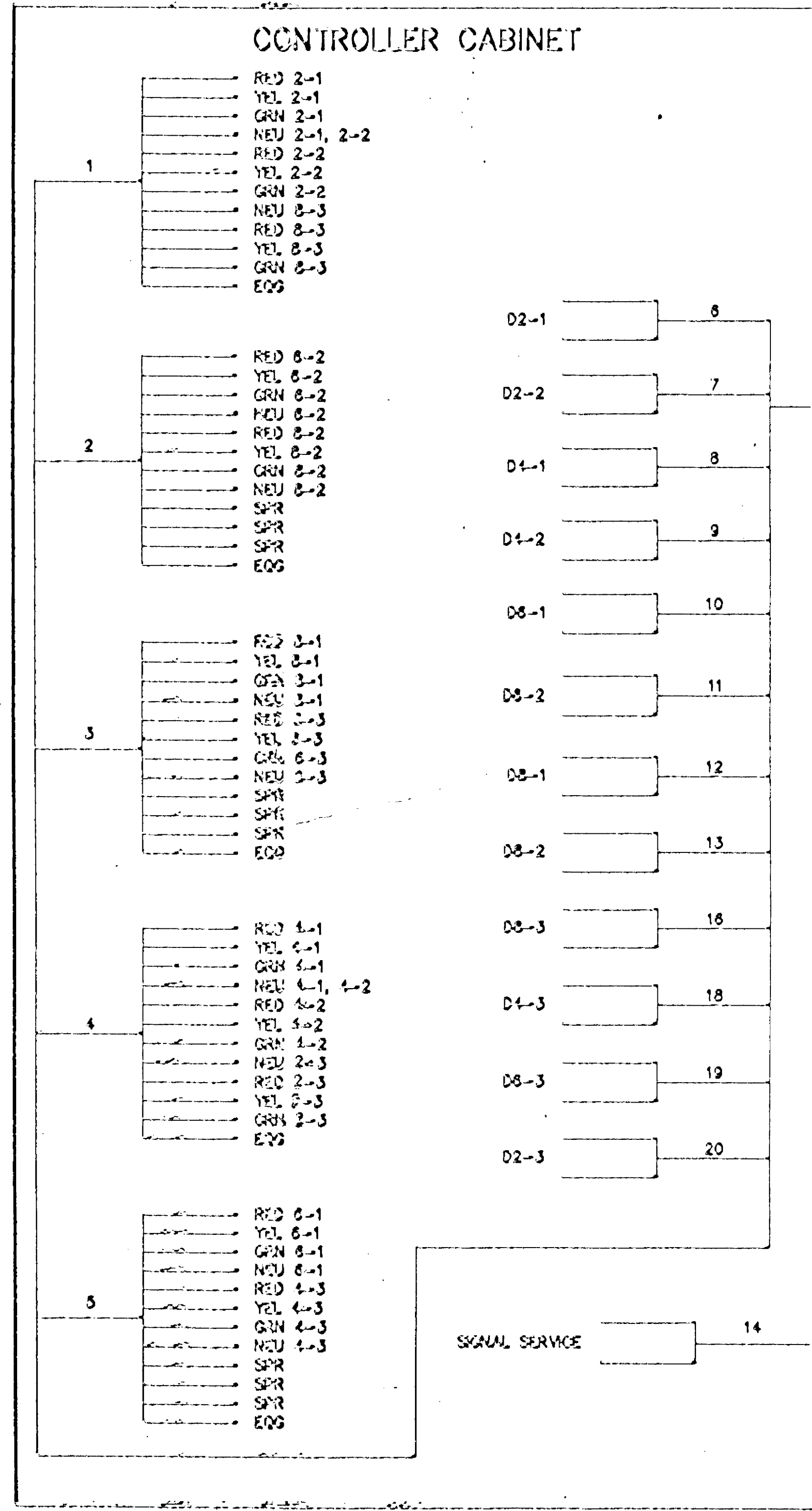
S.A.P. 02-614-14

S.P. _____ C.P. _____

Sheet No. 42 of 117 Sheets

BASE OVERLAYING NO.

Fed. Project No. _____



FOR INFORMATION ONLY

CONDUCTOR COLOR CODING

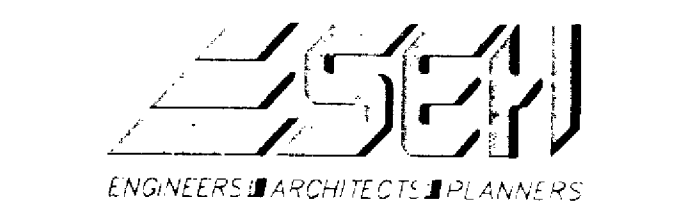
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O	WH	2-1/c#8
BL	WH	2-1/c#10
GR		
R/BLK	R	3/c#2
O/BLK	WH	
BL/BLK	BLK	
WH/BLK		
BLK/WH	BLK	2/c#14
O/BLK	CLEAR	

NOTE: ALL TERMINAL BLOCK CONNECTIONS SHALL BE ARRANGED AS SPECIFIED ABOVE.

NO.	BY	DATE	REVISIONS

"ELECTRICAL ENGINEER CERTIFICATION"
 I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
Robert A. Elber
 Date: 7-2-92 Reg. No. 5659

I hereby certify that this plan was prepared by me or under my direct supervision and that I am a duly Registered Professional Engineer under the laws of the state of Minnesota.
Thomas G. Lohmeyer
 Date: 6-16-92 Reg. No. 20943



ANOKA COUNTY, MINNESOTA
 CITY OF BLAINE

INPLACE SIGNAL SYSTEM "B"
 FIELD WIRING DIAGRAM
 MAIN STREET (CSAH 14) AT LEXINGTON AVENUE (CSAH 17)

FILE NO. 92219
 DATE

PERMANENT SIGN QUANTITIES							
M.U.T.C.D. CODE	PANEL SIZE (INCHES)	PANEL AREA (SQ. FT.)	# OF GROUND POSTS MOUNT INSTALLATIONS	# OF ISLAND MOUNT INSTALLATIONS	SIGN PANEL LEGEND	# POSTS/INSTALLATION	MOUNTING HEIGHT
R1-1	30" X 30"	6.25	2	0	STOP	1	7.0'
R2-1	36" X 48"	12.00	1	0	SPEED LIMIT 55	2	7.0'
R3-8F	36" X 30"	7.50	1	0	LANE DESIGNATION	2	7.0'
R3-X1	30" X 30"	6.25	9	0	RIGHT TURN LANE	1	7.0'
R4-7	30" X 36"	7.50	0	5	KEEP RIGHT	2	7.0'
R3-X2	30" X 30"	6.25	0	5	LEFT TURN LANE		
R4-7	30" X 36"	7.50	0	7	KEEP RIGHT	2	7.0'
X4-2	18" X 18"	4.50	0	14	HAZARD MARKERS		
TYPE III	8 FT.	24.00	4	0	BARRICADE	2	
R3-7(R)	30" X 30"	6.25	1	0	RIGHT LANE MUST TURN RIGHT	1	7.0'
R3-2	24" X 24"	4.00	0	1	NO LEFT TURN	1	7.0'
R5-1	30" X 30"	6.25	0	6	DO NOT ENTER	1	7.0'
	96" X 60"	10.00	1	0	RIGHT LANE ENDS 850' AHEAD	2	7.0'

MOUNTED BELOW R4-7

MOUNTED BELOW R4-7

PERMANENT SIGN QUANTITIES							
M.U.T.C.D. CODE	PANEL SIZE (INCHES)	PANEL AREA (SQ. FT.)	# OF GROUND POSTS MOUNT INSTALLATIONS	# OF ISLAND MOUNT INSTALLATIONS	SIGN PANEL LEGEND	# POSTS/INSTALLATION	MOUNTING HEIGHT
W1-7	48" X 24"	8.00	1	0	LARGE ARROW	2	7.0'
X4-2	18" X 18"	4.50	2	0	HAZARD MARKERS		
W3-3	48" X 48"	16.00	4	0	SIGNAL AHEAD	2	7.0'
W4-2	48" X 48"	16.00	6	0	LANE REDUCTION	2	7.0'
W14-3	36" X 48" X 48"	6.00	7	0	NO PASSING ZONE	2	7.0'
M1-6	24" X 24"	4.00	7	0	COUNTY ROUTE MARKER (CSAH 14)	1	7.0'
M1-6	24" X 24"	4.00	6	0	COUNTY ROUTE MARKER (CSAH 17)	1	7.0'
M1-6	24" X 24"	4.00	5	0	COUNTY ROUTE MARKER (C.R. 52)	1	7.0'
M3-1	24" X 12"	2.00	5	0	NORTH CARDINAL DIRECTION MARKER		
M3-2	24" X 12"	2.00	2	0	EAST CARDINAL DIRECTION MARKER		
M3-3	24" X 12"	2.00	4	0	SOUTH CARDINAL DIRECTION MARKER		
M3-4	24" X 12"	2.00	2	0	WEST CARDINAL DIRECTION MARKER		
M2-1	21" X 15"	2.19	2	0	JUNCTION MARKER		
M6-4	21" X 15"	2.19	5	0	DIRECTIONAL ARROW (DBL. HD.)		6.0' (MIN.)
M5-1R	21" X 15"	2.19	1	0	ADVANCE TURN ARROW (RIGHT)		6.0' (MIN.)
M5-1L	21" X 15"	2.19	1	0	ADVANCE TURN ARROW (LEFT)		6.0' (MIN.)
M6-1R	21" X 15"	2.19	2	0	DIRECTIONAL ARROW (RIGHT)		6.0' (MIN.)
M6-1L	21" X 15"	2.19	2	0	DIRECTIONAL ARROW (LEFT)		6.0' (MIN.)

MOUNTED BELOW W1-7

MOUNTED ABOVE M1-6

MOUNTED ABOVE M1-6

MOUNTED ABOVE M1-6

MOUNTED ABOVE M1-6

MOUNTED ABOVE M1-6

MOUNTED BELOW M1-6

MOUNTED BELOW M1-6

MOUNTED BELOW M1-6

MOUNTED BELOW M1-6

MOUNTED BELOW M1-6

MOUNTED BELOW M1-6

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PLOT DATE: 7-8-92

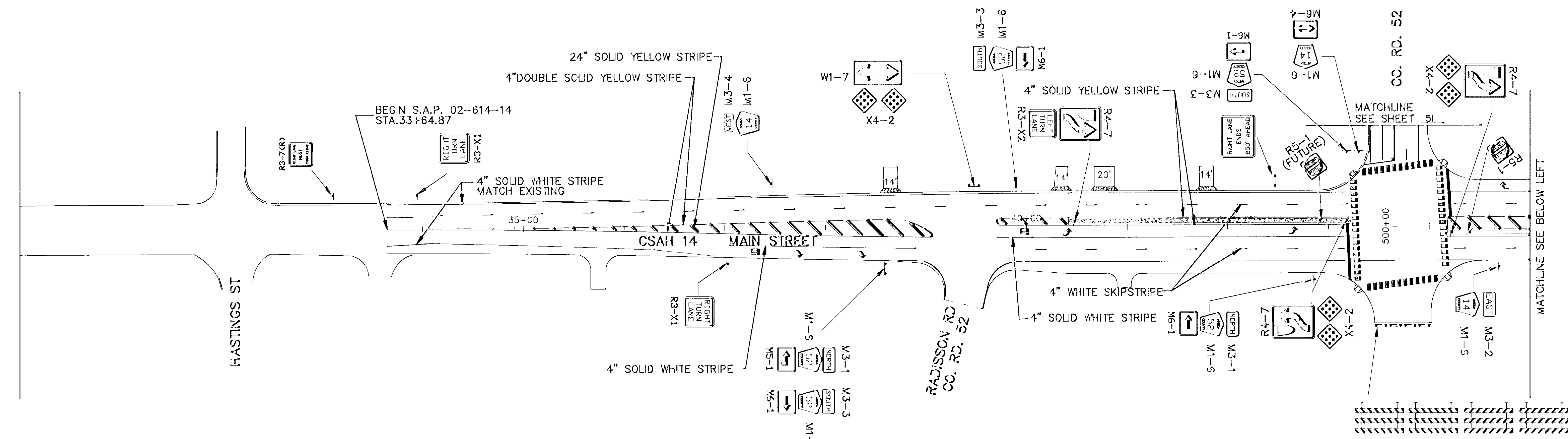
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CHECKED	KWH	AS				
DRAWN	SAP	SHCWN				

Barton-Aschman Associates, Inc.
111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55411

S.A.P. 02-614-14

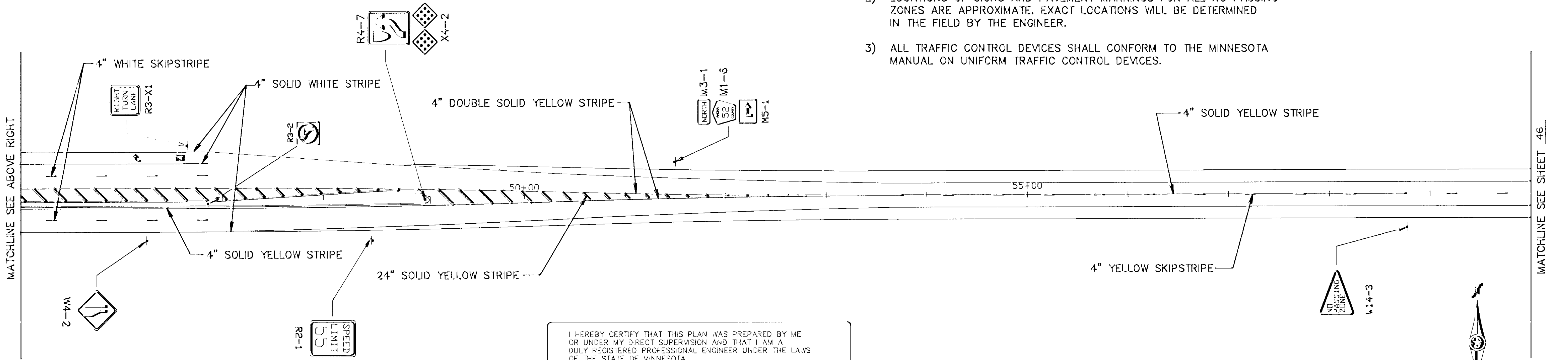
CSAH 14
PERMANENT SIGN TABULATIONS

FILE NO.	SHEET NO.
5006-61	44
DATE	OF
7-8-92	117



NOTES:

- 1) LOCATIONS OF ALL SIGNS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) LOCATIONS OF SIGNS AND PAVEMENT MARKINGS FOR ALL NO PASSING ZONES ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 3) ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.



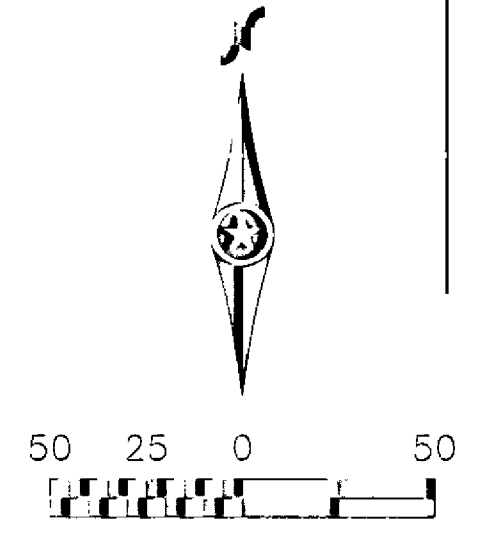
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

SIGNED Kenneth W. Hoyer

DATE 7-9-92 REG. NO. 10235

* APPLIES TO SIGNING & STRIPING PLANS, SHEETS 44 TO 52.

CSAH 14 (MAIN STREET)
STA. 30+00 TO STA. 60+00



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OPERATOR: CAP
PLOT SCALE: 1"=40'
DATE: 7-8-92

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CHECKED	KWH	AS SHOWN			
DRAWN	RRC				

Barton-Aschman Associates, Inc.
111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55401

S.A.P. 02--614--14

SIGNING & STRIPING PLAN

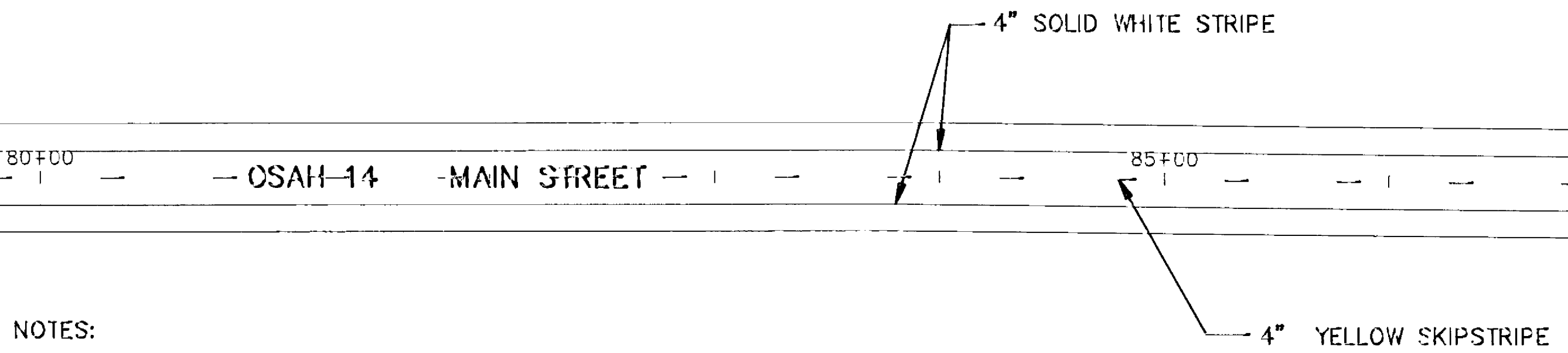
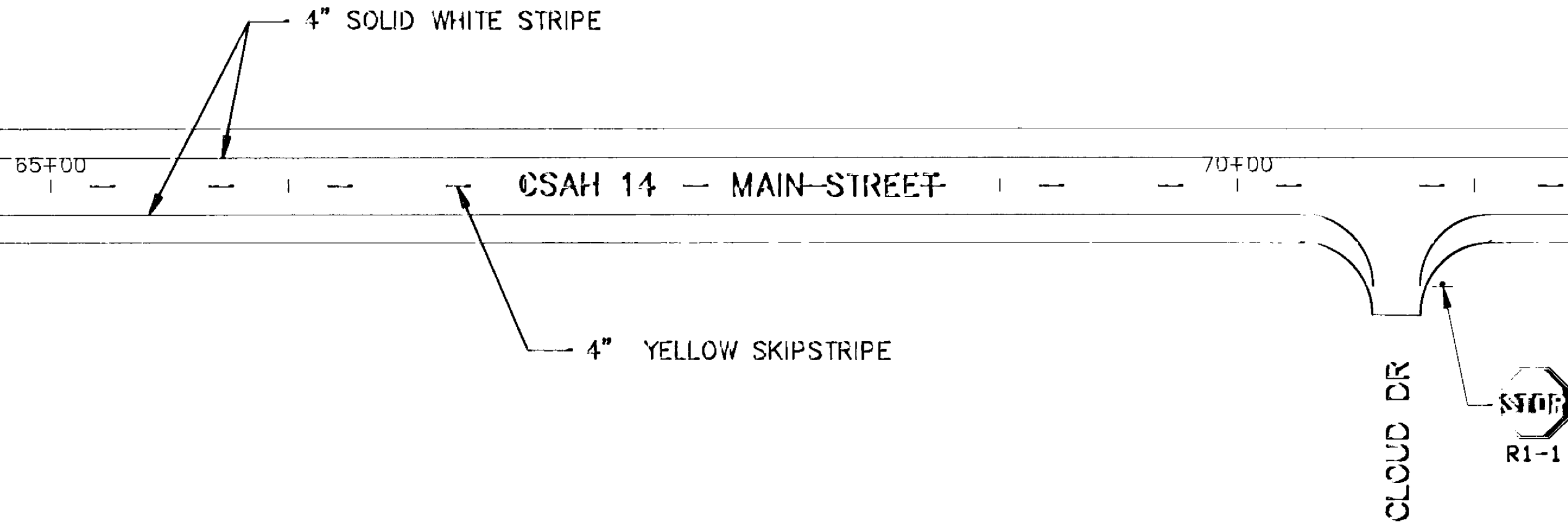
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5006-61	45
DATE	OF 117
7-8-92	

MATCHLINE SEE SHEET 45

MATCHLINE SEE BELOW LEFT

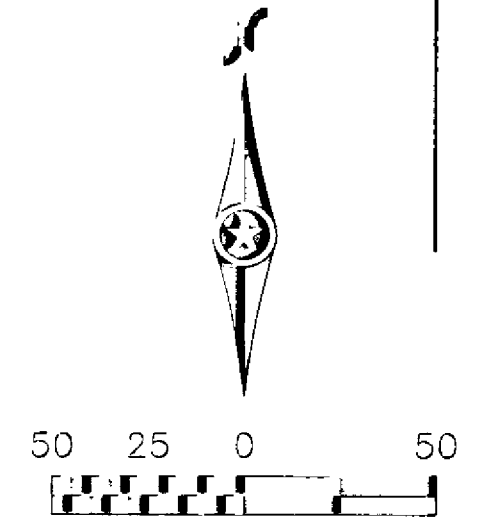
MATCHLINE SEE ABOVE RIGHT

MATCHLINE SEE SHEET 47



- NOTES:
- 1) LOCATIONS OF ALL SIGNS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
 - 2) LOCATIONS OF SIGNS AND PAVEMENT MARKINGS FOR ALL NO PASSING ZONES ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
 - 3) ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

CSAH 14 (MAIN STREET)
 STA. 60+00 TO STA. 90+00



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 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NÖ.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	RRC	SHCWN			

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 111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55401

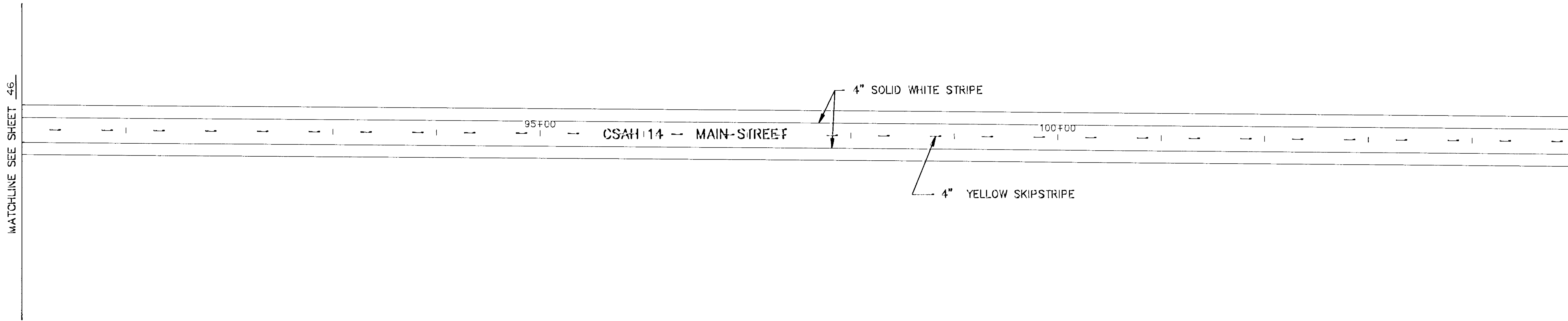
S.A.P. 02-614-14

SIGNING & STRIPING PLAN

FILE NO.	SHEET NO.
5006-61	46
DATE	OF
7-8-92	117

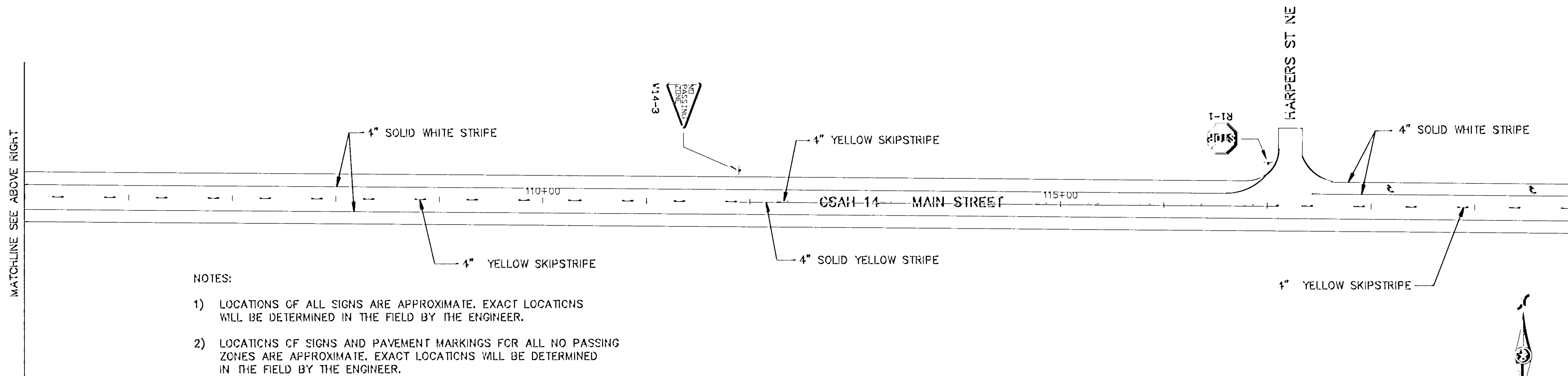
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MATCHLINE SEE ABOVE RIGHT

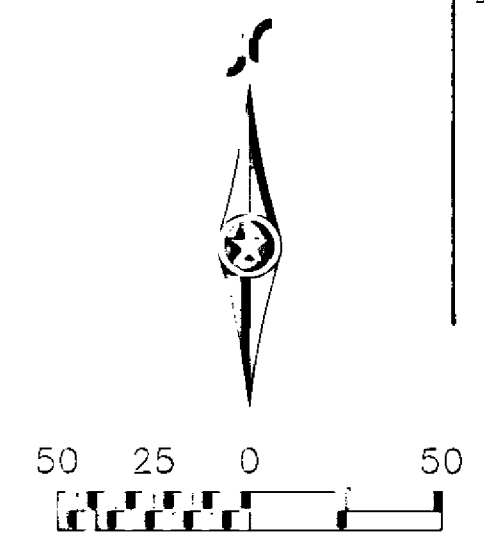
MATCHLINE SEE SHEET 48



NOTES:

- 1) LOCATIONS OF ALL SIGNS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) LOCATIONS OF SIGNS AND PAVEMENT MARKINGS FOR ALL NO PASSING ZONES ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 3) ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

CSAH 14 (MAIN STREET)
STA. 90+00 TO STA. 120+00



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PLOT SCALE: 1"=50'
DATE: 7-8-92

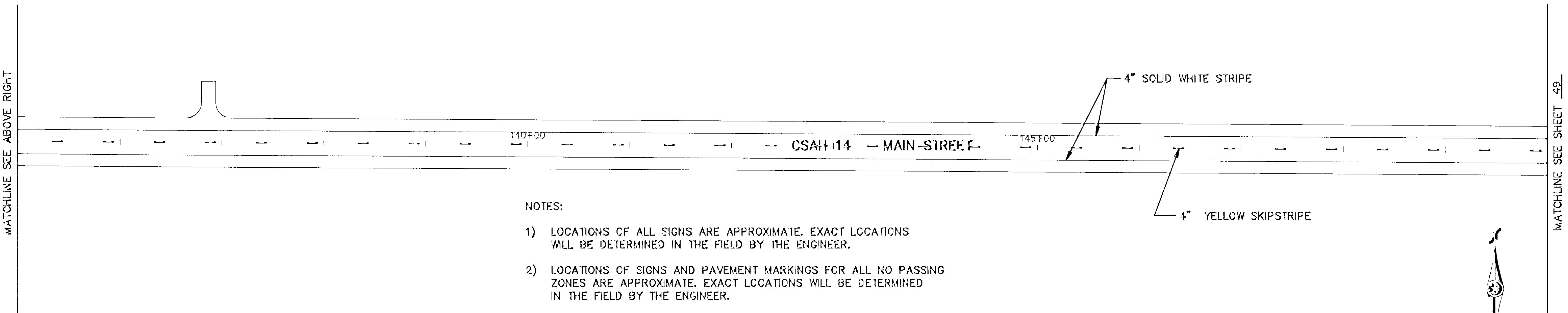
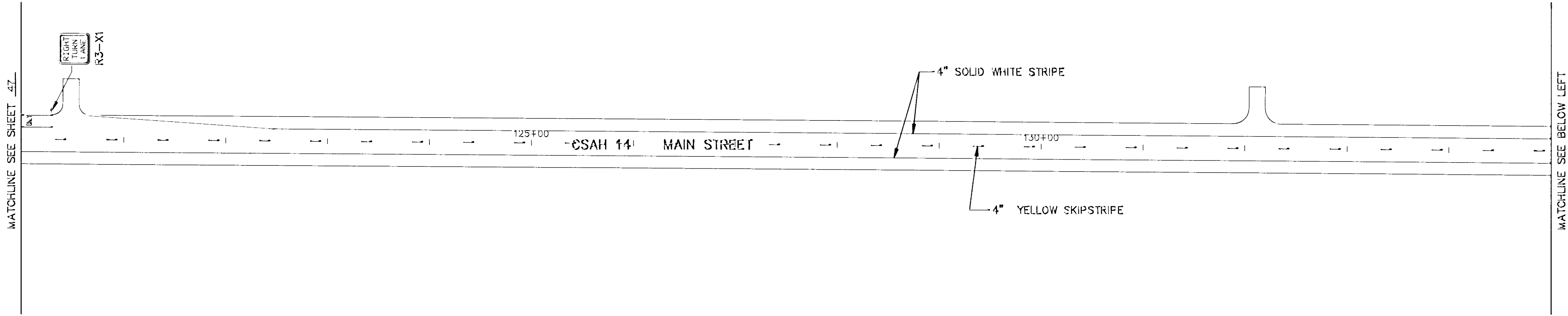
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S.A.P. 02-614-14

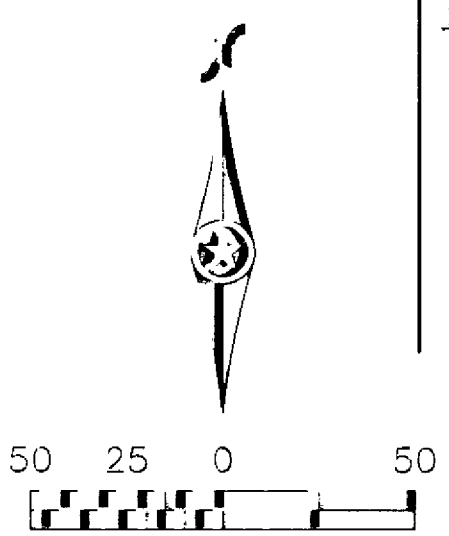
SIGNING & STRIPING PLAN

FILE NO.	SHEET NO.
50C6-61	47
DATE	OF
7-8-92	117



- NOTES:
- 1) LOCATIONS OF ALL SIGNS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
 - 2) LOCATIONS OF SIGNS AND PAVEMENT MARKINGS FOR ALL NO PASSING ZONES ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
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CSAH 14 (MAIN STREET)
STA. 120+00 TO STA. 150+00



DIR & FILE NAME: S:\WORK\5555.DWG
DATE: 7-8-92
PLOT SCALE: 1"=50'

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	RRC	SHCWN			

 **Barton-Aschman Associates, Inc.**
111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55461

S.A.P. 02-614-14

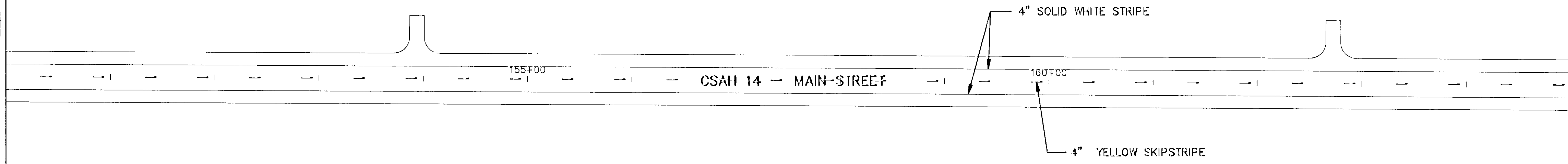
SIGNING & STRIPING PLAN

FILE NO.	5006-61	SHEET NO.	48
DATE	7-8-92		

OF 117

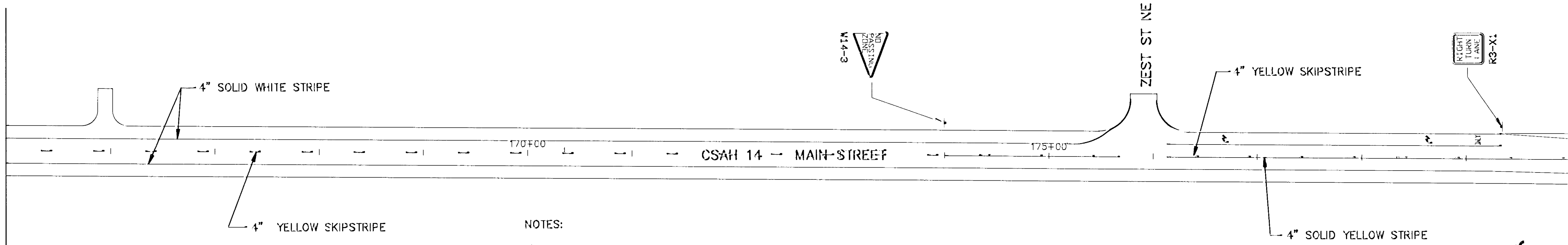
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MATCHLINE SEE ABOVE RIGHT

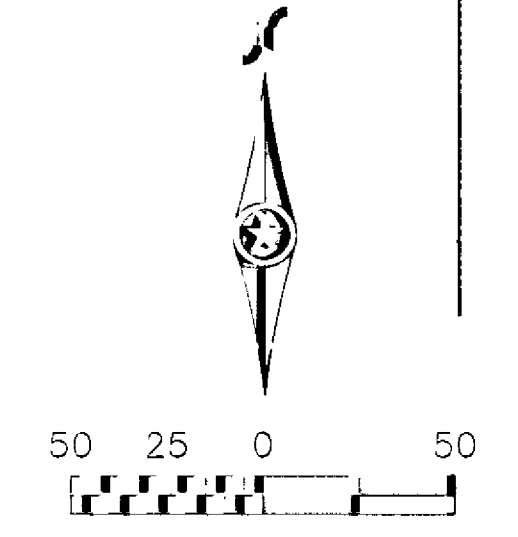
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NOTES:

- 1) LOCATIONS OF ALL SIGNS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) LOCATIONS OF SIGNS AND PAVEMENT MARKINGS FOR ALL NO PASSING ZONES ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 3) ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

CSAH 14 (MAIN STREET)
STA. 150+00 TO STA. 180+00



DIR & FILE NAME: C:\WORK\SSBJMG
PROJECT: 02-614-14
PROF: KWH
DATE: 7/8/92

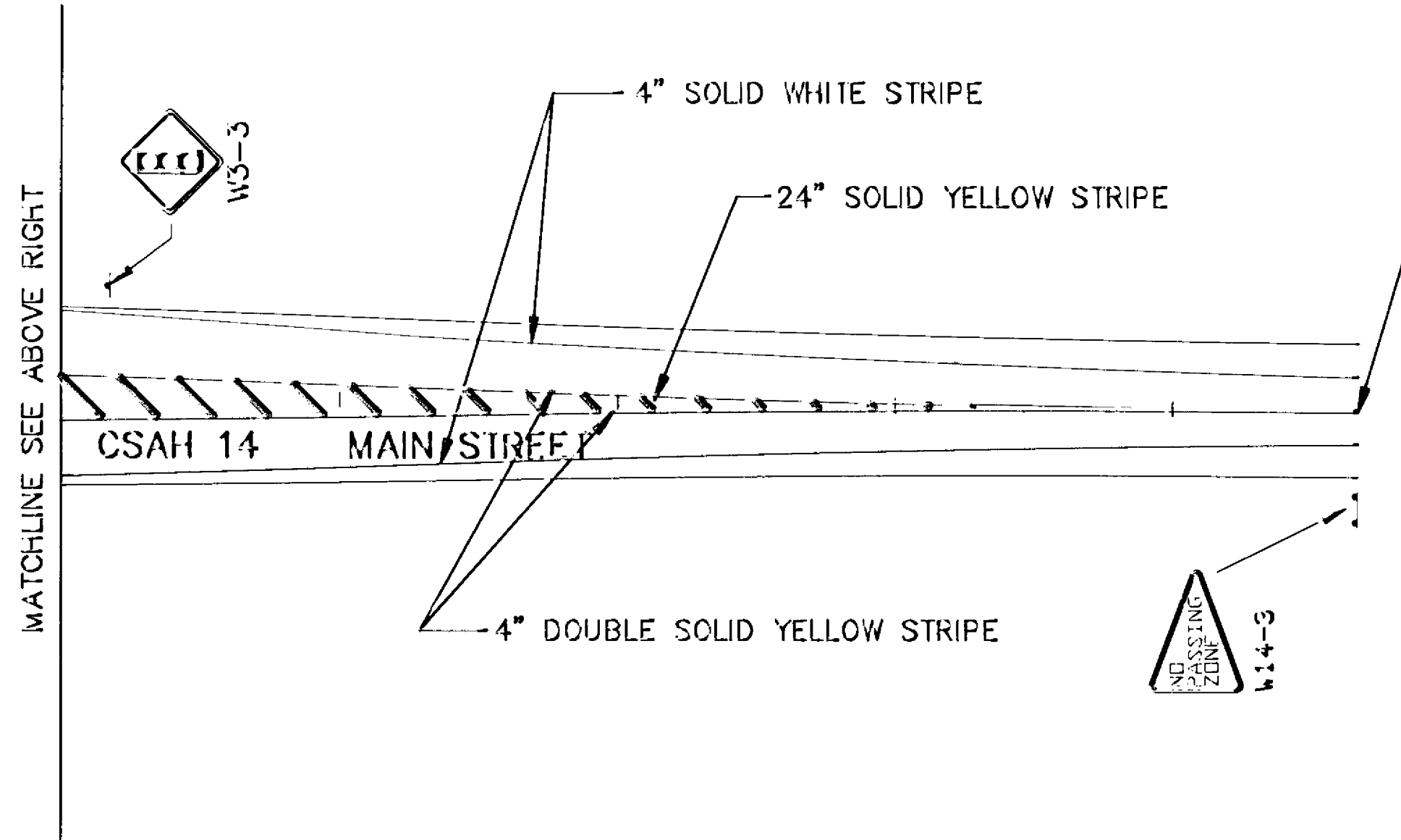
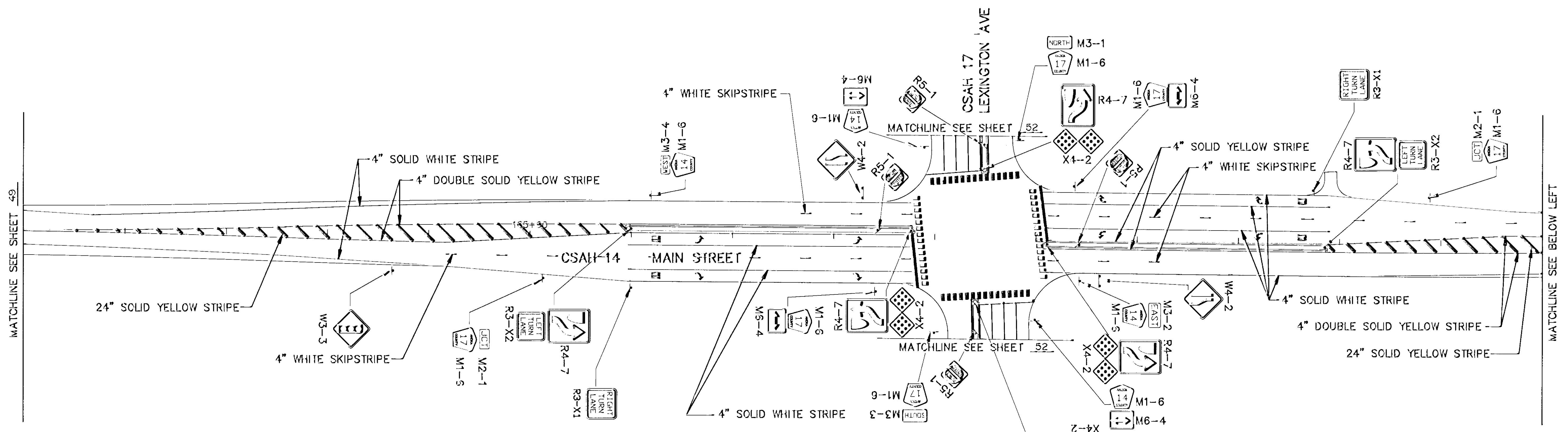
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	RRC	SHOWN			



S.A.P. 02-614-14

SIGNING & STRIPING PLAN

FILE NO.	SHEET NO.
5006-61	49
DATE	
7-8-92	OF 117

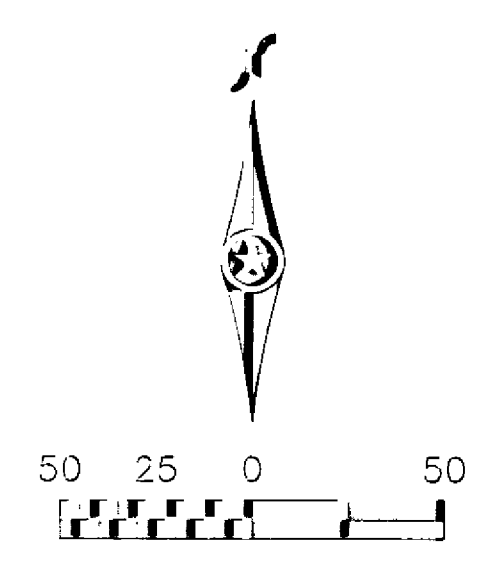


END S.A.P. 02-614-14
 STA. 199+66.82
 MATCH EXISTING

NOTES:

- 1) LOCATIONS OF ALL SIGNS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) LOCATIONS OF SIGNS AND PAVEMENT MARKINGS FOR ALL NO PASSING ZONES ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 3) ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

CSAH 14 (MAIN STREET)
 STA. 180+00 TO STA. 210+00



DIR & FILE NAME: C:\WORK\557.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

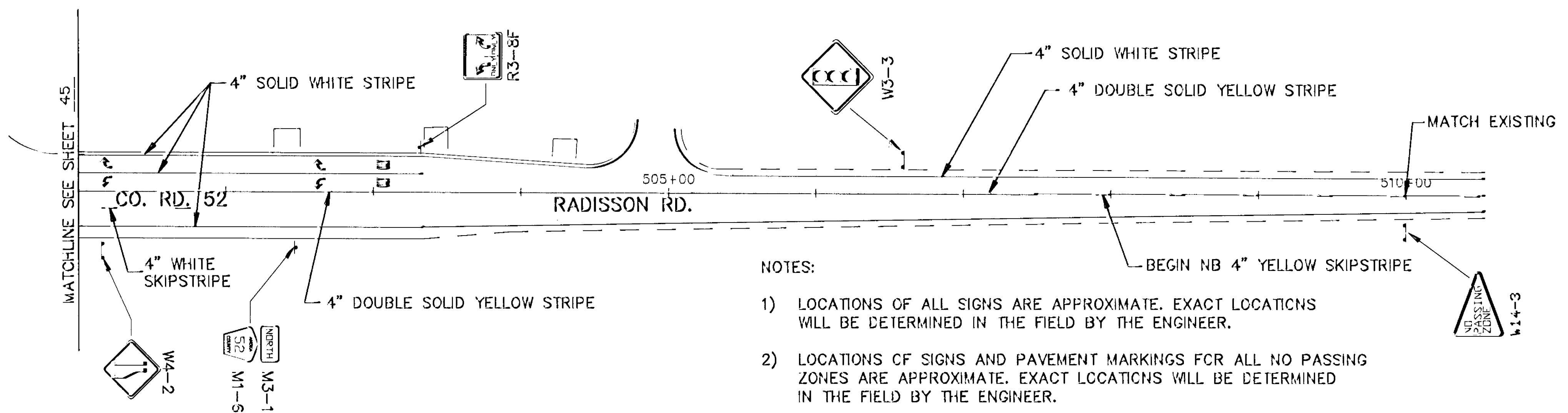
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	RRC	SHOWN			



S.A.P. 02-614-14

SIGNING & STRIPING PLAN

FILE NO.	SHEET NO.
5006-61	50
DATE	OF 117
7-8-92	



NOTES:

- 1) LOCATIONS OF ALL SIGNS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) LOCATIONS OF SIGNS AND PAVEMENT MARKINGS FOR ALL NO PASSING ZONES ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 3) ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

C.R. 52 (RADISSON ROAD)
 STA. 501+00 TO STA. 510+00



DIR & FILE NAME: C:\ANON\SSB.DWG
 OPERATOR: SAP
 PLOT DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	RRC	SHOWN			


Barton-Aschman Associates, Inc.
111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55401

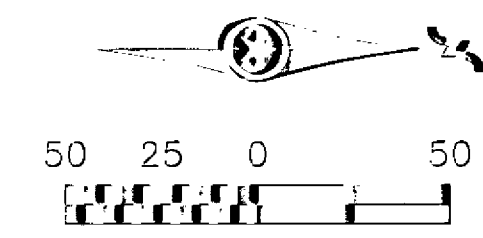
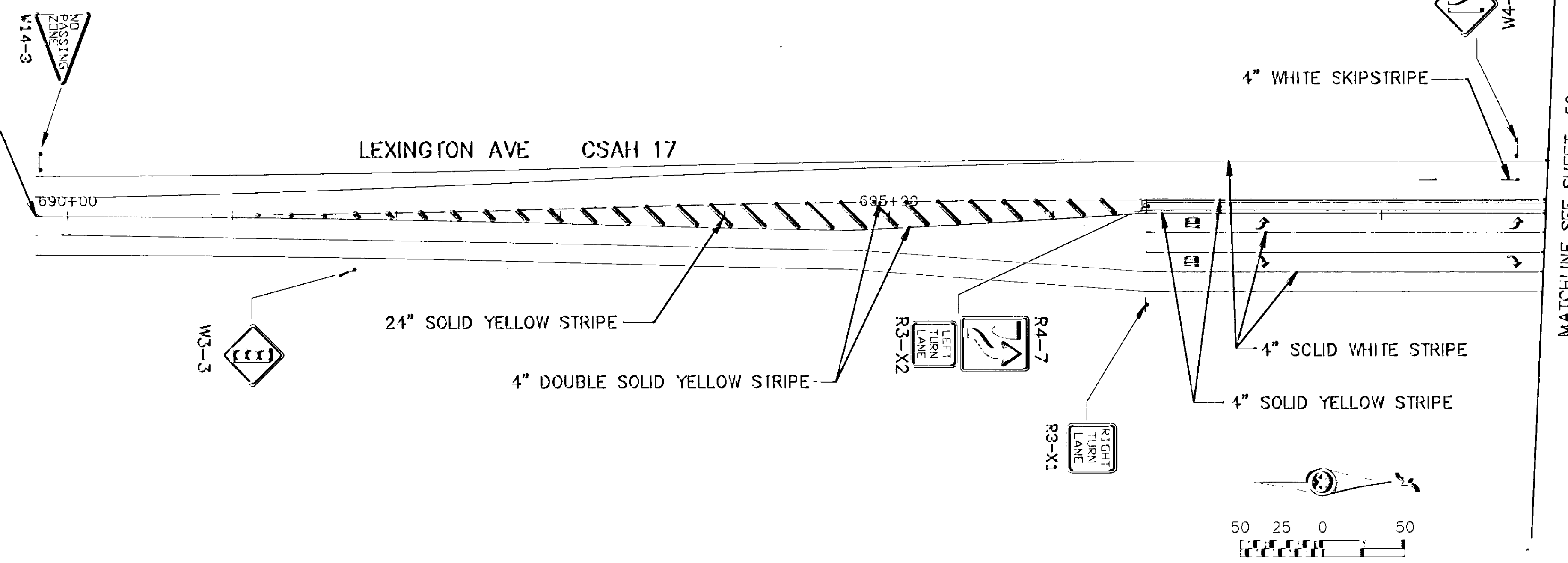
S.A.P. 02-614-14

SIGNING & STRIPING PLAN

FILE NO.	SHEET NO.
50C6-61	51
DATE	
7-8-92	OF 117

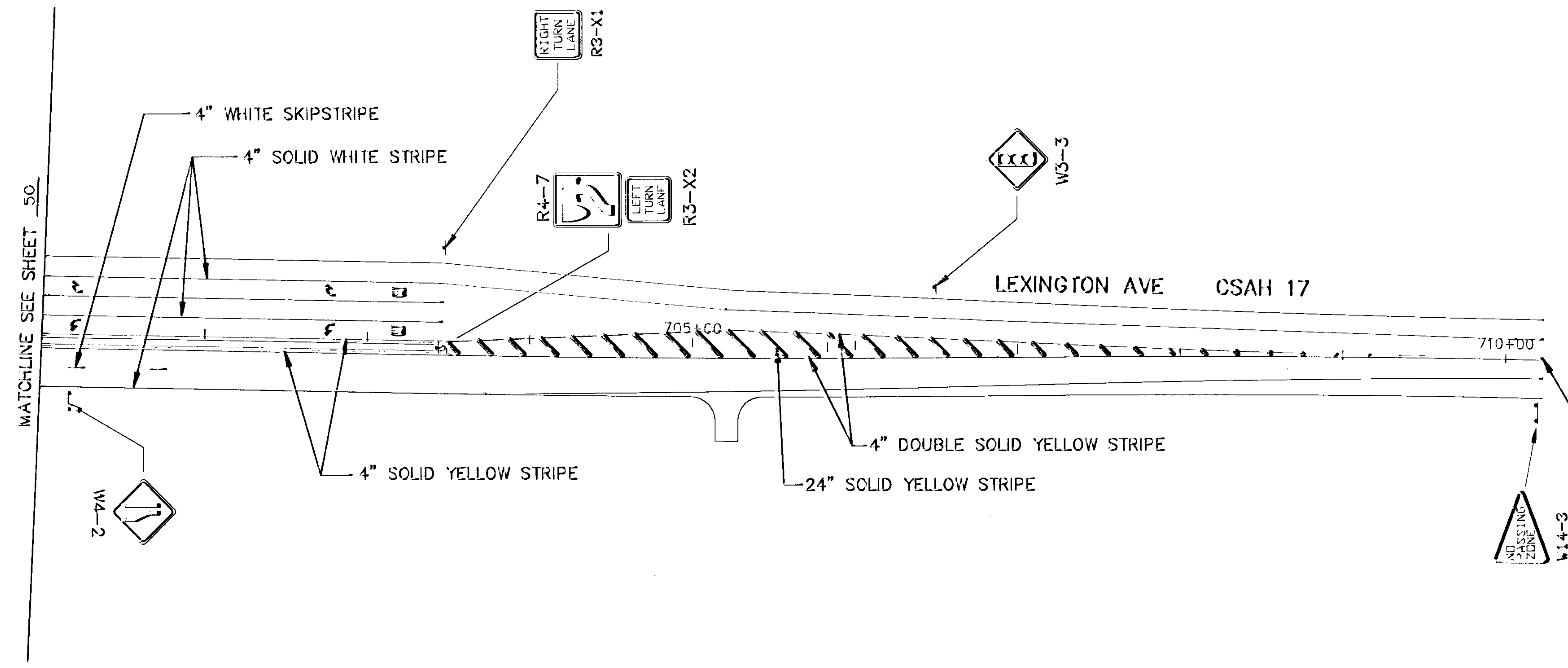
BEGIN S.A.P. 02-614-14
STA. 689+80.46
MATCH EXISTING

LEXINGTON AVE CSAH 17

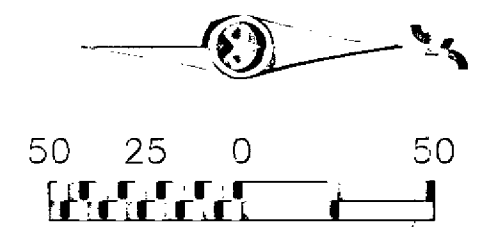


NOTES:

- 1) LOCATIONS OF ALL SIGNS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 2) LOCATIONS OF SIGNS AND PAVEMENT MARKINGS FOR ALL NO PASSING ZONES ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- 3) ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.



END S.A.P. 02-614-14
STA. 710+22.97
MATCH EXISTING



CSAH 17 (LEXINGTON AVE.)
STA. 689+80.46 TO STA. 699+00
STA. 701+00 TO STA. 710+22.97

UIR & FILE NAME: C:\AROKA\559.DWG
 OPERATOR: SAP
 PLOT DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	RRC				

Barton-Aschman Associates, Inc.
111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55401

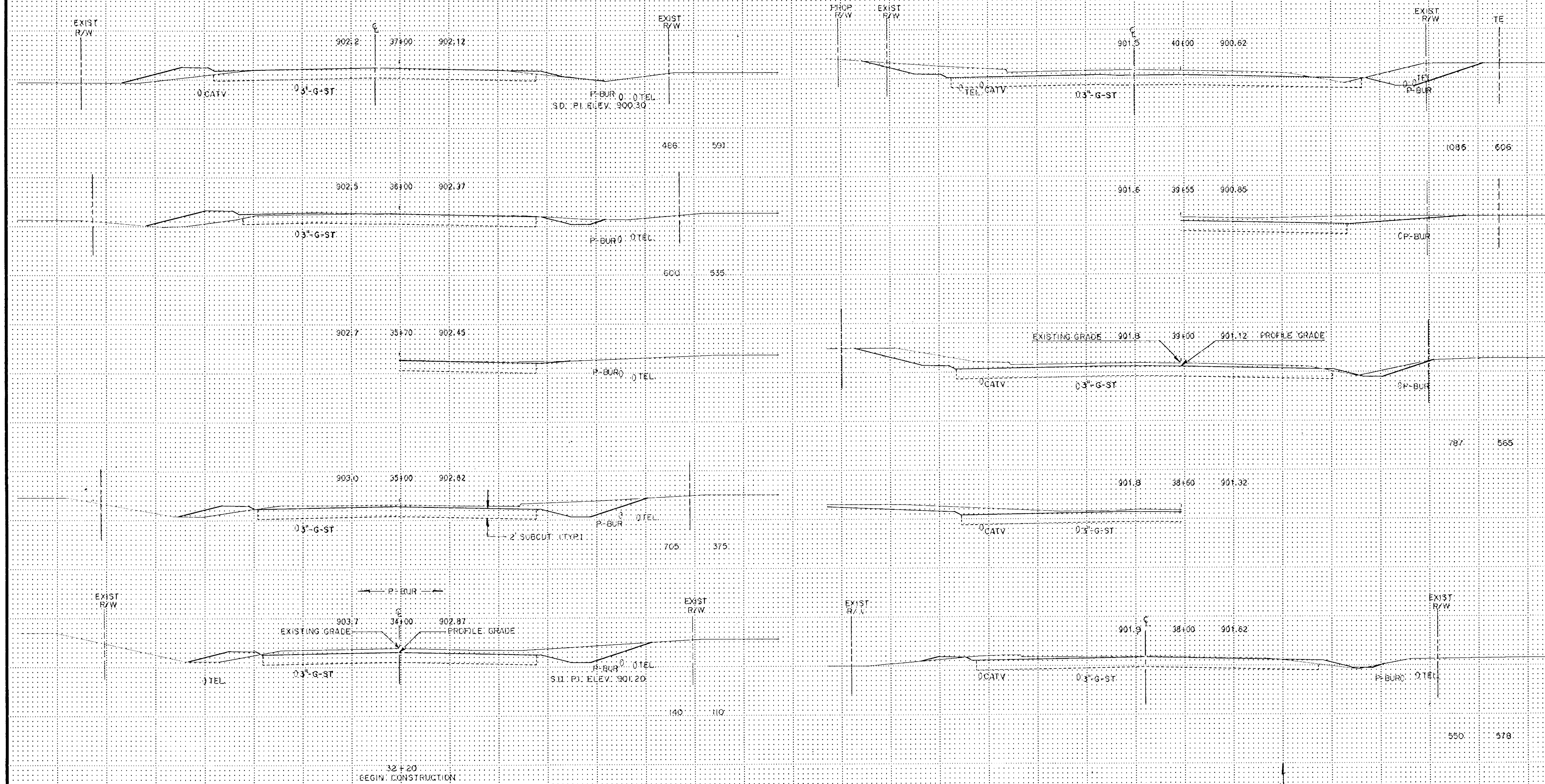
S.A.P. 02-614-14

SIGNING & STRIPING PLAN

FILE NO.	SHEET NO.
5006-61	52
DATE	OF
7-8-92	17

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

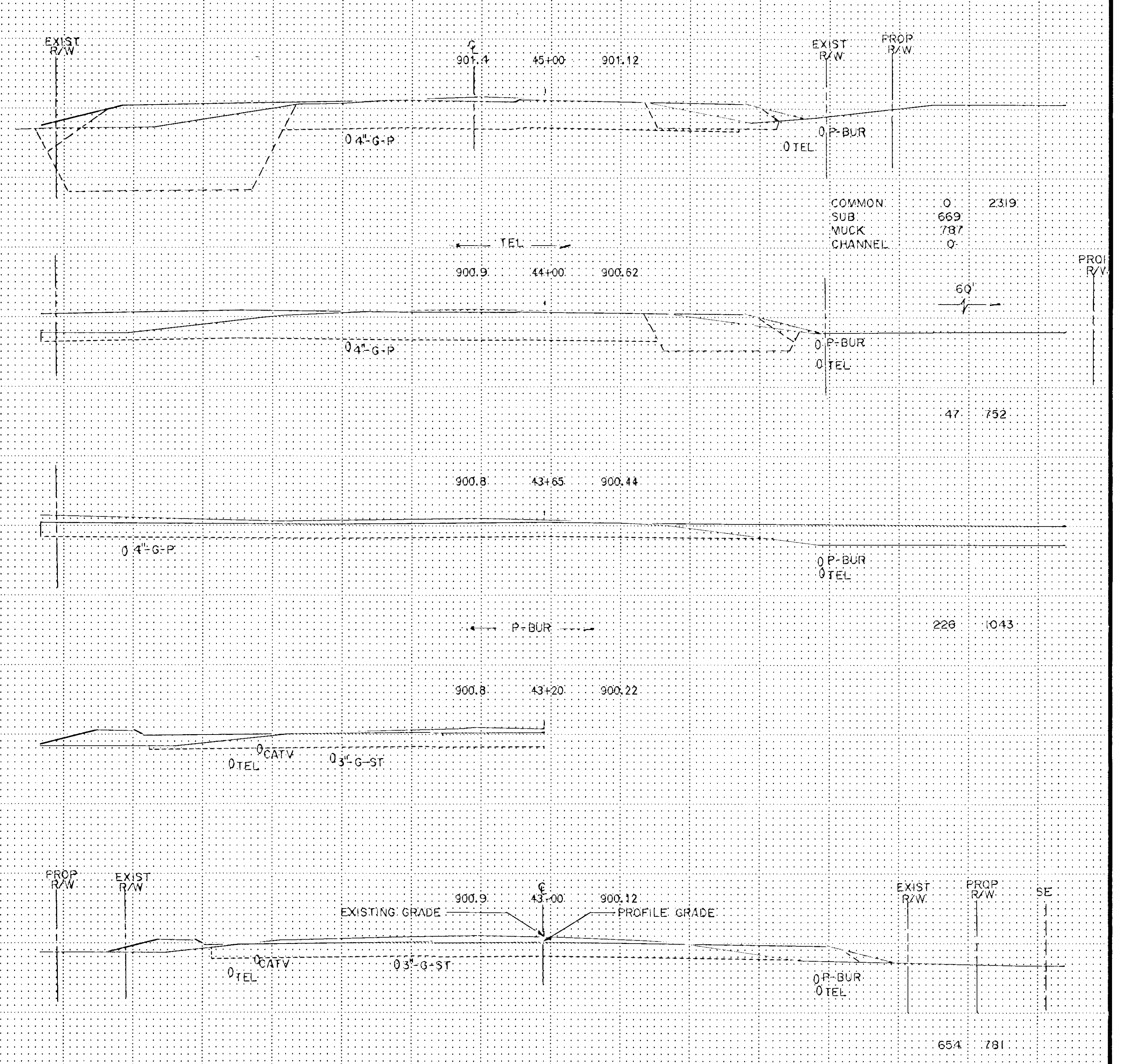
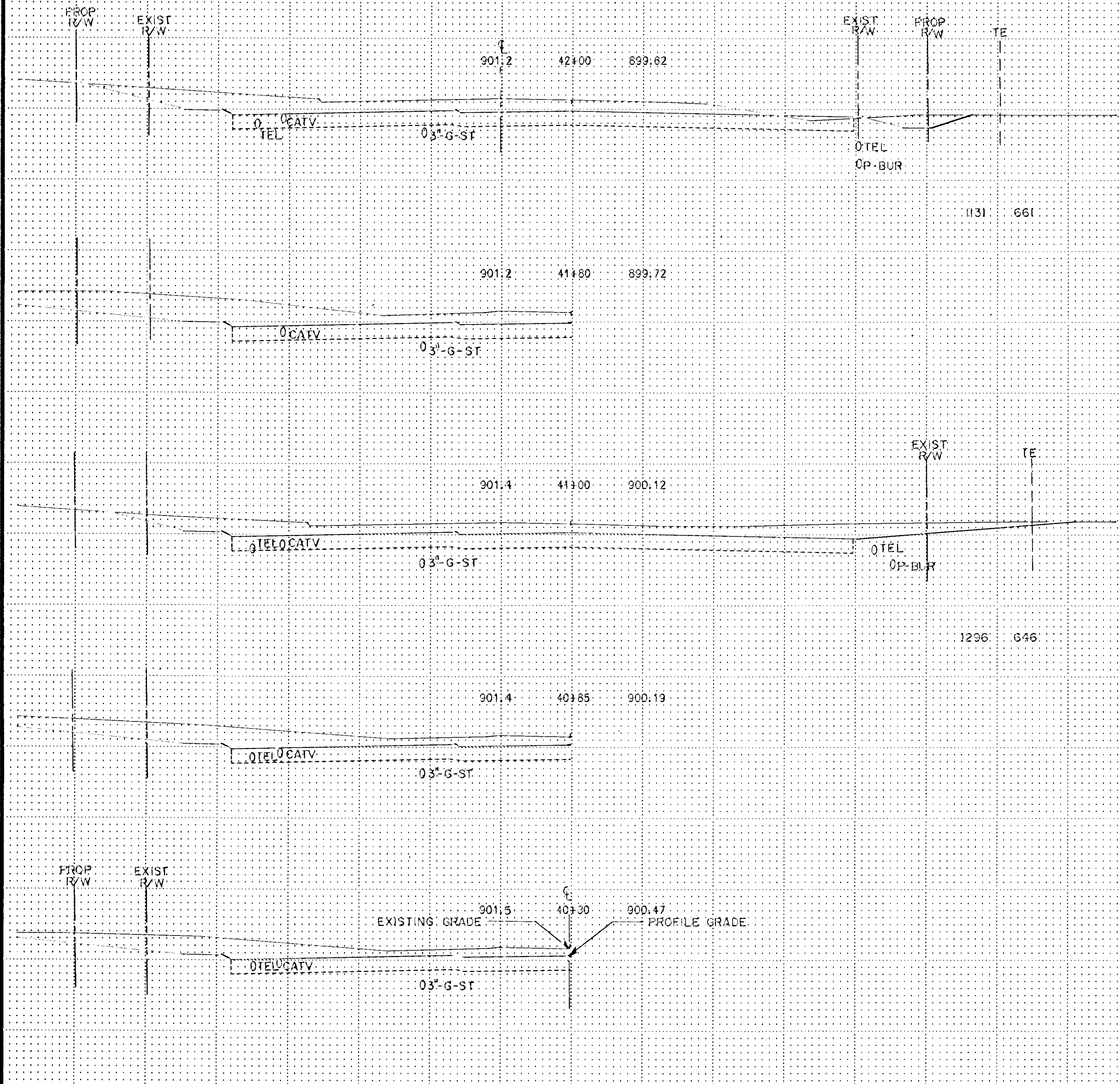
EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION
STA. 34+00 TO STA. 40+00

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

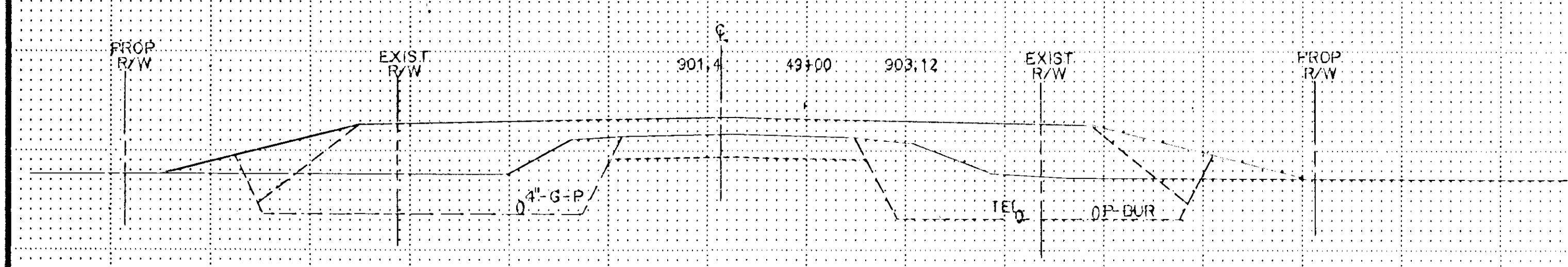


CROSS-SECTION
STA. 40+30 TO STA. 45+00

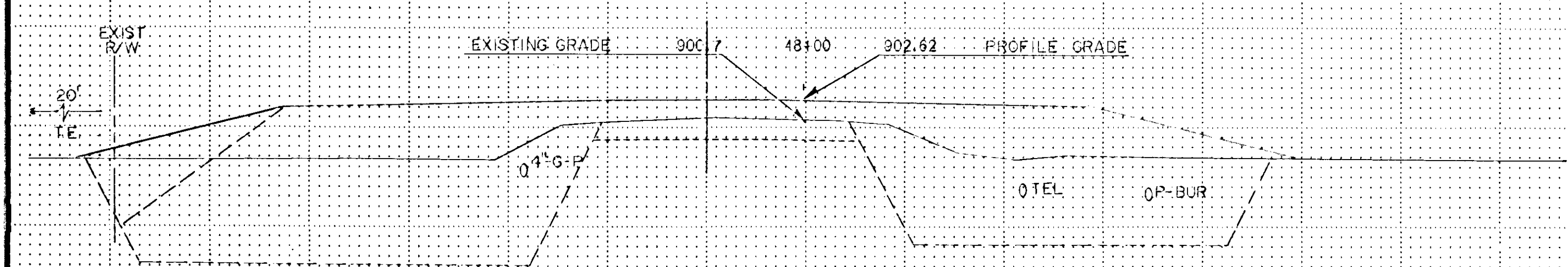
Copy Dimensions Form #1 8/84/85

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

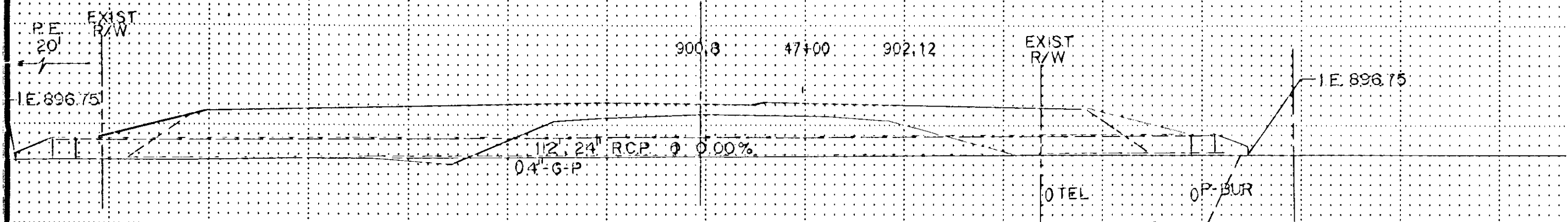
EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



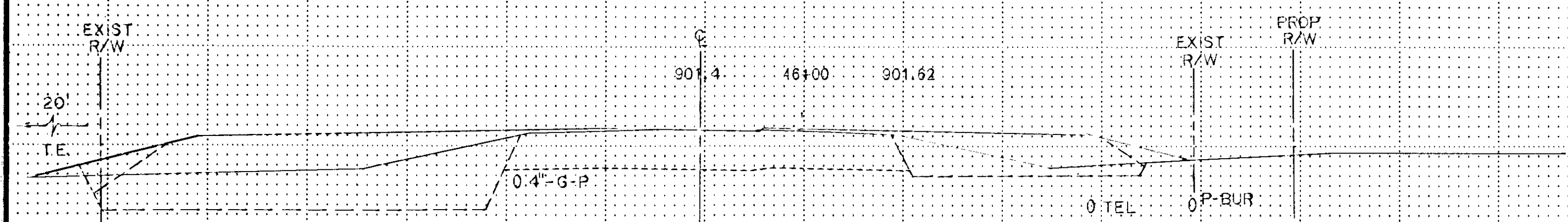
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SUB	294	
MUCK	2161	
CHANNEL	0	



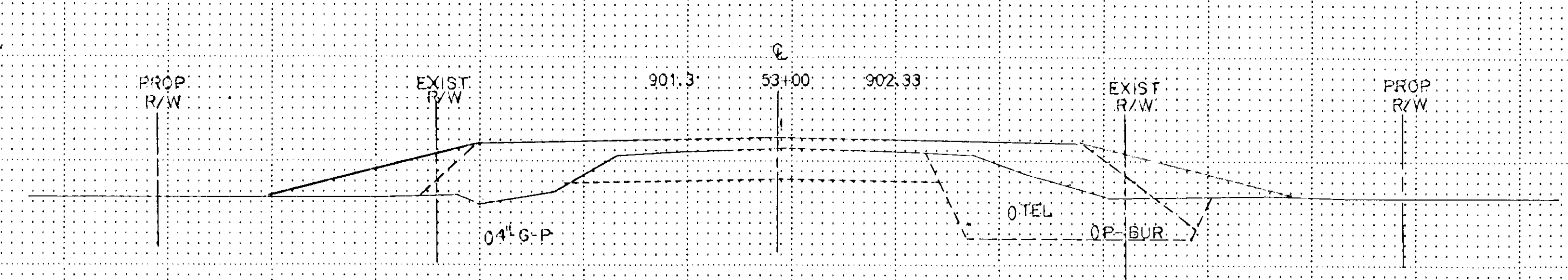
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	3807	
	0	



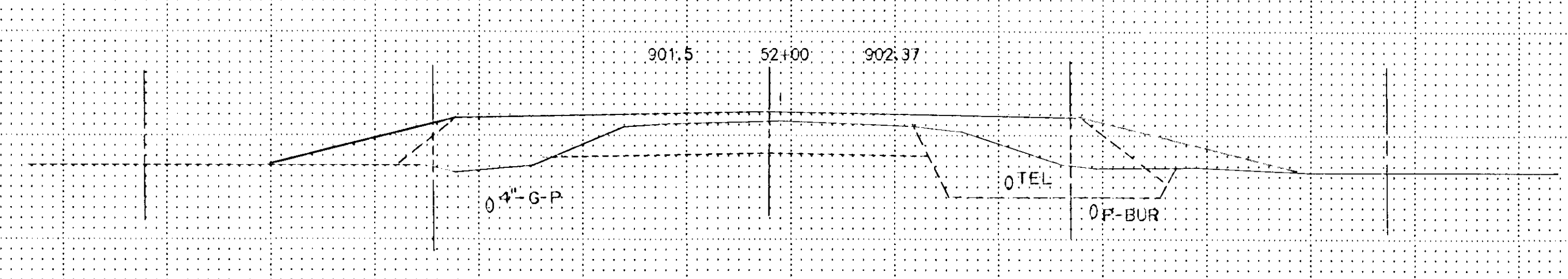
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	2979	
	0	



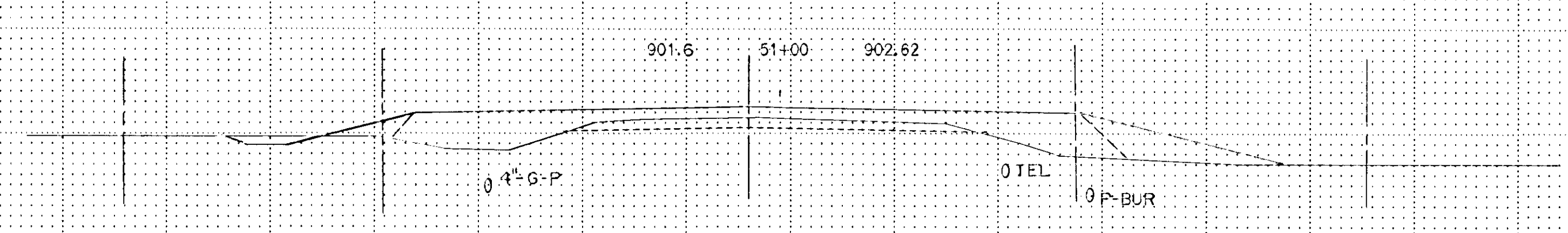
COMMON	0	2524
SUB	504	
MUCK	1081	
CHANNEL	0	



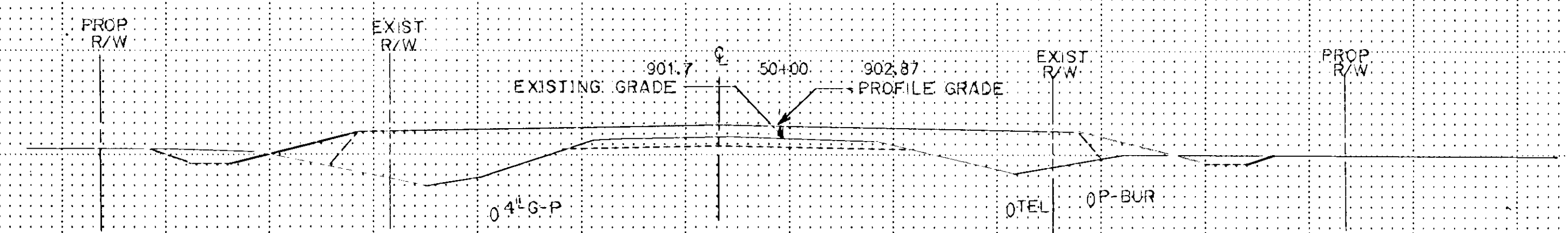
COMMON	0	1459
SUB	456	
MUCK	426	
CHANNEL	0	



	0	1004
	180	
	231	
	183	
	0	



	0	637
	409	

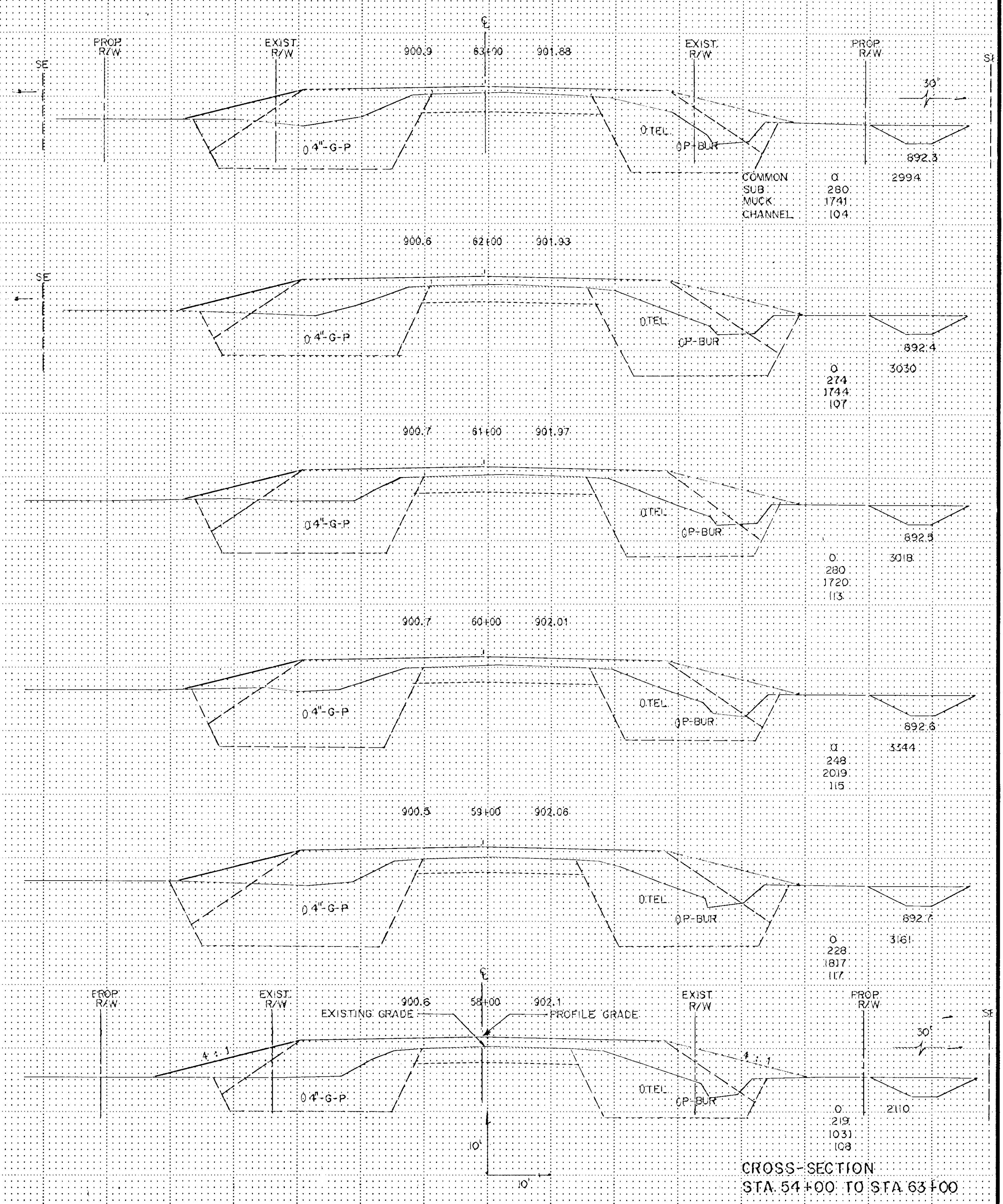
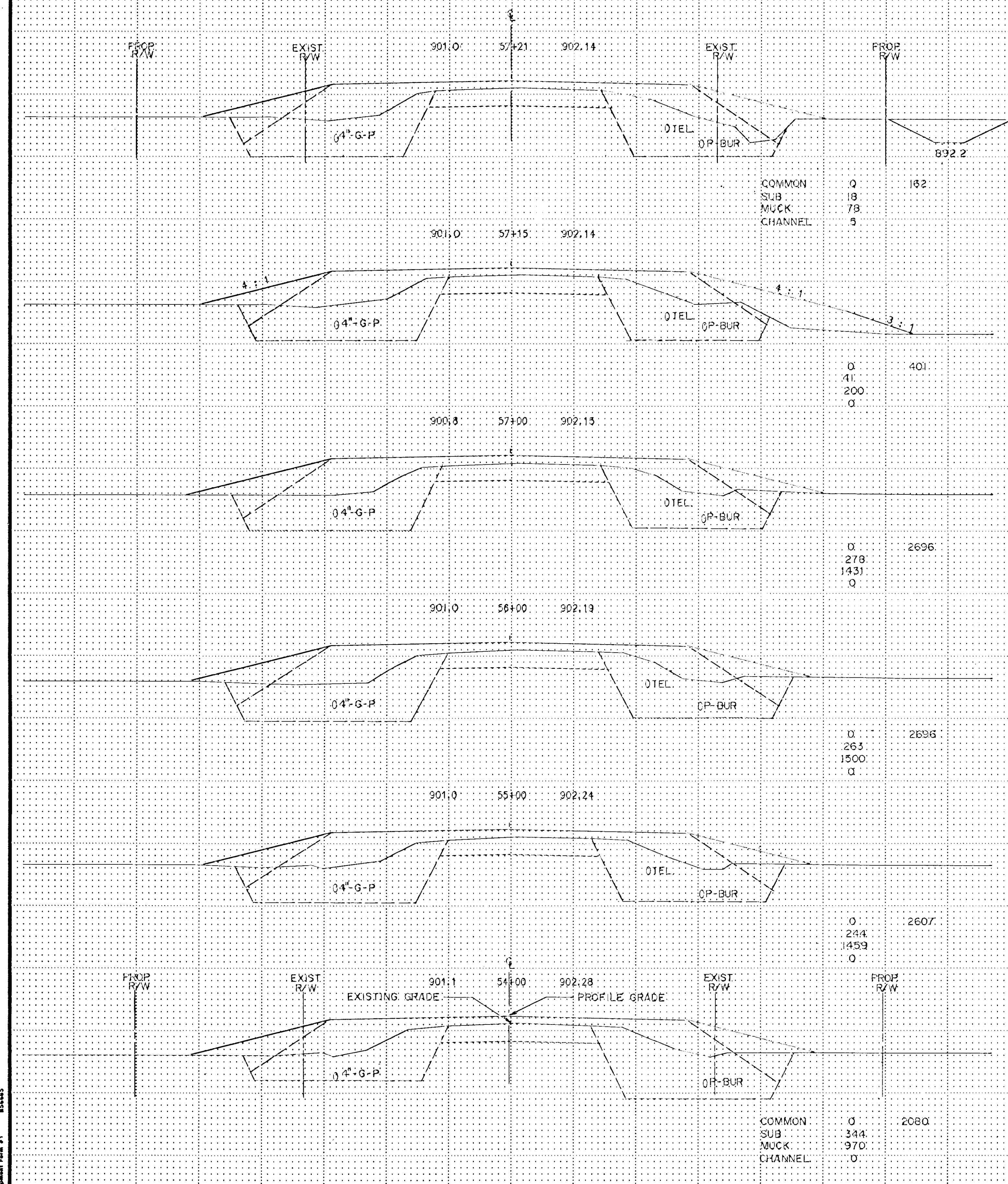


	0	1498
	230	
	170	
	606	
	0	

CROSS-SECTION
STA. 46+00 TO STA. 53+00

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

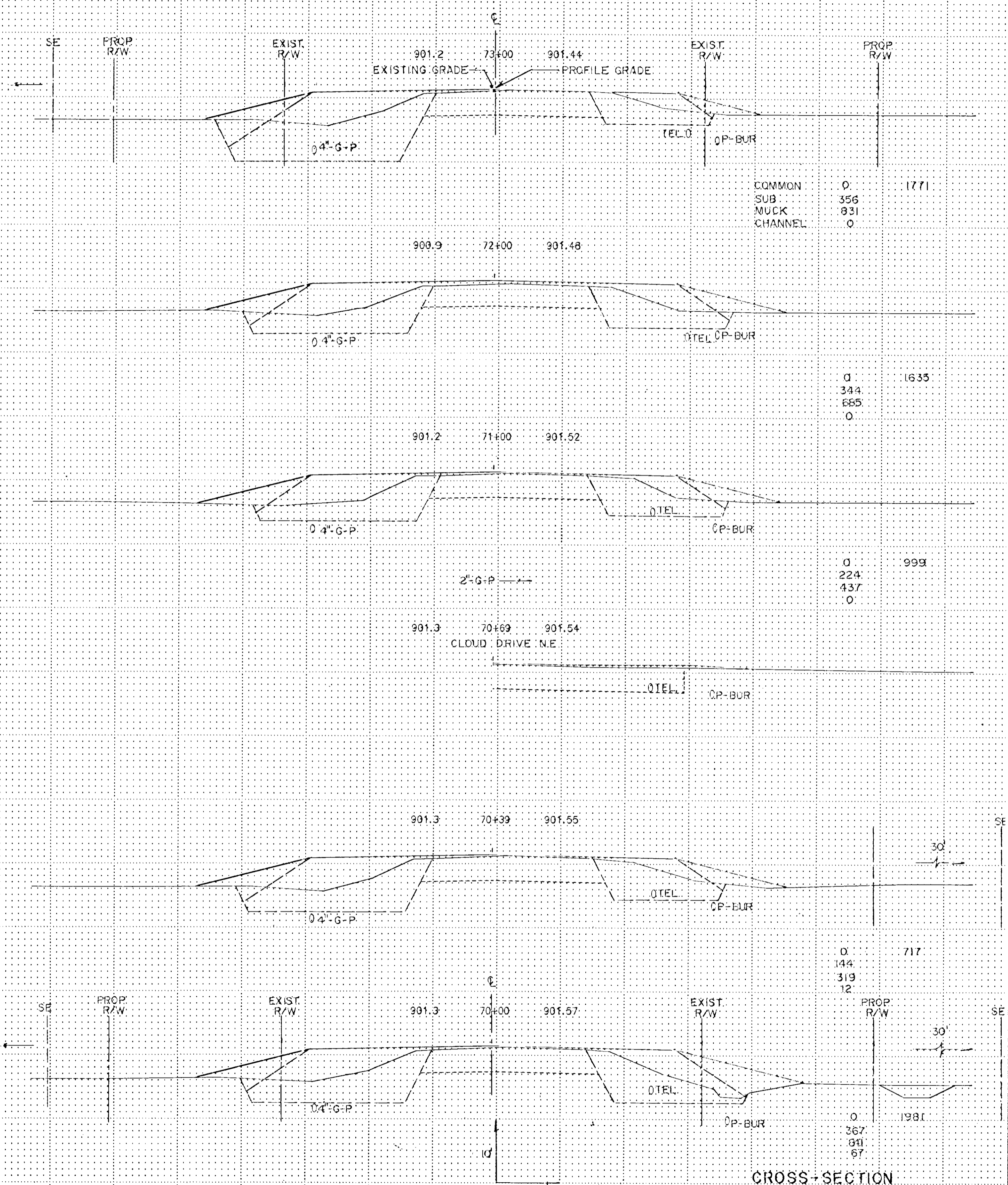
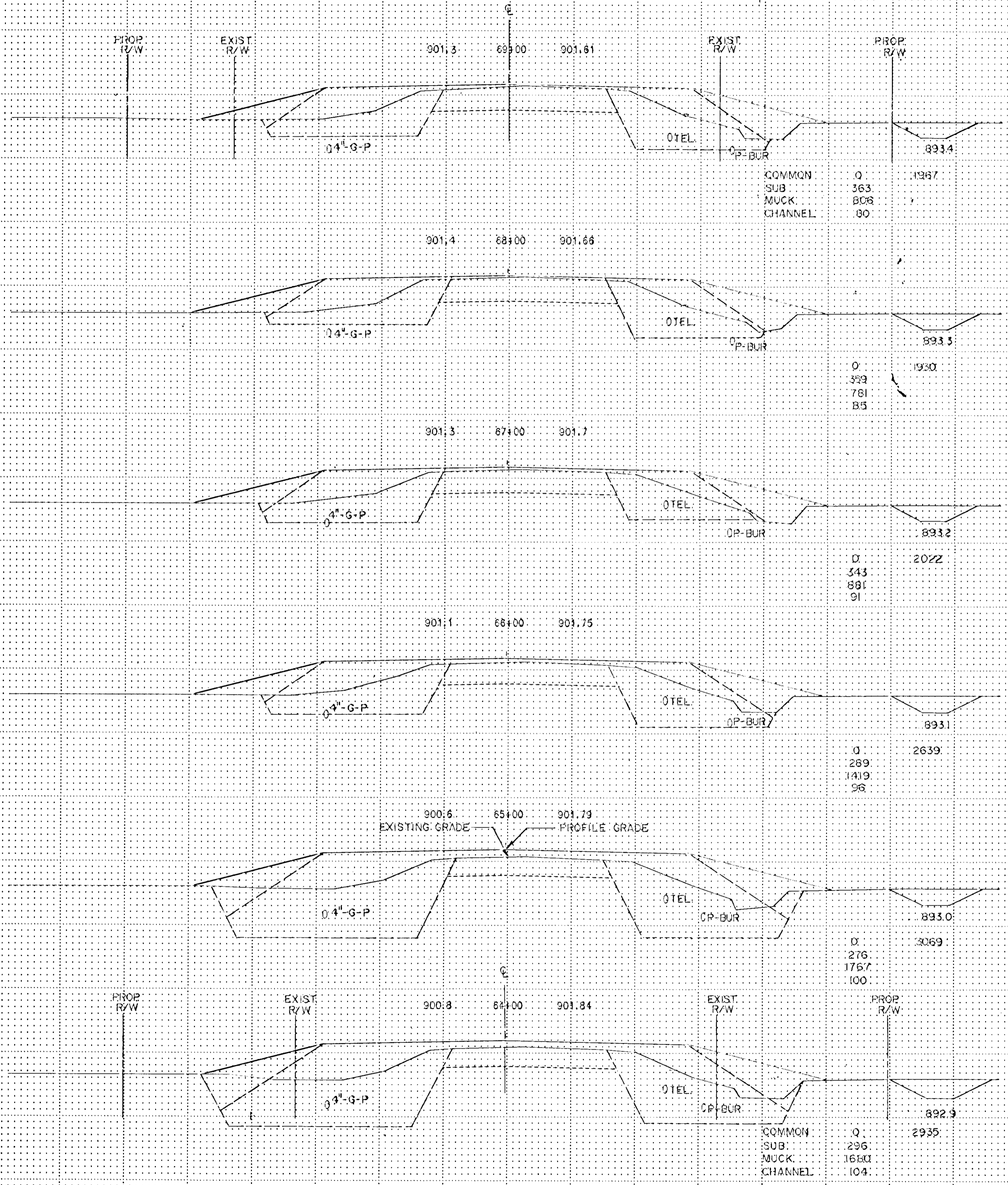


CROSS-SECTION
STA. 54+00 TO STA. 63+00

Copy Embankment Form #1

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION
STA. 64+00 TO STA. 73+00

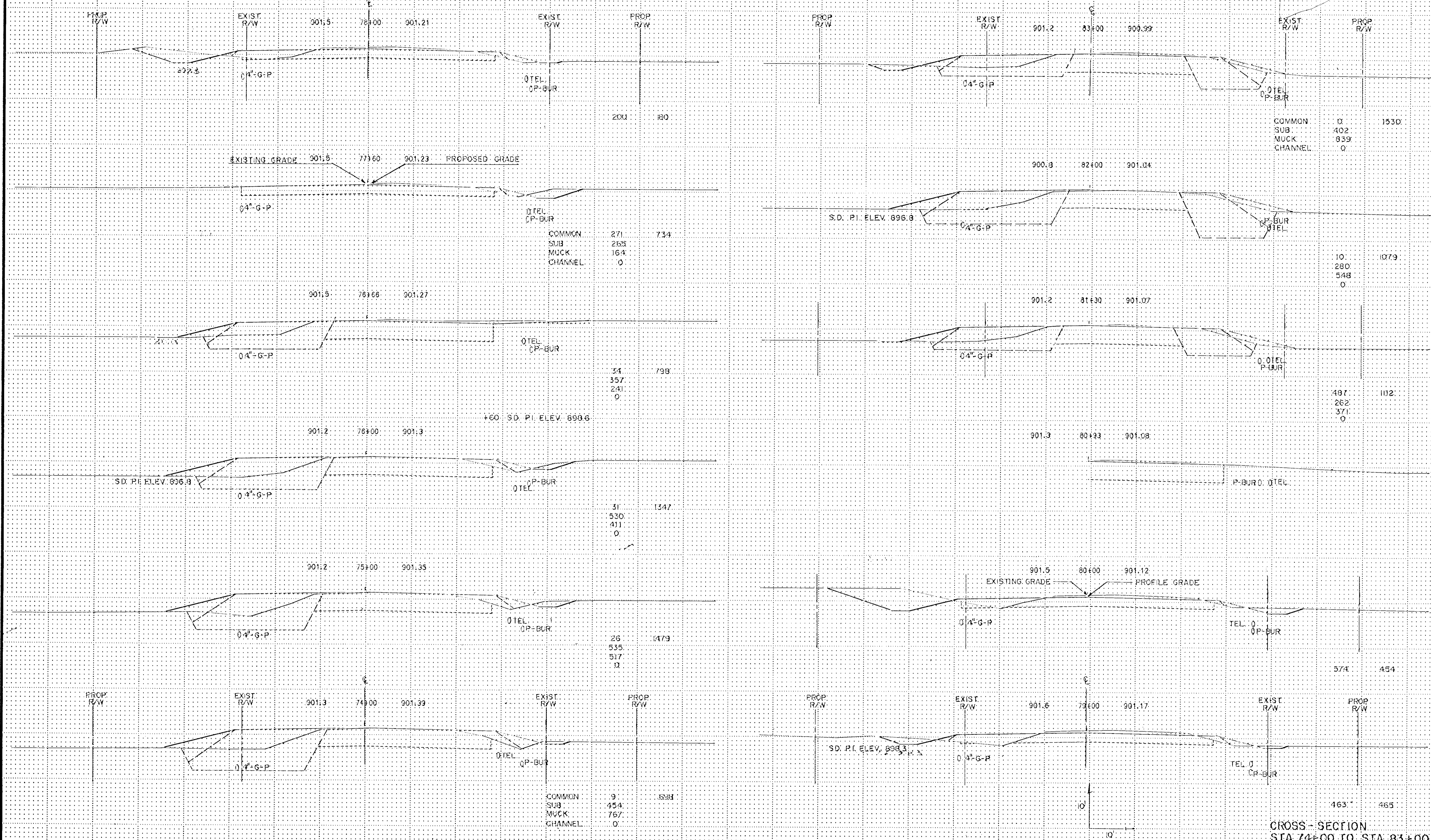
Excavation Form #1

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

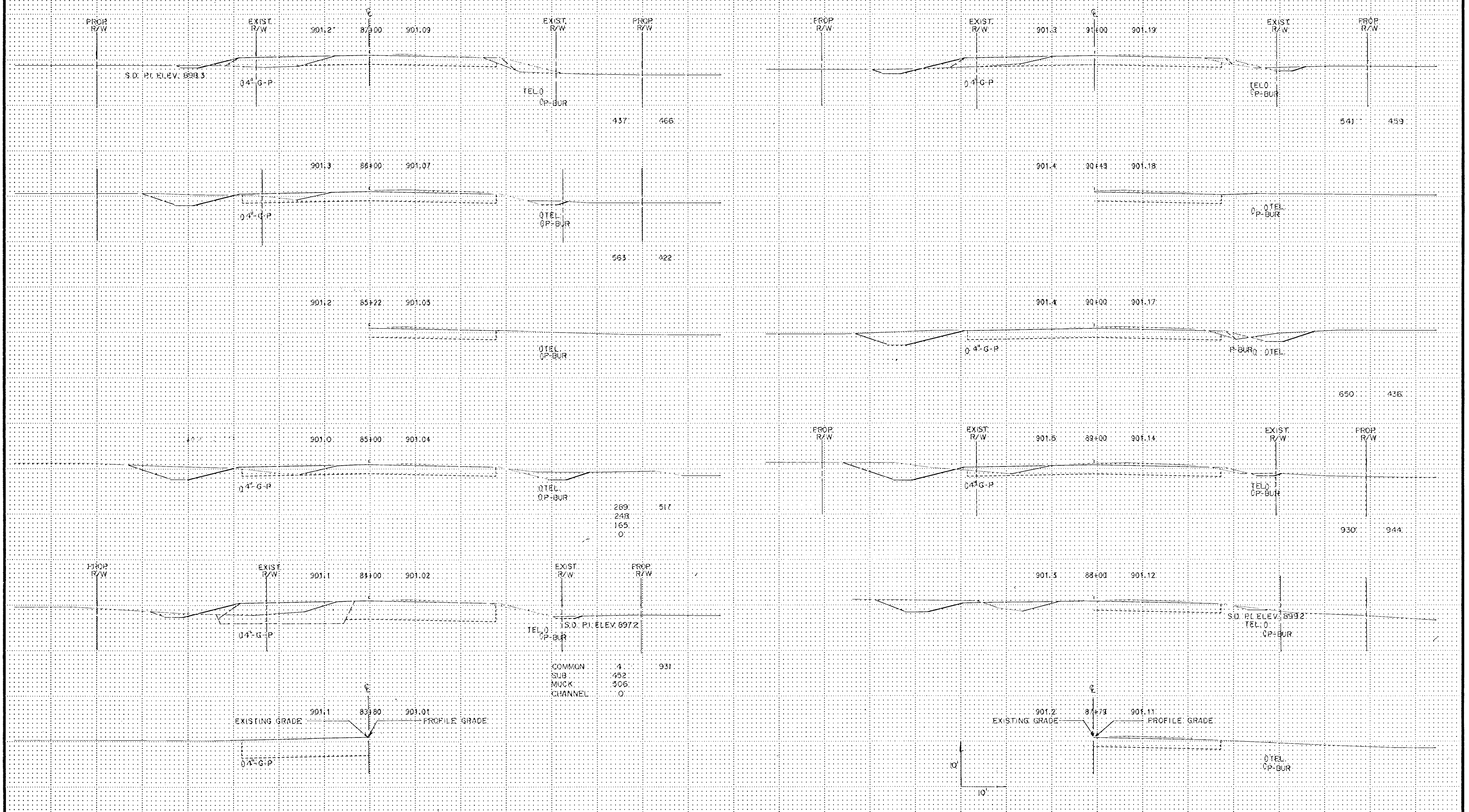


CROSS-SECTION
STA. 74+00 TO STA. 83+00

State Department of Transportation Form 41

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



COMMON
SUB
MUCK
CHANNEL

4
452
306
0

S.D. PI. ELEV. 697.2

EXISTING GRADE 901.1 83+80 901.01 PROFILE GRADE

EXISTING GRADE 901.2 87+73 901.11 PROFILE GRADE

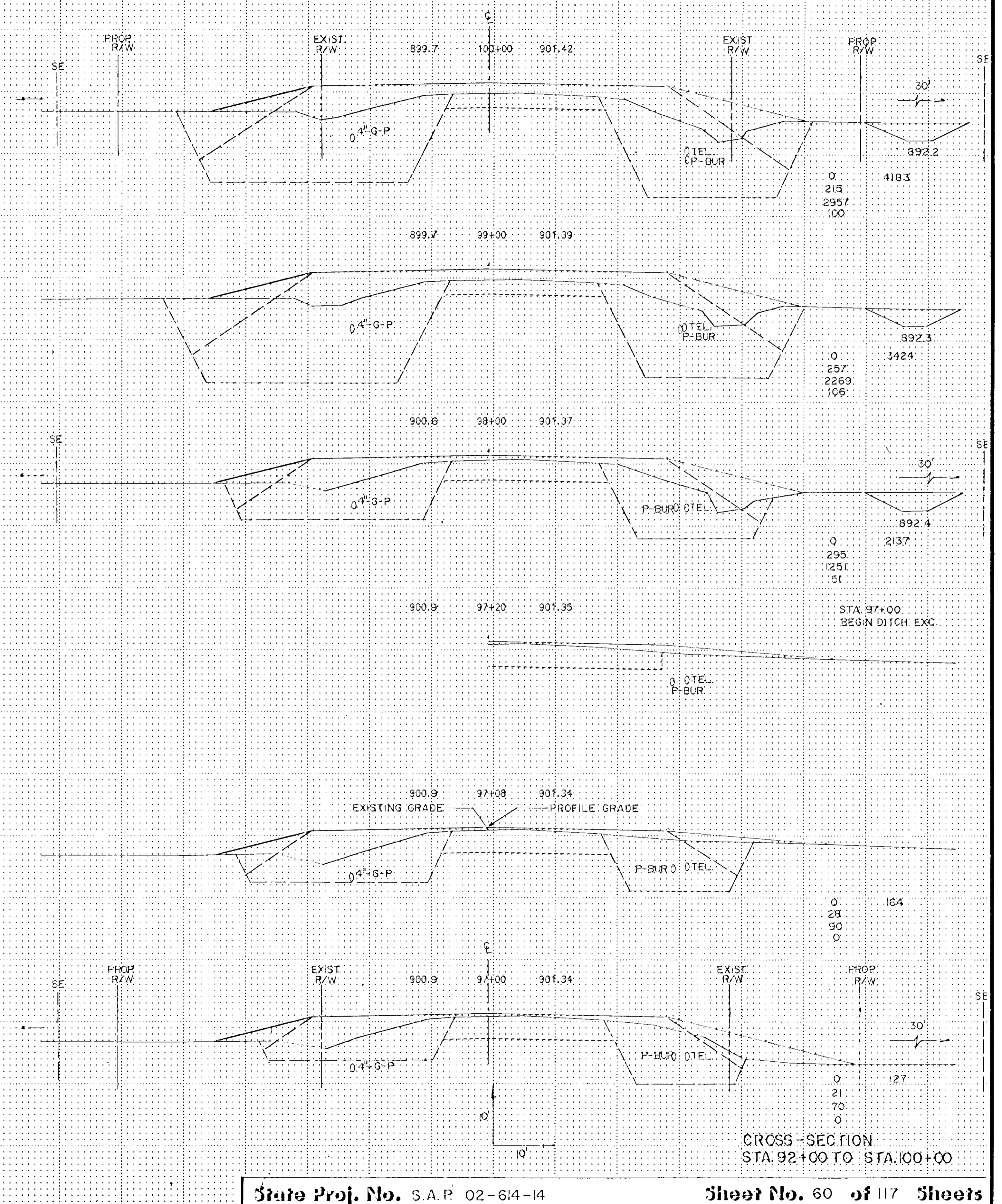
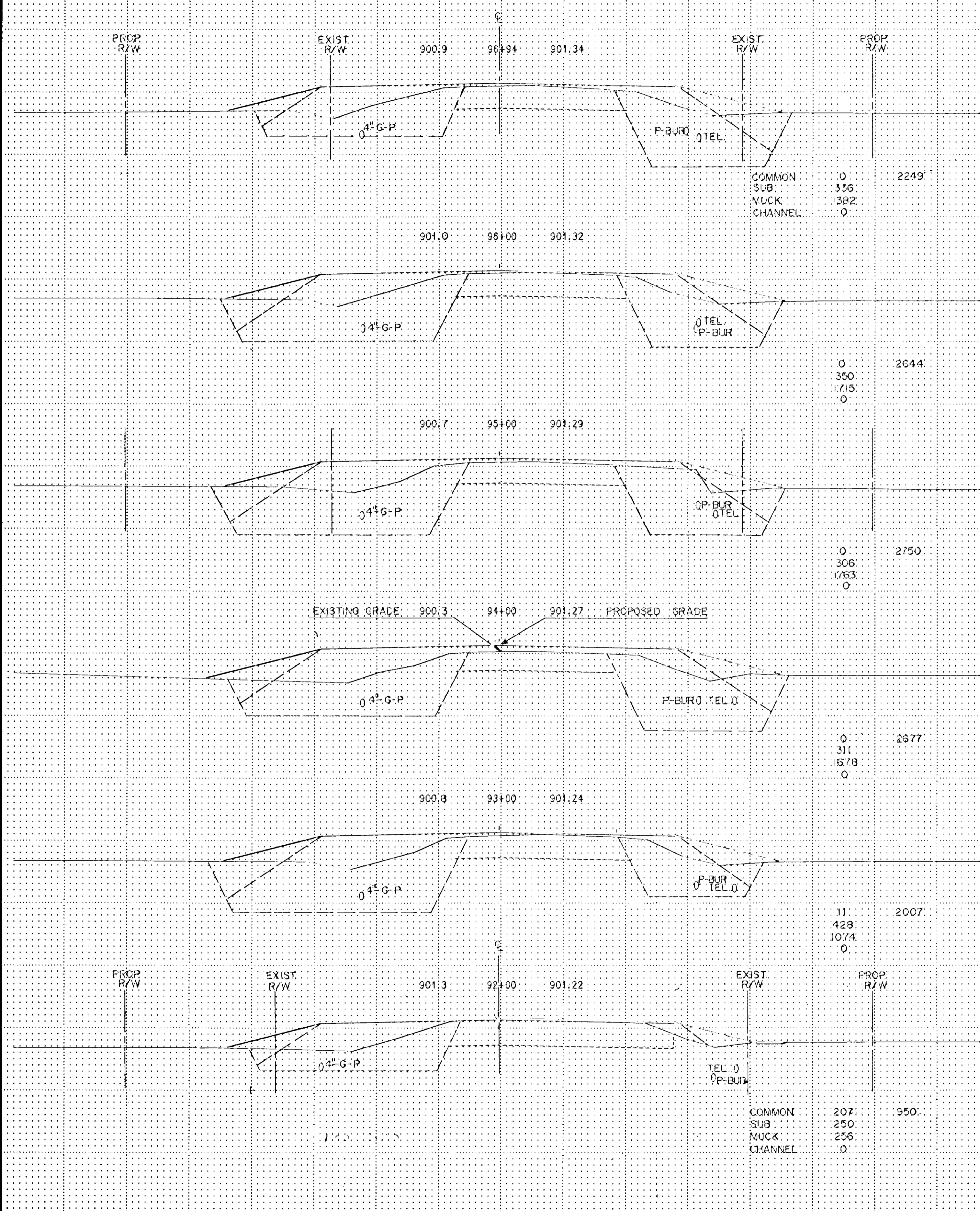
CROSS-SECTION
STA. 83+80 TO STA. 91+00

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

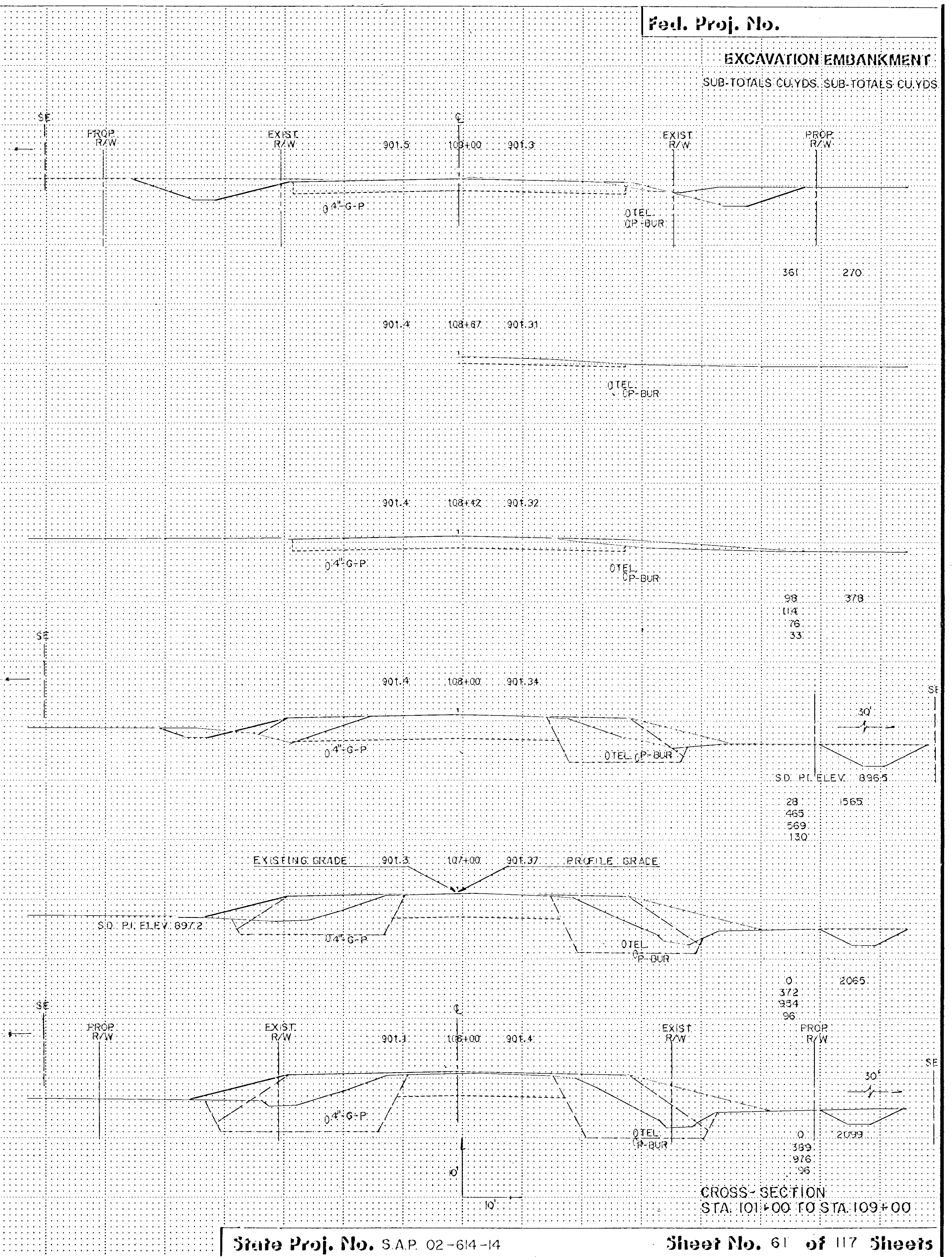
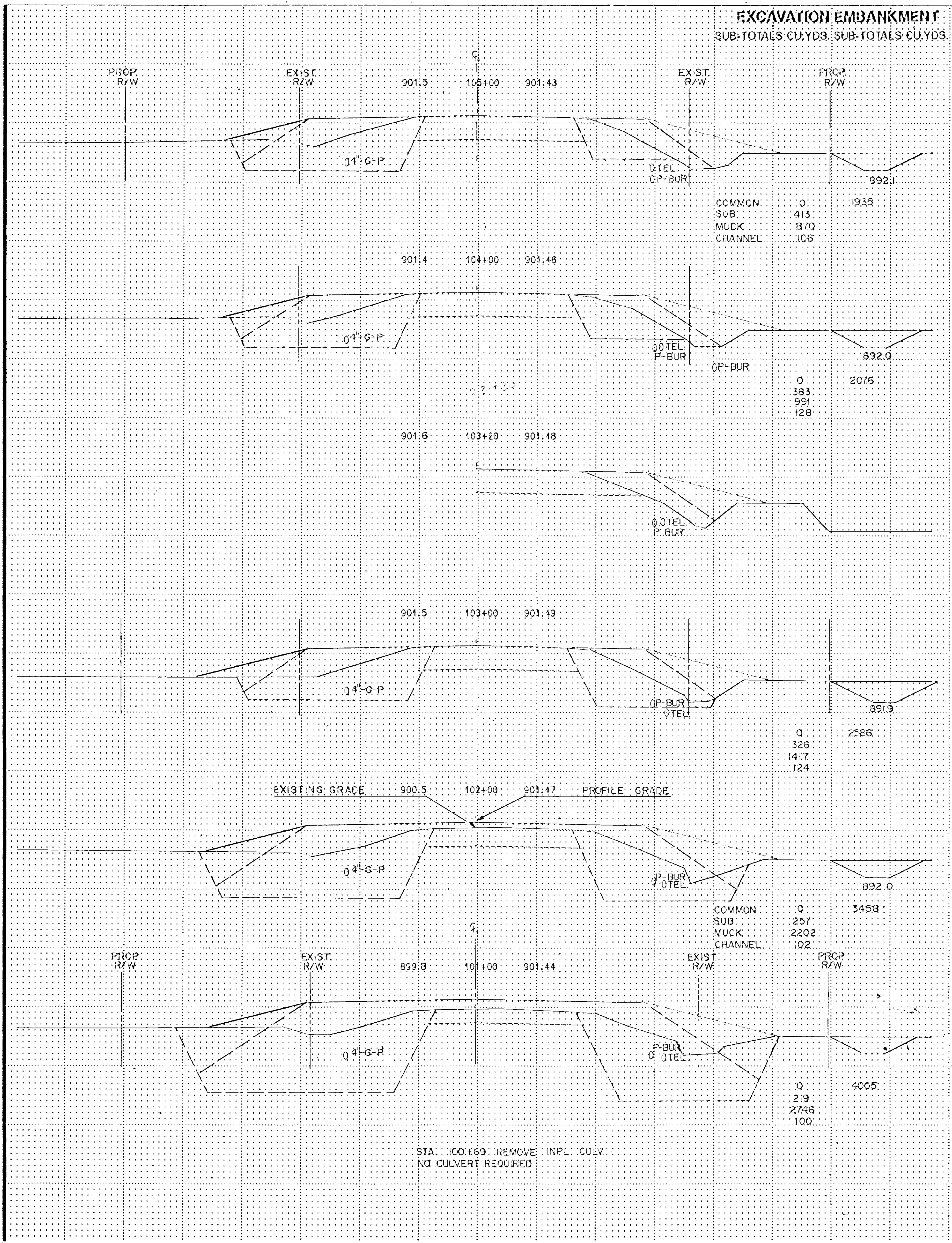


CROSS-SECTION
STA: 92+00 TO STA: 100+00

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

Fed. Proj. No.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION
STA. 101+00 TO STA. 109+00

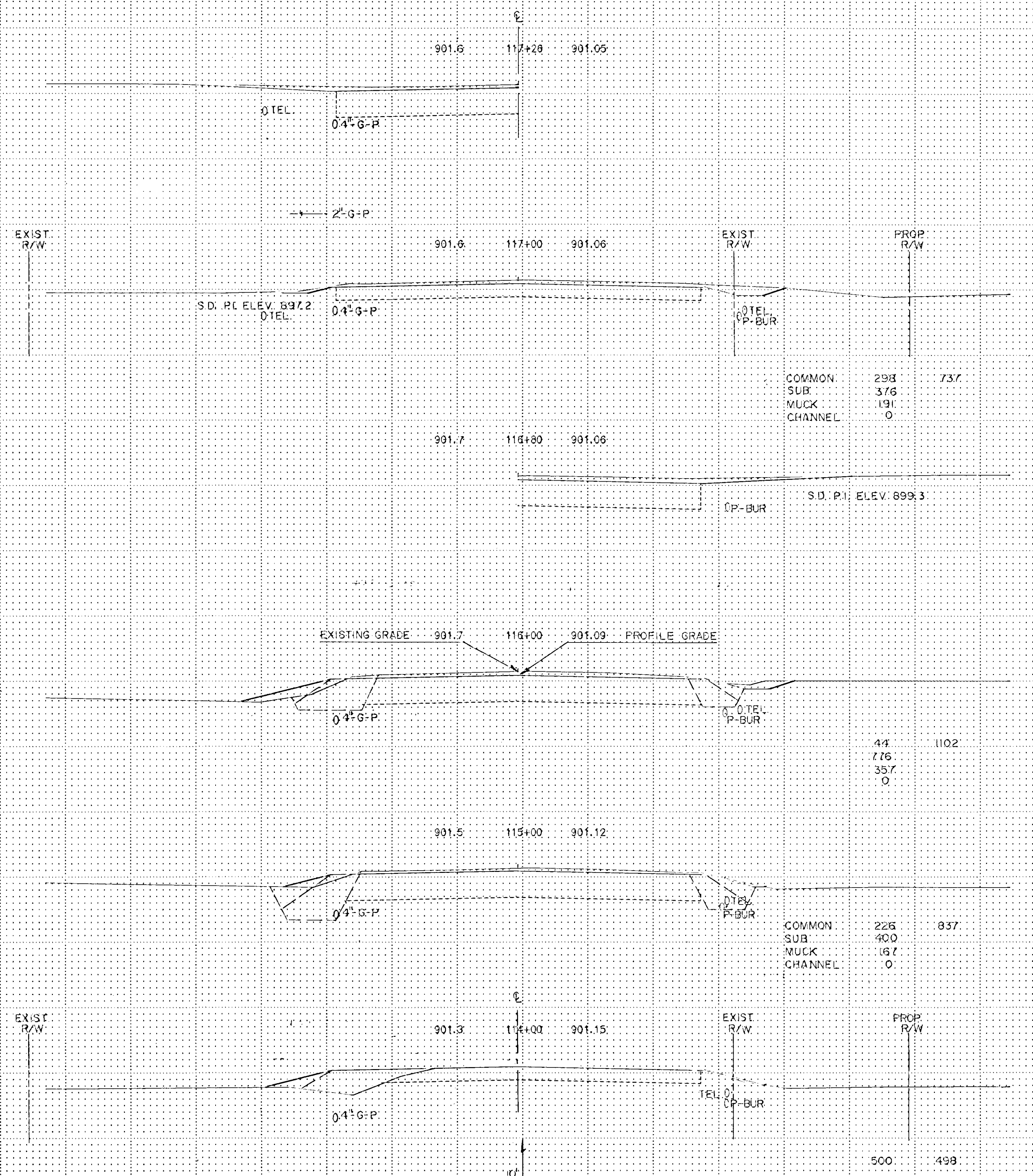
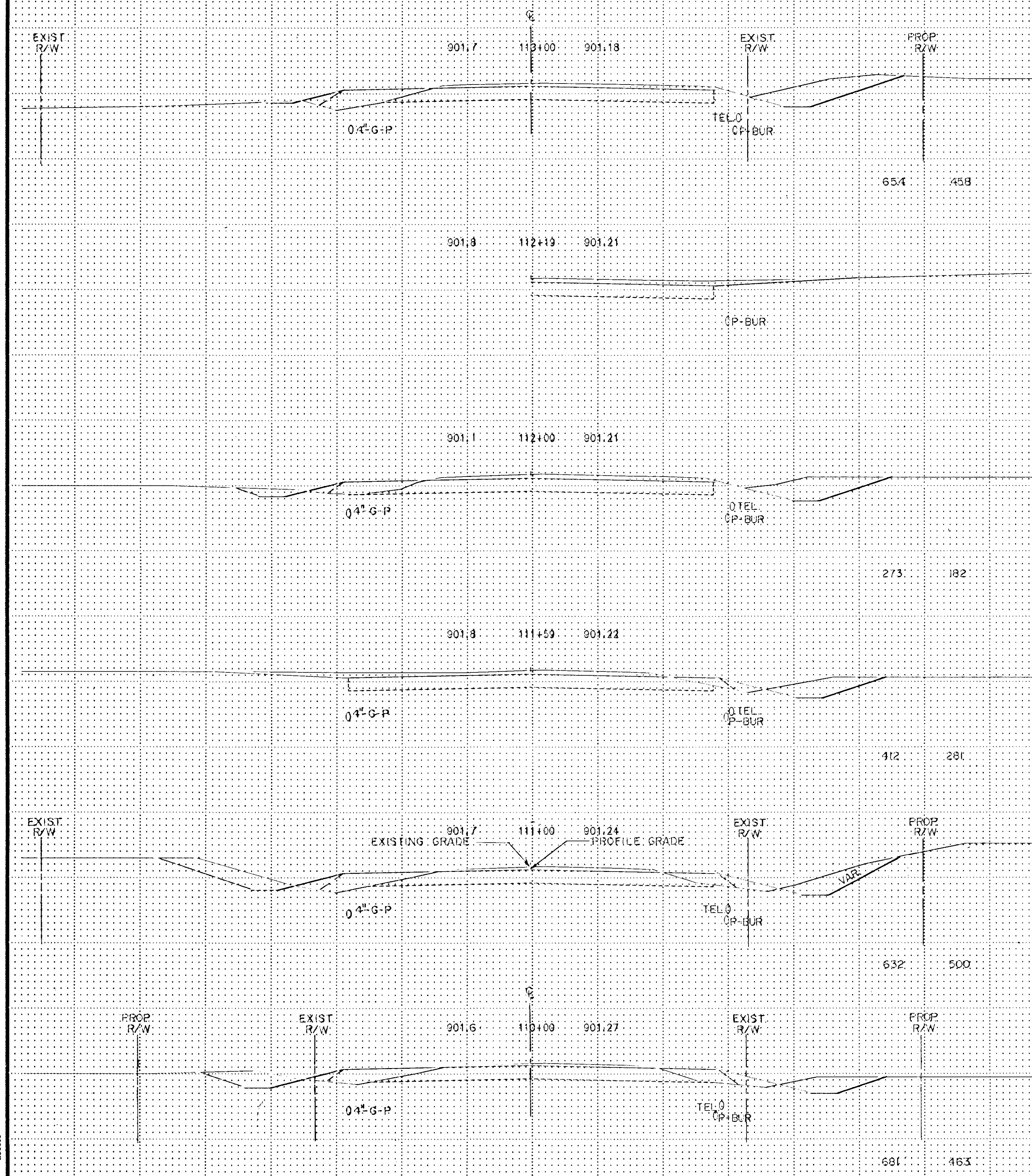
Copy Attachment Form #1

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



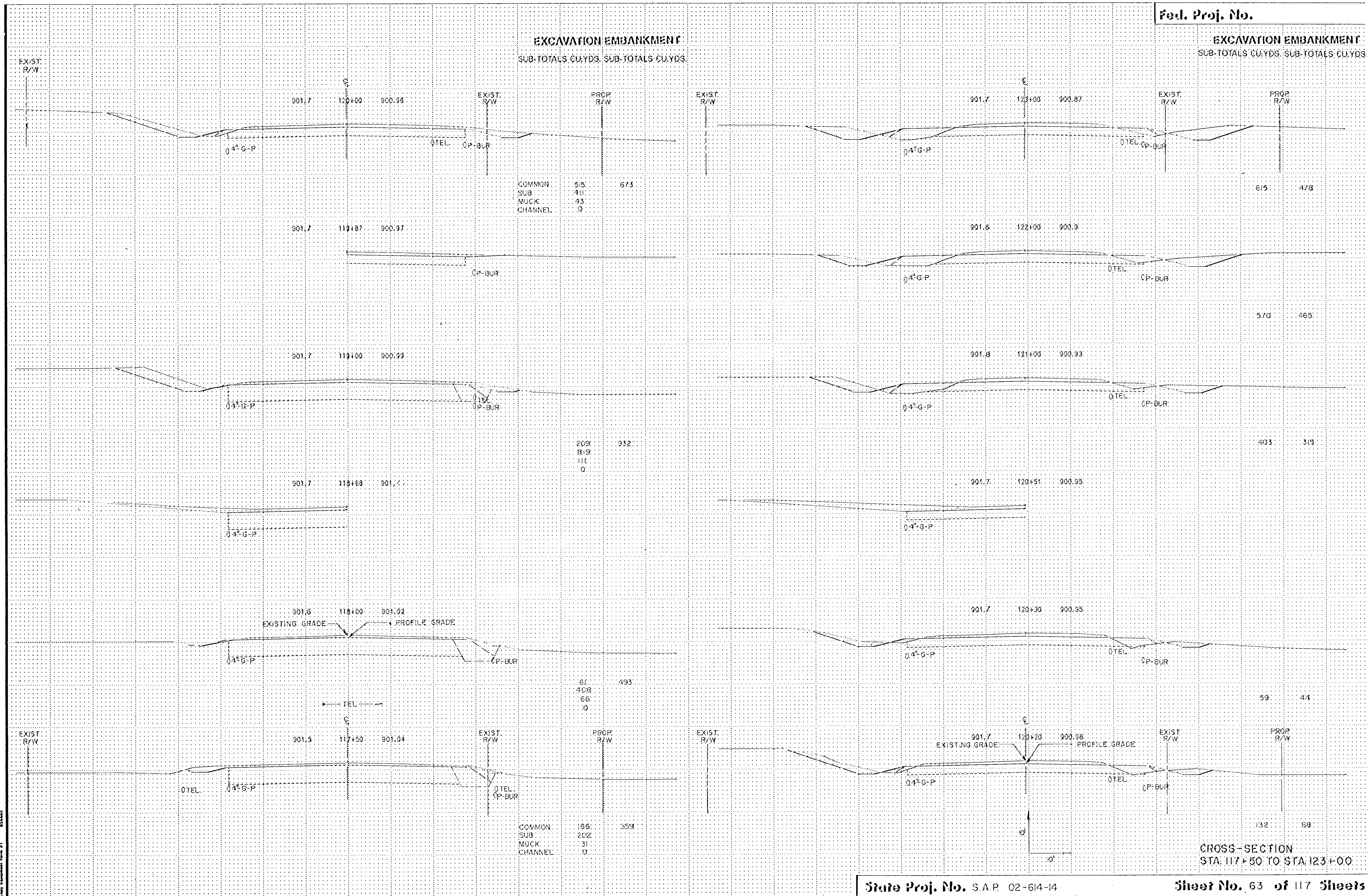
CROSS-SECTION
STA. 110+00 TO STA. 117+26

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT

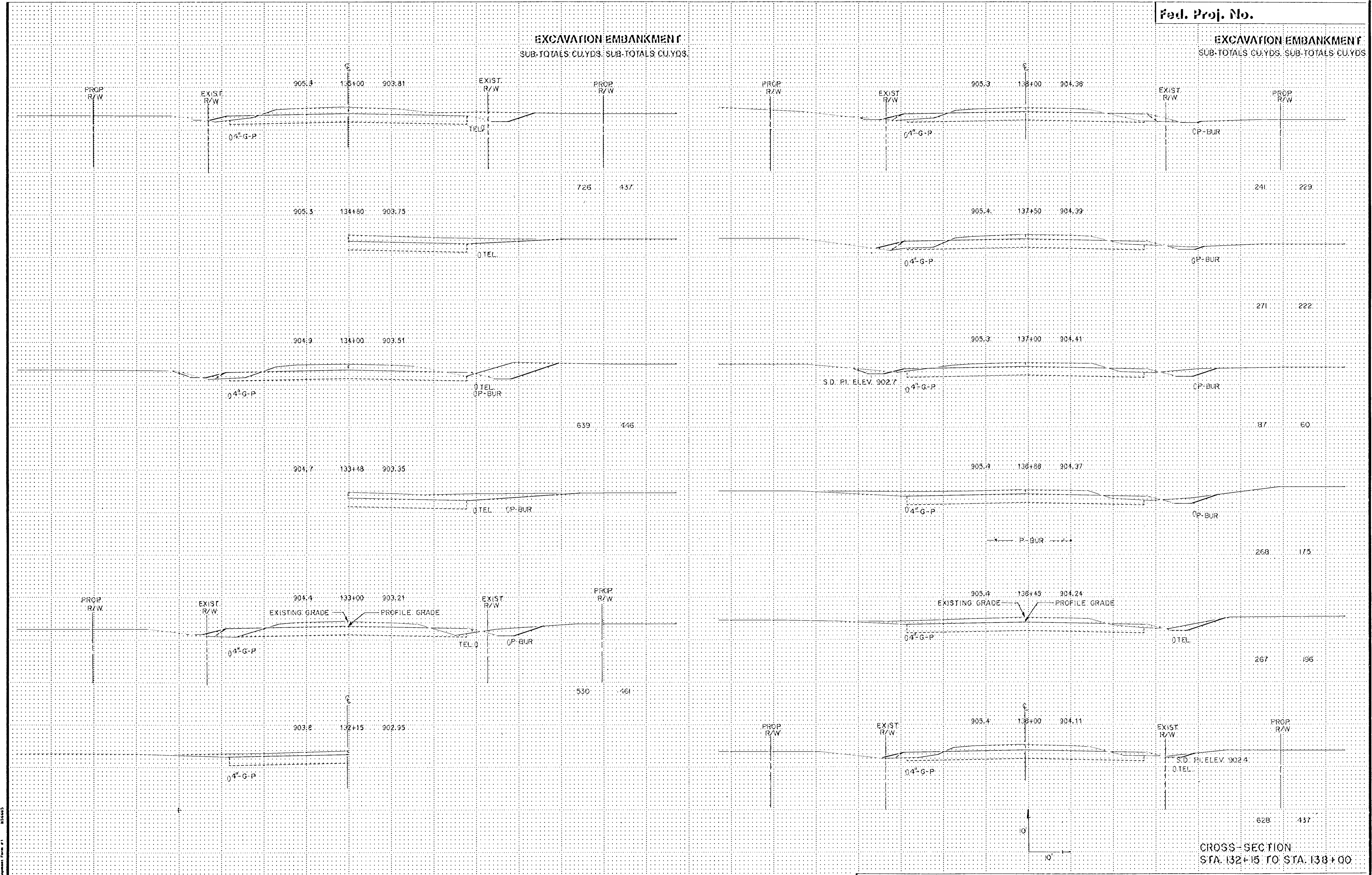
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION
STA: 117+50 TO STA: 123+00

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION
STA. 132+15 TO STA. 138+00

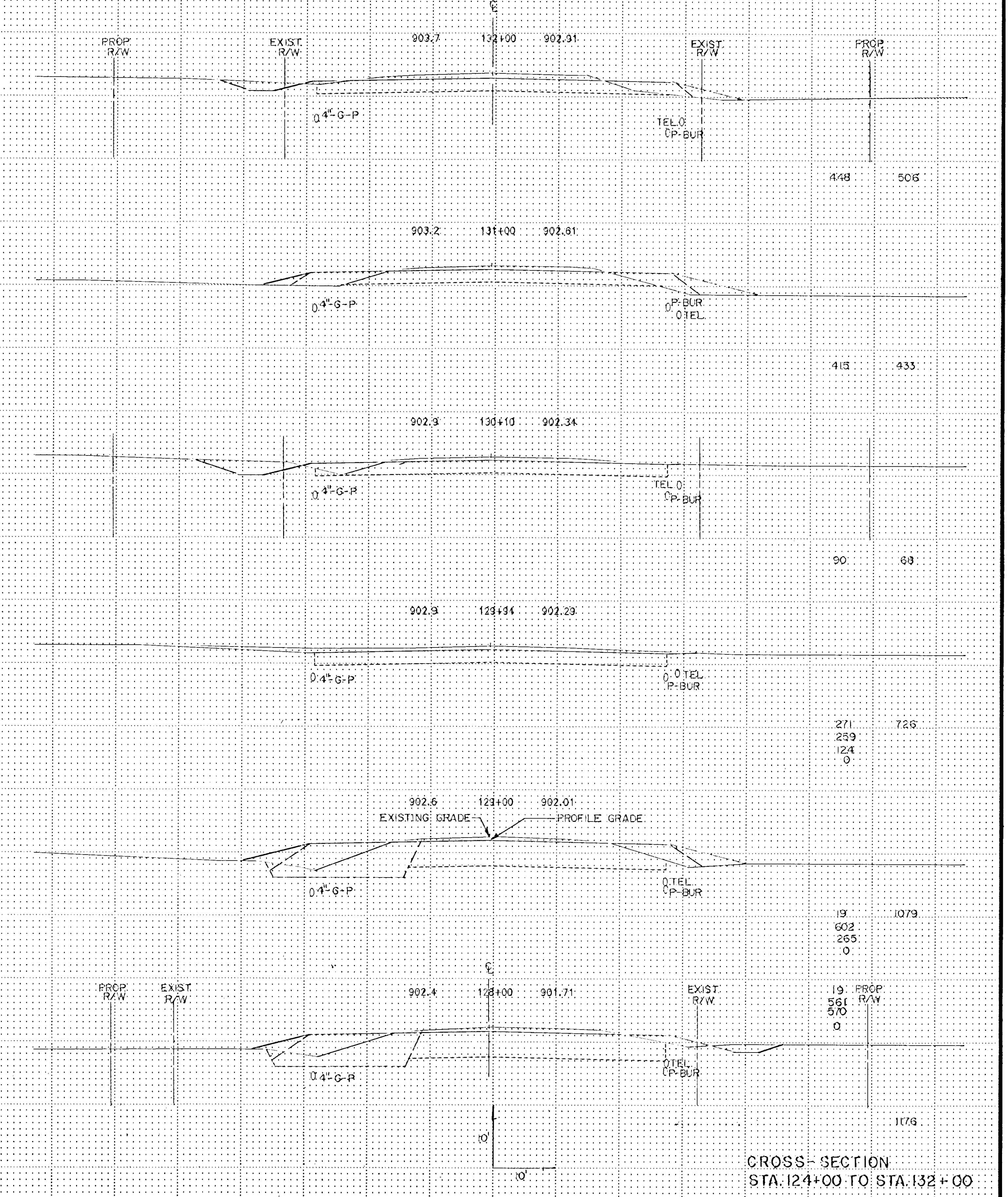
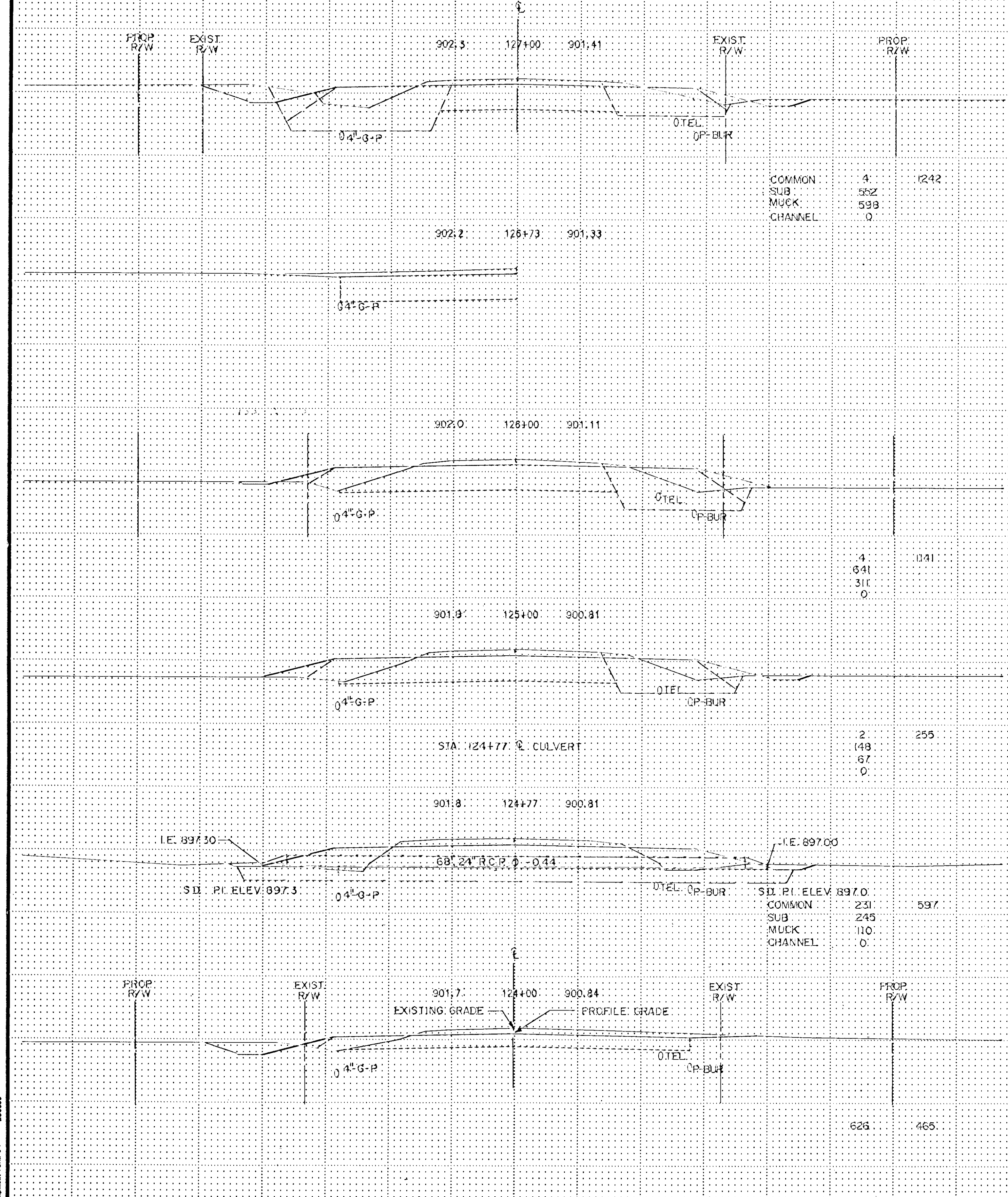
Copy Equipment Form #1 85445

EXCAVATION EMBANKMENT

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION STA. 124+00 TO STA. 132+00

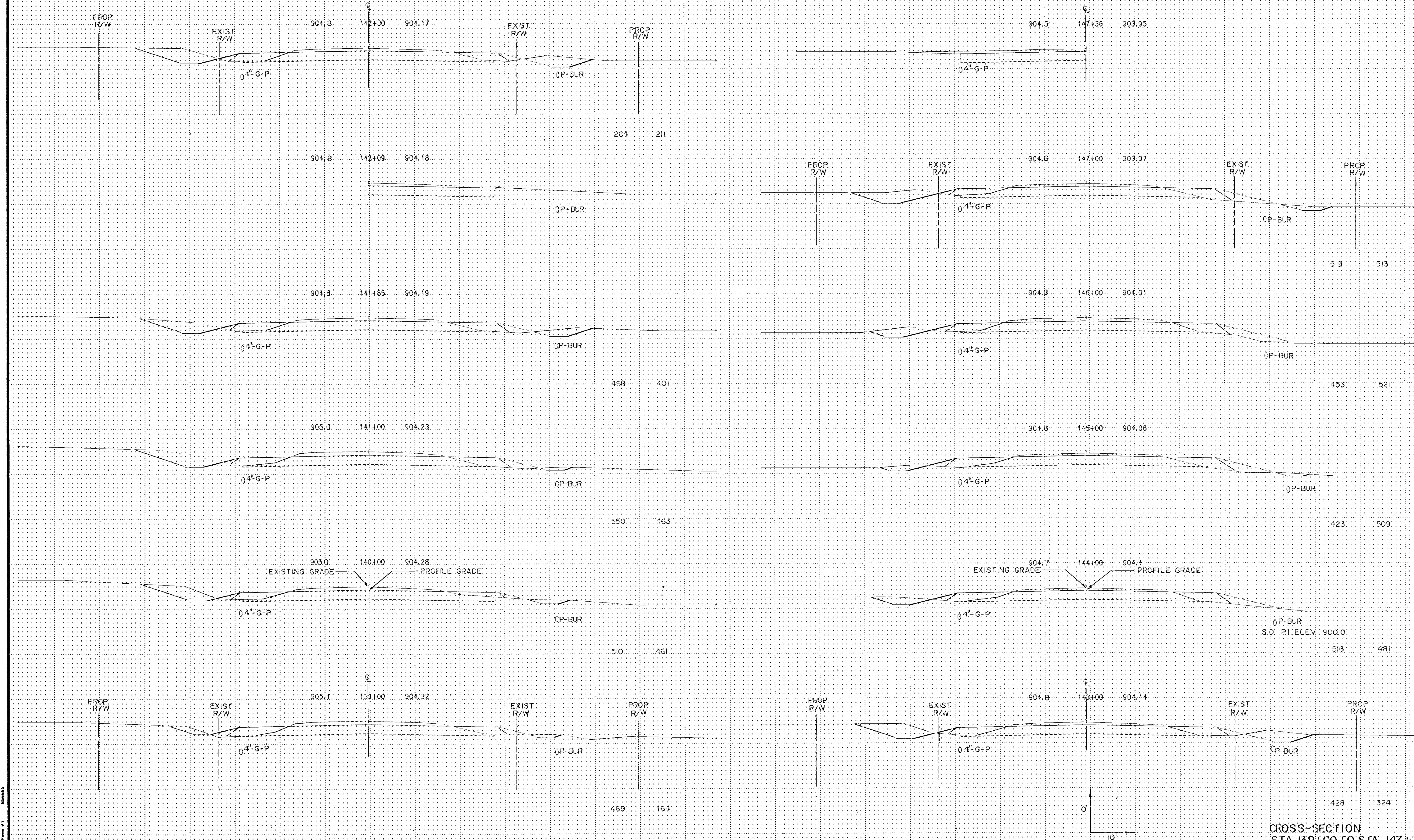
Cross Section Form #1

EXCAVATION EMBANKMENT

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION STA. 139+00 TO STA. 147+36

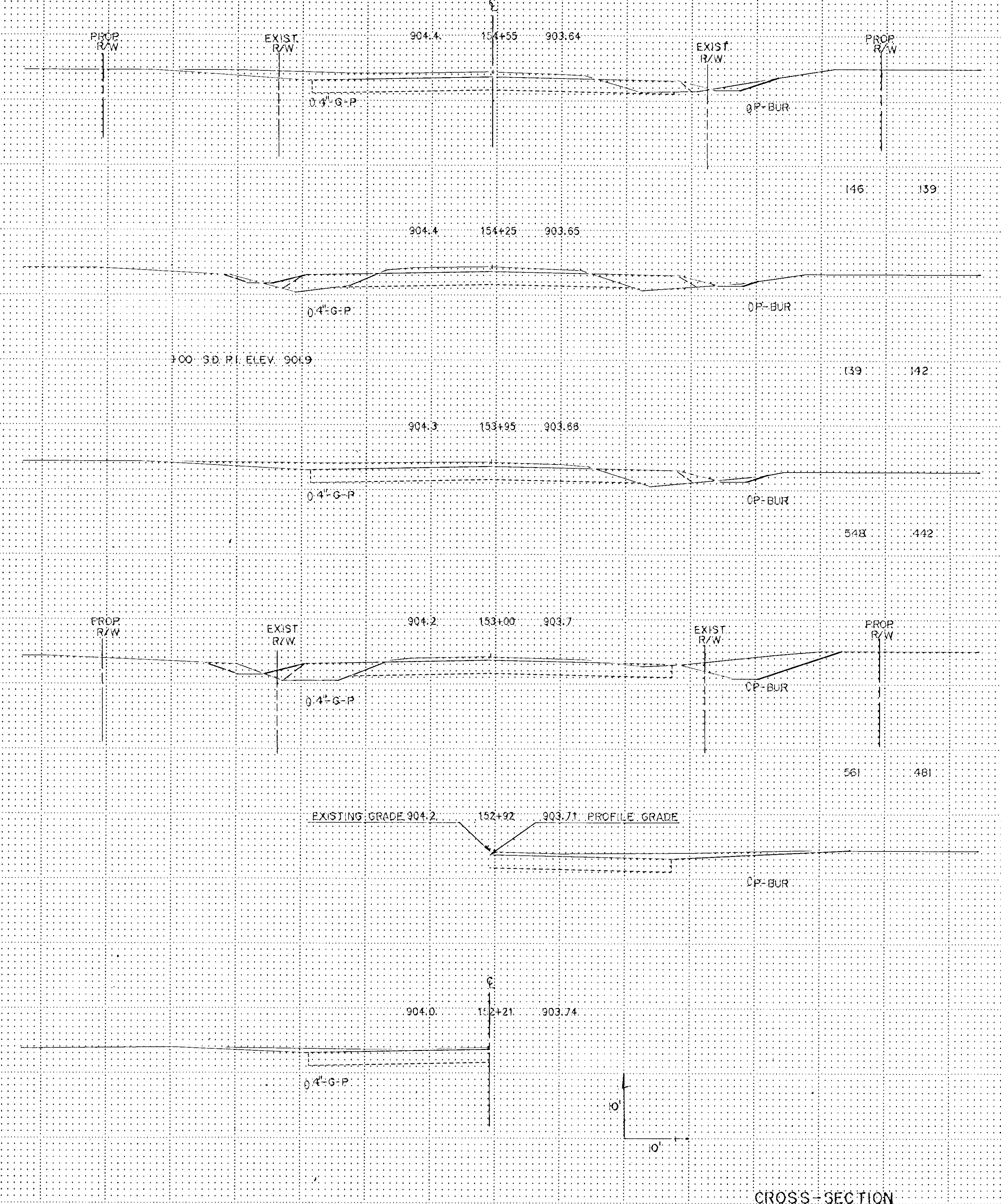
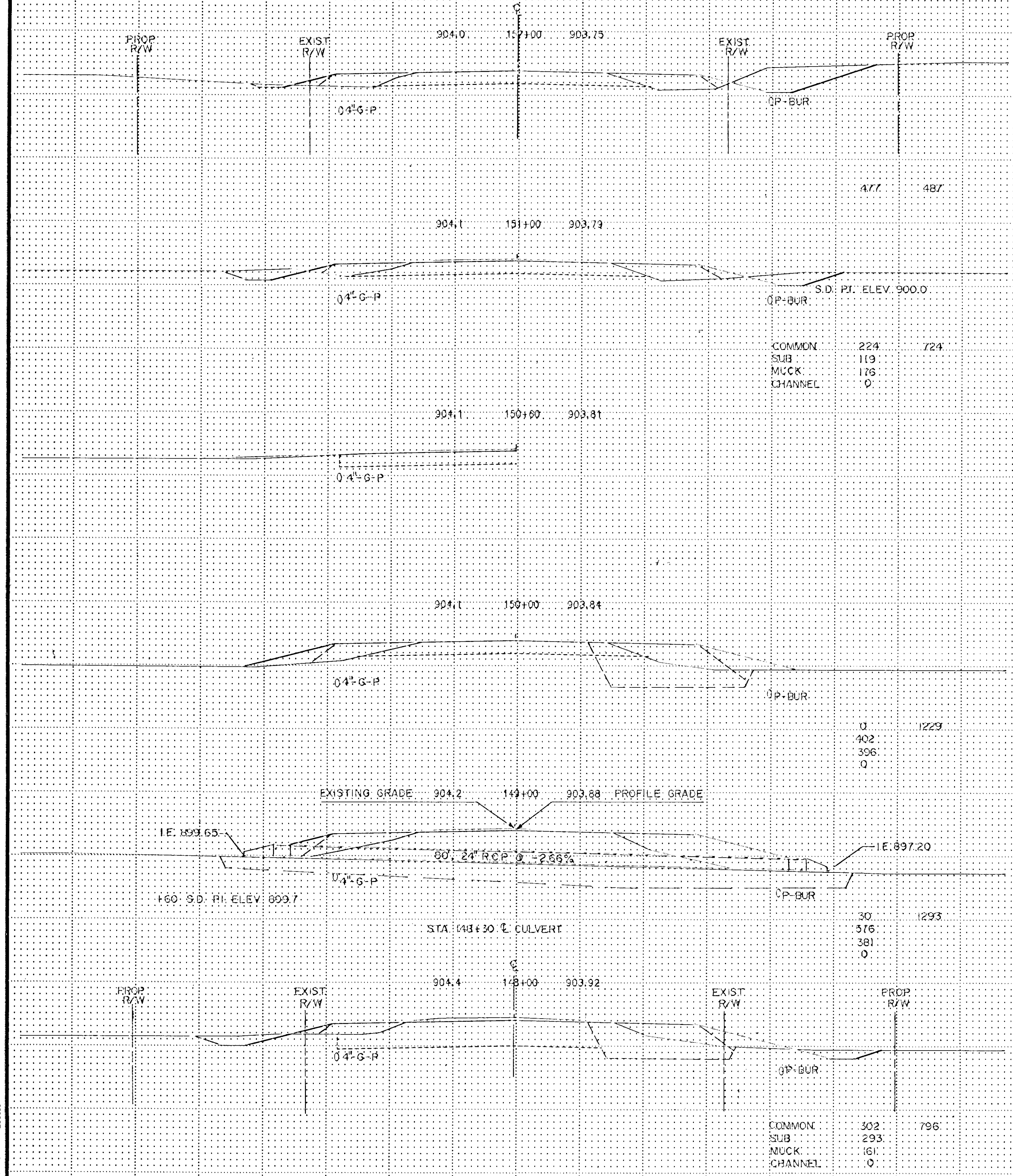
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EXCAVATION EMBANKMENT

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

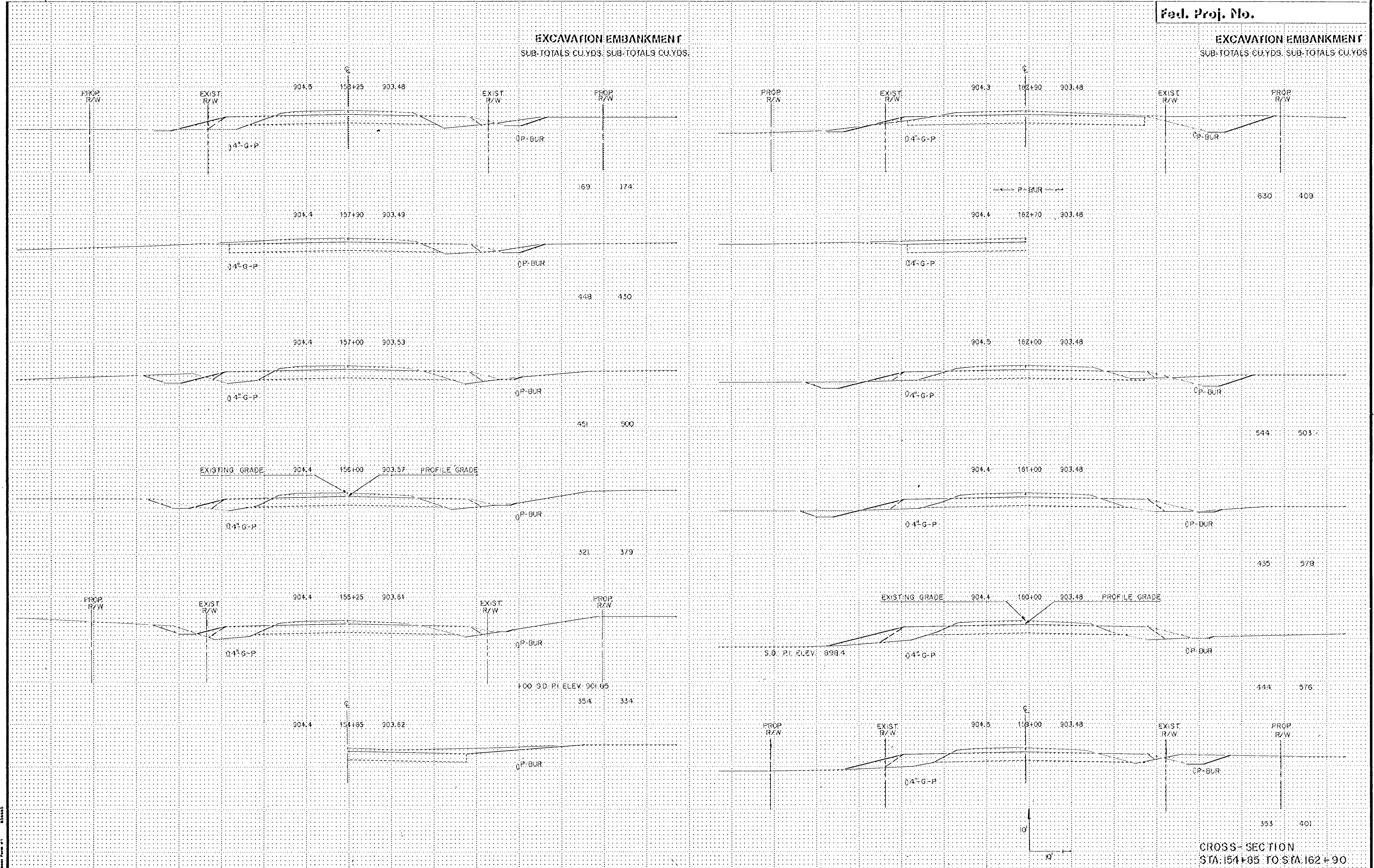


CROSS-SECTION STA. 148+00 TO STA. 154+55

Copy Equipment Form #1

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION
STA. 154+85 TO STA. 162+90

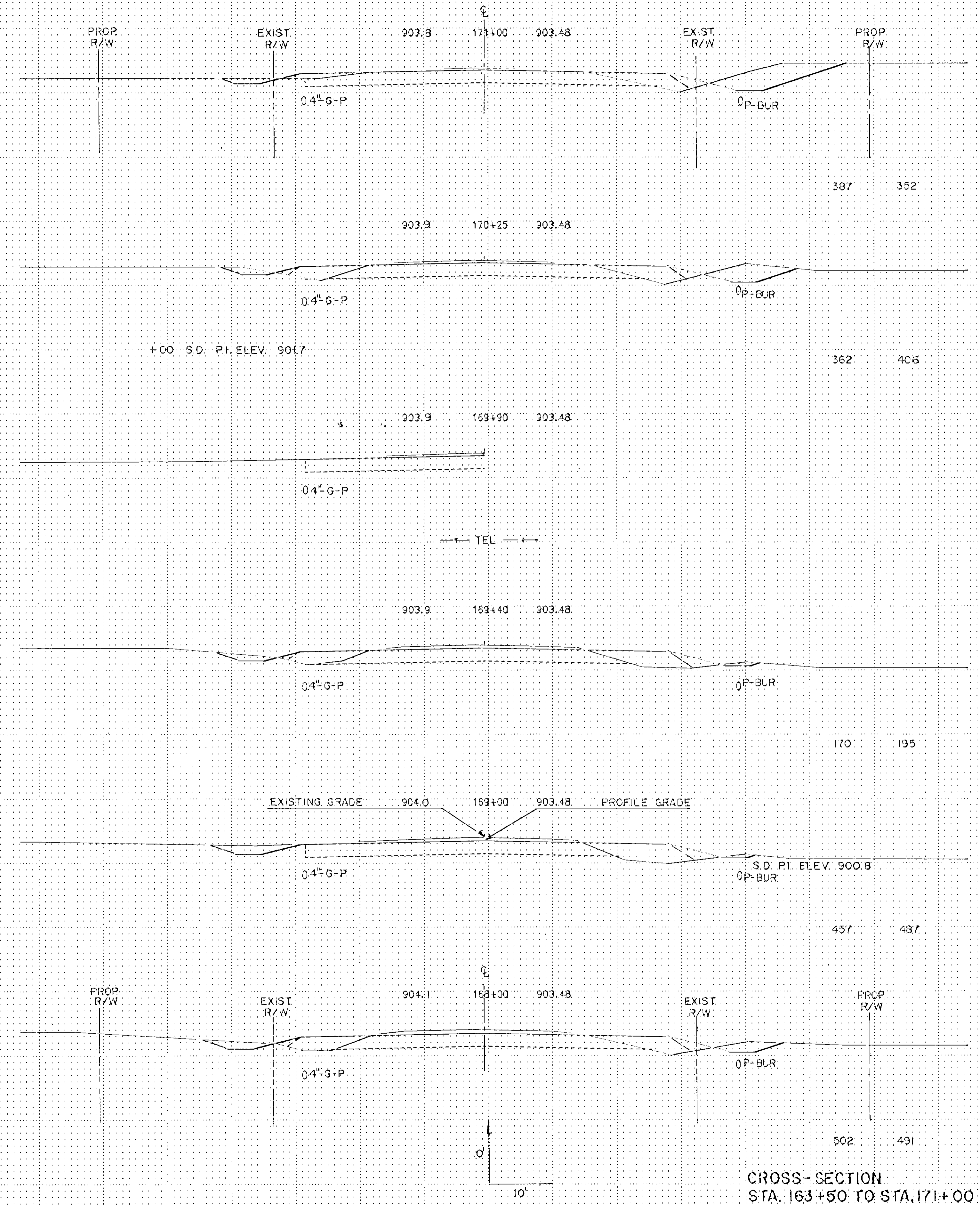
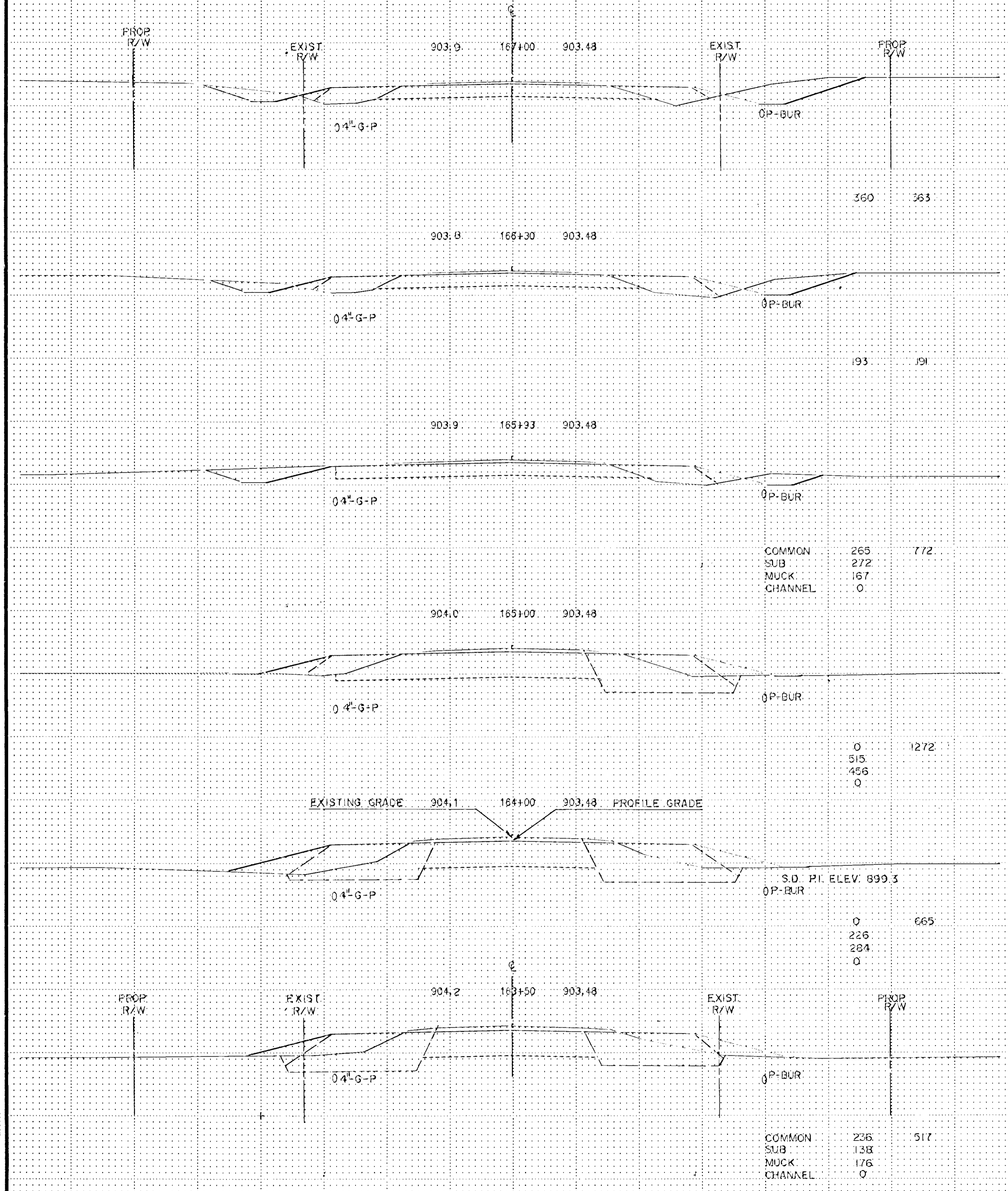
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EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



COMMON	265	772
SUB	272	
MUCK	167	
CHANNEL	0	

EXISTING GRADE 904.1 164+00 903.48 PROFILE GRADE

EXISTING GRADE 904.0 163+00 903.48 PROFILE GRADE

S.D. PT. ELEV. 899.3

S.D. PT. ELEV. 900.8

CROSS-SECTION
STA. 163+50 TO STA. 171+00

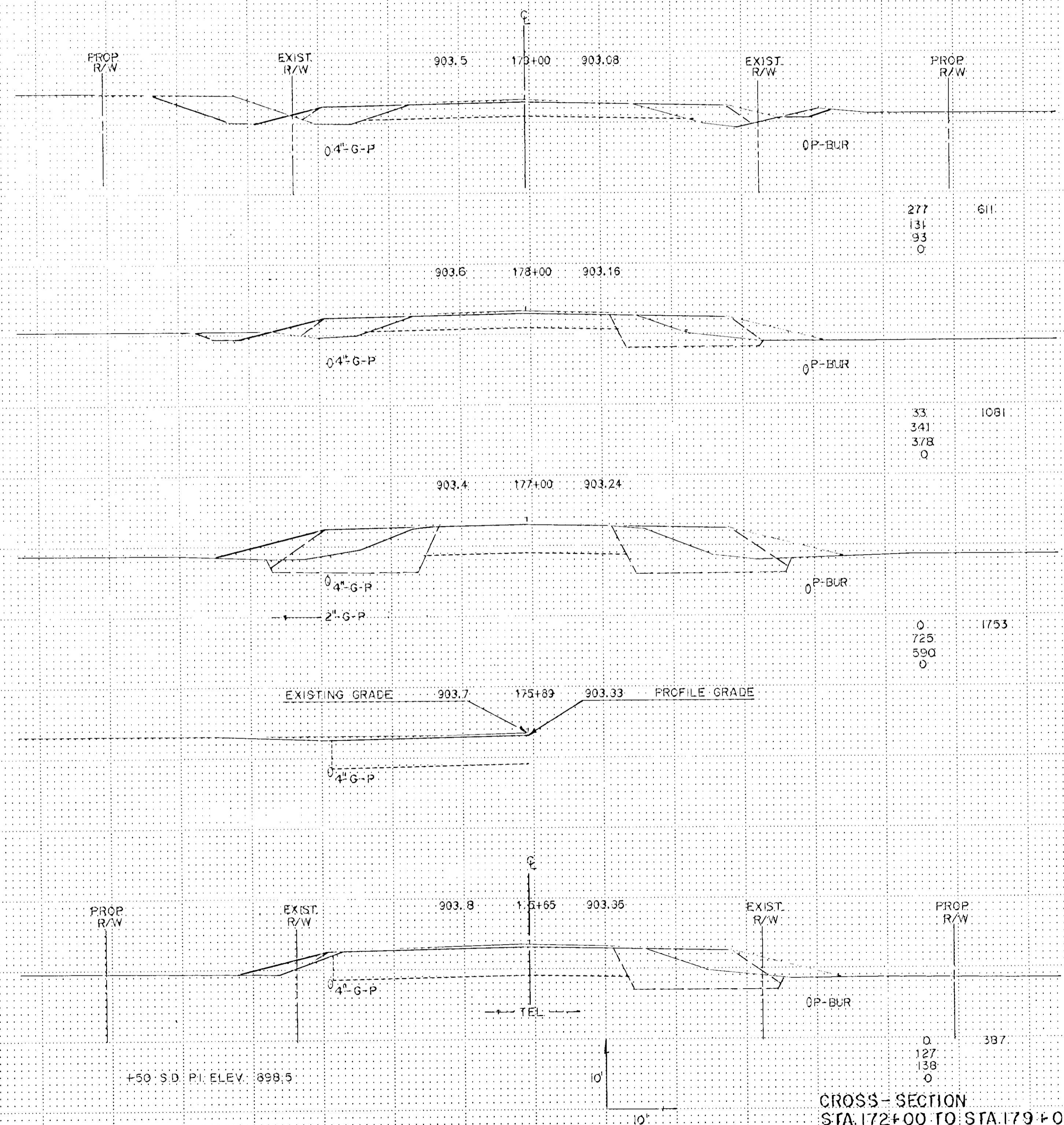
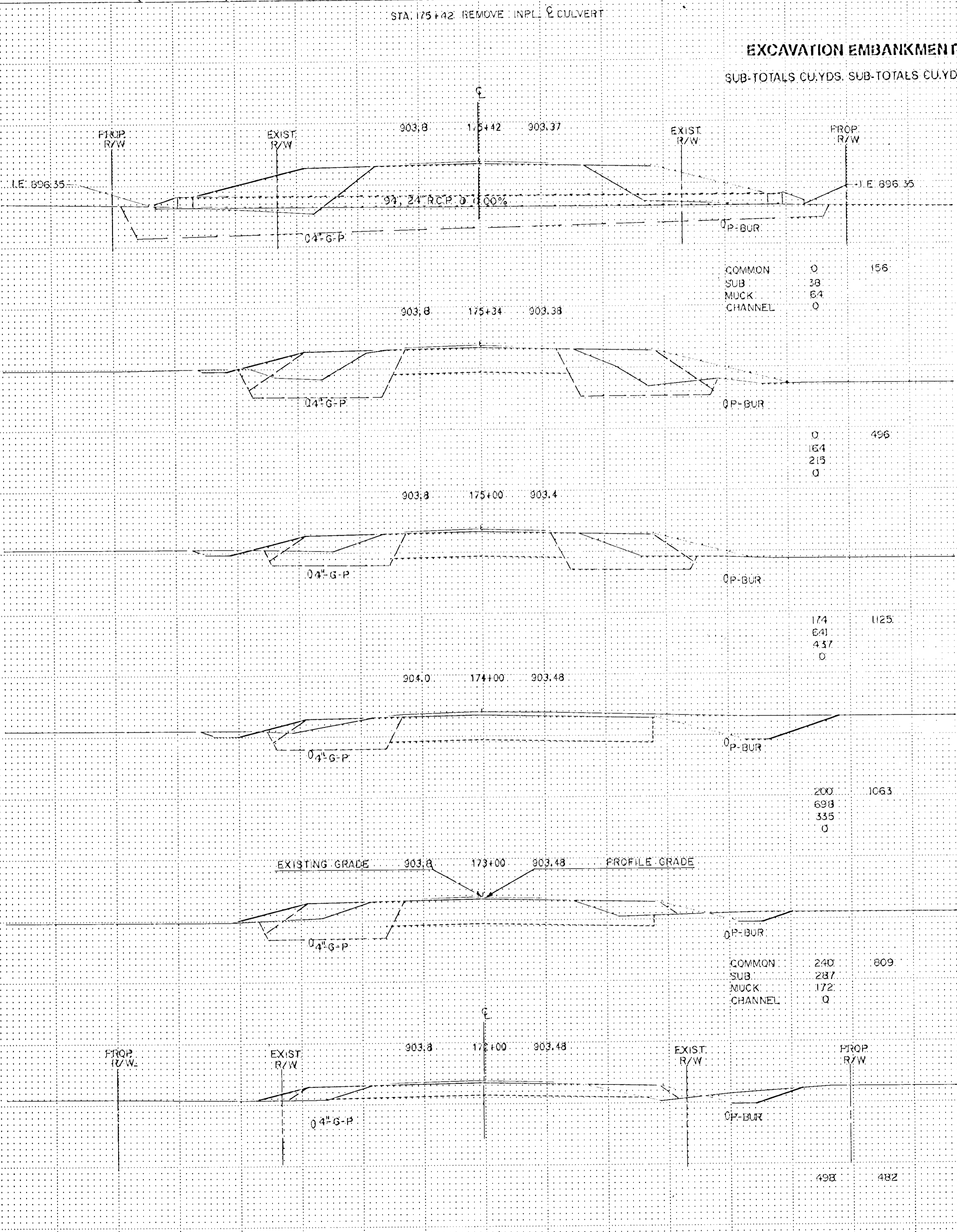
Copy Equipment Form #1 E5445

EXCAVATION EMBANKMENT

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



CROSS-SECTION
STA. 172+00 TO STA. 179+00

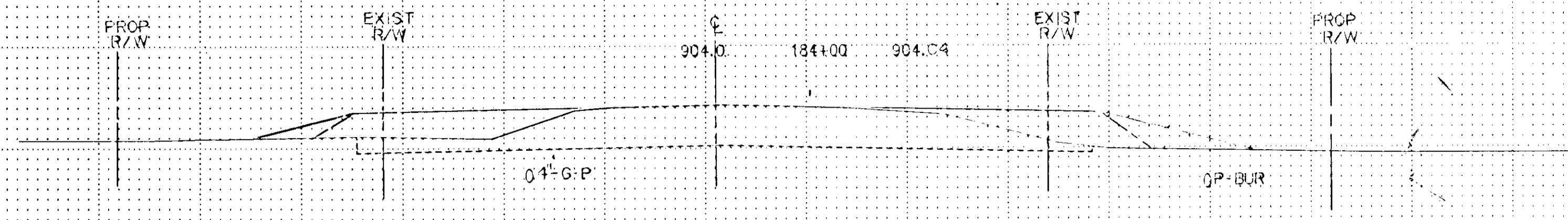
Group Engineering Form #1

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

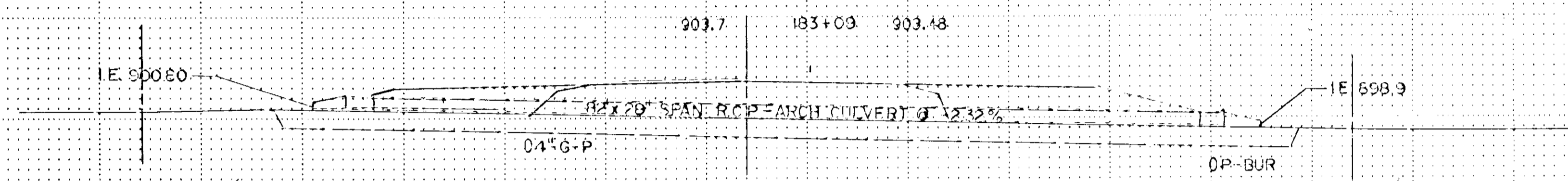
EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



STA. 183+09.2 CULVERT

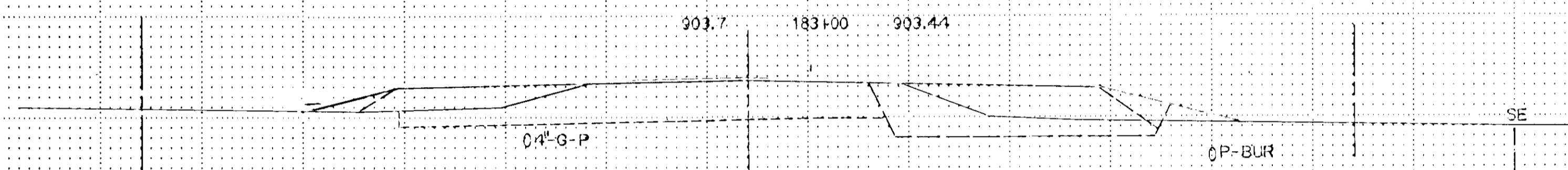
COMMON	14	1081
SUB	592	
MUCK	97	
CHANNEL	0	



STA. 183+09

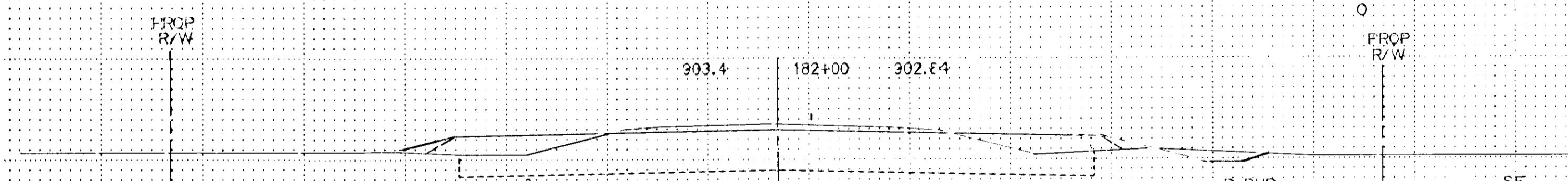
E 898.9

1	65
32	
15	
0	



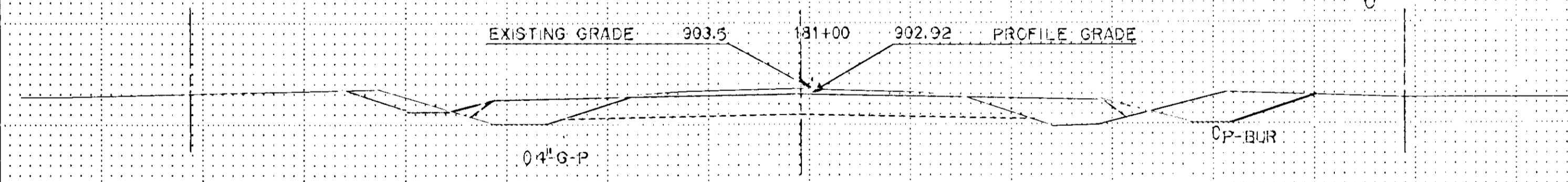
STA. 183+00

46	1030
687	
141	
0	



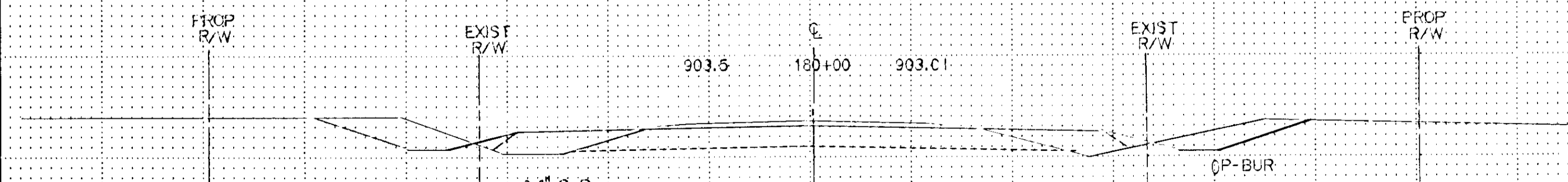
STA. 182+00

291	745
400	
0	
0	



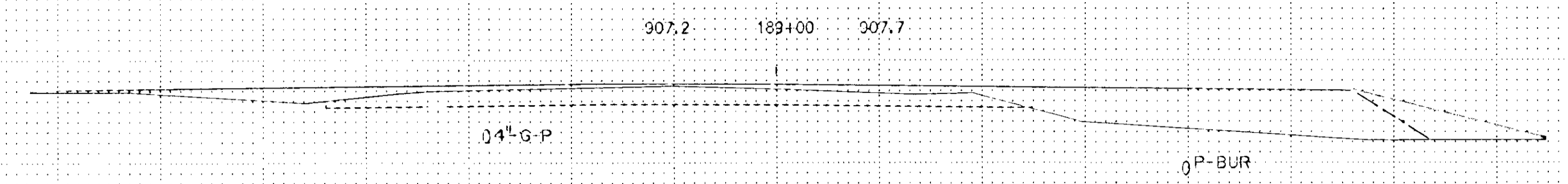
STA. 181+00

511	481
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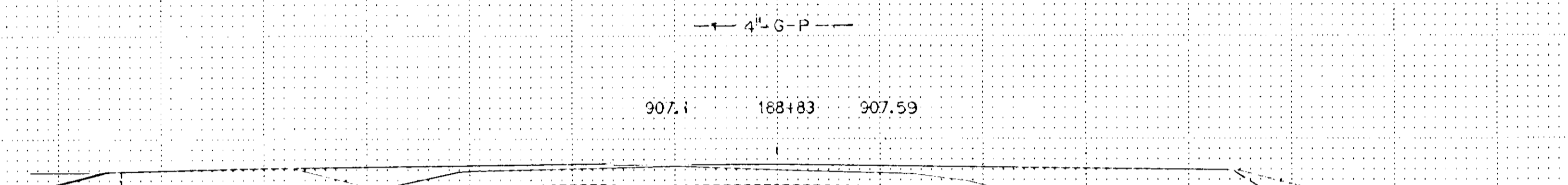
STA. 180+00

511	495
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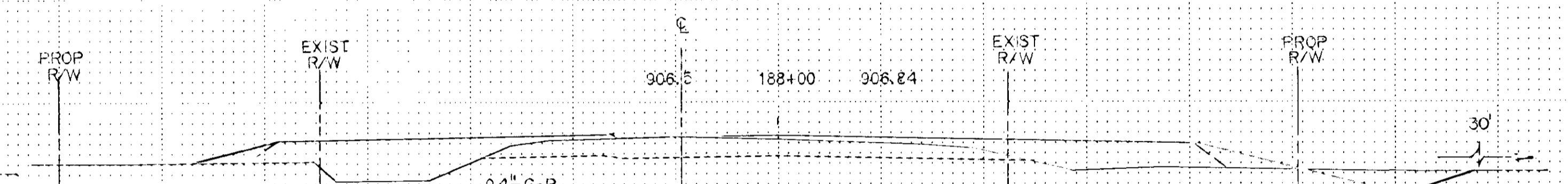
STA. 184+00

109	188
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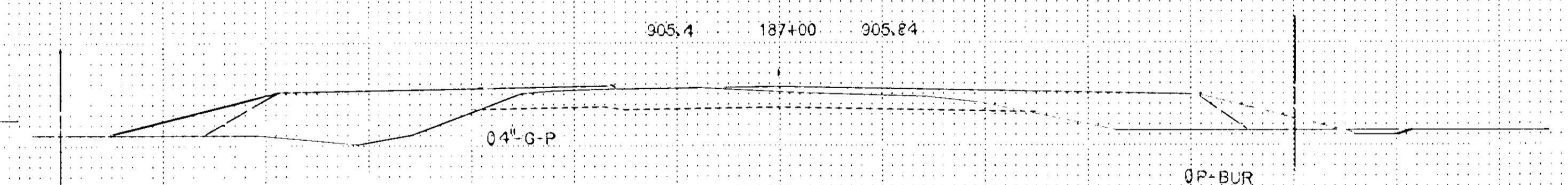
STA. 184+83

590	803
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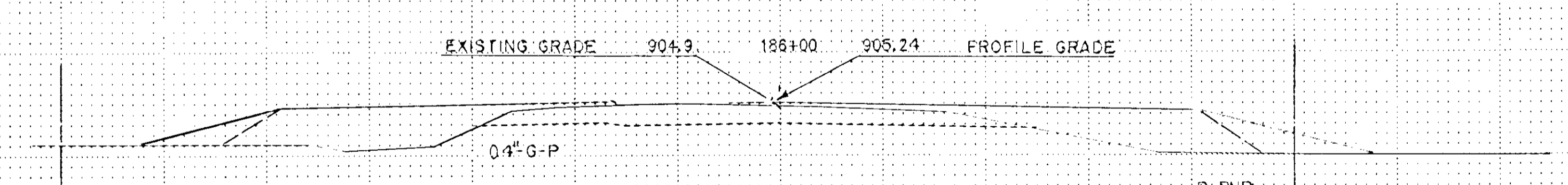
STA. 184+00

366	965
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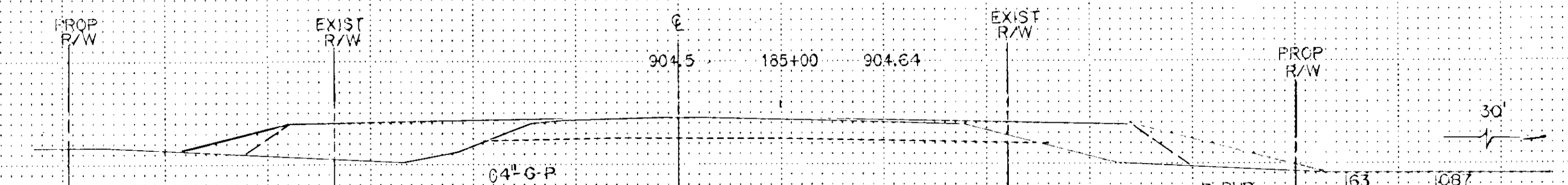
STA. 187+00

293	1104
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STA. 188+00

309	1013
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STA. 185+00

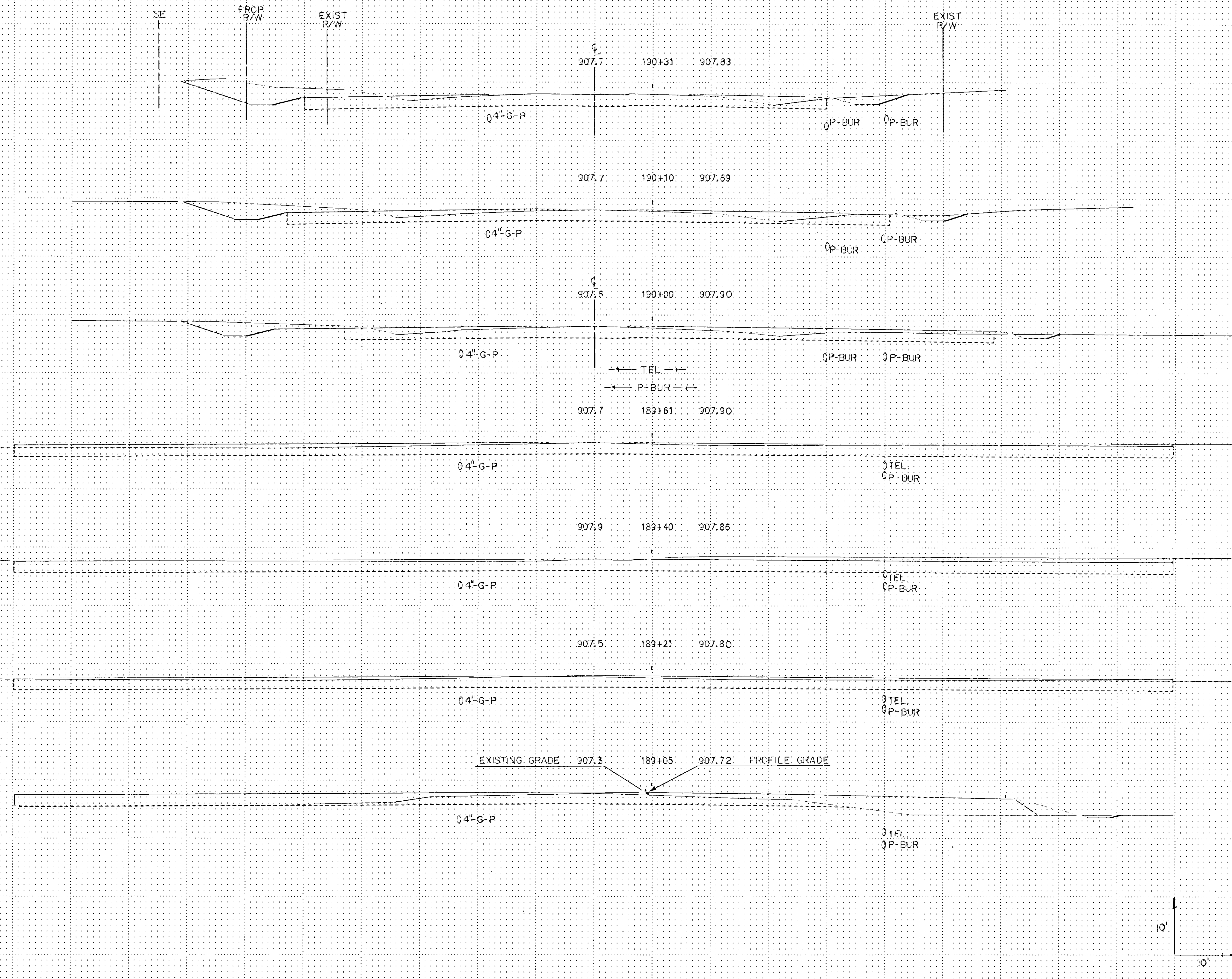
163	1087
389	
0	
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CROSS-SECTION STA. 180+00 TO STA. 189+00

Copy Equipment Form #1

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS



178	152
71	79
377	448
306	311
272	261
134	232
18	65

CROSS-SECTION
STA. 189+05 TO STA. 190+31

Copy Statement Form #1 5/14/65

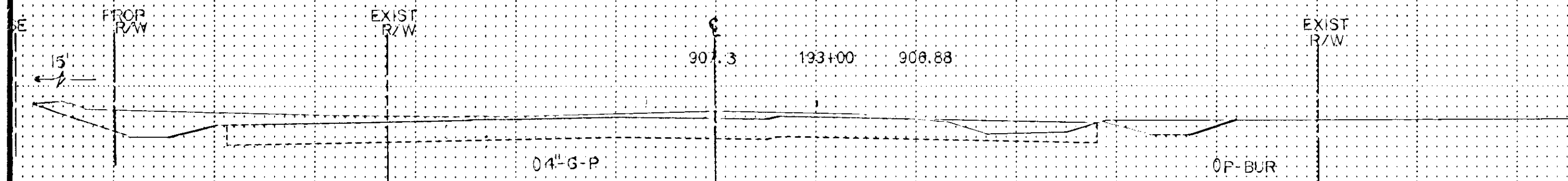
EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

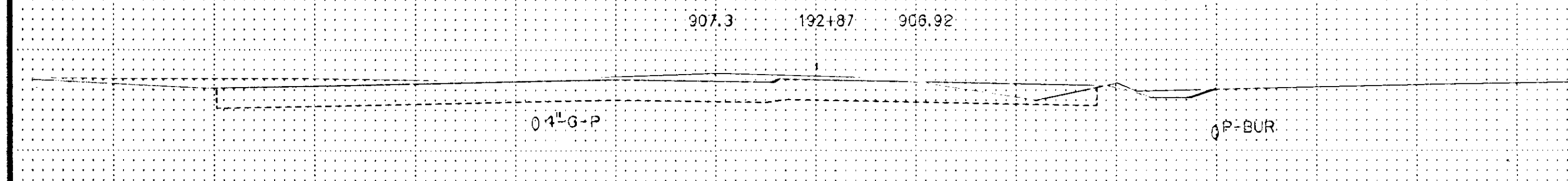
Fed. Proj. No.

EXCAVATION EMBANKMENT

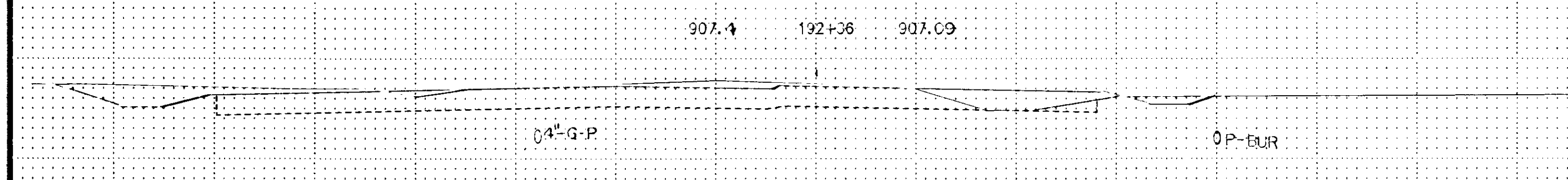
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



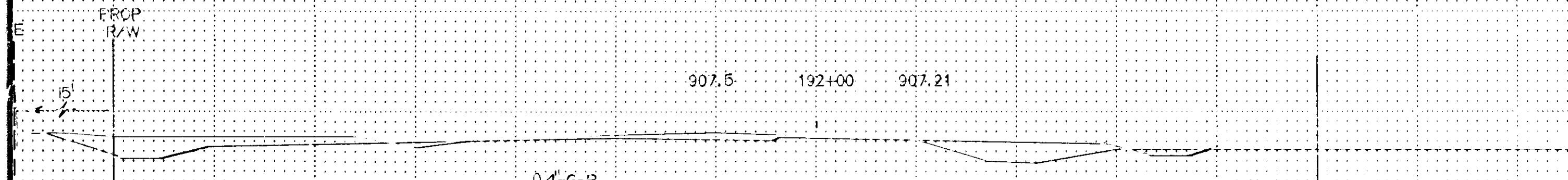
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393 338

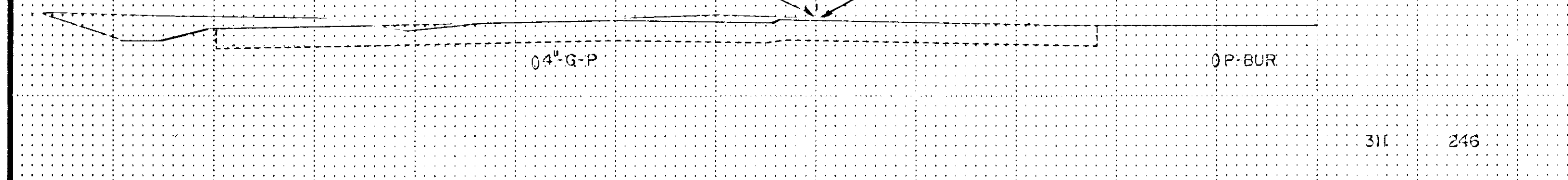


269 238

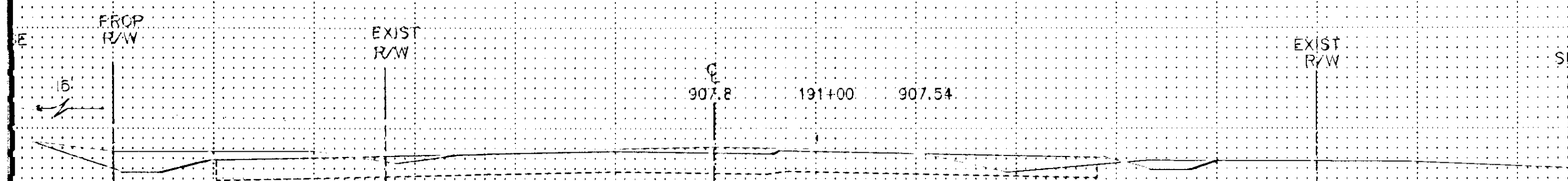


440 347

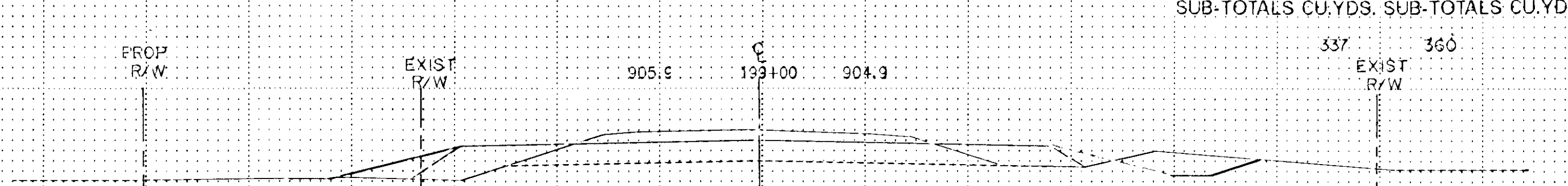
EXISTING GRADE 907.7 191+41 907.41 PROFILE GRADE



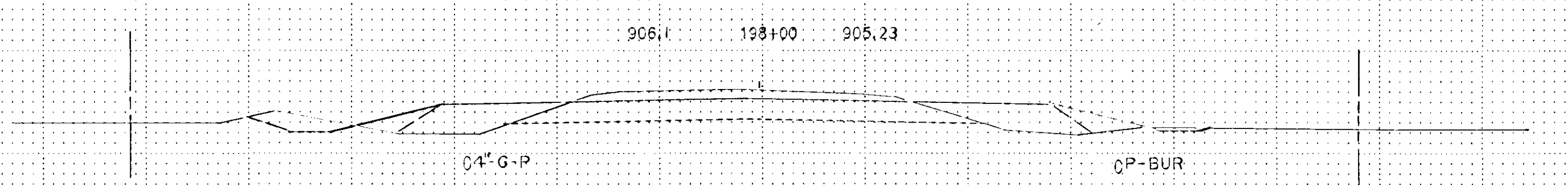
311 246



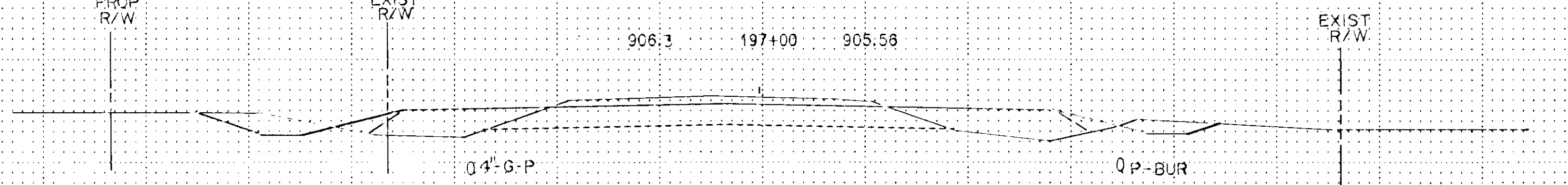
581 461



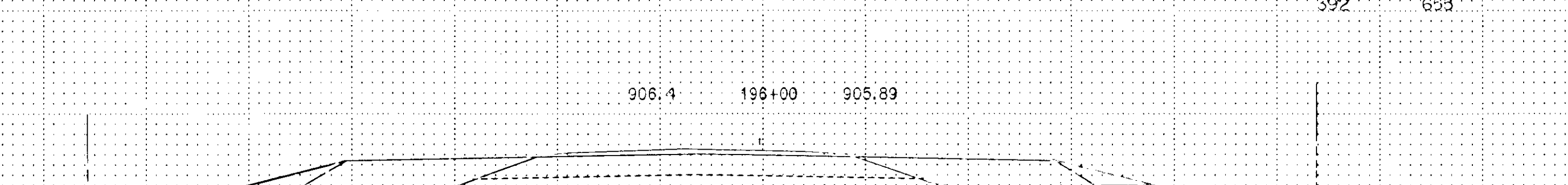
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450 574

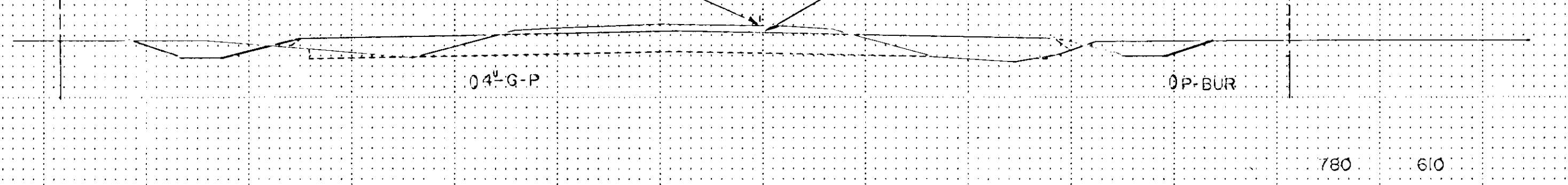


392 693

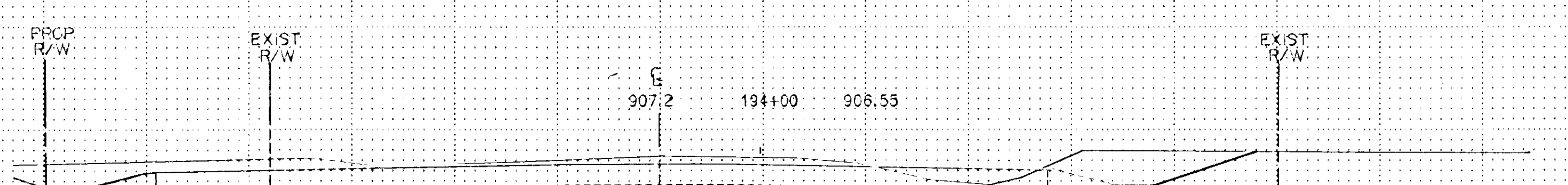


419 652

EXISTING GRADE 906.9 195+00 906.22 PROFILE GRADE



780 610



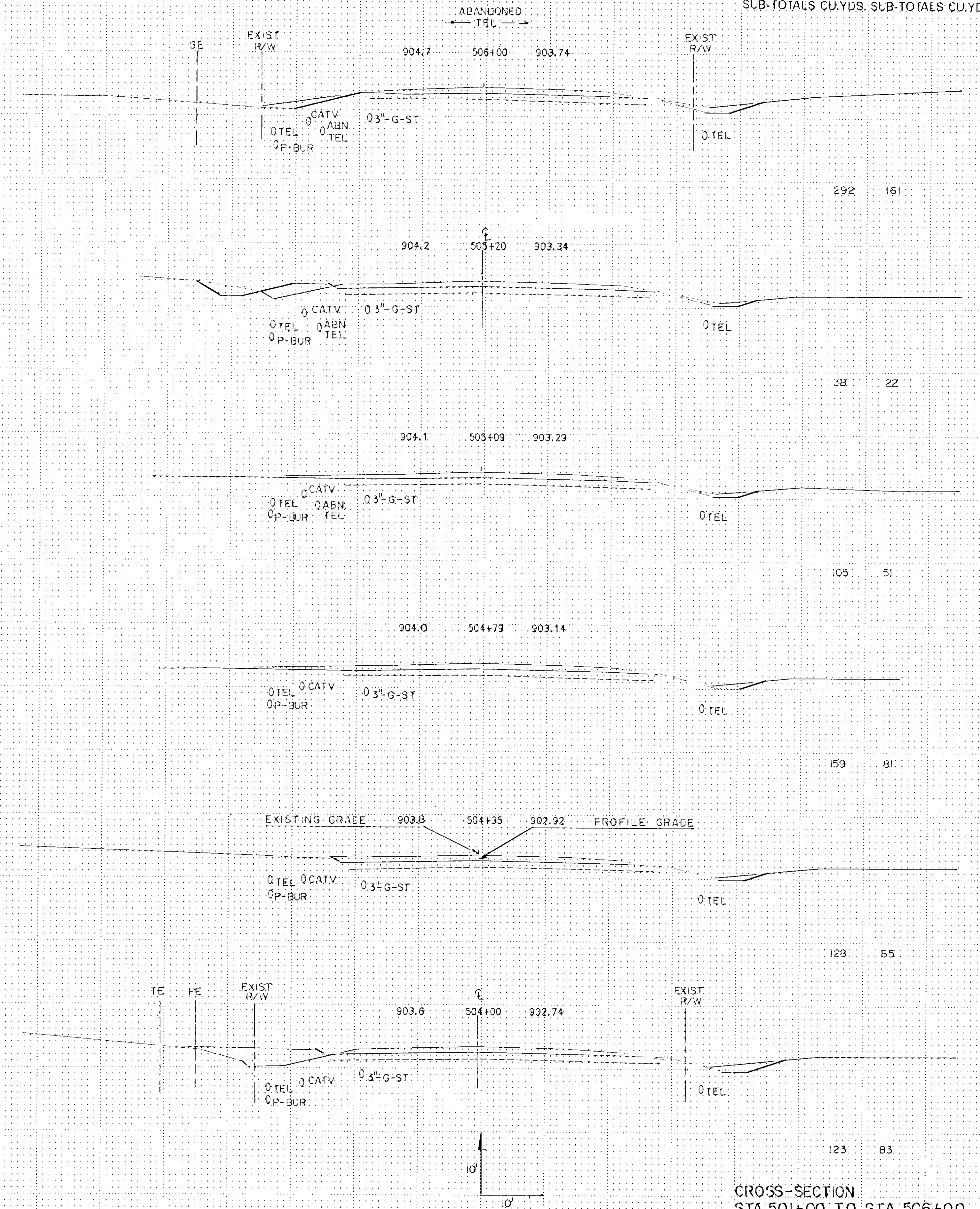
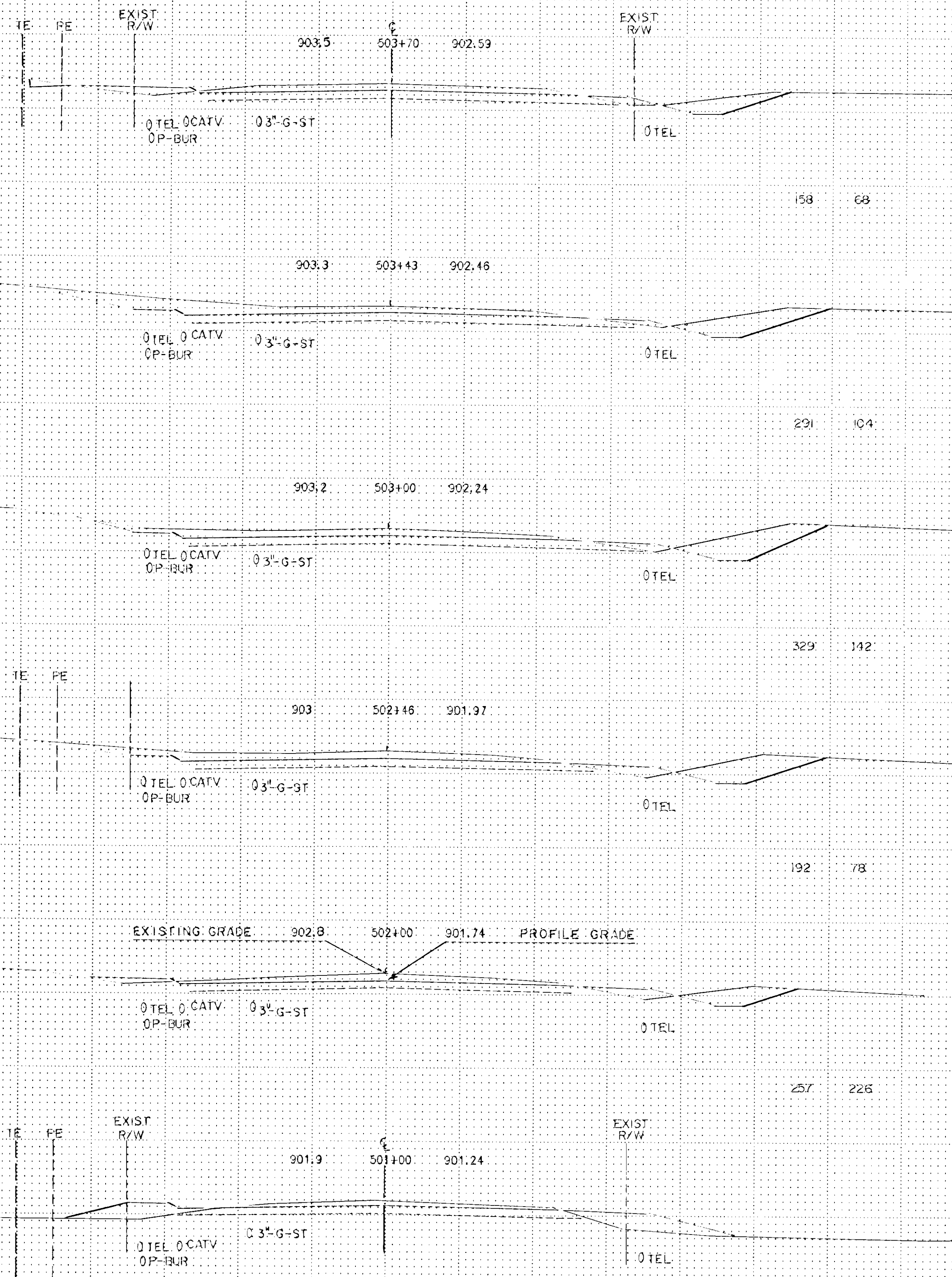
983 654

CROSS-SECTION STA. 191+00 TO STA. 199+00

Copy Equipment Form #1

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

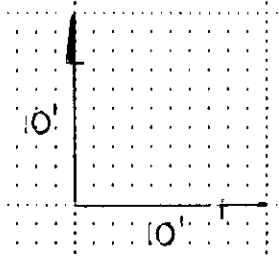
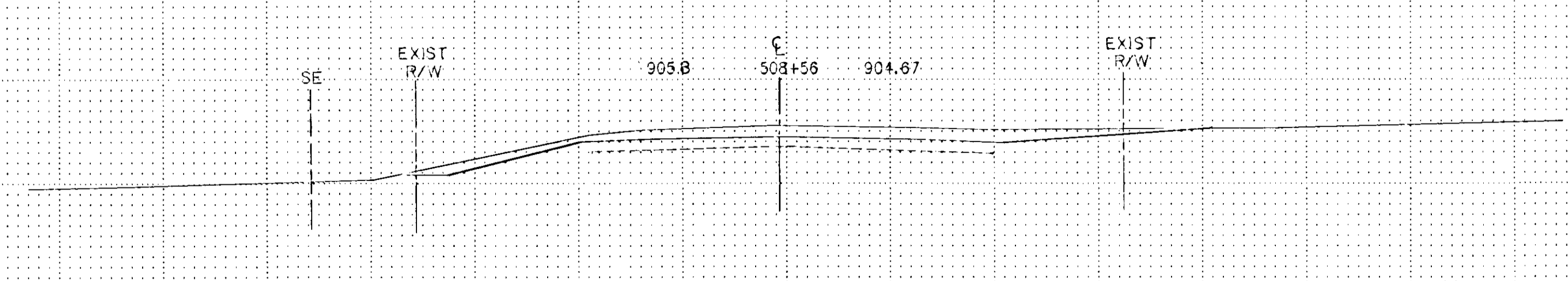
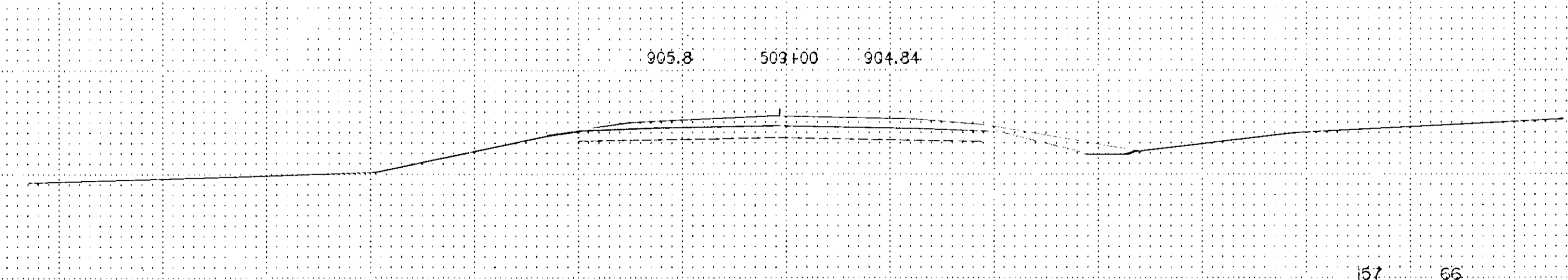
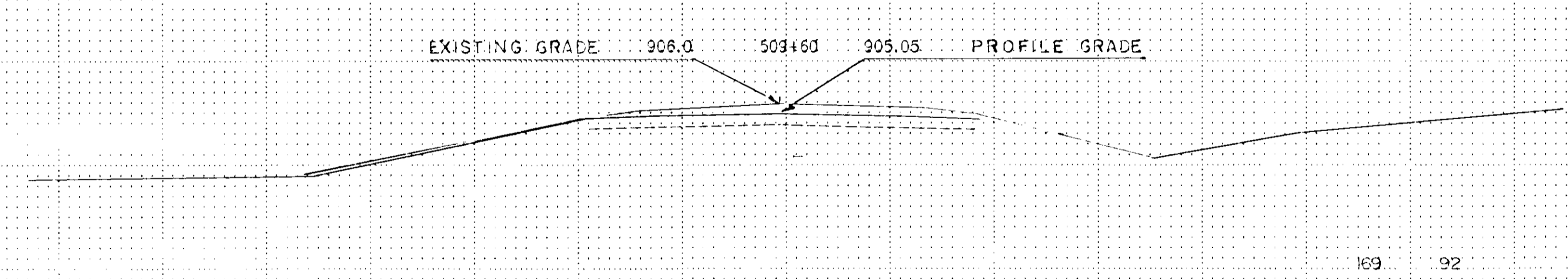
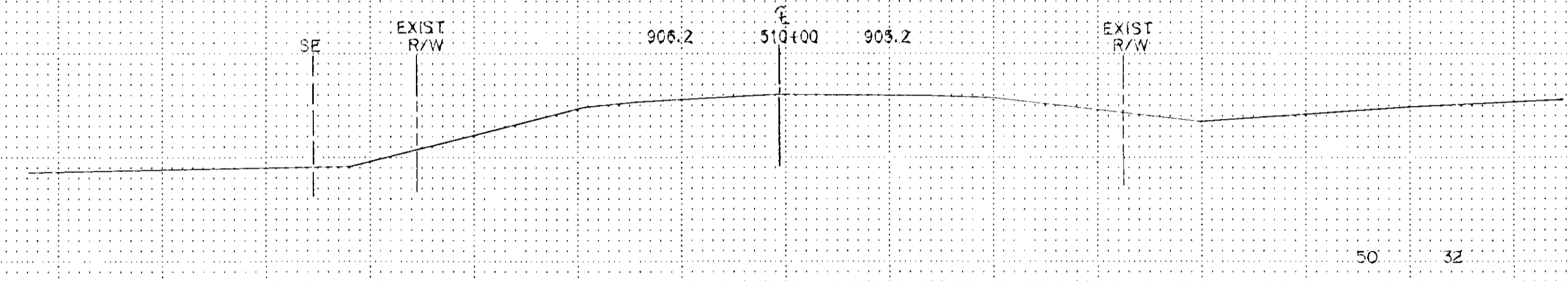
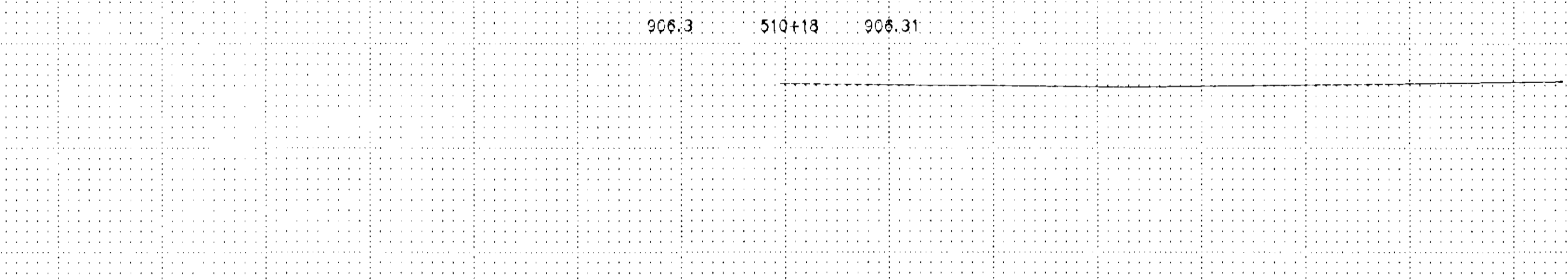
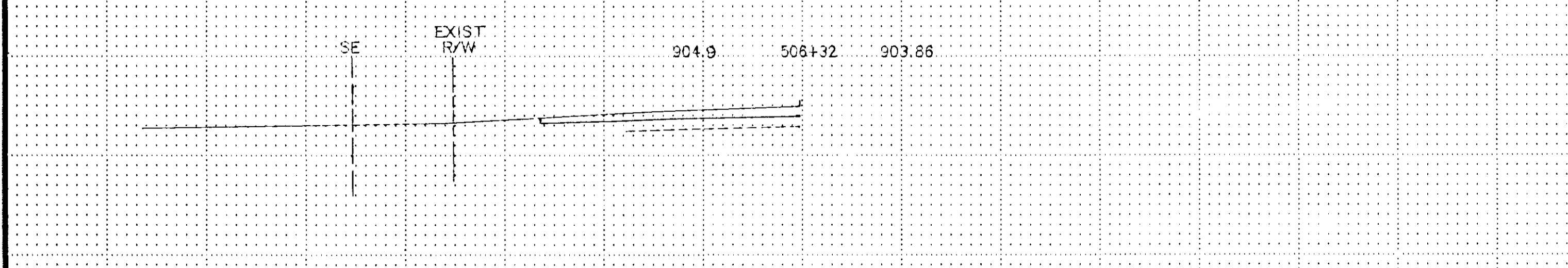
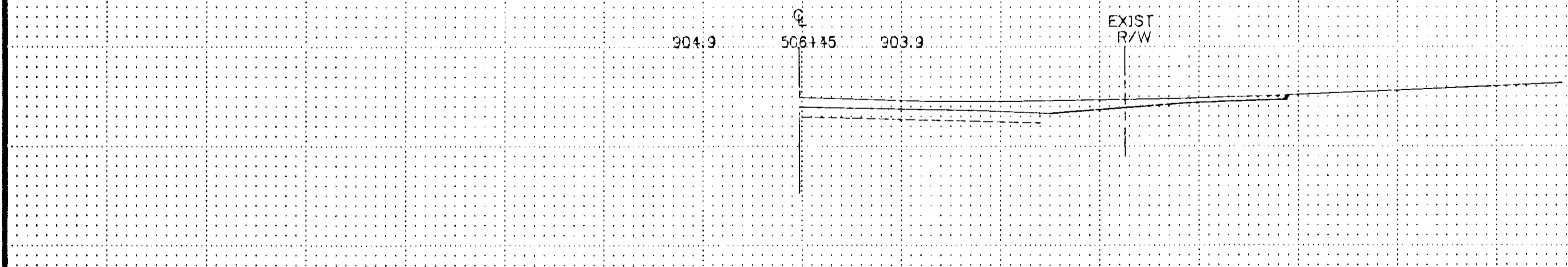
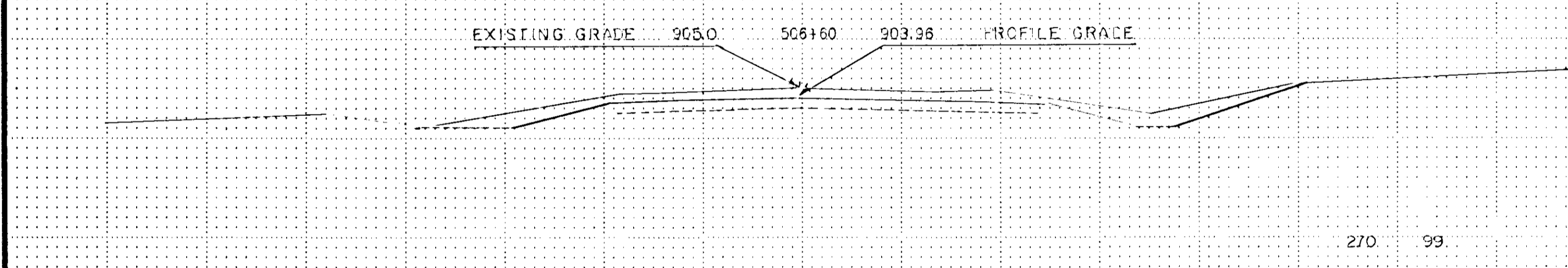
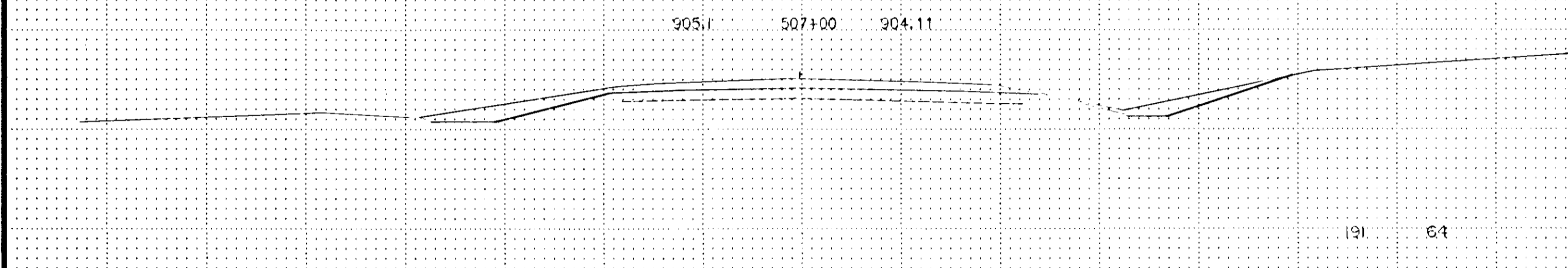
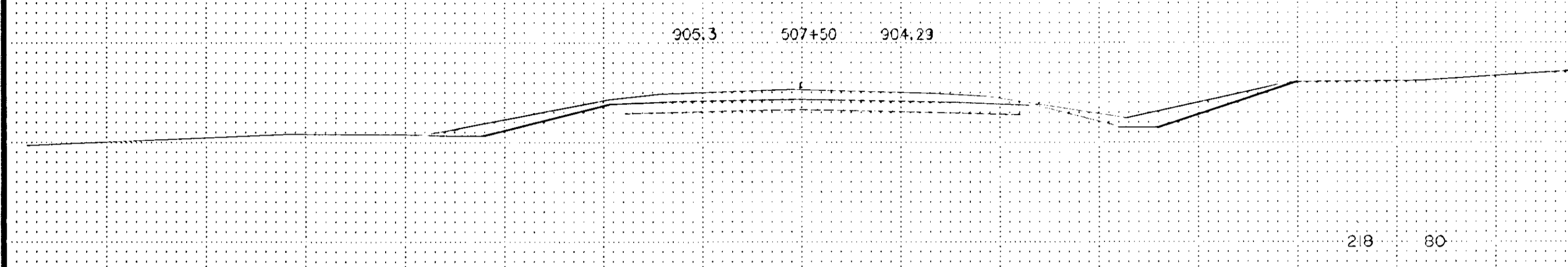
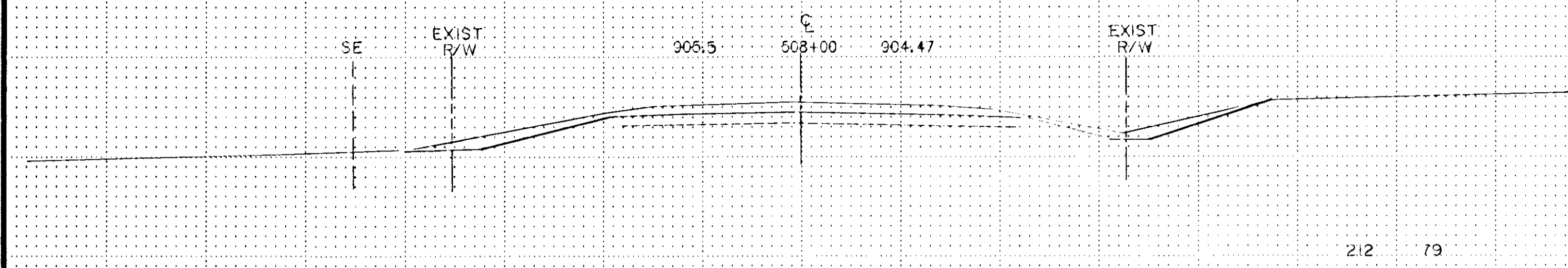


CROSS-SECTION
STA. 501+00 TO STA. 506+00

Copy Equipment Form #1 554445

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.

EXCAVATION EMBANKMENT
SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS.



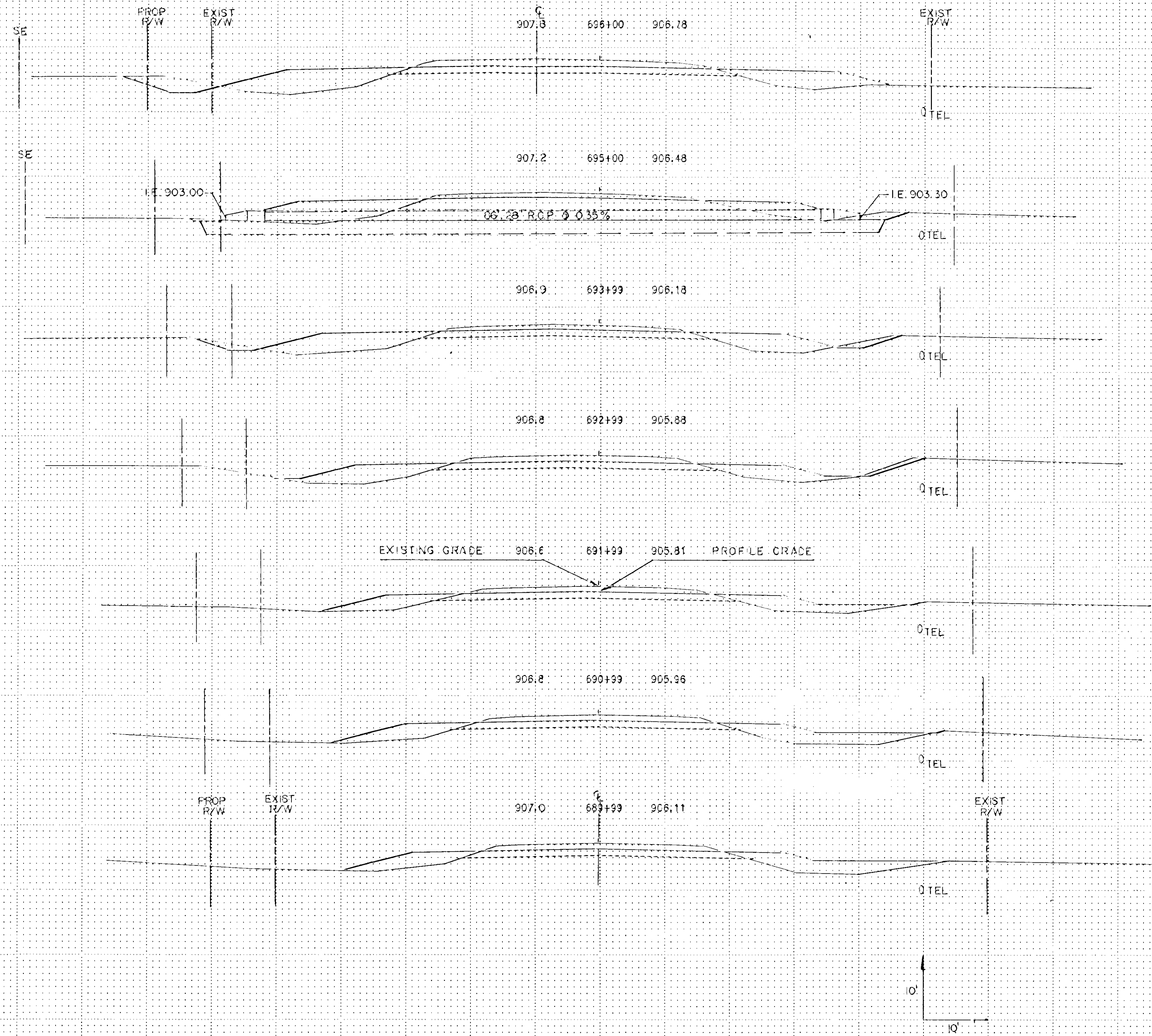
CROSS-SECTION
STA. 506+32 TO STA. 510+18

Copy Equipment Form #1 B15445

Fed. Proj. No.

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS



363 571

295 521

287 472

273 437

256 505

256 557

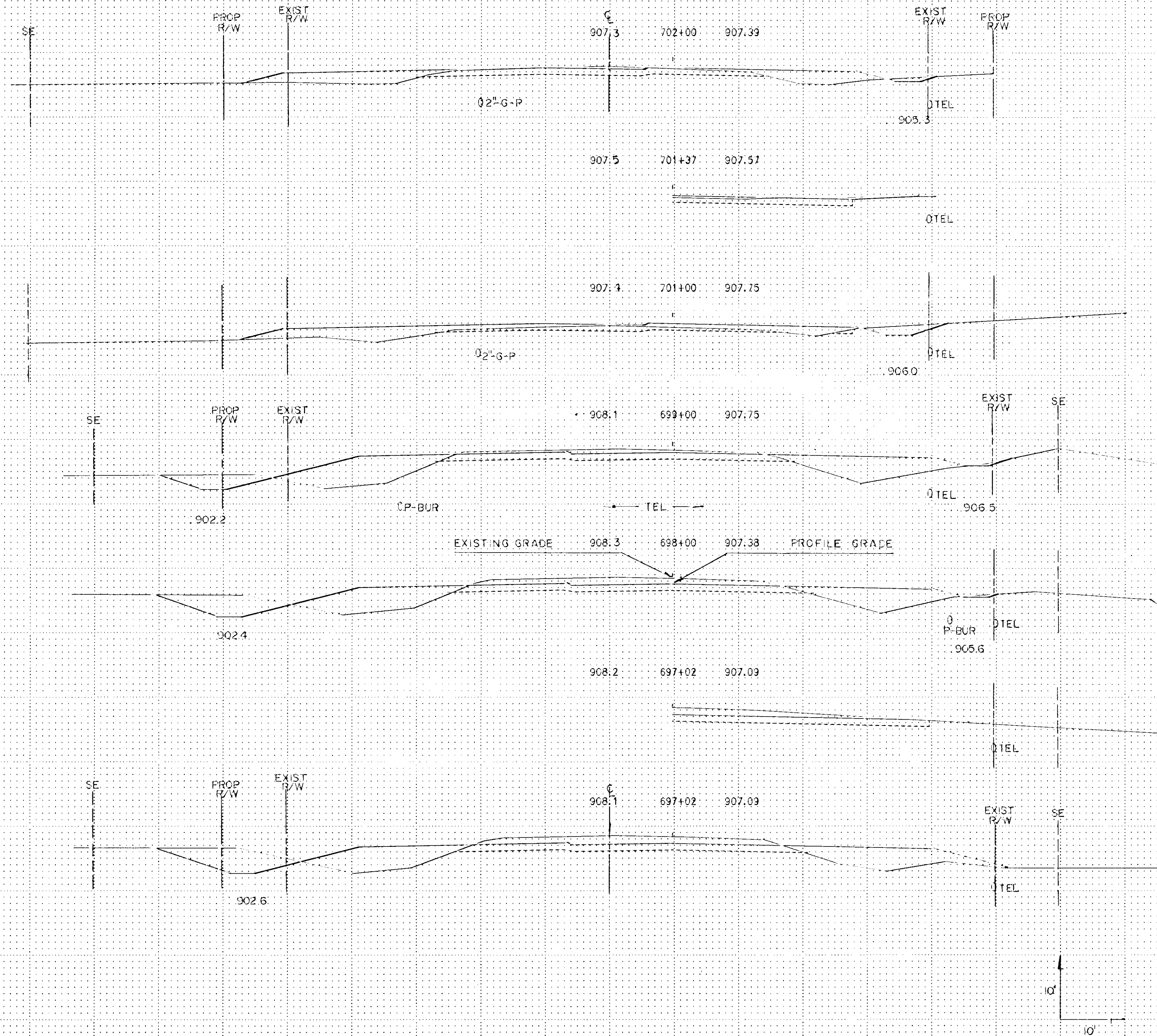
28 56

CROSS-SECTION
STA. 689+99 TO STA. 696+00

Fed. Proj. No.

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS



287 434

441 702

532 671

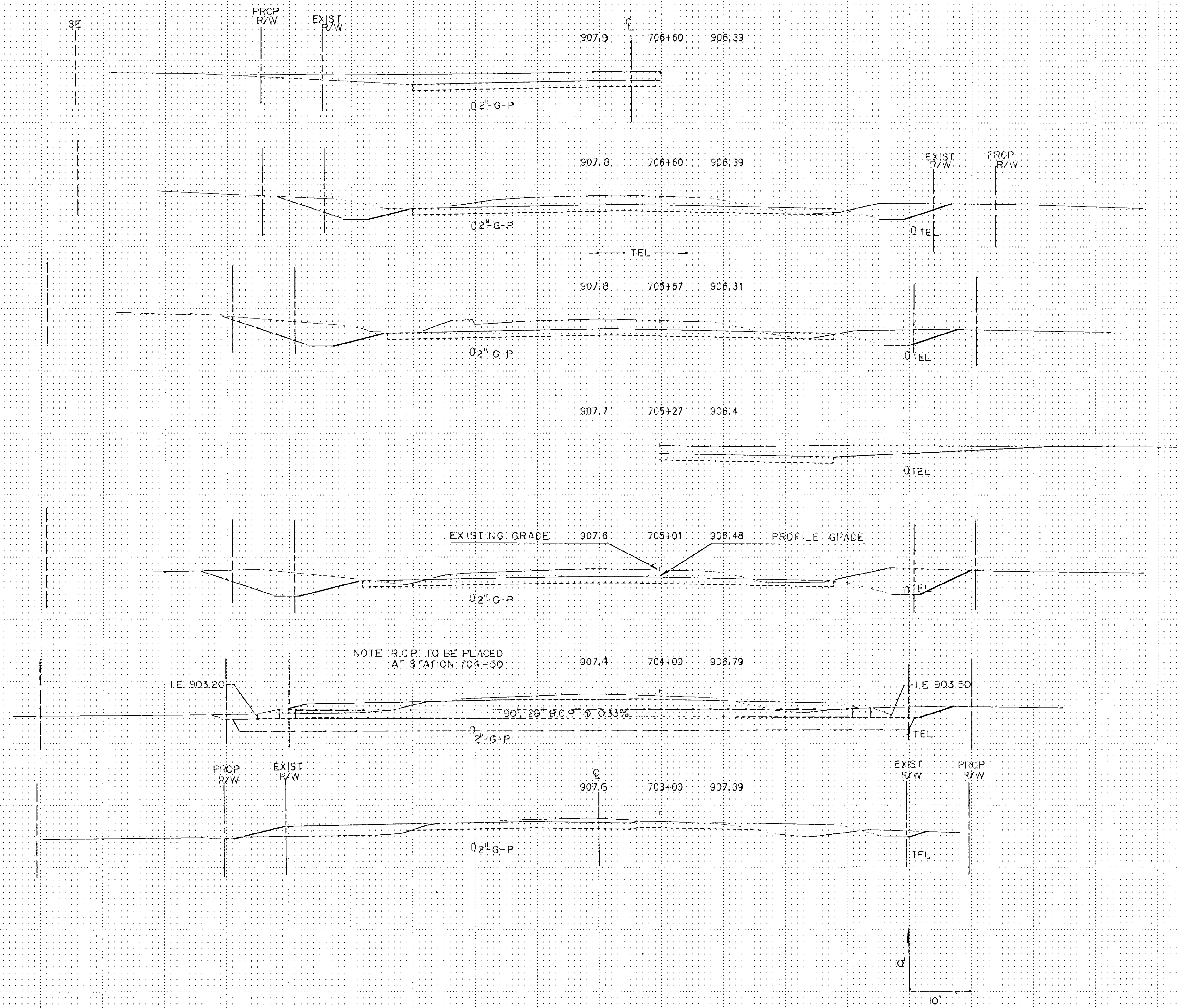
789 643

CROSS-SECTION
STA. 697+02 TO STA. 702+00

Fed. Proj. No.

EXCAVATION EMBANKMENT

SUB-TOTALS CU.YDS. SUB-TOTALS CU.YDS



712 245

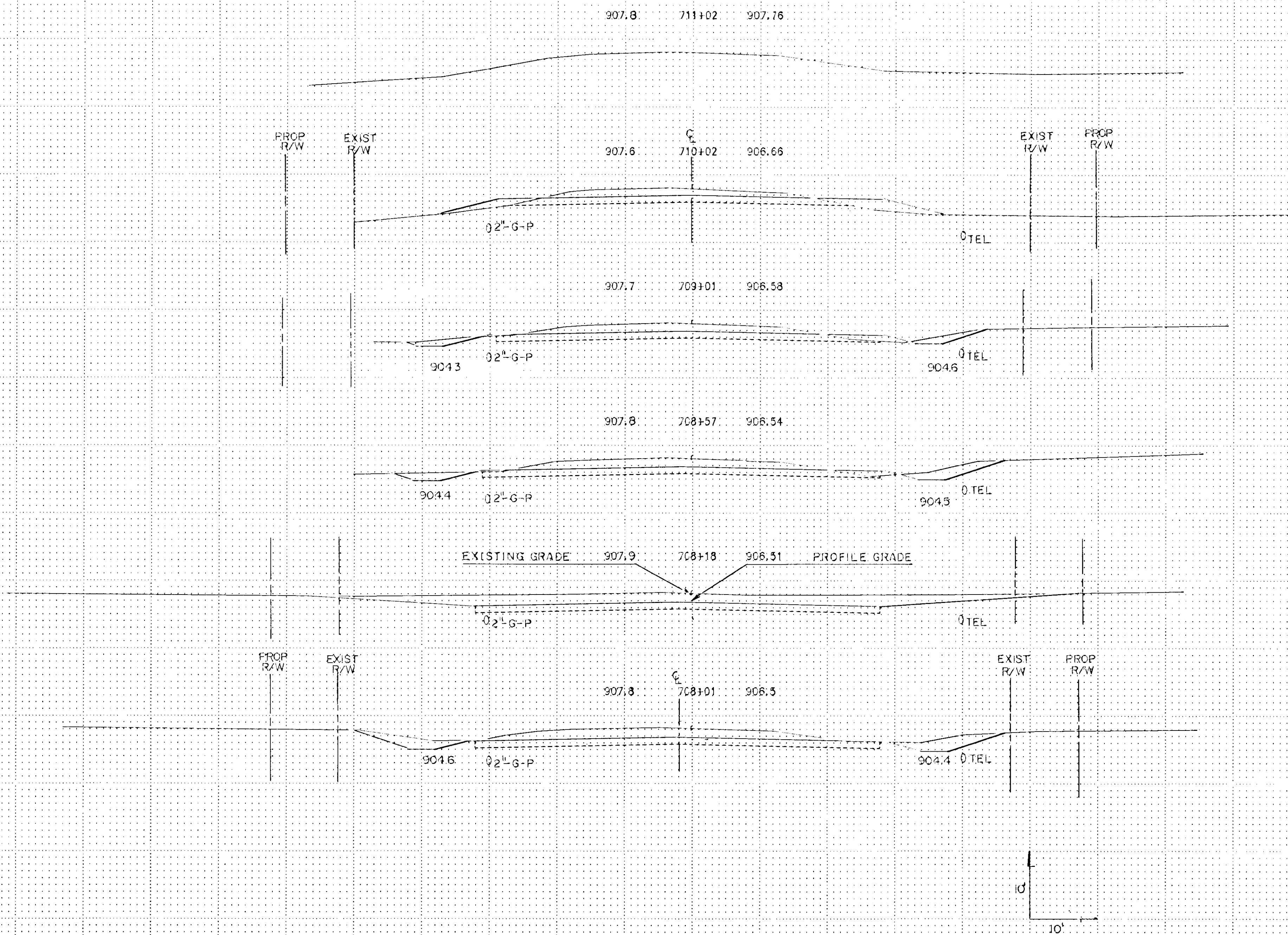
591 186

659 333

358 402

311 446

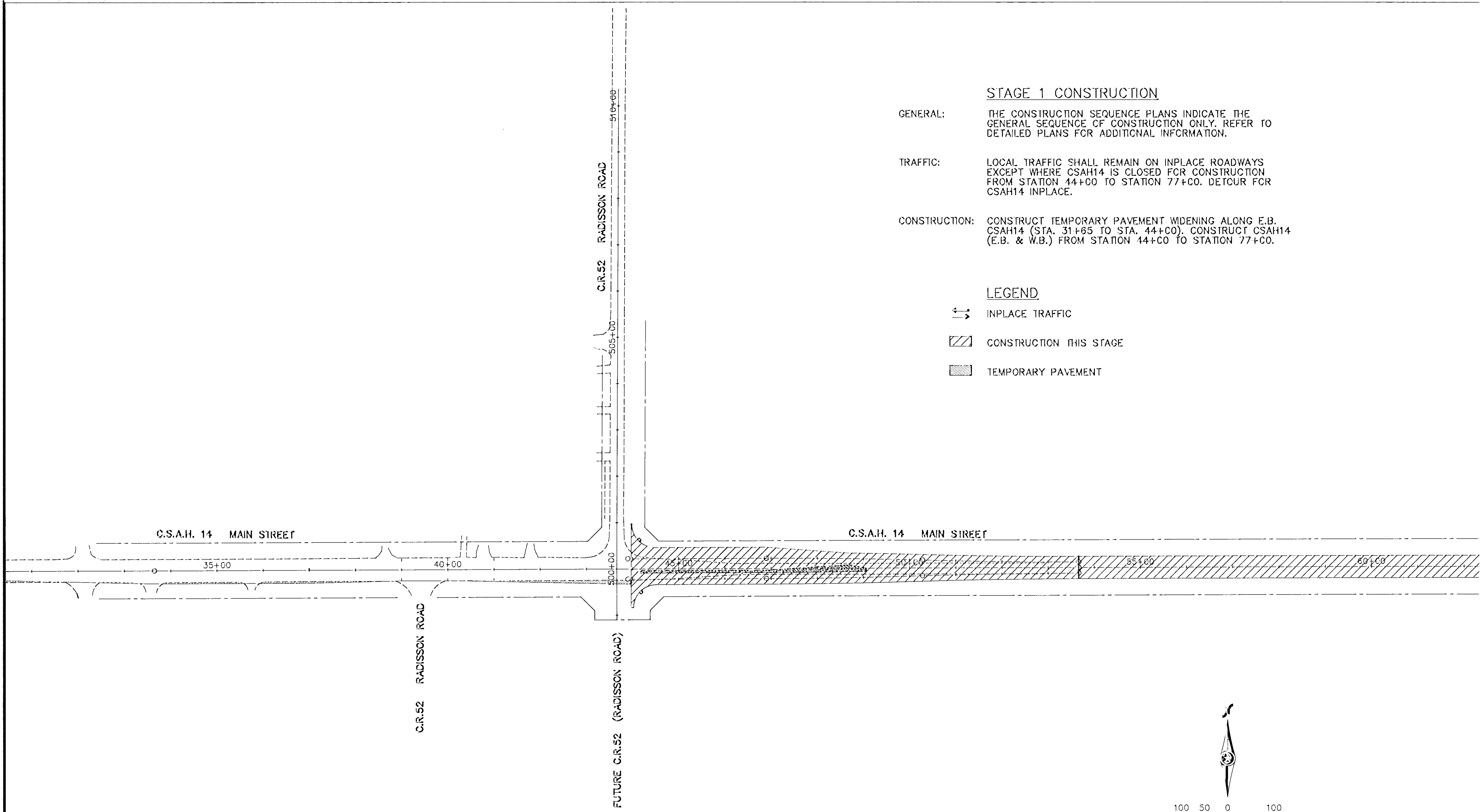
CROSS-SECTION STA. 703+00 TO STA. 706+60



48	26
41	22.6
19	96
233	87
118	41
834	337

CROSS-SECTION
STA. 708+01 TO STA. 711+02

Cross Section Form #1 854445

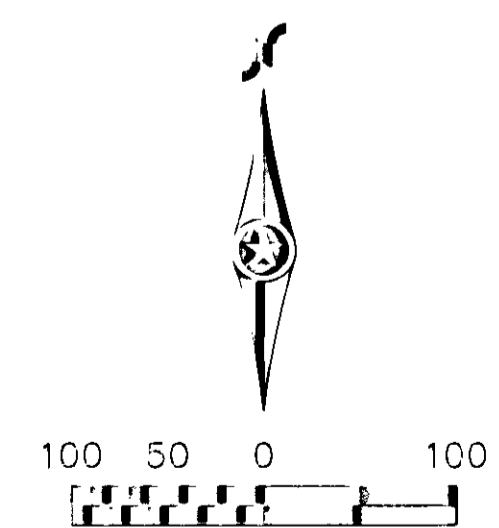


STAGE 1 CONSTRUCTION

- GENERAL:** THE CONSTRUCTION SEQUENCE PLANS INDICATE THE GENERAL SEQUENCE OF CONSTRUCTION ONLY. REFER TO DETAILED PLANS FOR ADDITIONAL INFORMATION.
- TRAFFIC:** LOCAL TRAFFIC SHALL REMAIN ON INPLACE ROADWAYS EXCEPT WHERE CSAH14 IS CLOSED FOR CONSTRUCTION FROM STATION 44+00 TO STATION 77+00. DETOUR FOR CSAH14 INPLACE.
- CONSTRUCTION:** CONSTRUCT TEMPORARY PAVEMENT WIDENING ALONG E.B. CSAH14 (STA. 31+65 TO STA. 44+00), CONSTRUCT CSAH14 (E.B. & W.B.) FROM STATION 44+00 TO STATION 77+00.

LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY PAVEMENT



D:\K & FILE NAME: c:\nokia\vac-5101.dwg
 OPERATOR: JMW
 PLOT DATE: 7-8-92

DESIGN	JMW	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	JMW				



S.A.P. 02--614--14

**STAGE 1
SEQUENCE OF CONSTRUCTION**




FILE NO.	SHEET NO.
5006-61	80
DATE	OF 117
7-8-92	

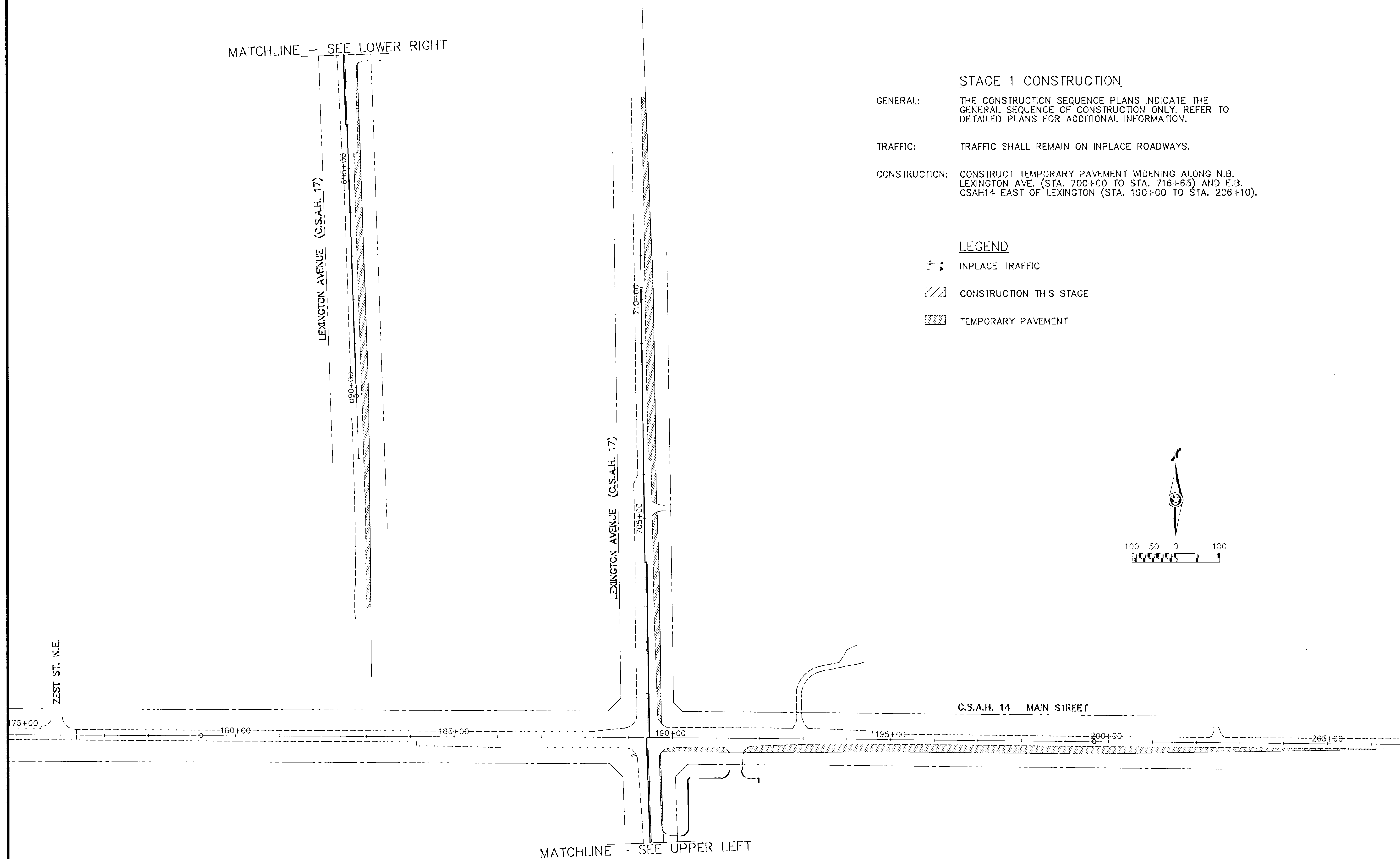
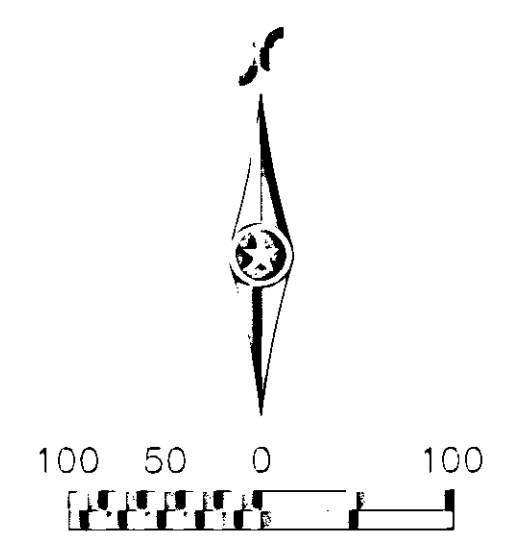
MATCHLINE - SEE LOWER RIGHT

STAGE 1 CONSTRUCTION

- GENERAL:** THE CONSTRUCTION SEQUENCE PLANS INDICATE THE GENERAL SEQUENCE OF CONSTRUCTION ONLY. REFER TO DETAILED PLANS FOR ADDITIONAL INFORMATION.
- TRAFFIC:** TRAFFIC SHALL REMAIN ON INPLACE ROADWAYS.
- CONSTRUCTION:** CONSTRUCT TEMPORARY PAVEMENT WIDENING ALONG N.B. LEXINGTON AVE. (STA. 700+00 TO STA. 716+65) AND E.B. CSAH14 EAST OF LEXINGTON (STA. 190+00 TO STA. 206+10).

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY PAVEMENT



MATCHLINE - SEE UPPER LEFT

D:\K & FILE NAME: c:\work\AC-STG1.DWG
 PLOT SCALE: 1"=100'
 DATE: 7-8-92

	DESIGN	JMW	SCALE	AS	NO.	DATE	REVISION
CHECKED	KJWH		SHOWN				
DRAWN	JMW						


Barton-Aschman Associates, Inc.
 111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55401

S.A.P. 02-614-14




STAGE 1
SEQUENCE OF CONSTRUCTION

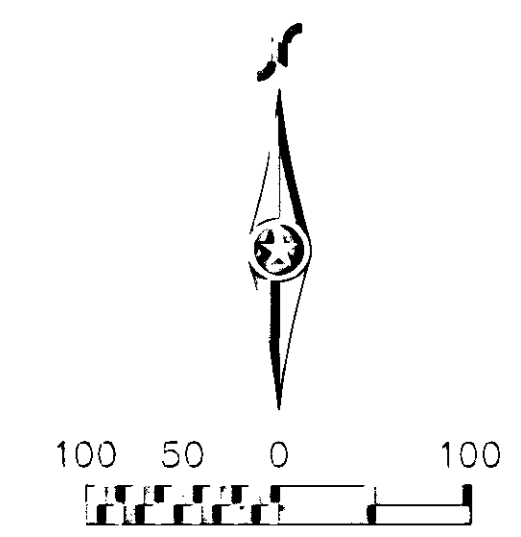
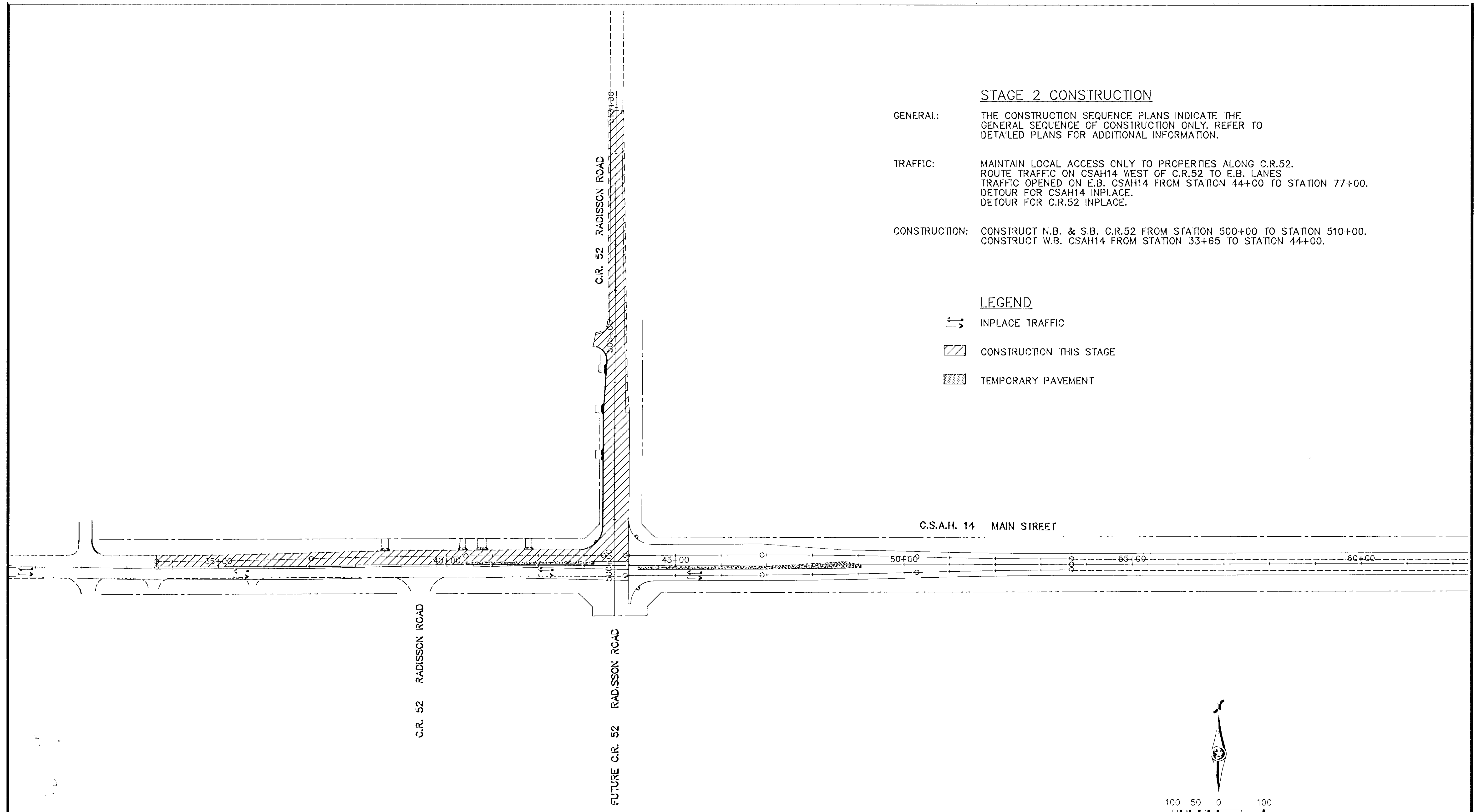
FILE NO.	50C6-61	SHEET NO.	81
DATE	7-8-92	OF	117

STAGE 2 CONSTRUCTION

- GENERAL:** THE CONSTRUCTION SEQUENCE PLANS INDICATE THE GENERAL SEQUENCE OF CONSTRUCTION ONLY. REFER TO DETAILED PLANS FOR ADDITIONAL INFORMATION.
- TRAFFIC:** MAINTAIN LOCAL ACCESS ONLY TO PROPERTIES ALONG C.R.52. ROUTE TRAFFIC ON CSAH14 WEST OF C.R.52 TO E.B. LANES. TRAFFIC OPENED ON E.B. CSAH14 FROM STATION 44+00 TO STATION 77+00. DETOUR FOR CSAH14 INPLACE. DETOUR FOR C.R.52 INPLACE.
- CONSTRUCTION:** CONSTRUCT N.B. & S.B. C.R.52 FROM STATION 500+00 TO STATION 510+00. CONSTRUCT W.B. CSAH14 FROM STATION 33+65 TO STATION 44+00.

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY PAVEMENT



DIR & FILE NAME: C:\vnuwa\AC-5102.dwg
 PLOT SCALE: 1"=100'
 DATE: 7-8-92

DESIGN	JMW	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	JMW				



S.A.P. 02-614-14

**STAGE 2
 SEQUENCE OF CONSTRUCTION**


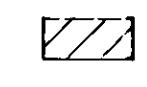


FILE NO.	SHEET NO.
50C6-61	82
DATE	OF
7-8-92	117

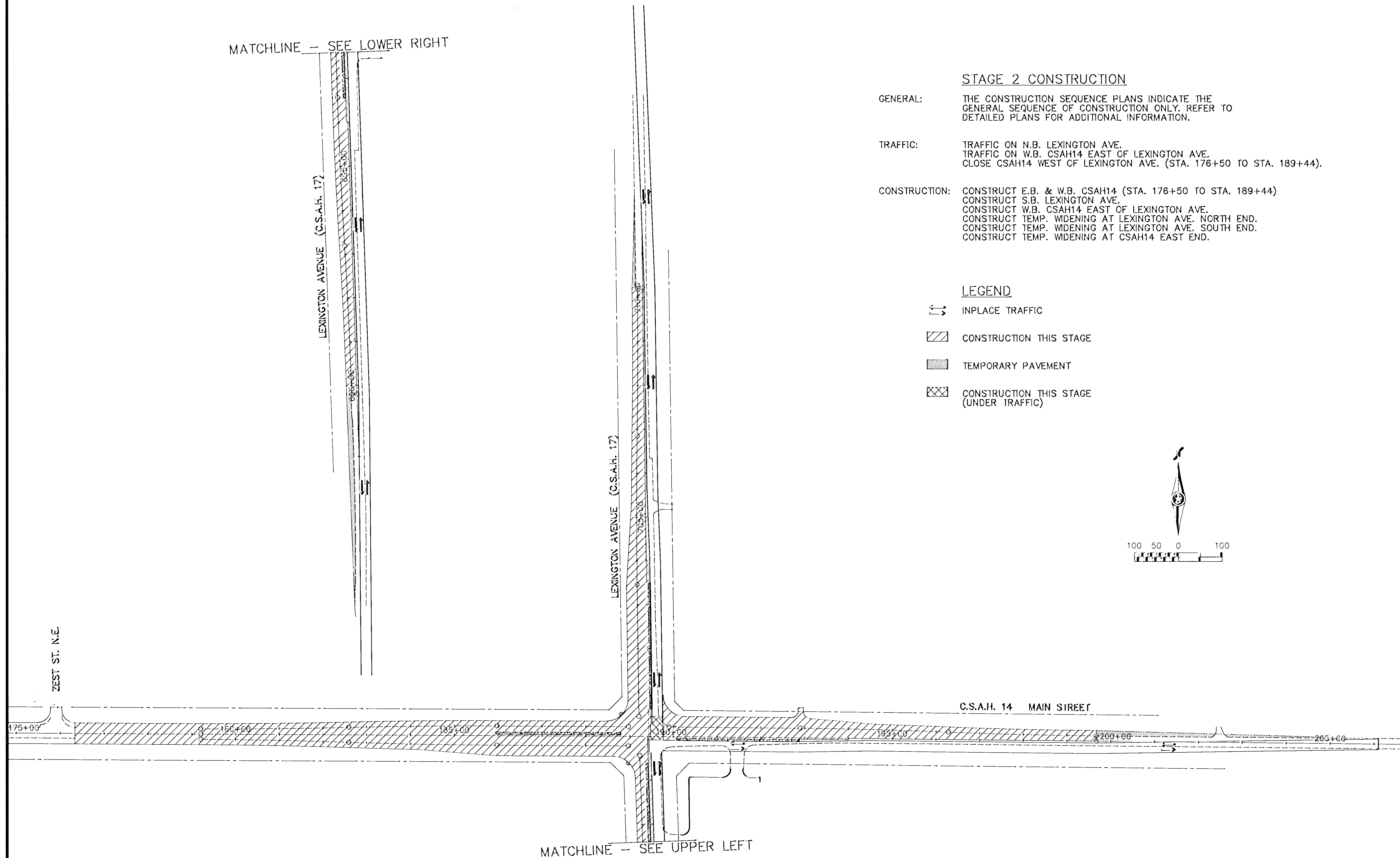
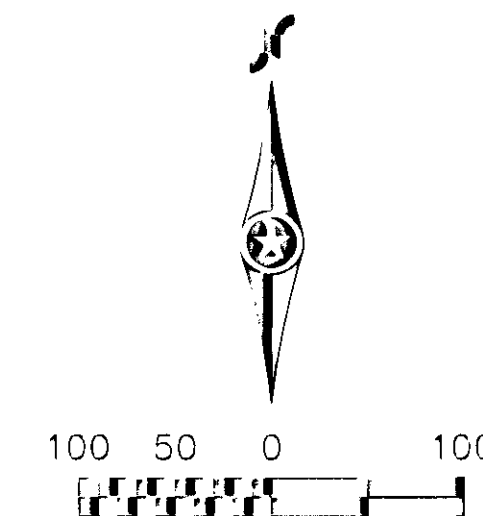
MATCHLINE - SEE LOWER RIGHT

STAGE 2 CONSTRUCTION

- GENERAL:** THE CONSTRUCTION SEQUENCE PLANS INDICATE THE GENERAL SEQUENCE OF CONSTRUCTION ONLY. REFER TO DETAILED PLANS FOR ADDITIONAL INFORMATION.
- TRAFFIC:** TRAFFIC ON N.B. LEXINGTON AVE.
TRAFFIC ON W.B. CSAH14 EAST OF LEXINGTON AVE.
CLOSE CSAH14 WEST OF LEXINGTON AVE. (STA. 176+50 TO STA. 189+44).
- CONSTRUCTION:** CONSTRUCT E.B. & W.B. CSAH14 (STA. 176+50 TO STA. 189+44)
CONSTRUCT S.B. LEXINGTON AVE.
CONSTRUCT W.B. CSAH14 EAST OF LEXINGTON AVE.
CONSTRUCT TEMP. WIDENING AT LEXINGTON AVE. NORTH END.
CONSTRUCT TEMP. WIDENING AT LEXINGTON AVE. SOUTH END.
CONSTRUCT TEMP. WIDENING AT CSAH14 EAST END.

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY PAVEMENT
-  CONSTRUCTION THIS STAGE (UNDER TRAFFIC)



MATCHLINE - SEE UPPER LEFT

D:\K & FILE NAME: C:\vorking\AC-5702.DWG
 OPERATOR: JMW
 PLOT DATE: 7-8-92
 DATE: 7-8-92

DESIGN	JMW	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	JMW				


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 111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55401

S.A.P. 02-614-14

STAGE 2
SEQUENCE OF CONSTRUCTION

FILE NO.	SHEET NO.
5006-61	83
DATE	
7-8-92	117





STAGE 3 CONSTRUCTION

GENERAL: THE CONSTRUCTION SEQUENCE PLANS INDICATE THE GENERAL SEQUENCE OF CONSTRUCTION ONLY. REFER TO DETAILED PLANS FOR ADDITIONAL INFORMATION.

TRAFFIC: TRAFFIC OPENED ON N.B. & S.B. C.R.52 (STA. 500+00 TO STA. 510+00). TRAFFIC ON CSAH14 WEST OF STA. 49+00 ROUTED TO W.B. LANES. DETOUR FOR CSAH14 INPLACE.

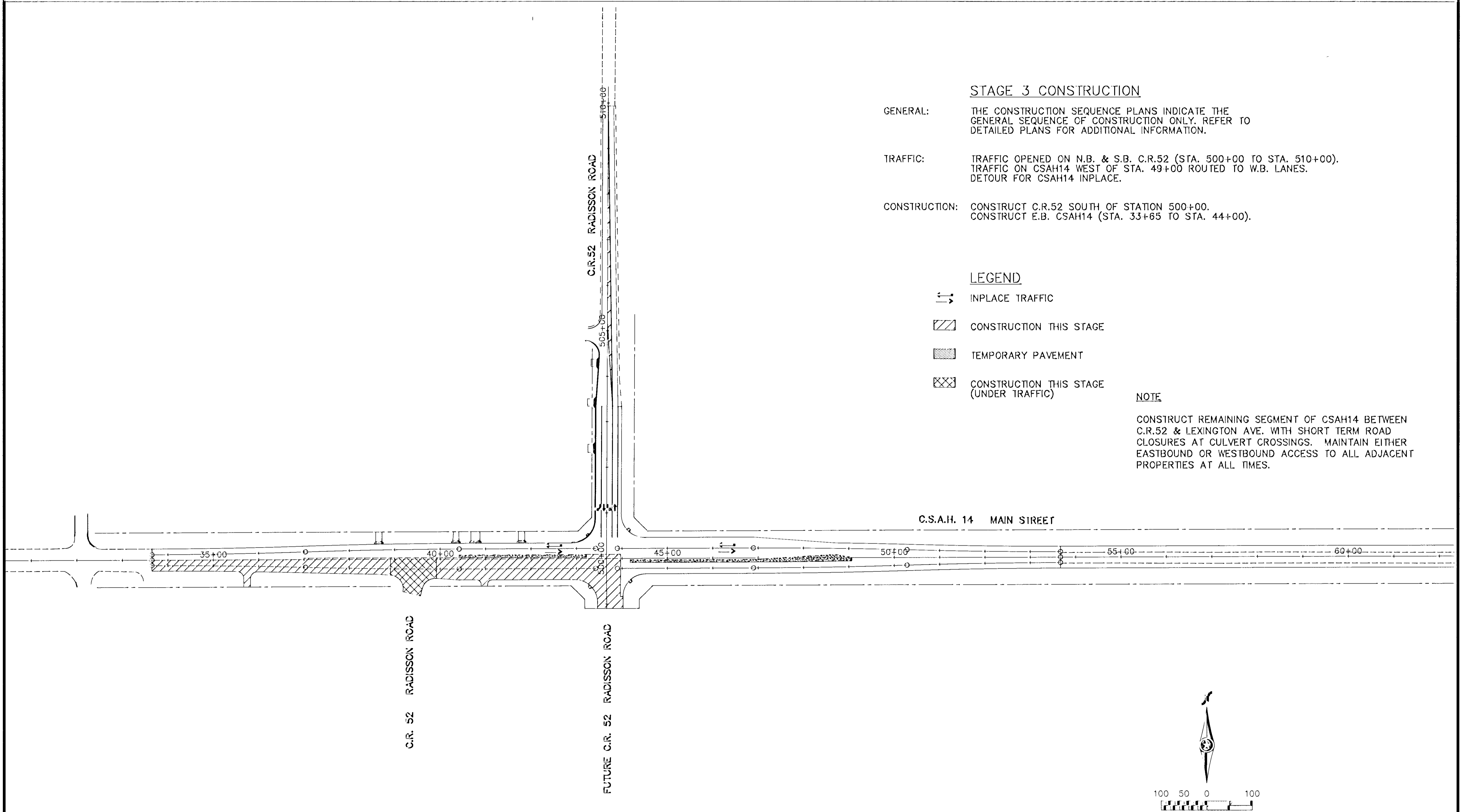
CONSTRUCTION: CONSTRUCT C.R.52 SOUTH OF STATION 500+00. CONSTRUCT E.B. CSAH14 (STA. 33+65 TO STA. 44+00).

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY PAVEMENT
-  CONSTRUCTION THIS STAGE (UNDER TRAFFIC)

NOTE

CONSTRUCT REMAINING SEGMENT OF CSAH14 BETWEEN C.R.52 & LEXINGTON AVE. WITH SHORT TERM ROAD CLOSURES AT CULVERT CROSSINGS. MAINTAIN EITHER EASTBOUND OR WESTBOUND ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES.



DIR & FILE NAME: c:\mxd\mc-stg3.dwg
 OPERATOR: JMW
 PLOT SCALE: 1"=100'
 DATE: 7-8-92

	DESIGN	CHECKED	DRAWN	SCALE	NO.	DATE	REVISION
	JMW	KWH	JMW	AS			
				SHCWN			



S.A.P. 02-614-14

STAGE 3
SEQUENCE OF CONSTRUCTION




FILE NO.	SHEET NO.
5008-61	34
DATE	OF 117
7-8-92	

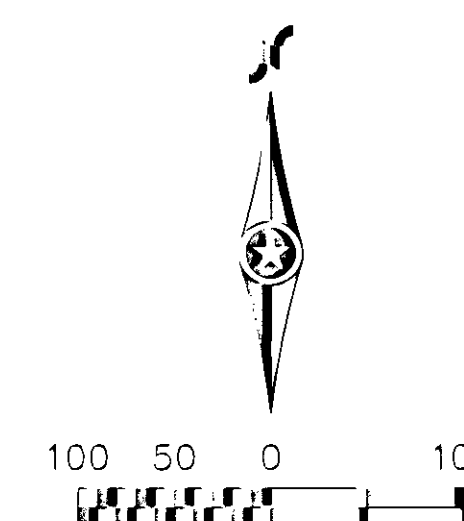
MATCHLINE - SEE LOWER RIGHT

STAGE 3 CONSTRUCTION

- GENERAL:** THE CONSTRUCTION SEQUENCE PLANS INDICATE THE GENERAL SEQUENCE OF CONSTRUCTION ONLY. REFER TO DETAILED PLANS FOR ADDITIONAL INFORMATION.
- TRAFFIC:** TRAFFIC OPENED ON CSAH14 (STA. 176+50 TO STA. 189+44) .
 TRAFFIC ON S.B. LEXINGTON AVE.
 TRAFFIC ON W.B. CSAH14 EAST OF LEXINGTON AVE.
 MAINTAIN ACCESS TO PROPERTY ON S.E. CORNER AT ALL TIMES.
- CONSTRUCTION:** CONSTRUCT N.B. LEXINGTON AVE.
 CONSTRUCT E.B. CSAH14 EAST OF LEXINGTON AVE.

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY PAVEMENT



NOTE

CONSTRUCT REMAINING SEGMENT OF CSAH14 BETWEEN C.R.52 & LEXINGTON AVE. WITH SHORT TERM ROAD CLOSURES AT CULVERT CROSSINGS. MAINTAIN EITHER EASTBOUND OR WESTBOUND ACCESS TO ALL ADJACENT PROPERTIES AT ALL TIMES.

ZEST ST. N.E.

LEXINGTON AVENUE (C.S.A.H. 17)

C.S.A.H. 14 MAIN STREET

MATCHLINE - SEE UPPER LEFT

DIR & FILE NAME: c:\arundel\AC-ST03.DWG
 OPERATOR: JMW
 PLOT SCALE: 1"=100'
 DATE: 7-8-92

DESIGN	JMW	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	JMW				



S.A.P. 02--614--14

**STAGE 3
 SEQUENCE OF CONSTRUCTION**

FILE NO.	SHEET NO.
5006-61	85
DATE	OF 117
7-8-92	

CSAH 14 DETOUR "A" ®			
M.U.T.C.D. CODE	SIZE	INSERT	QTY.
G20-X1	60" X 48"	THIS ROAD WILL BE CLOSED BEGINNING (DATE)	2
G20-X2	96" X 84"	ROAD WORK LEXINGTON AVE TO RADISSON RD FOLLOW DETOUR	4
G20-X2	96" X 84"	ROAD WORK RADISSON RD TO LEXINGTON AVE FOLLOW DETOUR	3
M4-8	24" X 12"		9
M3-4	24" X 12"		3
M1-6	24" X 24"		2
M1-6	24" X 24"		1
M4-8	24" X 12"		2
M3-2	24" X 12"		3
M1-6	24" X 24"		2
M1-6	24" X 24"		2
M1-6	24" X 12"		6
M4-8	24" X 12"		5
M1-6	24" X 24"		2
M1-6	24" X 12"		3
M4-8	24" X 12"		2
M1-6	24" X 24"		2
M1-6	24" X 12"		2
M4-8	24" X 12"		2
M1-6	24" X 24"		2
FLASHER	48" X 18"		1
TYPE III	8 FT.		
FLASHER	48" X 18"		1
TYPE III	8 FT.		
FLASHER	60" X 30"		3
TYPE III	8 FT.		
FLASHER	48" X 18"		1
TYPE III	8 FT.		
FLASHER	48" X 48"		4
TYPE III	8 FT.		

CSAH 14 DETOUR "A" CONTINUED ®			
M.U.T.C.D. CODE	SIZE	INSERT	QTY.
FLASHER	48" X 48"		3
W20-2	48" X 48"		1
FLASHER	48" X 48"		2
W20-3	48" X 48"		2
FLASHER	48" X 48"		3
W20-3	48" X 48"		
R1-1	36" X 36"		4
R1-1	48" X 48"		4
R1-4	18" X 6"		8
G20-2	60" X 24"		4

CO. RD. 52 DETOUR "B"			
M.U.T.C.D. CODE	SIZE	INSERT	QTY.
FLASHER	48" X 48"		2
W20-2	48" X 48"		
FLASHER	48" X 48"		1
W20-3	48" X 48"		1
W20-3	48" X 48"		1

CO. RD. 52 DETOUR "B" CONTINUED ®			
M.U.T.C.D. CODE	SIZE	INSERT	QTY.
G20-X2	96" X 84"		1
G20-X2	96" X 84"		1
M4-8	24" X 12"		3
M3-1	24" X 12"		1
M1-6	24" X 24"		3
M4-8	24" X 12"		3
M3-3	24" X 12"		2
M1-6	24" X 24"		1
M4-6	24" X 12"		3
M4-8	24" X 12"		2
M1-6	24" X 24"		1
FLASHER	48" X 18"		1
TYPE III	8 FT.		
FLASHER	48" X 18"		1
TYPE III	8 FT.		
FLASHER	60" X 30"		2
TYPE III	8 FT.		
FLASHER	48" X 30"		5
TYPE III	8 FT.		
FLASHER	60" X 30"		1
TYPE III	8 FT.		
FLASHER	48" X 30"		3
TYPE III	8 FT.		

DIR & FILE NAME: C:\VANDERKAMP\DESIGN.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=1'
 DATE: 7-8-92

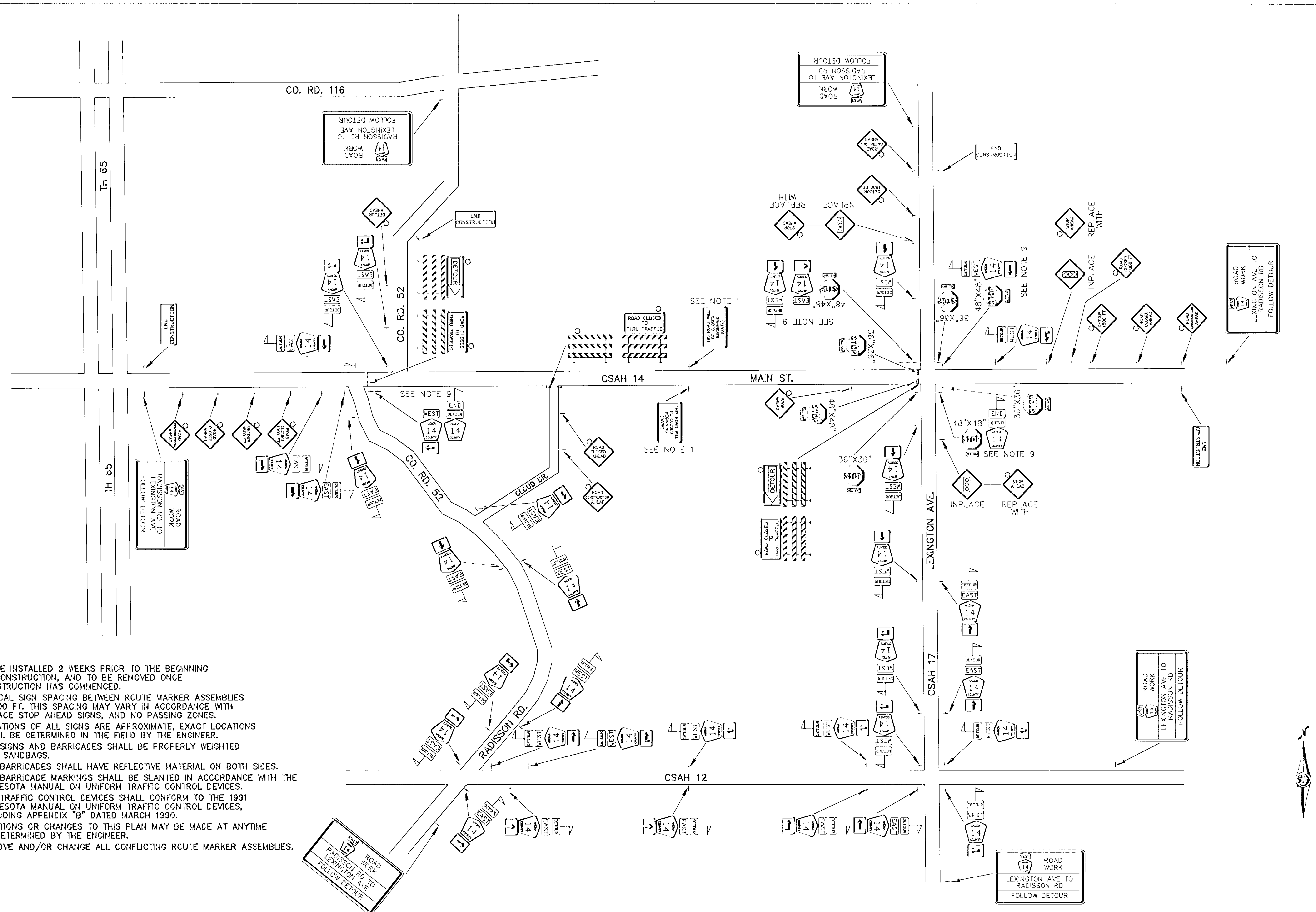
DESIGN	SAP	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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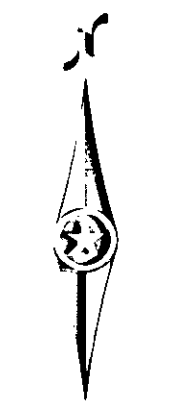
S.A.P. 02-614-14

CSAH 14 DETOUR "A" AND "B"
SIGN TABULATIONS

FILE NO.	SHEET NO.
5006-61	86
DATE	7-8-92



- NOTES:
1. TO BE INSTALLED 2 WEEKS PRIOR TO THE BEGINNING OF CONSTRUCTION, AND TO BE REMOVED ONCE CONSTRUCTION HAS COMMENCED.
 2. TYPICAL SIGN SPACING BETWEEN ROUTE MARKER ASSEMBLIES IS 500 FT. THIS SPACING MAY VARY IN ACCORDANCE WITH INPLACE STOP AHEAD SIGNS, AND NO PASSING ZONES.
 3. LOCATIONS OF ALL SIGNS ARE APPROXIMATE, EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
 4. ALL SIGNS AND BARRICADES SHALL BE PROPERLY WEIGHTED WITH SANDBAGS.
 5. ALL BARRICADES SHALL HAVE REFLECTIVE MATERIAL ON BOTH SIDES.
 6. ALL BARRICADE MARKINGS SHALL BE SLANTED IN ACCORDANCE WITH THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 7. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE 1991 MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING APPENDIX "B" DATED MARCH 1990.
 8. ADDITIONS OR CHANGES TO THIS PLAN MAY BE MADE AT ANYTIME AS DETERMINED BY THE ENGINEER.
 9. REMOVE AND/OR CHANGE ALL CONFLICTING ROUTE MARKER ASSEMBLIES.



DIR & FILE NAME: C:\VANDOKA\DETALD.WG
 OPERATOR: SAP
 PLOT SCALE: 1"=1'
 DATE: 7-8-92

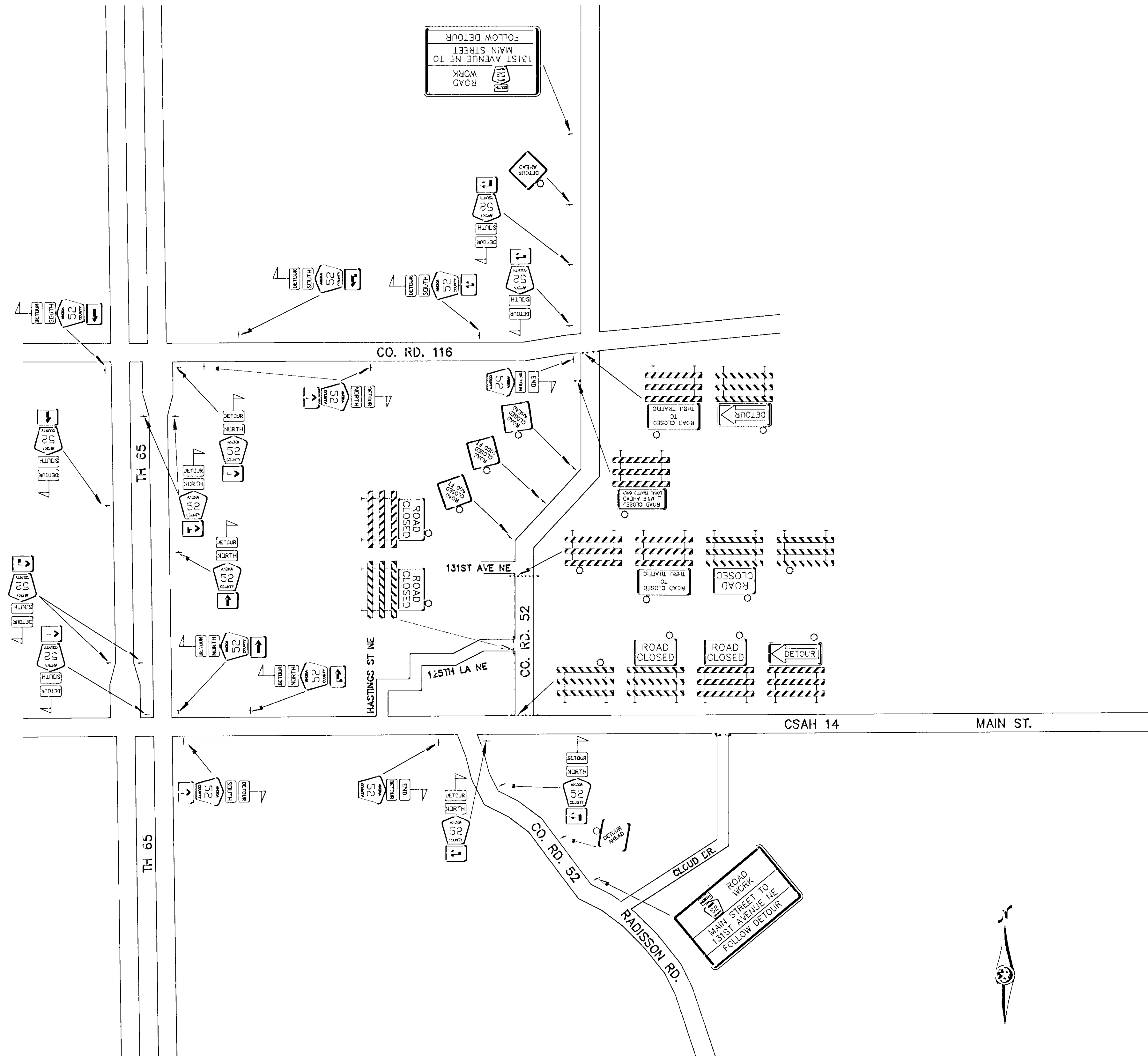
DESIGN	SAP	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			


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S.A.P. 02-614-92

CSAH 14 DETOUR "A"

FILE NO.	SHEET NO.
50C6-61	87
DATE	
7-8-92	OF 117



- NOTES:
1. TYPICAL SIGN SPACING BETWEEN ROUTE MARKER ASSEMBLIES IS 500 FT. THIS SPACING MAY VARY IN ACCORDANCE WITH INPLACE STOP AHEAD SIGNS, AND NO PASSING ZONES.
 2. LOCATIONS OF ALL SIGNS ARE APPROXIMATE, EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
 3. ALL SIGNS AND BARRICADES SHALL BE PROPERLY WEIGHTED WITH SANDBAGS.
 4. ALL BARRICADES SHALL HAVE REFLECTIVE MATERIAL ON BOTH SIDES.
 5. ALL BARRICADE MARKINGS SHALL BE SLANTED IN ACCORDANCE WITH THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 6. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE 1991 MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING APPENDIX "B" DATED MARCH 1990.
 7. ADDITIONS OR CHANGES TO THIS PLAN MAY BE MADE AT ANYTIME AS DETERMINED BY THE ENGINEER.
 8. REMOVE AND/OR CHANGE ALL CONFLICTING ROUTE MARKER ASSEMBLIES.

DIR & FILE NAME: C:\ANOKA\DETOUR.DWG
 PLOT SCALE: 1"=100'
 DATE: 7-8-92

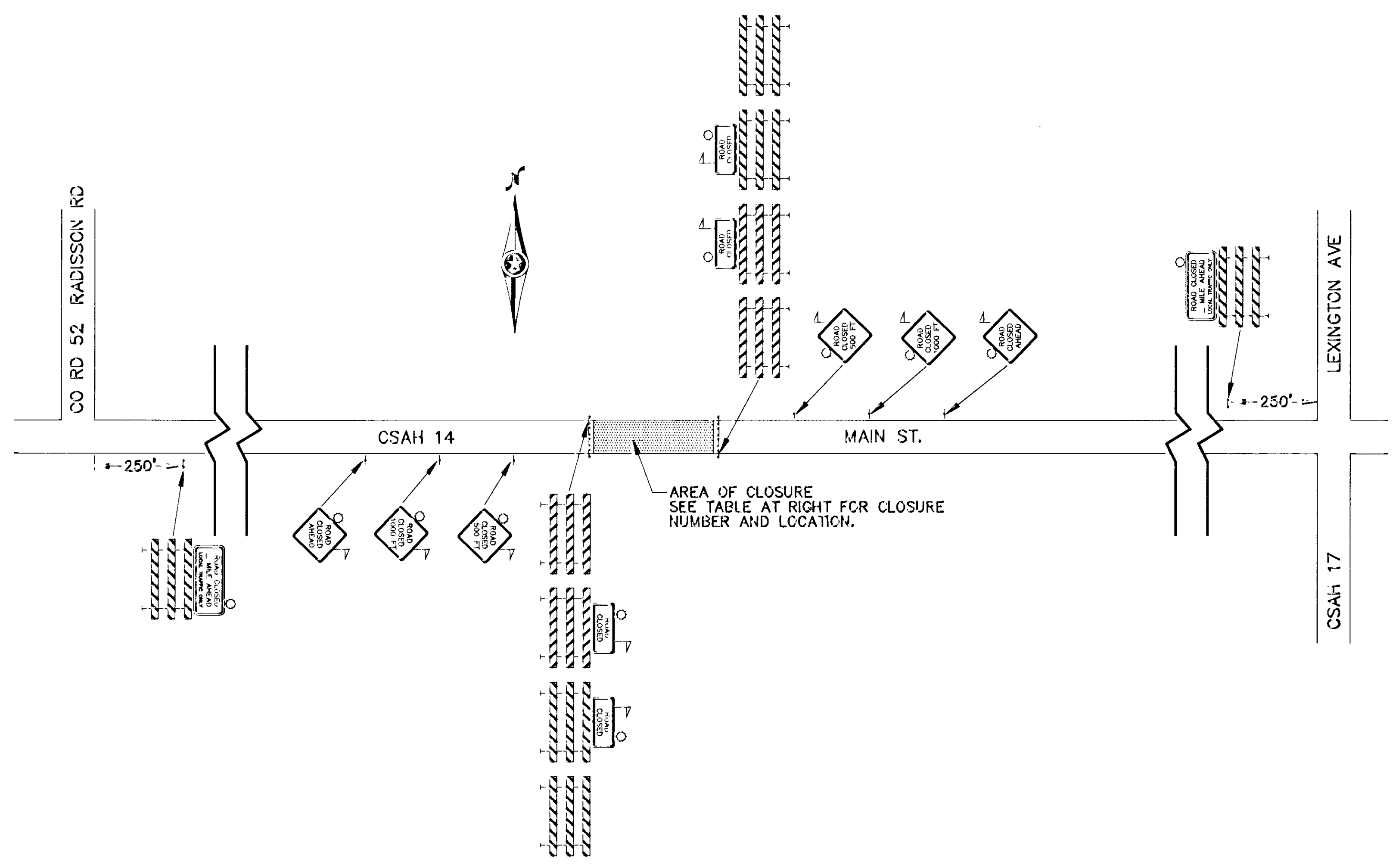
DESIGN	SAP	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	SAP				


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S.A.P. 02-614-14

CSAH 14 DETOUR "B"

FILE NO.	5006-61	SHEET NO.	88
DATE	7-8-92	OF	117



TYPICAL SHORT TERM ROAD CLOSURE

M.U.T.C.D. CODE	SIZE	INSERT	QTY.																																				
FLASHER W20-3	48" X 48"	ROAD CLOSED 500 FT AHEAD	2																																				
		ROAD CLOSED 1000 FT AHEAD	2																																				
		ROAD CLOSED AHEAD	2																																				
FLASHER R11-2	48" X 30"	ROAD CLOSED	4																																				
TYPE III	8 FT.	BARRICADE	4																																				
FLASHER TYPE III	8 FT.	BARRICADE	4																																				
FLASHER R11-3	60" X 30"	ROAD CLOSED * MILE AHEAD LOCAL TRAFFIC ONLY	1 EA																																				
TYPE III	8 FT.	BARRICADE	1 EA																																				
		<table border="1"> <thead> <tr> <th>SHORT TERM CLOSURE NO.</th> <th>CO.RD. 52 *</th> <th>CSAH 17 *</th> <th>QTY.</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>2.5</td> <td>0.2</td> <td>1 EA</td> </tr> <tr> <td>2</td> <td>2.2</td> <td>0.5</td> <td>1 EA</td> </tr> <tr> <td>3</td> <td>2</td> <td>0.7</td> <td>1 EA</td> </tr> <tr> <td>4</td> <td>1.5</td> <td>1.1</td> <td>1 EA</td> </tr> <tr> <td>5</td> <td>1.4</td> <td>1.3</td> <td>1 EA</td> </tr> <tr> <td>6</td> <td>1.3</td> <td>1.4</td> <td>1 EA</td> </tr> <tr> <td>7</td> <td>0.9</td> <td>1.5</td> <td>1 EA</td> </tr> <tr> <td>8</td> <td>0.7</td> <td>2</td> <td>1 EA</td> </tr> </tbody> </table>	SHORT TERM CLOSURE NO.	CO.RD. 52 *	CSAH 17 *	QTY.	1	2.5	0.2	1 EA	2	2.2	0.5	1 EA	3	2	0.7	1 EA	4	1.5	1.1	1 EA	5	1.4	1.3	1 EA	6	1.3	1.4	1 EA	7	0.9	1.5	1 EA	8	0.7	2	1 EA	
SHORT TERM CLOSURE NO.	CO.RD. 52 *	CSAH 17 *	QTY.																																				
1	2.5	0.2	1 EA																																				
2	2.2	0.5	1 EA																																				
3	2	0.7	1 EA																																				
4	1.5	1.1	1 EA																																				
5	1.4	1.3	1 EA																																				
6	1.3	1.4	1 EA																																				
7	0.9	1.5	1 EA																																				
8	0.7	2	1 EA																																				

SHORT TERM CLOSURE NO.	LOCATION OF CLOSURE CSAH 14 MAIN STREET
1	STA 174+50 TO STA 176+00
2	STA 163+00 TO STA 165+50
3	STA 146+50 TO STA 149+50
4	STA 124+50 TO STA 130+00
5	STA 117+50 TO STA 120+50
6	STA 113+50 TO STA 116+50
7	STA 91+00 TO STA 108+50
8	STA 81+00 TO STA 84+50

NOTES:

1. LOCATIONS OF ALL SIGNS ARE APPROXIMATE, EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER.
2. ALL SIGNS AND BARRICADES SHALL BE PROPERLY WEIGHTED WITH SANDBAGS.
3. ALL BARRICADES SHALL HAVE REFLECTIVE MATERIAL ON BOTH SIDES.
4. ALL BARRICADE MARKINGS SHALL BE SLANTED IN ACCORDANCE WITH THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
5. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE 1991 MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING APPENDIX "B" DATED MARCH 1990.
6. ADDITIONS OR CHANGES TO THIS PLAN MAY BE MADE AT ANYTIME AS DETERMINED BY THE ENGINEER.

DIR & FILE NAME: C:\WORK\AGETS\10WG
 OPERATOR: SAP
 PLOT SCALE: 1"=1'
 DATE: 7-8-92

DESIGN	SAP	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCW			



S.A.P. 02-614-14

CSAH 14
SHORT TERM ROAD CLOSURES

FILE NO. 50C6-61	SHEET NO. 89
DATE 7-8-92	OF 117

CSAH 14 TRAFFIC CONTROL						①
M.U.T.C.D. CODE	SIZE	INSERT	STAGE 1 QTY.	STAGE 2 QTY.	STAGE 3 QTY.	
R1-1	48" X 48"				2	
R3-X1	24" X 30"				2	
R3-7	24" X 30"		1			
FLASHER R11-2	60" X 30"		6	5	6	
TYPE III	8 FT.					
FLASHER R11-3	60" X 30"		* 2			
TYPE III	3 FT.		* 2.5	1		
			* 1/2		1	
FLASHER W1-4R	48" X 48"			5	2	
W13-1	24" X 24"					
FLASHER W1-4L	48" X 48"			2	4	
W13-1	24" X 24"					
FLASHER W20-3	48" X 48"		AHEAD	2		1
			500 FT	3	2	2
			1000 FT	1	2	2
FLASHER W20-X6	48" X 48"		500 FT		7	4
W13-1	24" X 24"		1000 FT		7	4
FLASHER M4-10L	48" X 18"				1	
TYPE III	8 FT.					
FLASHER TYPE III	8 FT.			6	16	16
REBOUNDABLE NON-METALIC DRUM LIKE CHANNELIZER				123	336	266

DIS & FILE NAME: C:\WORK\DESIGN\DWG
 OPERATOR: SAP
 PLOT SCALE: 1=1
 DATE: 7-8-92

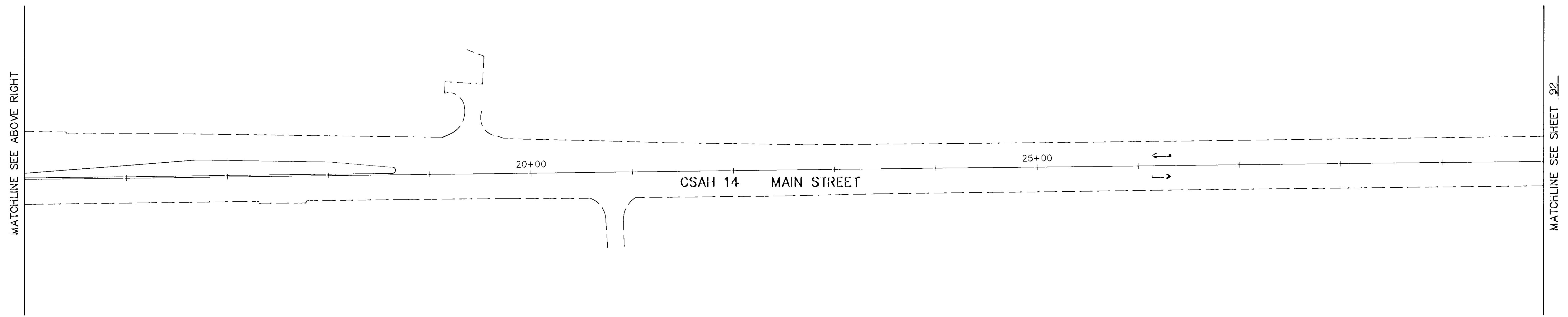
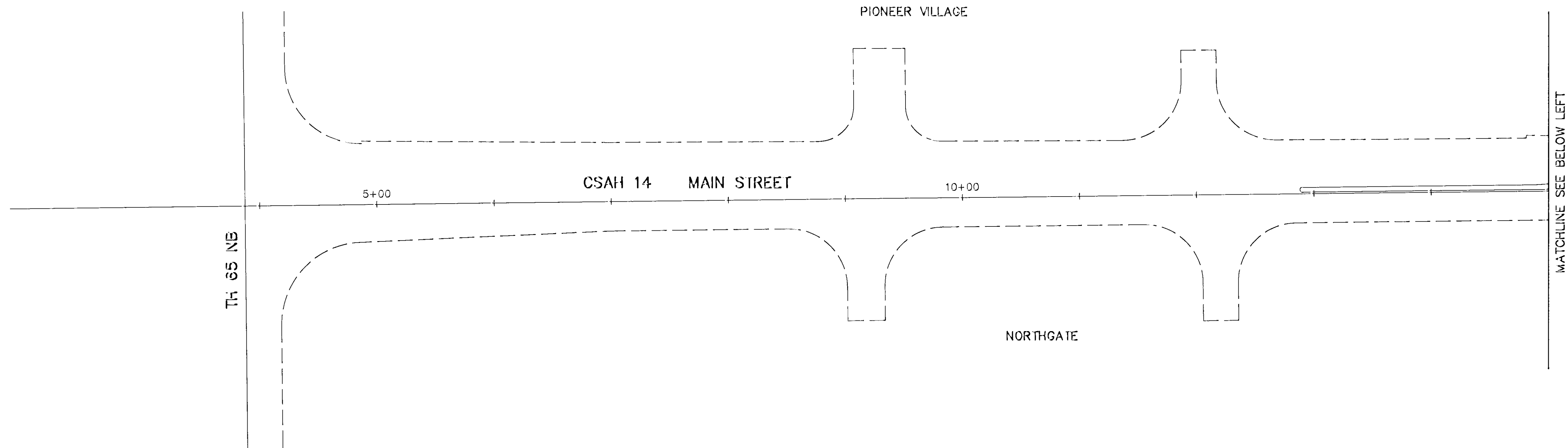
DESIGN	SAP	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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S.A.P. 02-614-14

CSAH 14
 TRAFFIC CONTROL
 SIGN TABULATIONS

FILE NO.	SHEET NO.
5006-61	90
DATE	
7-8-92	OF 117



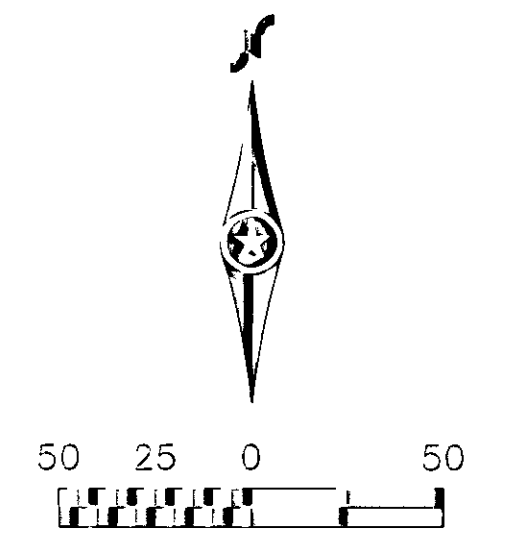
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

SIGNED Kenneth W. Harris

DATE 7-9-92 REG. NO. 18235

* APPLIES TO TRAFFIC CONTROL PLANS, SHEETS 80 TO 117.

- LEGEND**
- INPLACE TRAFFIC
 - CONSTRUCTION THIS STAGE
 - TEMPORARY WIDENING



DIR & FILE NAME: C:\MORVAN\ST1-1.DWG
OPERATOR: SAP
PLOT SCALE: 1"=50'
DATE: 7-8-92

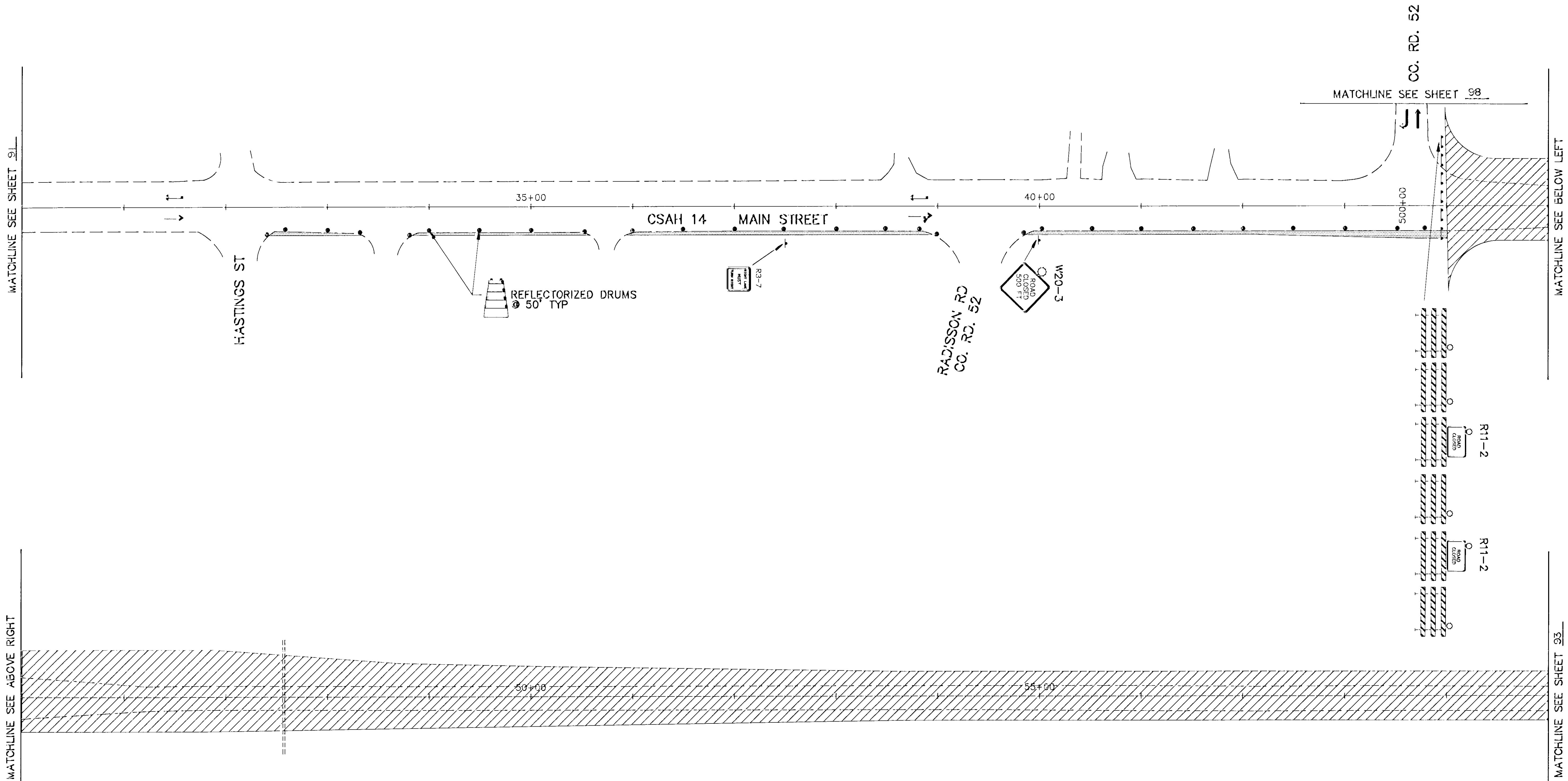
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	SAP				

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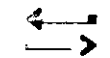

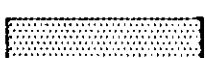
S.A.P. 02-614-14

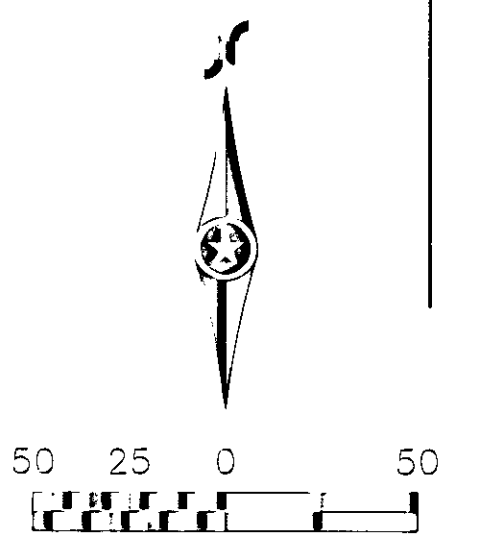
STAGE 1
TRAFFIC CONTROL

FILE NO.	SHEET NO.
5006-61	91
DATE	OF
7-8-92	117



LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DIR & FILE NAME: C:\ANOKA\ST1-2.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			


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S.A.P. 02-614-14

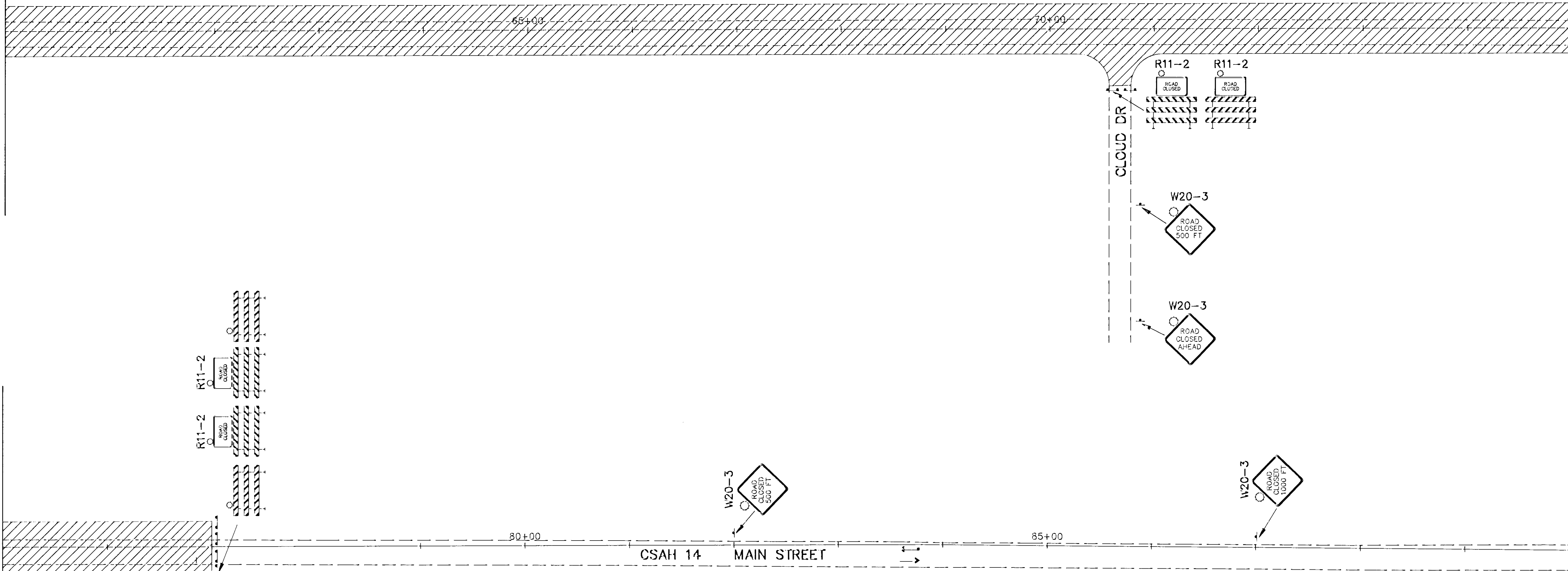
**STAGE 1
TRAFFIC CONTROL**

FILE NO.	SHEET NO.
50C6-61	92
DATE	
7-8-92	OF 117

DIR & FILE NAME: C:\WORK\A311-3.DWG
 USER: KWH
 PLOT DATE: 7-8-92

MATCHLINE SEE SHEET 92

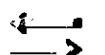
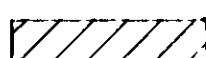
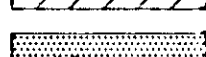
MATCHLINE SEE ABOVE RIGHT

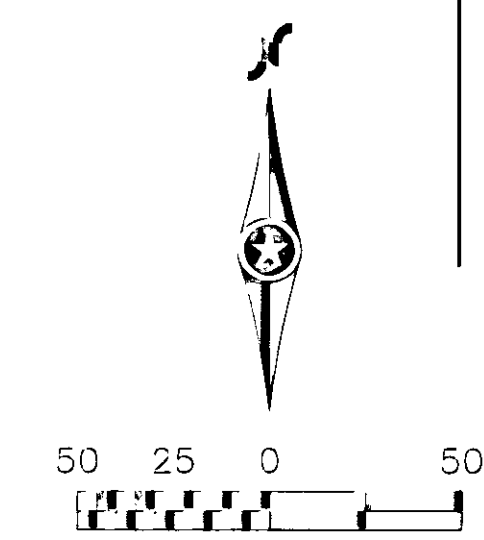


MATCHLINE SEE BELOW LEFT

MATCHLINE SEE SHEET 94

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	SAP				

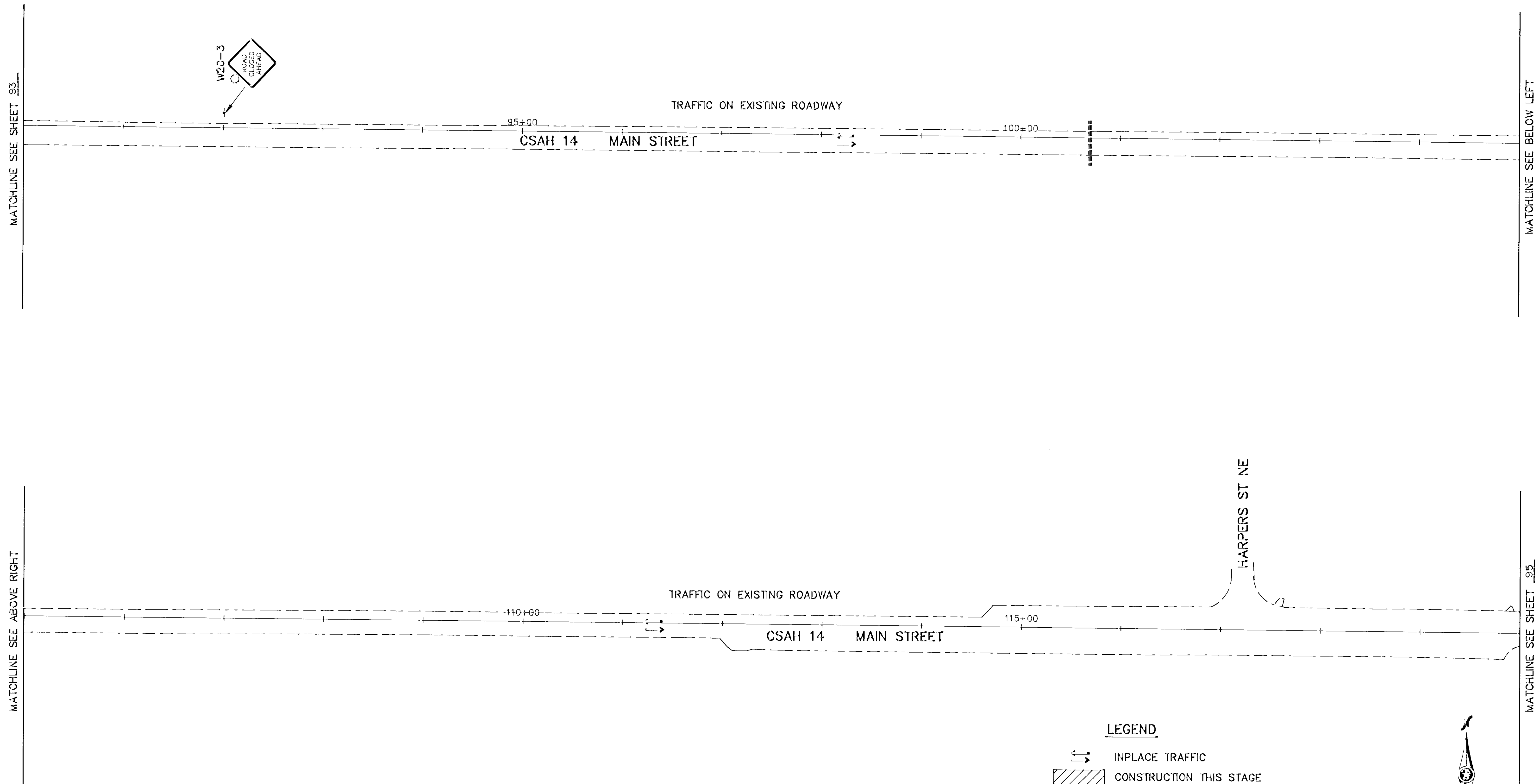
Barton-Aschman Associates, Inc.
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S.A.P. 02-614-14

STAGE 1
TRAFFIC CONTROL

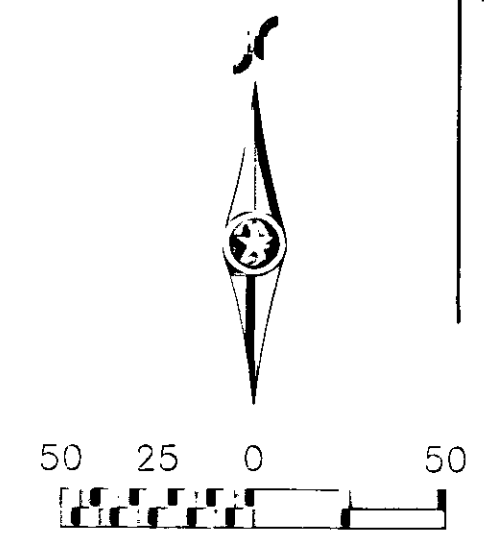
FILE NO.	SHEET NO.
50C6-61	93
DATE	OF
7-8-92	117

DIR & FILE NAME: C:\ANGKA\ST1-4.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-6-92



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

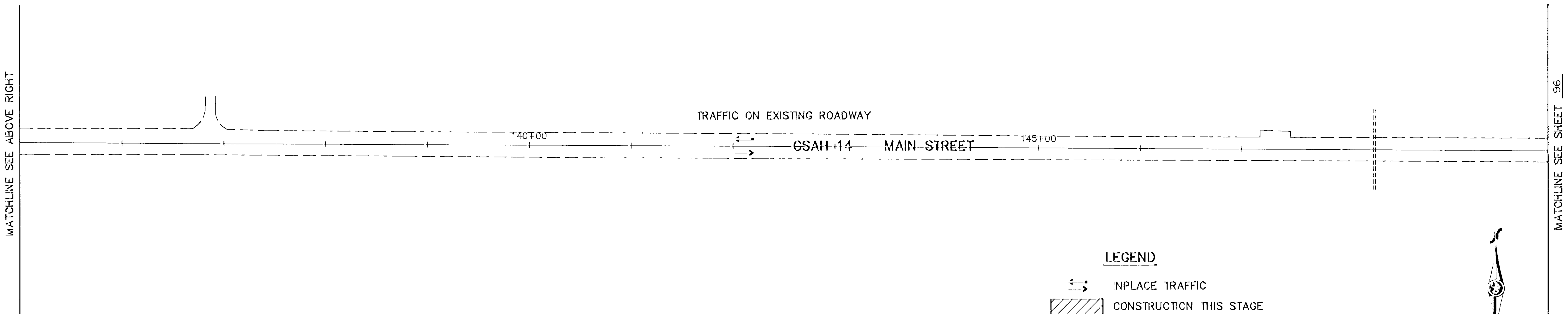
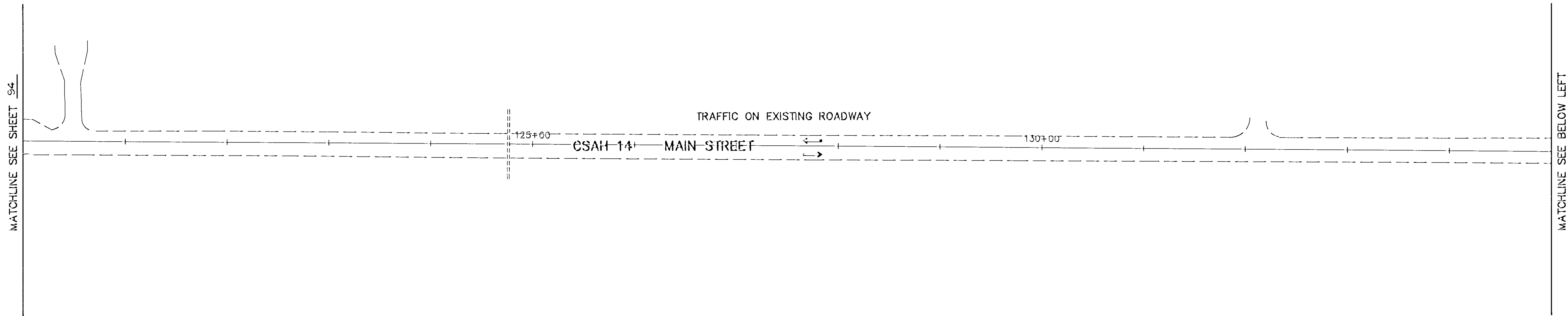
Barton-Aschman Associates, Inc.
 111 Third Avenue South, Suite 350 Minneapolis, Minnesota 55401

S.A.P. 02-614-14


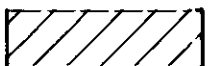
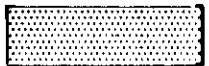
STAGE 1
TRAFFIC CONTROL

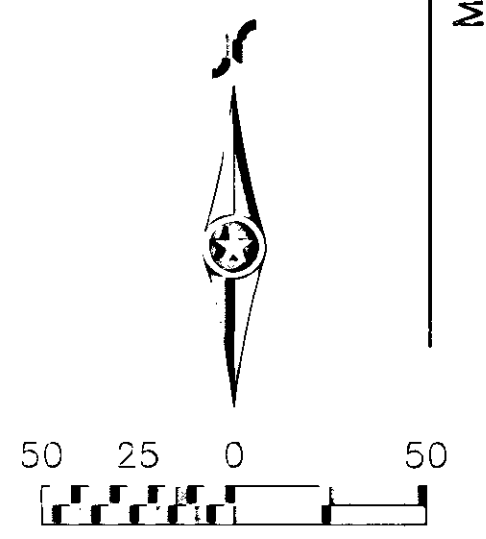
FILE NO.	SHEET NO.
50C6-61	94
DATE	
7-8-92	

OF 117



LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DIR & FILE NAME: C:\ANDRKA\311-5.DWG
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	SAP				


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S.A.P. 02-614-14

STAGE 1
TRAFFIC CONTROL

FILE NO.	SHEET NO.
50C6-61	95
DATE	
7-8-92	OF 117

MATCHLINE SEE SHEET 95

MATCHLINE SEE BELOW LEFT

TRAFFIC ON EXISTING ROADWAY

155+00

CSAH 14 MAIN STREET

160+00

MATCHLINE SEE ABOVE RIGHT

MATCHLINE SEE SHEET 97

TRAFFIC ON EXISTING ROADWAY

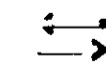
170+00

CSAH 14 MAIN STREET

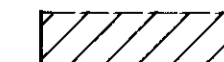
175+00

ZEST ST NE

LEGEND



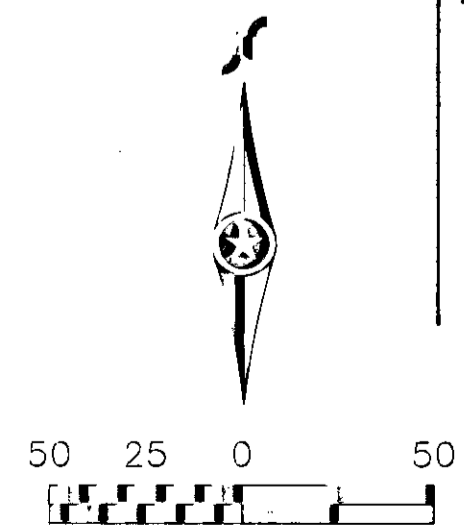
INPLACE TRAFFIC



CONSTRUCTION THIS STAGE



TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\ST-6.DWG
OPERATOR: SAP
PLOT DATE: 7-8-92

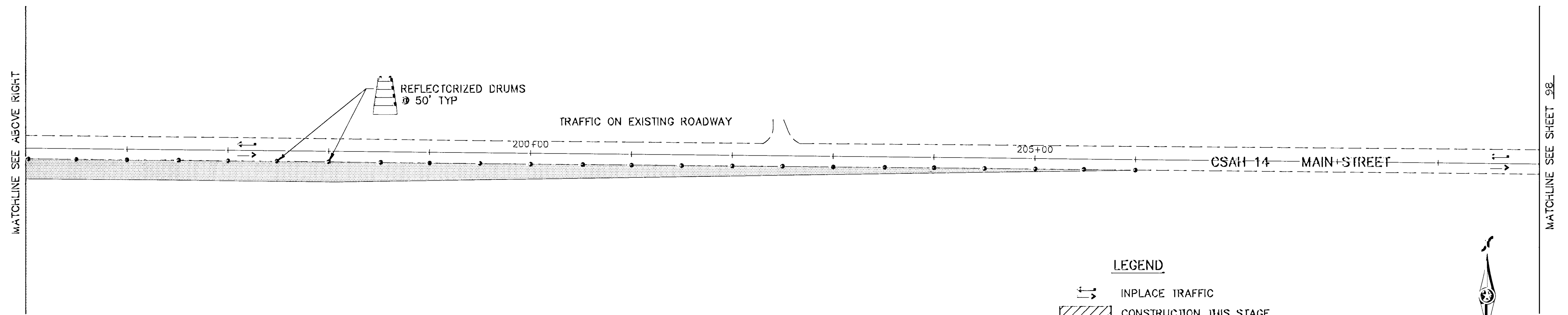
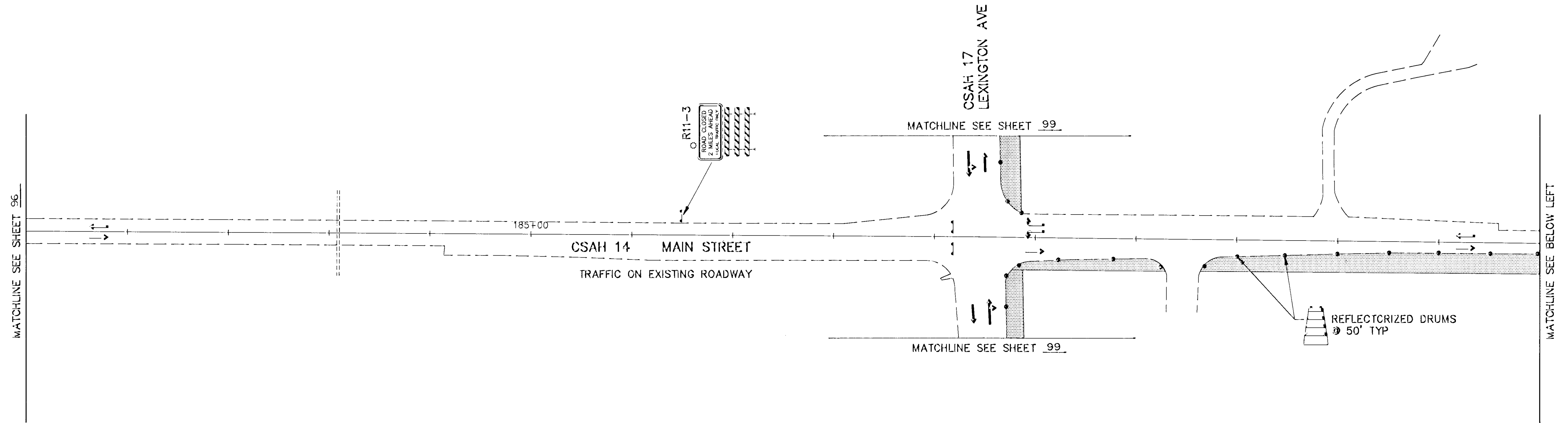
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	SAP				

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S.A.P. 02-614-14

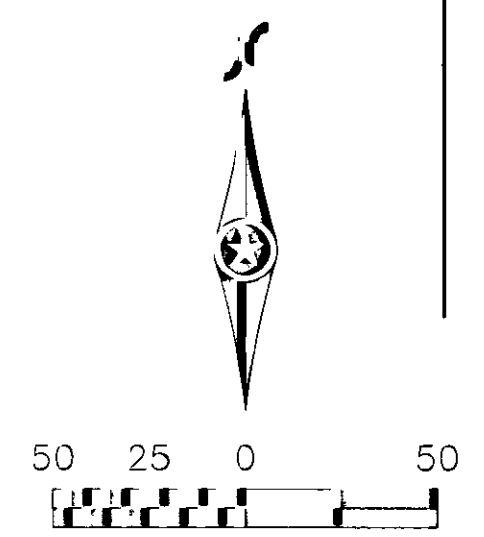
STAGE 1
TRAFFIC CONTROL

FILE NO.	SHEET
5006-61	96
DATE	OF
7-8-92	117



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\ASTH-7.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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S.A.P. 02-614-14

STAGE 1
TRAFFIC CONTROL

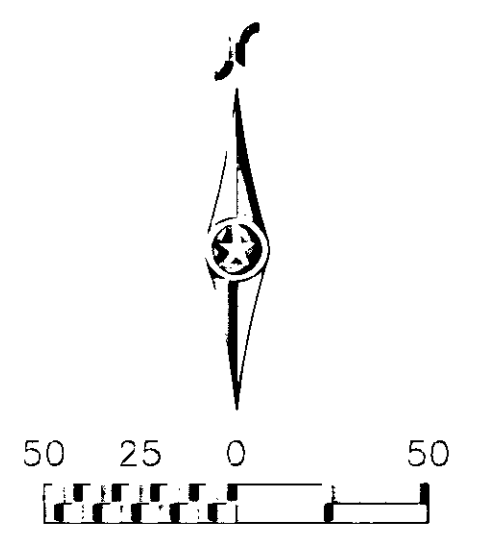
FILE NO.	SHEET NO.
5006-61	97
DATE	OF
7-8-92	117

MATCHLINE SEE SHEET 97

TRAFFIC ON EXISTING ROADWAY

215+00

CSAH 14 MAIN STREET



MATCHLINE SEE SHEET 92

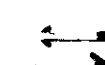
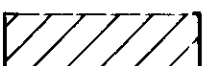

TRAFFIC ON EXISTING ROADWAY

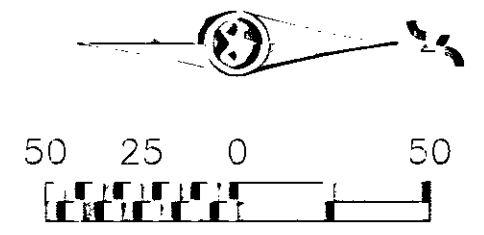
505+00

CO. RD. 52 RADISSON RD

510+00

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DIR & FILE NAME: C:\ANOKA\ST1-B.DWG
OPERATOR: SAP
PLOT SCALE: 1"=50'
DATE: 7-8-92

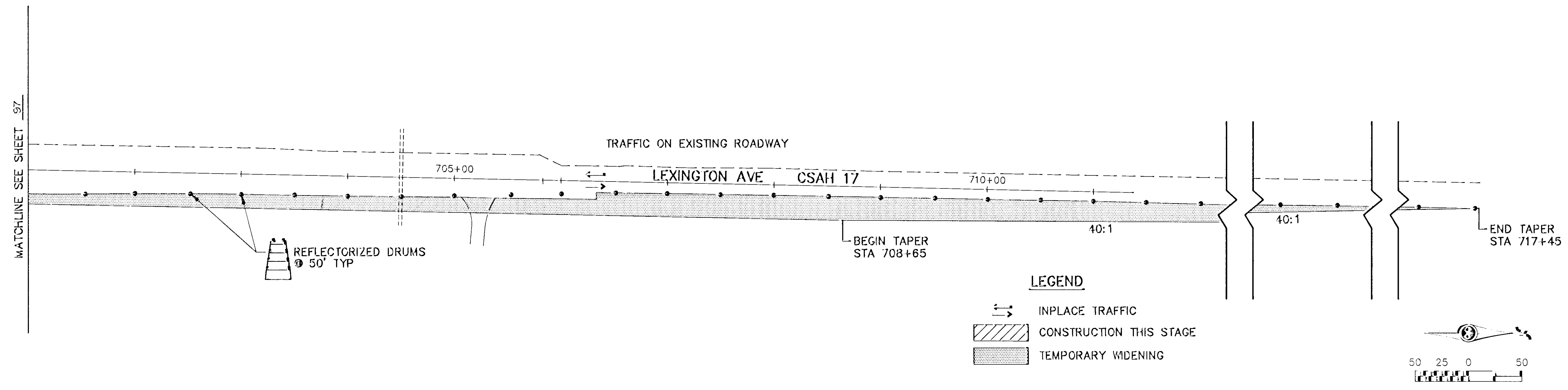
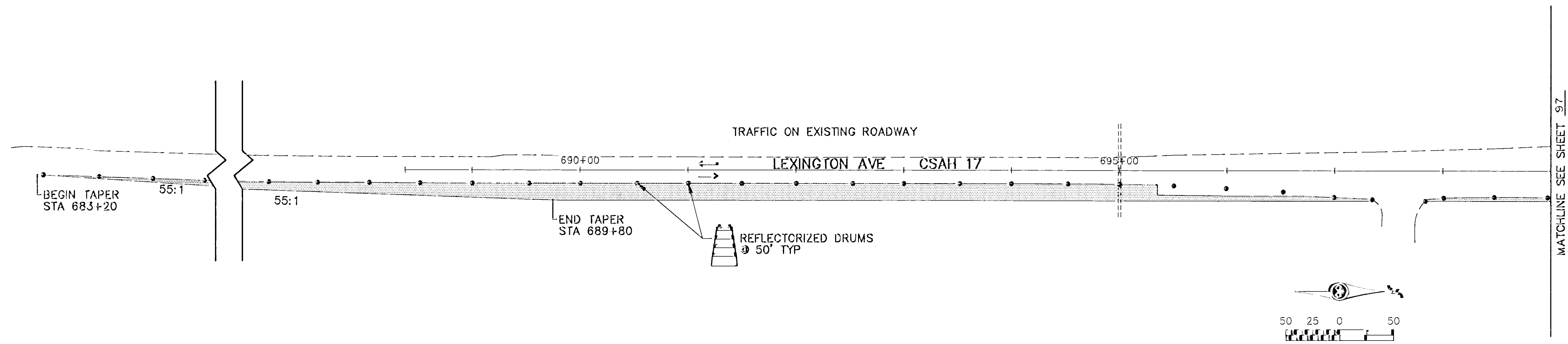
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			



S.A.P. 02-614-14

STAGE 1
TRAFFIC CONTROL

FILE NO.	SHEET NO.
5006-61	98
DATE	
7-8-92	OF 117



DIR & FILE NAME: C:\WORK\ASTI-9.DWG
 PLOT DATE: 7-8-92
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

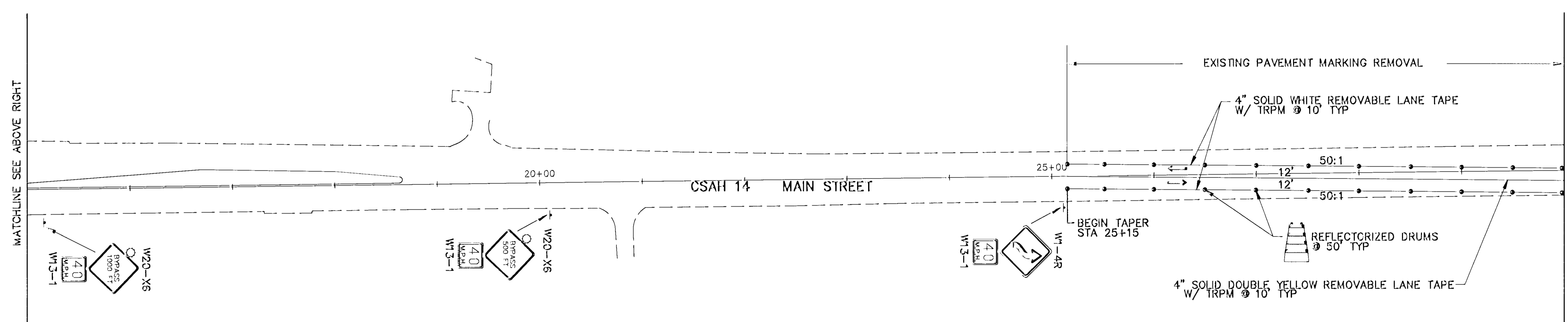
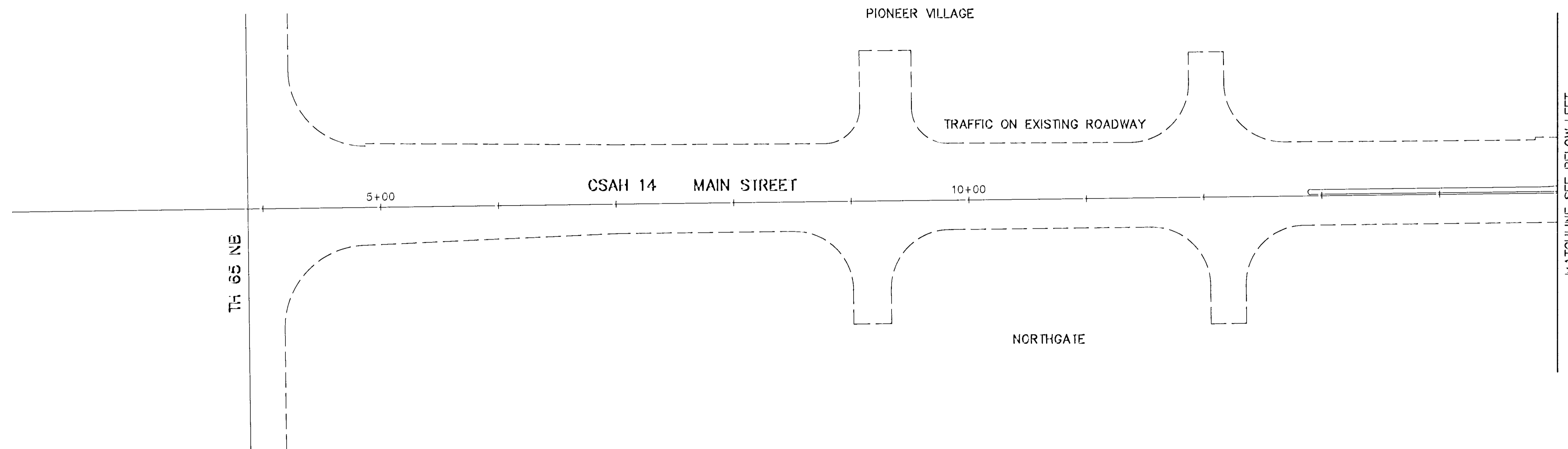
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			



S.A.P. 02-614-14

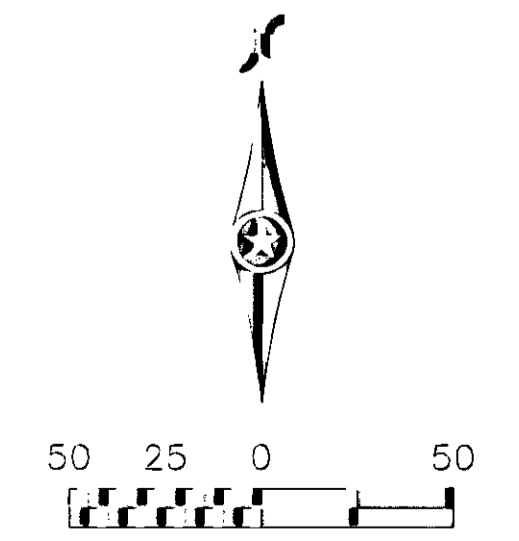
STAGE 1
TRAFFIC CONTROL

FILE NO.	SHEET NO.
5006-61	99
DATE	OF
7-8-92	117



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DIR & FILE NAME: C:\ANDOKA\512-1.DWG
 PLOT DATE: 7-8-92
 DATE: 7-8-92

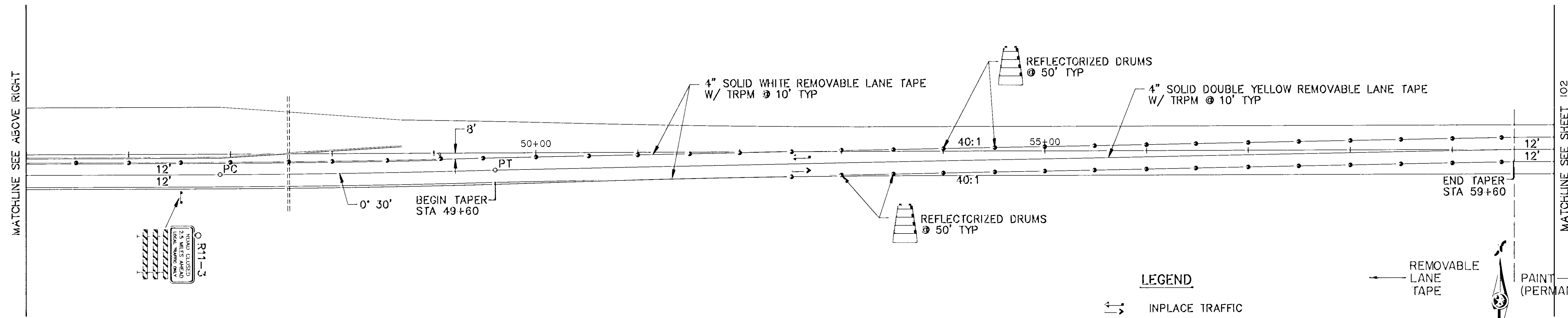
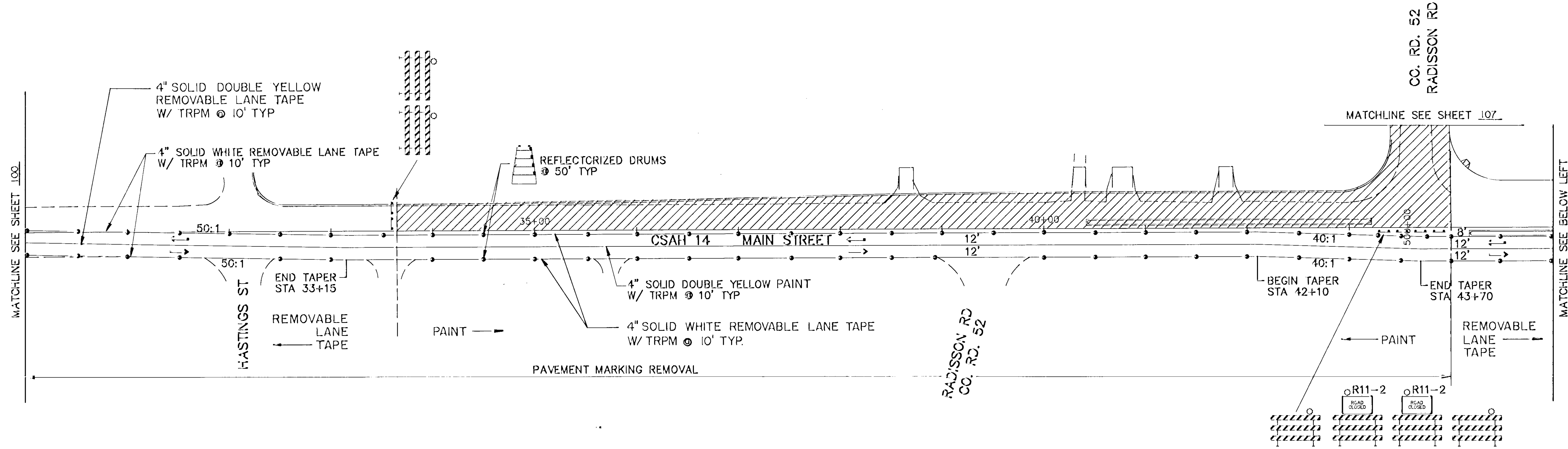
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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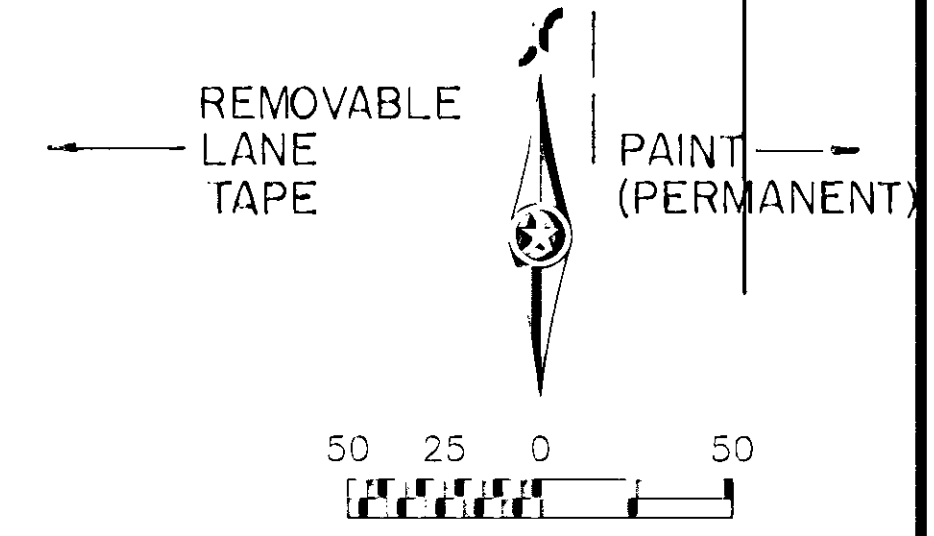
S.A.P. 02-614-14

**STAGE 2
TRAFFIC CONTROL**

FILE NO.	SHEET NO.
5006-61	100
DATE	OF
7-8-92	117



- LEGEND**
- INPLACE TRAFFIC
 - CONSTRUCTION THIS STAGE
 - TEMPORARY WIDENING



DIR & FILE NAME: S:\WORK\12-2.DWG
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

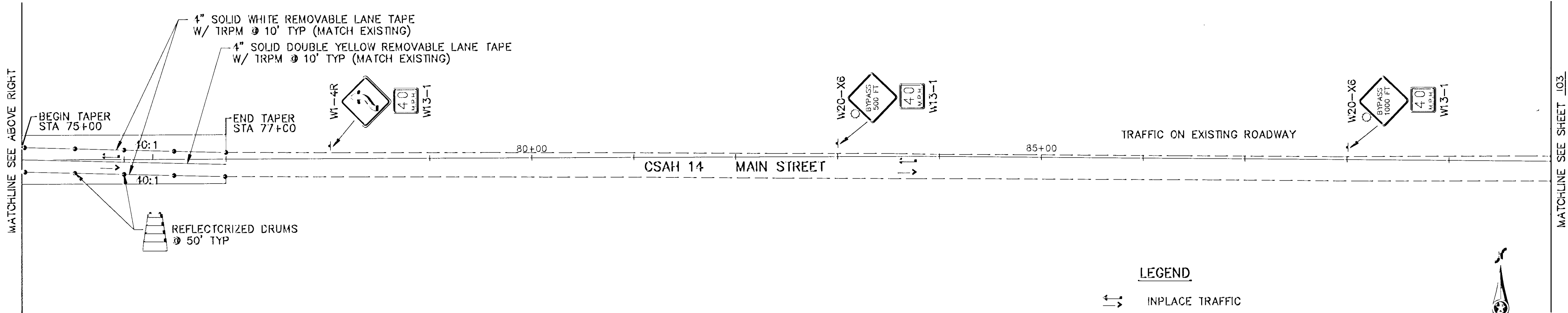
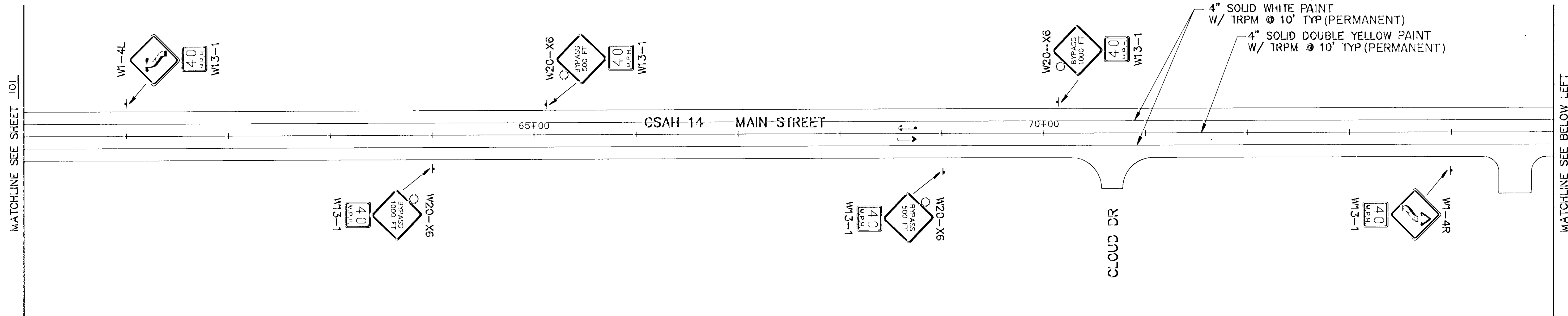
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	SAP				

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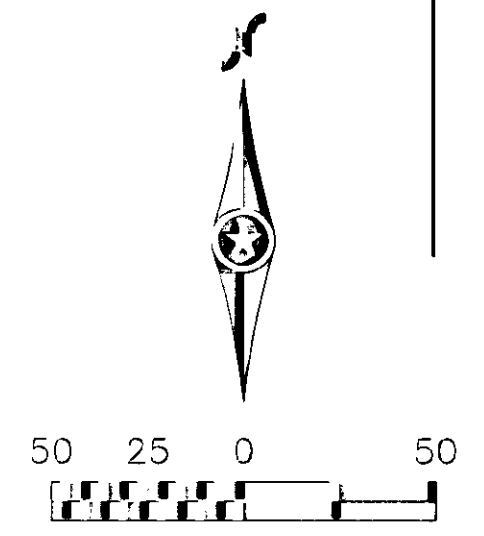
STAGE 2
TRAFFIC CONTROL

FILE NO.	SHEET NO.
5006-61	101
DATE	OF
7-8-92	117



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DIR & FILE NAME: C:\ANDKKA\ST2-3.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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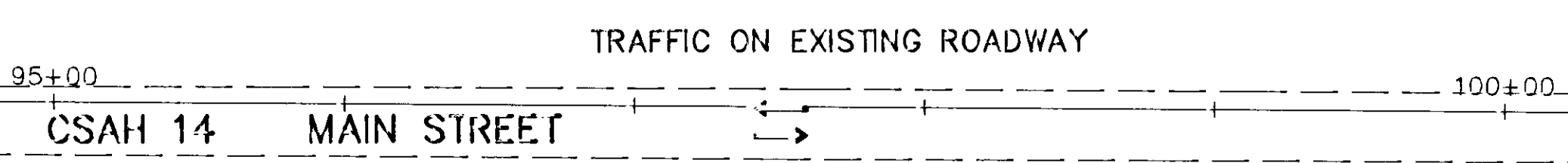
S.A.P. 02-614-14

STAGE 2
TRAFFIC CONTROL

FILE NO.	SHEET NO.
5006-61	102
DATE	OF 117
7-8-92	

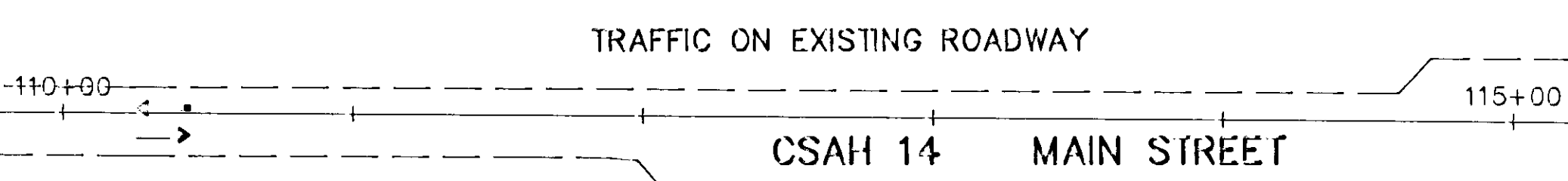
MATCHLINE SEE SHEET 102

MATCHLINE SEE BELOW LEFT



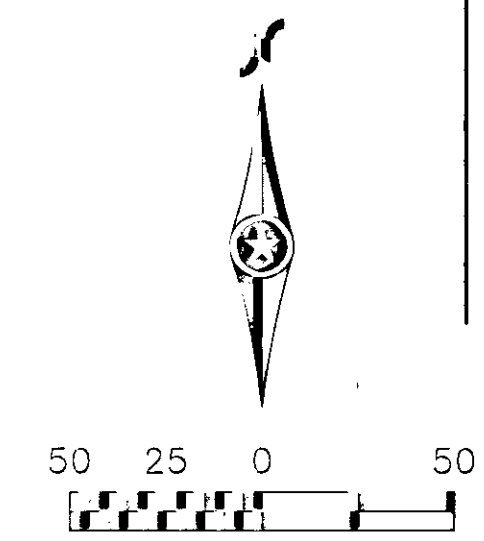
MATCHLINE SEE ABOVE RIGHT

MATCHLINE SEE SHEET 104



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\AS172-4.DWG
DRAWN BY: SAP
CHECKED BY: SAP
DATE: 7-8-92

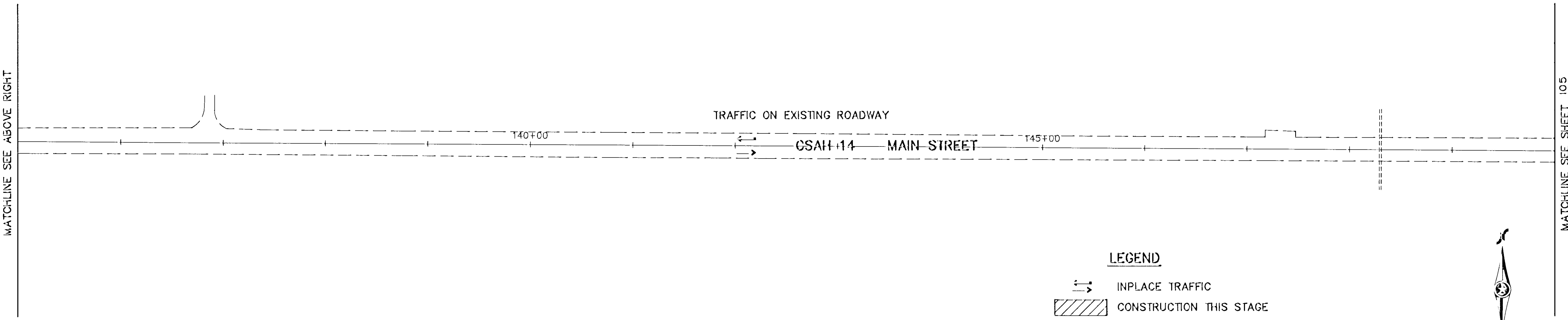
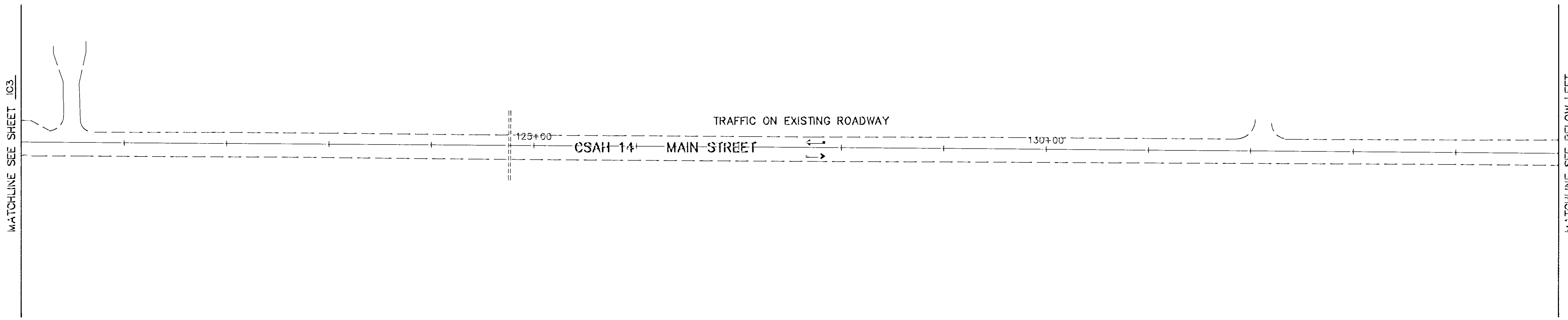
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHOWN			

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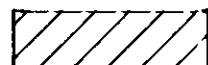

S.A.P. 02-614-14

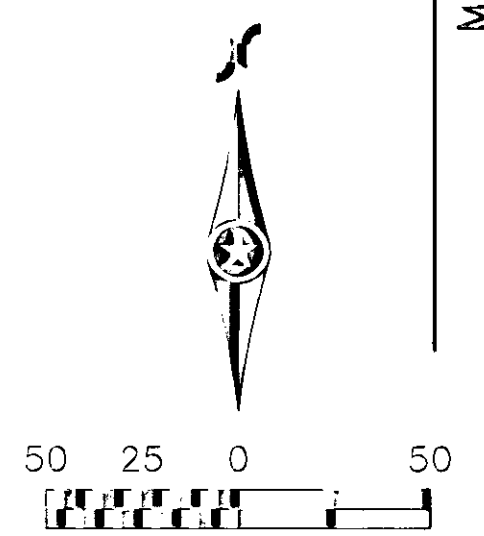
**STAGE 2
TRAFFIC CONTROL**

FILE NO.	SHEET NO.
50C6-61	103
DATE	
7-8-92	OF 117



LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\102-50WG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS SHOWN			
DRAWN	SAP				


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S.A.P. 02--614--14

STAGE 2
TRAFFIC CONTROL

FILE NO.	SHEET NO.
5006-61	104
DATE	OF
7-8-92	117

MATCHLINE SEE SHEET 104

MATCHLINE SEE BELOW LEFT

MATCHLINE SEE ABOVE RIGHT

MATCHLINE SEE SHEET 106

TRAFFIC ON EXISTING ROADWAY

CSAH 14 MAIN STREET

TRAFFIC ON EXISTING ROADWAY

CSAH 14 MAIN STREET


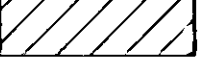

155+00

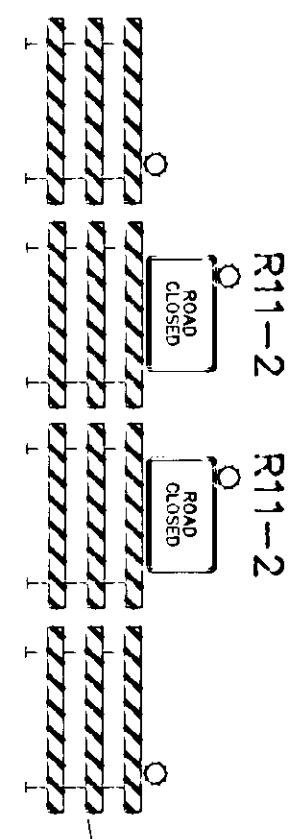
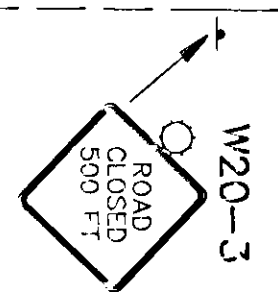
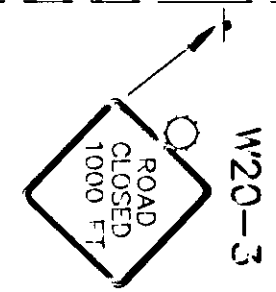
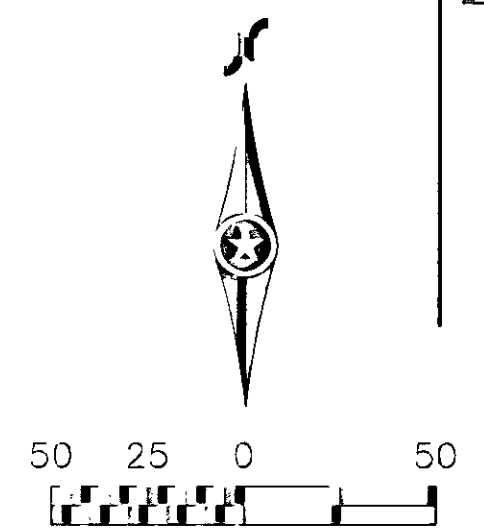
160+00

170+00

175+00

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DIR & FILE NAME: C:\ANOKA\ST2-BDING
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

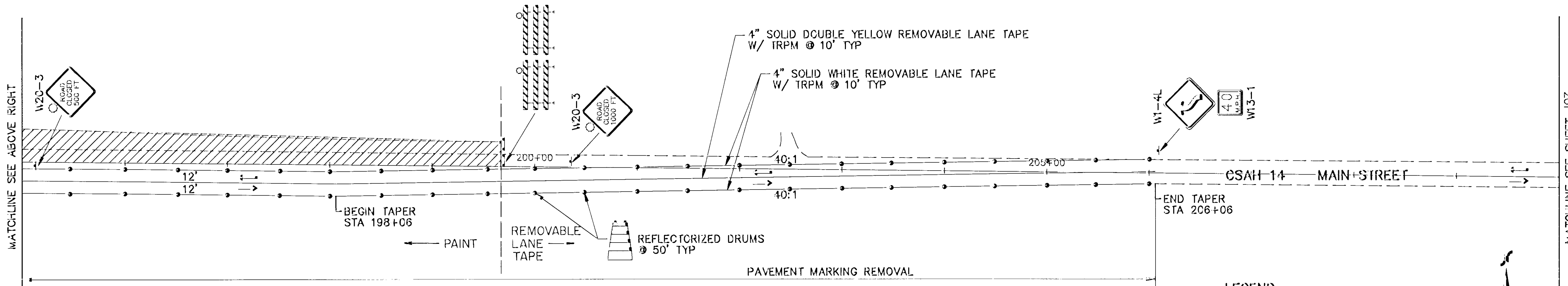
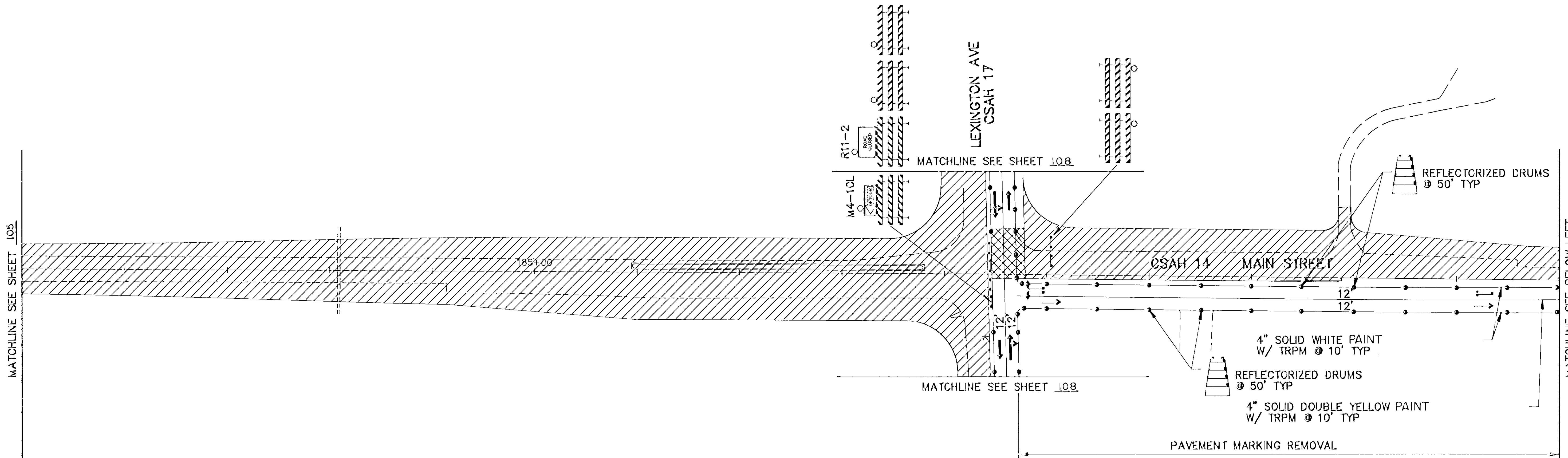
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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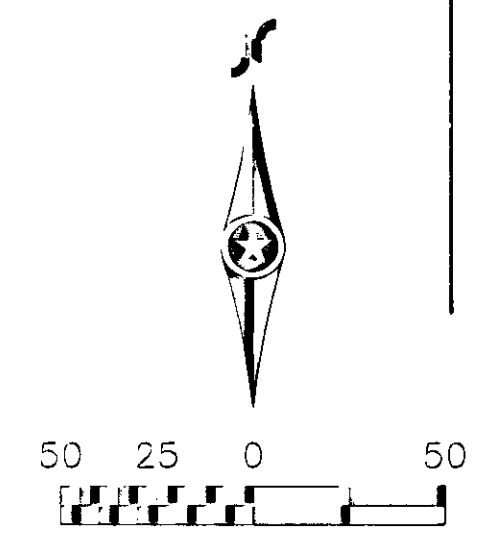
STAGE 2
TRAFFIC CONTROL

FILE NO. 5006-61
 SHEET NO. 105
 DATE 7-8-92
 OF 117



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING
- CONSTRUCTION UNDER TRAFFIC



DIR & FILE NAME: S:\WORK\1572-7\DWG
 CREATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHOWN			

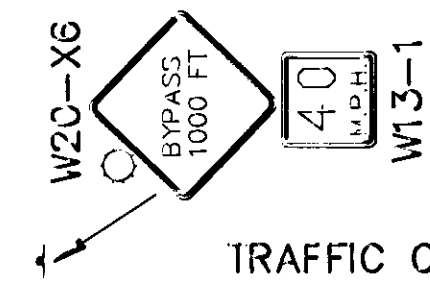
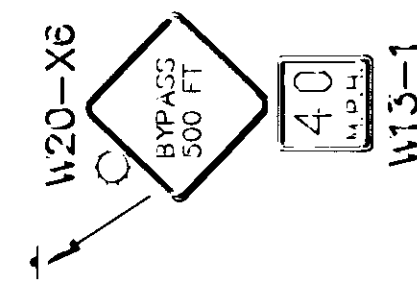
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**STAGE 2
TRAFFIC CONTROL**

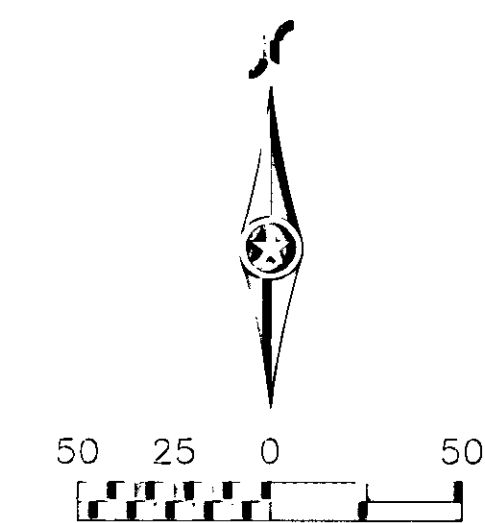
FILE NO.	5008-61	SHEET NO.	106
DATE	7-8-92	OF	117

MATCHLINE SEE SHEET 106

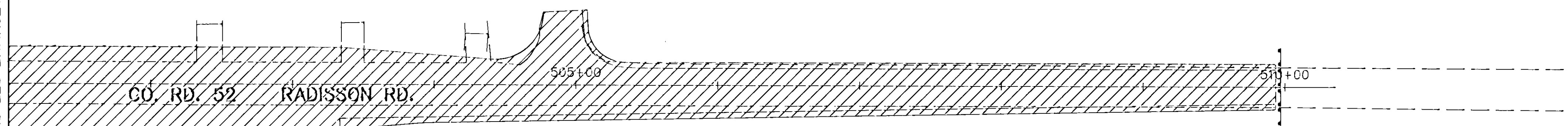


TRAFFIC ON EXISTING ROADWAY
CSAH 14 MAIN STREET

215+00



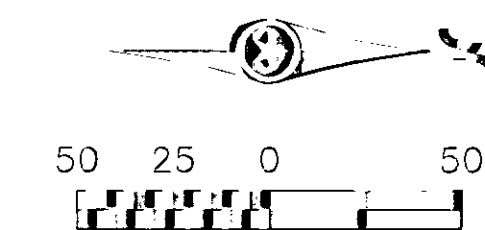
MATCHLINE SEE SHEET 101



NOTE:
TRAFFIC CONTROL DEVICES FOR THIS PORTION
OF CO. RD. 52 CONSTRUCTION ARE INCLUDED IN
DETOUR "B".

LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\1212-BJWG
PLOT SCALE: 1"=50'
DATE: 7-8-92

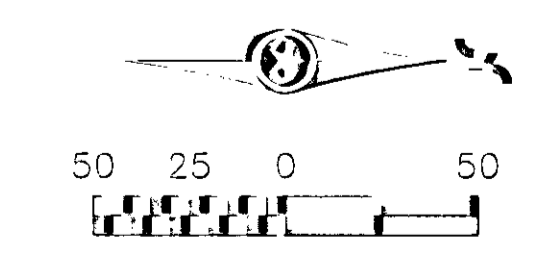
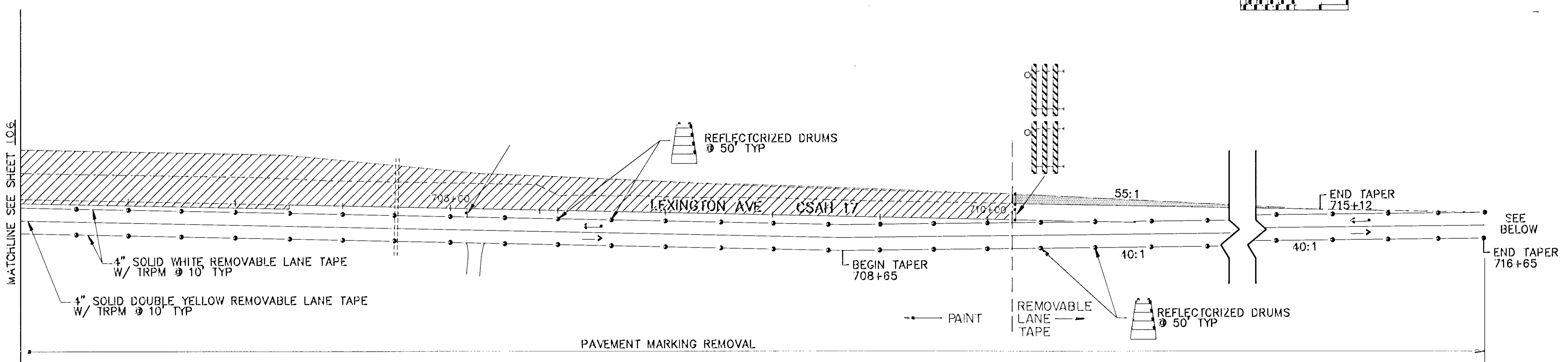
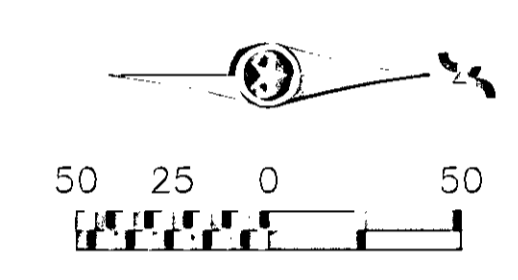
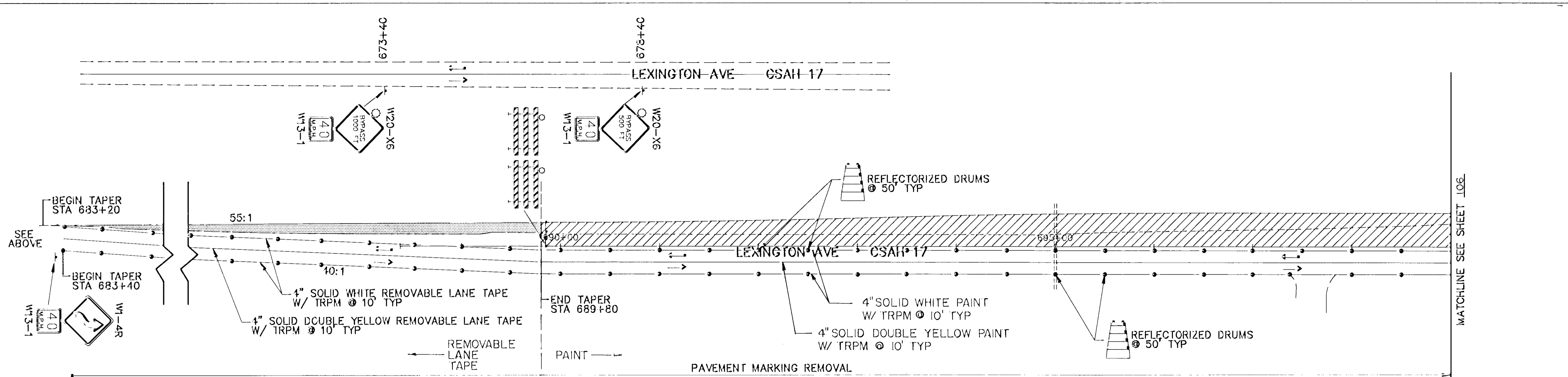
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHOWN			

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S.A.P. 02-614-14

STAGE 2
TRAFFIC CONTROL

FILE NO. 5006-61	SHEET NO. 107
DATE 7-8-92	OF 117



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPRCARY WIDENING

DIR & FILE NAME: C:\ANOKA\1212-8.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

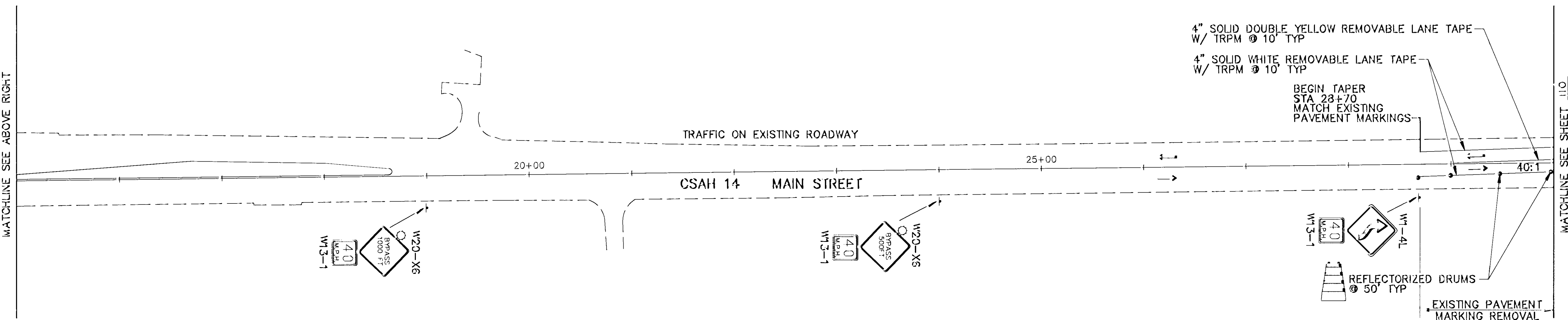
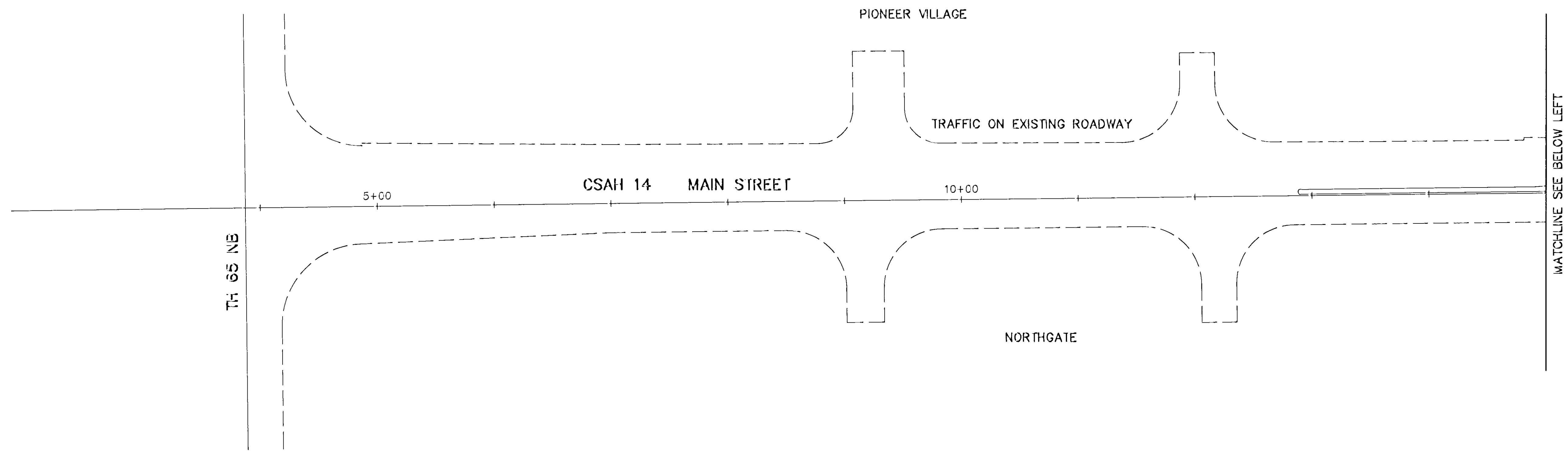
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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STAGE 2
TRAFFIC CONTROL

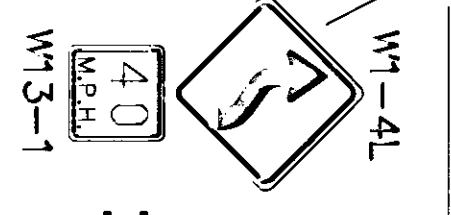
FILE NO.	SHEET NO.
5006-61	108
DATE	OF
7-8-92	117



4" SOLID DOUBLE YELLOW REMOVABLE LANE TAPE
W/ TRPM @ 10' TYP

4" SOLID WHITE REMOVABLE LANE TAPE
W/ TRPM @ 10' TYP

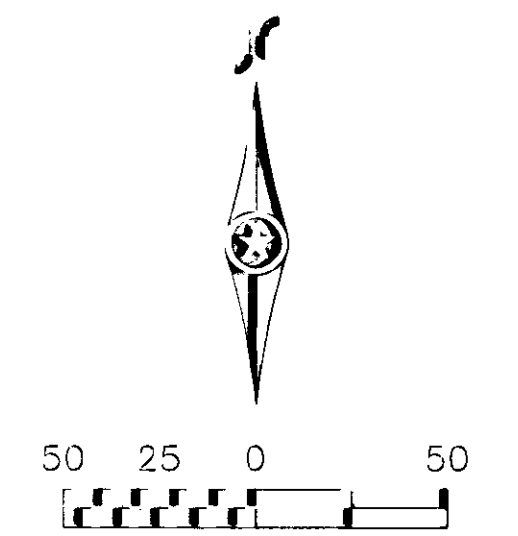
BEGIN TAPER
STA 28+70
MATCH EXISTING
PAVEMENT MARKINGS



EXISTING PAVEMENT MARKING REMOVAL

LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\STD-11.DWG
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

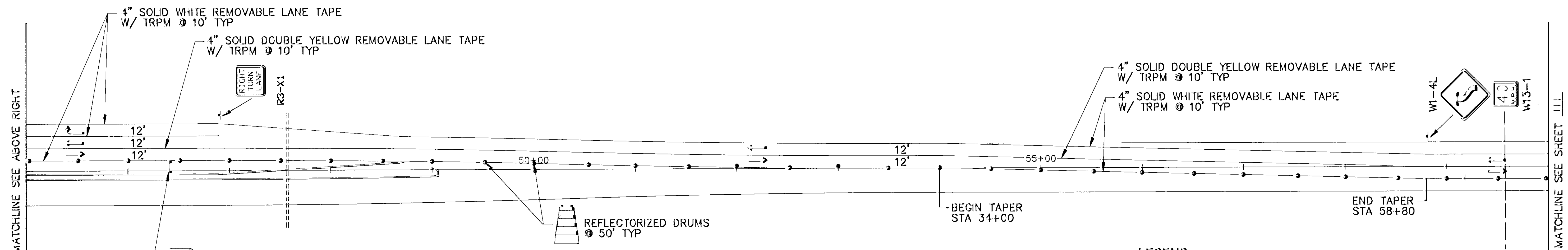
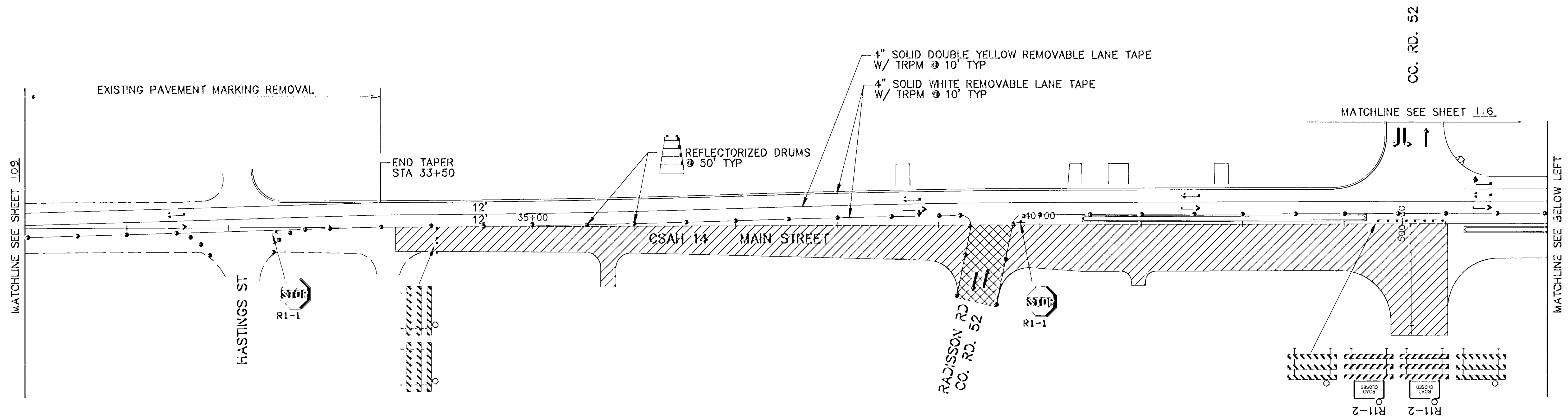
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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**STAGE 3
TRAFFIC CONTROL**

FILE NO.	SHEET NO.
5006-61	109
DATE	
7-8-92	OF 117



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING
- CONSTRUCTION UNDER TRAFFIC

REMOVABLE LANE TAPE

PAINT (FROM STAGE II)

DIR & FILE NAME: C:\WORK\ST3-2.DWG
 OPERATOR: SAP
 PLOT DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHOWN			

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S.A.P. 02-614-14

STAGE 3
TRAFFIC CONTROL

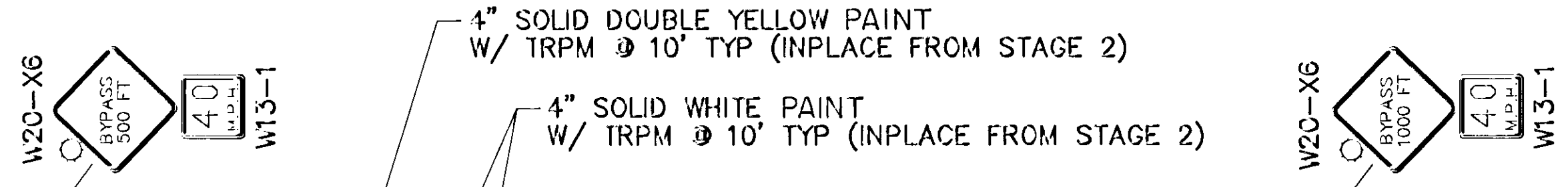
FILE NO.	SHEET NO.
50C6-61	110
DATE	OF
7-8-92	117

MATCHLINE SEE SHEET 110

MATCHLINE SEE BELOW LEFT

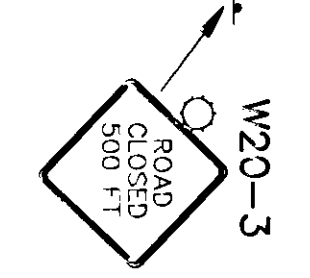
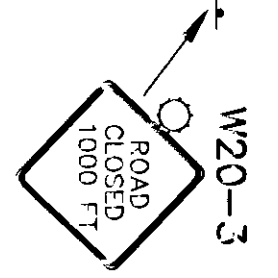
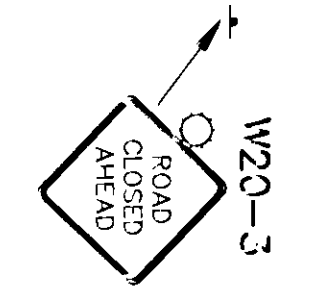
MATCHLINE SEE ABOVE RIGHT

MATCHLINE SEE SHEET 112

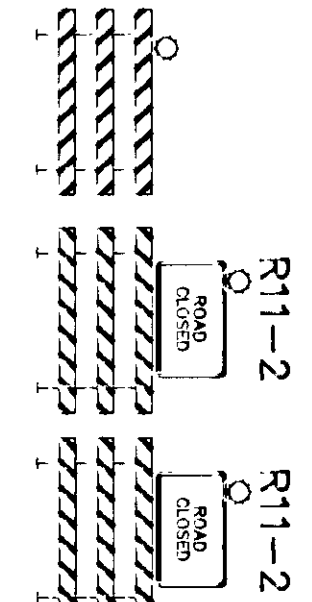


4" SOLID DOUBLE YELLOW PAINT
W/ TRPM @ 10' TYP (INPLACE FROM STAGE 2)

4" SOLID WHITE PAINT
W/ TRPM @ 10' TYP (INPLACE FROM STAGE 2)



CLOUD DR

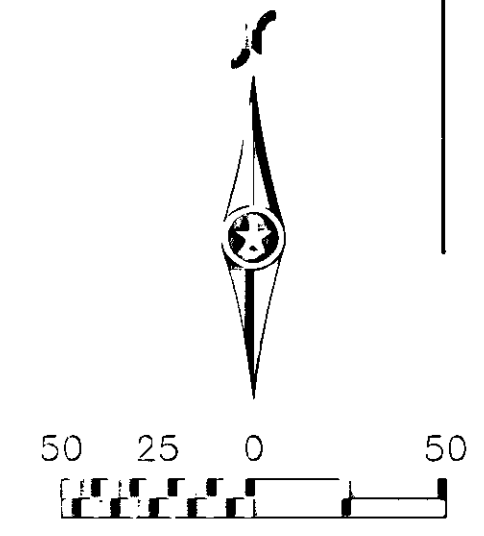


NOTE: CSAH 14 WILL BE CLOSED FOR CONSTRUCTION IN SEGMENTS FROM STA. 77+00 TO STA. 175+40. SEE SHORT TERM ROAD CLOSURES PLAN AND LOCATION TABULATION. MAINTAIN ACCESS FROM THE EAST OR WEST TO ALL PROPERTIES ALONG CSAH 14 AT ALL TIMES.



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\AST3-3.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			



S.A.P. 02-614-14

STAGE 3
TRAFFIC CONTROL

FILE NO.	5006-61	SHEET NO.	111
DATE	7-8-92	OF	117

MATCHLINE SEE SHEET 111

MATCHLINE SEE BELOW LEFT

MATCHLINE SEE ABOVE RIGHT

MATCHLINE SEE SHEET 113

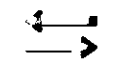

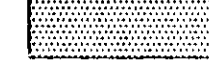
95+00
CSAH 14 MAIN STREET
100+00

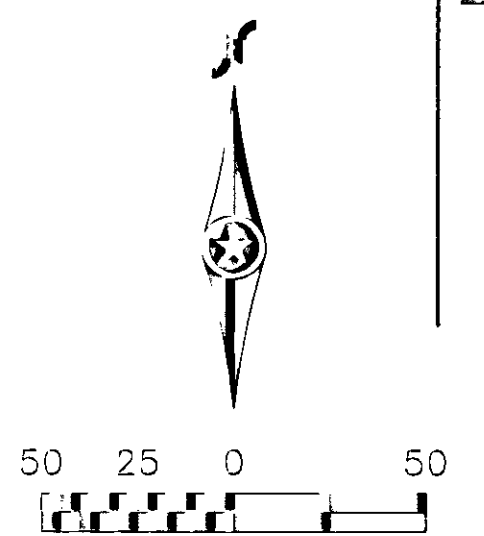
NOTE: CSAH 14 WILL BE CLOSED FOR CONSTRUCTION IN SEGMENTS FROM STA. 77+00 TO STA. 175+40. SEE SHORT TERM ROAD CLOSURES PLAN AND LOCATION TABULATION. MAINTAIN ACCESS FROM THE EAST OR WEST TO ALL PROPERTIES ALONG CSAH 14 AT ALL TIMES.

110+00
CSAH 14 MAIN STREET
115+00

HARPERS ST NE

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\AST3-ADJING
DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			



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S.A.P. 02-614-14

STAGE 3
TRAFFIC CONTROL

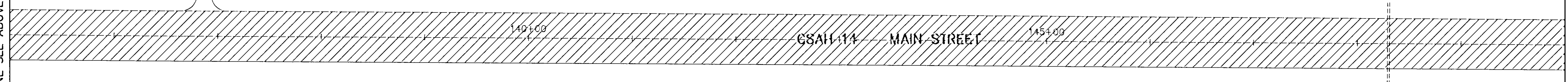
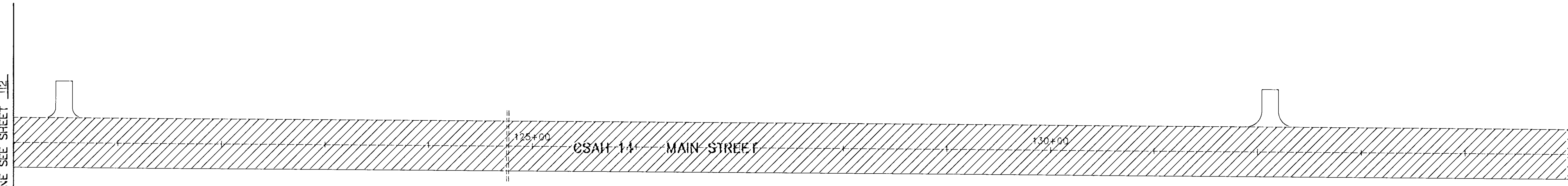
FILE NO.	SHEET NO.
5006-61	112
DATE	OF 117
7-8-92	

MATCHLINE SEE SHEET 112

MATCHLINE SEE BELOW LEFT


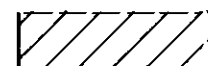
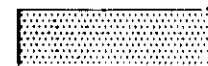
MATCHLINE SEE ABOVE RIGHT

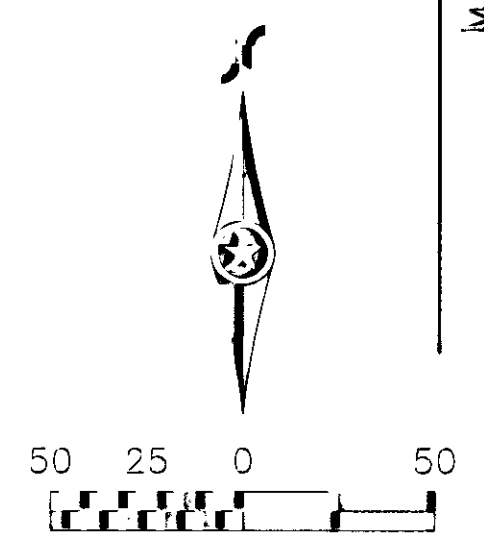
MATCHLINE SEE SHEET 114



NOTE: CSAH 14 WILL BE CLOSED FOR CONSTRUCTION IN SEGMENTS FROM STA. 77+00 TO STA. 175+40. SEE SHORT TERM ROAD CLOSURES PLAN AND LOCATION TABULATION. MAINTAIN ACCESS FROM THE EAST OR WEST TO ALL PROPERTIES ALONG CSAH 14 AT ALL TIMES.

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DIR & FILE NAME: C:\ANOKA\313-0.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			


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**STAGE 3
TRAFFIC CONTROL**

FILE NO.	SHEET NO.
5006-61	113
DATE	
7-8-92	OF 117

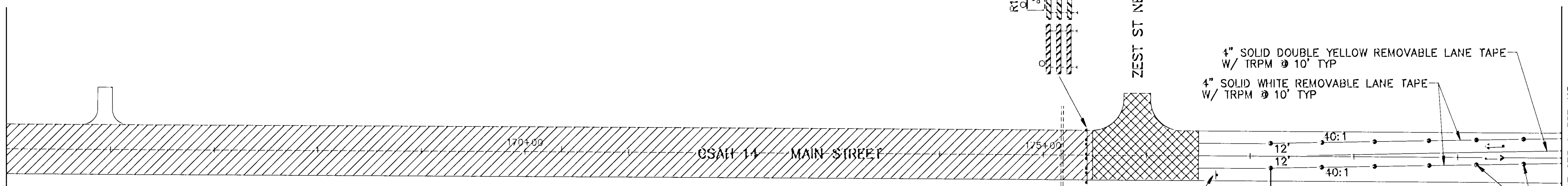
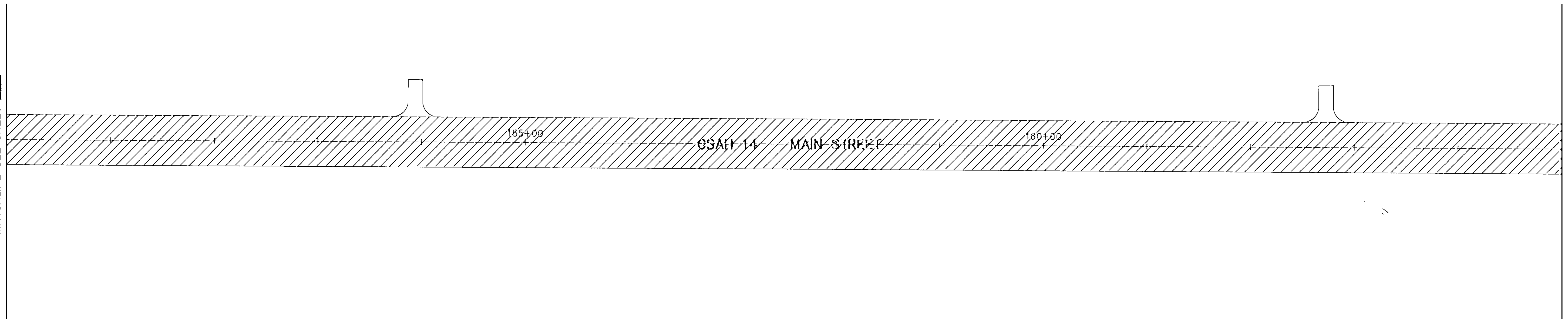
MATCHLINE SEE SHEET 113

MATCHLINE SEE BELOW LEFT

MATCHLINE SEE ABOVE RIGHT

MATCHLINE SEE SHEET 115

NOTE: CSAH 14 WILL BE CLOSED FOR CONSTRUCTION IN SEGMENTS FROM STA. 77+00 TO STA. 175+40. SEE SHORT TERM ROAD CLOSURES PLAN AND LOCATION TABULATION. MAINTAIN ACCESS FROM THE EAST OR WEST TO ALL PROPERTIES ALONG CSAH 14 AT ALL TIMES.

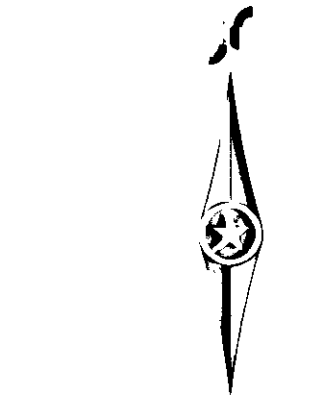


LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING
- CONSTRUCTION UNDER TRAFFIC

REFLECTORIZED DRUMS @ 50' TYP

BEGIN TAPER STA 177+17



DIR & FILE NAME: C:\ANOKA\STD-6.DWG
PLOT SCALE: 1"=50'
DATE: 7-8-92

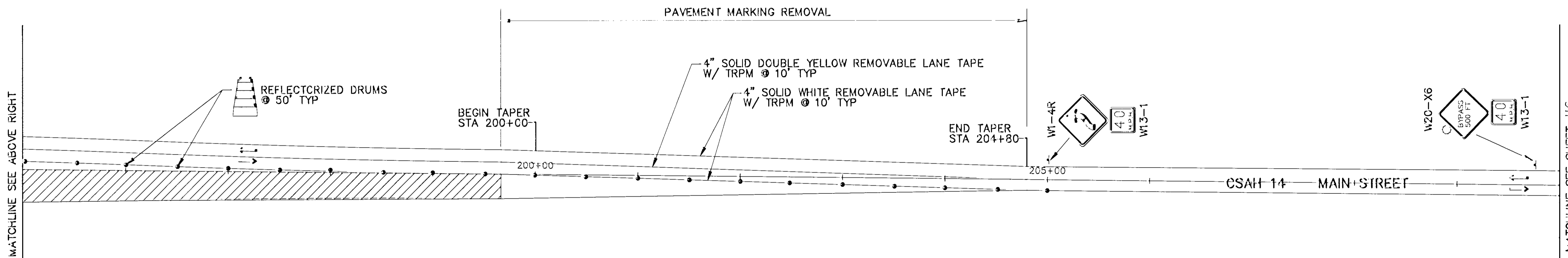
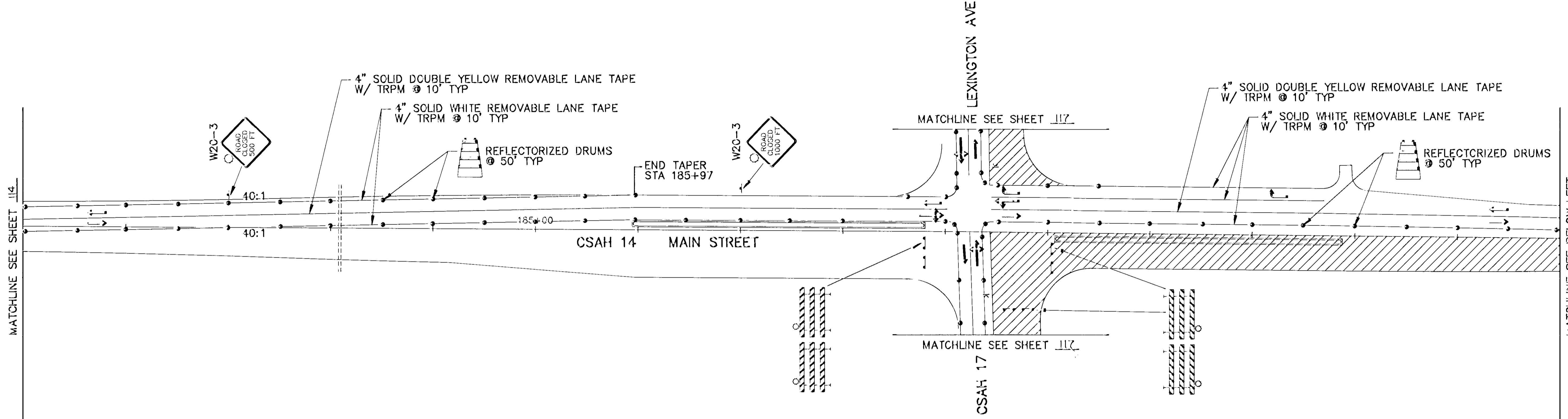
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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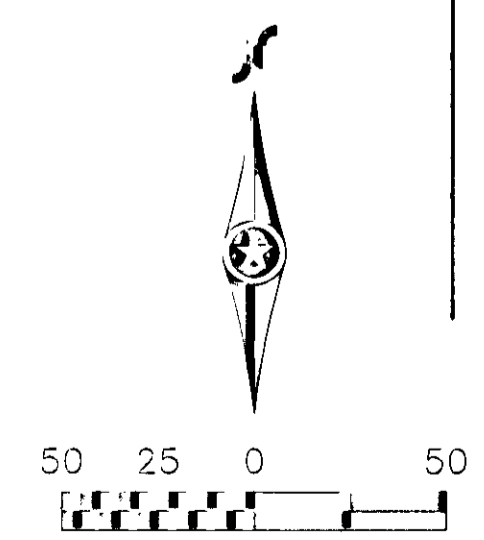
STAGE 3
TRAFFIC CONTROL

FILE NO.	SHEET NO.
50C6-61	114
DATE	
7-8-92	OF 117



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPORARY WIDENING



DIR & FILE NAME: C:\WORK\AS13-7.DWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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**STAGE 3
 TRAFFIC CONTROL**

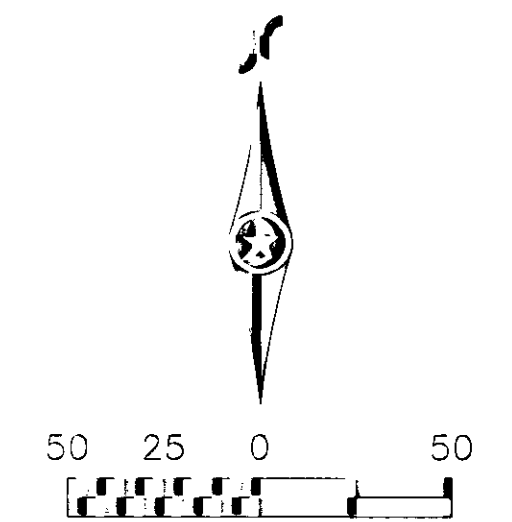
FILE NO.	SHEET NO.
5006-61	115
DATE	
7-8-92	OF 117

MATCHLINE SEE SHEET 115

TRAFFIC ON EXISTING ROADWAY

CSAH 14 MAIN STREET

215+00



MATCHLINE SEE SHEET 110

4" SOLID DOUBLE YELLOW PAINT STRIPE (PERMANENT)

4" SOLID WHITE PAINT STRIPE (PERMANENT)


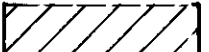

RIGHT TURN LANE R3-X1

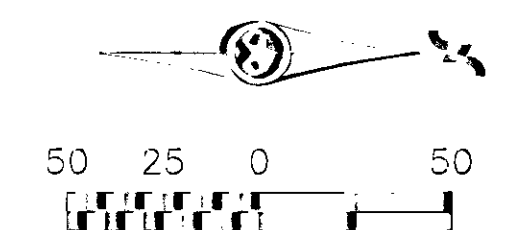
CO. RD. 52 RADISSON RD

505+00

510+00

LEGEND

-  INPLACE TRAFFIC
-  CONSTRUCTION THIS STAGE
-  TEMPORARY WIDENING



DIR & FILE NAME: C:\ANDOKA\113-8.DWG
PLOT DATE: 7-8-92
PLOT SCALE: 1"=50'

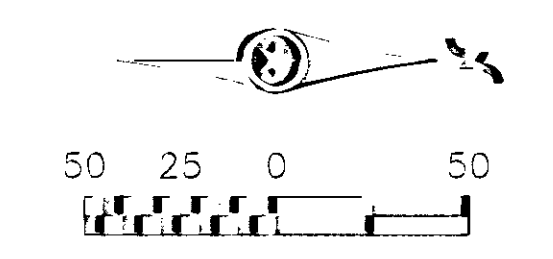
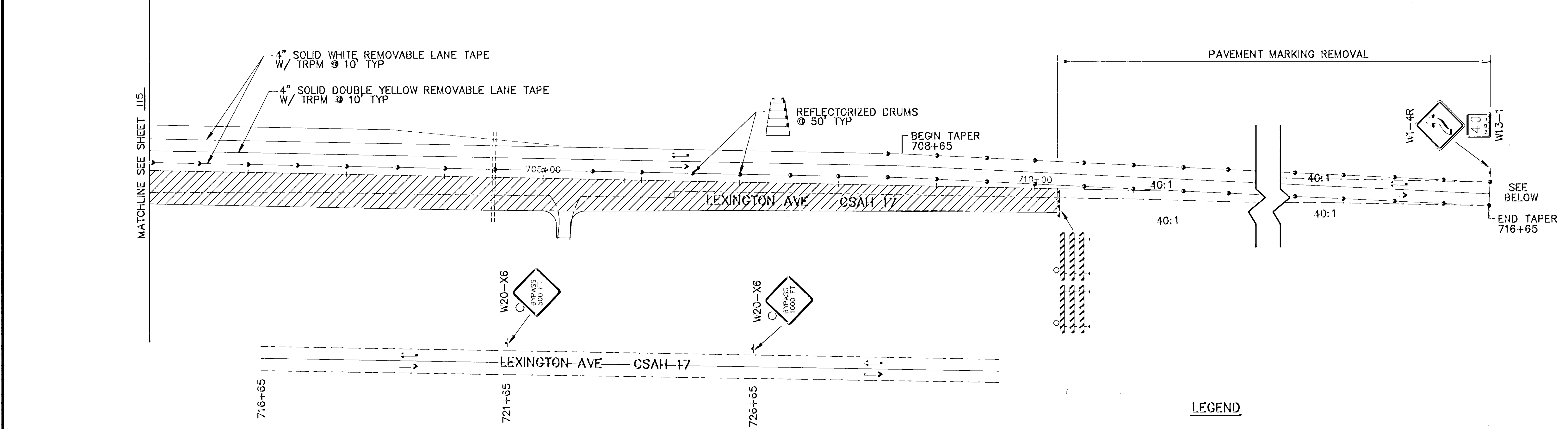
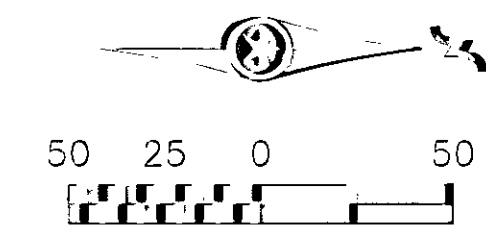
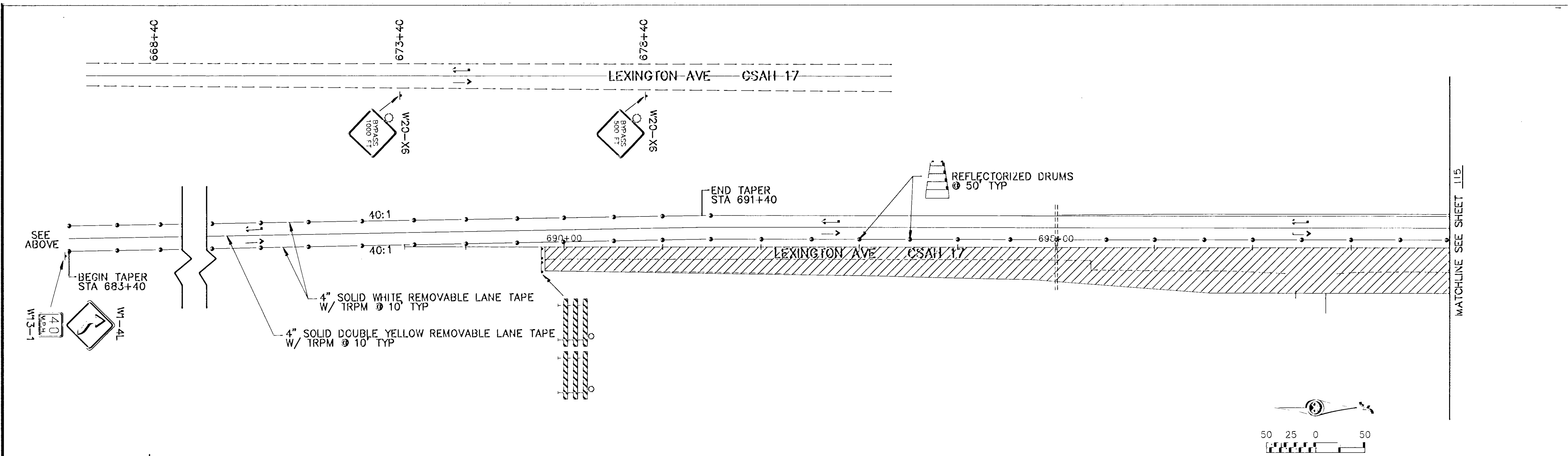
DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHCWN			

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STAGE 3
TRAFFIC CONTROL

FILE NO.	SHEET NO.
50C6-61	116
DATE	
7-8-92	OF 117



LEGEND

- INPLACE TRAFFIC
- CONSTRUCTION THIS STAGE
- TEMPRCARY WIDENING

DIR & FILE NAME: C:\ANOKA\STD-BLDWG
 OPERATOR: SAP
 PLOT SCALE: 1"=50'
 DATE: 7-8-92

DESIGN	RRC	SCALE	NO.	DATE	REVISION
CHECKED	KWH	AS			
DRAWN	SAP	SHOWN			

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STAGE 3
TRAFFIC CONTROL

FILE NO.	SHEET NO.
50C6-61	117
DATE	OF 117
7-8-92	

