

STATEMENT OF ESTIMATED QUANTITIES

NOTES	ITEM NUMBER	ITEM DESCRIPTION	UNIT	TOTAL PROJECT QUANTITIES ESTIMATED
	2021.501	MOBILIZATION	LUMP SUM	1
1,2	2104.503	SAWING CONCRETE PAVEMENT (FULL DEPTH)	LIN FT	43
1,2	2104.503	SAWING BIT PAVEMENT (FULL DEPTH)	LIN FT	195
2	2104.504	REMOVE CONCRETE DRIVEWAY PAVEMENT	SQ YD	123
3	2104.504	REMOVE BITUMINOUS DRIVEWAY PAVEMENT	SQ YD	606
3	2104.504	REMOVE BITUMINOUS PAVEMENT	SQ YD	162
4,5	2123.510	MOTOR GRADER	hour	12
	2130.523	WATER	M GALLON	277
6	2211.509	AGGREGATE BASE CLASS 5	TON	169
	2215.504	FULL DEPTH RECLAMATION	SQ YD	39549
4,5	2215.504	HAUL FULL DEPTH RECLAMATION (LV)	CU YD	174
7	2221.509	SHOULDER BASE AGGREGATE CLASS 5	TON	4335
8	2232.604	MILL BITUMINOUS PAVEMENT (SPECIAL)	SQ YD	95
9	2301.602	DRILL & GROUT DOWEL BAR (EPOXY COATED)	EACH	25
	2357.506	BITUMINOUS MATERIAL FOR TACK COAT	GALLON	1986
10	2360.509	TYPE SP 9.5 WEARING COURSE MIX (3;B)	TON	70
11	2360.509	TYPE SP 12.5 WEARING COURSE MIX (4;C)	TON	37
	2360.509	TYPE SP 12.5 WEARING COURSE MIX (4;C)	TON	9096
	2531.504	6" CONCRETE DRIVEWAY PAVEMENT	SQ YD	123
12	2540.602	MAIL BOX SUPPORT	EACH	40
13	2563.601	TRAFFIC CONTROL	LUMP SUM	1
14	2563.602	PORTABLE CHANGEABLE MESSAGE SIGN	EACH	20
	2574.507	COMMON TOPSOIL BORROW	CU YD	53
	2575.508	HYDRAULIC REINFORCED FIBER MATRIX	POUND	1774
15	2581.503	REMOVABLE PREFORM PAVEMENT MARKING TAPE	LIN FT	548
16	2582.503	4" SOLID LINE MULTI-COMPONENT GROUND IN	LIN FT	32290
16	2582.503	4" BROKEN LINE MULTI-COMPONENT GROUND IN	LIN FT	1760
16	2582.503	4" DOUBLE SOLID LINE MULTI-COMPONENT GROUND IN	LIN FT	4600

CONSTRUCTION NOTES

1	REFERENCE DETAILS FOR REMOVAL DETAILS.
2	CONTRACTOR IS RESPONSIBLE FOR CONTACTING PROPERTY OWNER 48 HOURS BEFORE STARTING OPERATION.
3	ITEM FOR BITUMINOUS DRIVEWAYS AND STREET APPROACHES. CONTRACTOR IS RESPONSIBLE FOR CONTACTING PROPERTY OWNER 48 HOURS BEFORE STARTING OPERATION
4	ITEM FOR SUPERELEVATION / GRADE CORRECTION.
5	ITEM USED TO MOVE EXCESS RECLAIM MATERIAL AT THE RECLAIM AREA LIMITS TO CREATE A SMOOTH TRANSITION BETWEEN THE PROPOSED AND EXISTING PAVEMENT.
6	GRAVEL BASE FOR CONCRETE AND BITUMINOUS STREET APPROACHES AND DRIVEWAYS.
7	ITEM INCLUDES 7 TONS FOR EACH GRAVEL ENTRANCE AND GRAVEL STREET APPROACH.
8	ITEM INCLUDES 2" DEEP MILL AT BEGINNING AND END OF PROJECT, ALL PAVED STREET APPROACHES, AND DRIVEWAYS; SEE DETAILS FOR DIMENSIONS.
9	ITEM USED FOR CONCRETE DRIVEWAYS
10	ITEM FOR BITUMINOUS DRIVEWAYS. DRIVEWAYS SHALL BE PAVED AFTER MAINLINE AND BEFORE FINAL STRIPING.
11	ITEM FOR STREET APPROACHES. STREET APPROACHES SHALL BE PAVED AFTER MAINLINE, AND BEFORE FINAL STRIPING. SEE BIT SUMMARY
12	MAILBOXES ARE TO BE INSTALLED AT THE EXISTING MAILBOX LOCATION OR AS DIRECTED BY THE LOCAL POST AUTHORITY, CONTRACTOR IS RESPONSIBLE FOR CONTRACTING. MAILBOX REMOVAL AND ALL MATERIALS ARE INCIDENTAL TO INSTALLATION.
13	ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO, AND BE INSTALLED IN ACCORDANCE WITH, THE MOST CURRENT REVISION OF THE "MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES". "DO NOT PASS, PASS WITH CARE, NO CENTER STRIPE, AND STOP HERE ON RED SIGNS SHALL BE INPLACE WHENEVER PERMANENT PAVEMENT MARKINGS ARE NOT PRESENT.
14	2 MESSAGE BOARDS, ONE ON EACH END OF PROJECT, SHALL BE INSTALLED 10 DAYS PRIOR TO ANY CONSTRUCTION; REFERENCE STRIPING PLAN FOR DETAILS.
15	CENTERLINE AND LANE DESIGNATION SKIPS TO BE APPLIED AS SOON AS POSSIBLE ON MILLED SURFACE AND EACH NEW LIFT OF PAVEMENT; SKIPS MUST BE INPLACE BEFORE THE CONTRACTOR LEAVES FOR THE DAY. CONTRACTOR IS TO REMOVE PRIOR TO FINAL STRIPING.
16	FINAL STRIPING SHALL BE INSTALLED WITHIN 72 HOURS OF COMPLETION OF MAINLINE WEAR COURSE PAVING.

BITUMINOUS STREET SUMMARY

LOCATION	BITUMINOUS	NOTES
	2360 TYPE SP 12.5 WEAR (4,C)	
	TON	
Rabbit ST NW	21	[1]
Xenolith ST NW	17	[1]
PROJECT TOTAL	37	

BITUMINOUS SUMMARY NOTES:

[1] QUANTITY ESTIMATED FOR 2 LIFTS

THE FOLLOWING STANDARD PLATES, APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION, SHALL APPLY ON THIS PROJECT.

MNDOT STANDARD PLATES	
PLATE NO.	DESCRIPTION
8000J	CHANNELIZERS
9350A	MAILBOX SUPPORT (SWING-AWAY TYPE)

BASIS OF PLANNED QUANTITIES


2357	BITUMINOUS MATERIAL FOR TACK COAT	0.05 GAL / SQ YD
2211	AGGREGATE BASE CLASS 5	1.8 TONS / CU YD
2360	ALL BITUMINOUS PAVEMENT	115 LBS / SQ YD / IN THICKNESS
2581	REMOVABLE PREFORM PAVEMENT MARKING TAPE	2' AT 50' INTERVALS
2575	SEED MIXTURE 25-121	61 LBS./ ACRE
2574	FERTILIZER TYPE 3	350 LBS./ ACRE

NO	DATE	BY	CKD	APPR	REVISION	
	02/28/2023					11:10:25 AM

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I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GERALD J. AUGER JR.

SIGNATURE: 

DATE: 12-16-2022 LICENSE NO. 26511

DRAWN BY: MAR DATE: 01/04/2021

DESIGN BY: MAR DATE: 01/04/2021

CHECKED BY: CO DATE: 12/06/2022



**ANOKA COUNTY
HIGHWAY DEPT.**

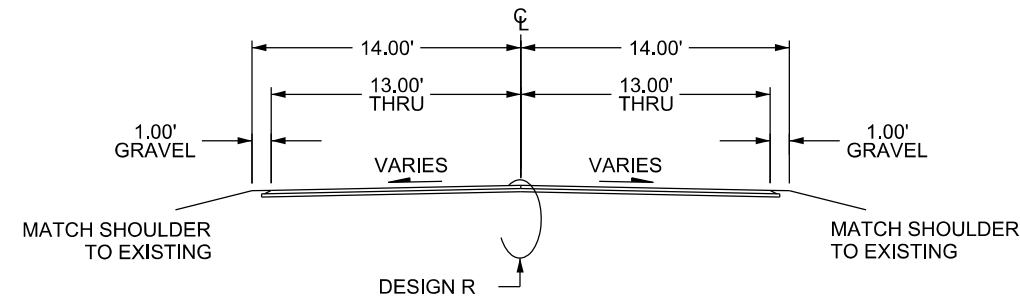
COUNTY PROJECT 21-36-64

STATEMENT OF ESTIMATED QUANTITIES

Sheet 2 of 18 Sheets

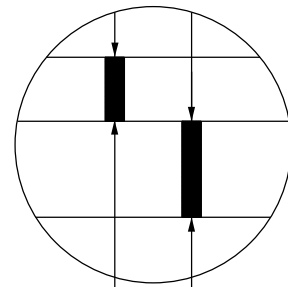
CR 64 - 181ST AVE NW
EXISTING / PROPOSED SECTION

11+05.00 - 147+95.00



NOTE: 1' BUMPOUT AT LOW END OF SUPER

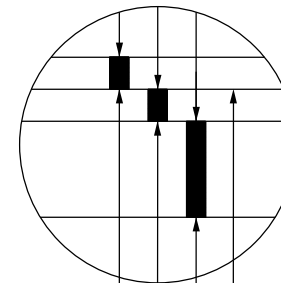
EXISTING SECTION



6.5" +/- EXISTING BITUMINOUS

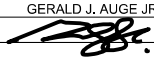
EXISTING SUBGRADE

**DESIGN R
 RECLAIM SECTION**



2.0" BITUMINOUS WEAR(SPWEB440C)
 2.0" BITUMINOUS WEAR(SPWEB440C)
 STABILIZED FULL DEPTH RECLAMATION
 BIT MATERIAL FOR TACK COAT MNDOT SPEC 2357

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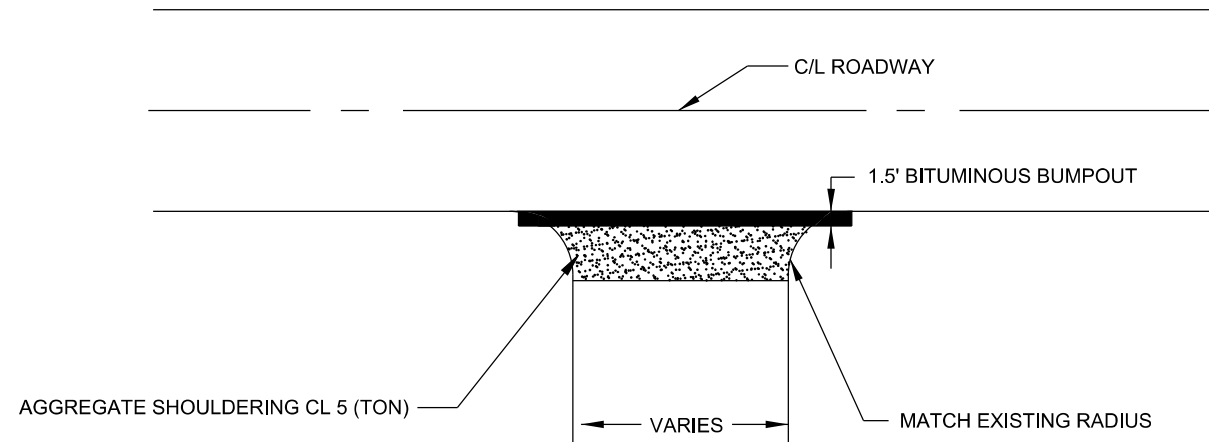


**ANOKA COUNTY
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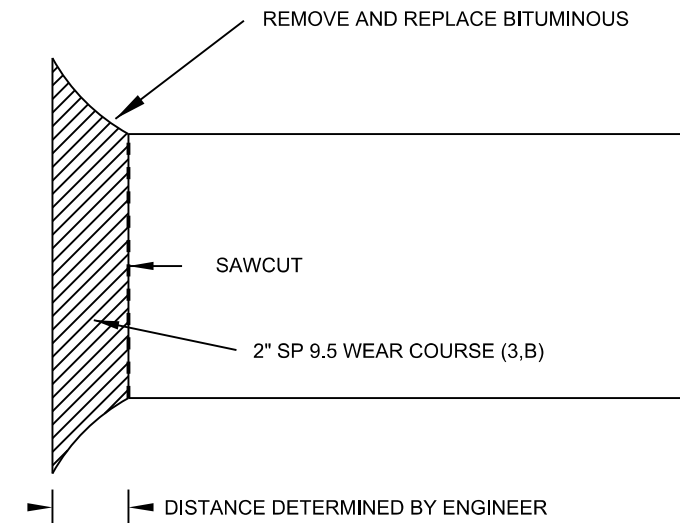
COUNTY PROJECT 21-36-64

TYPICAL SECTIONS
 Sheet 3 of 18 Sheets

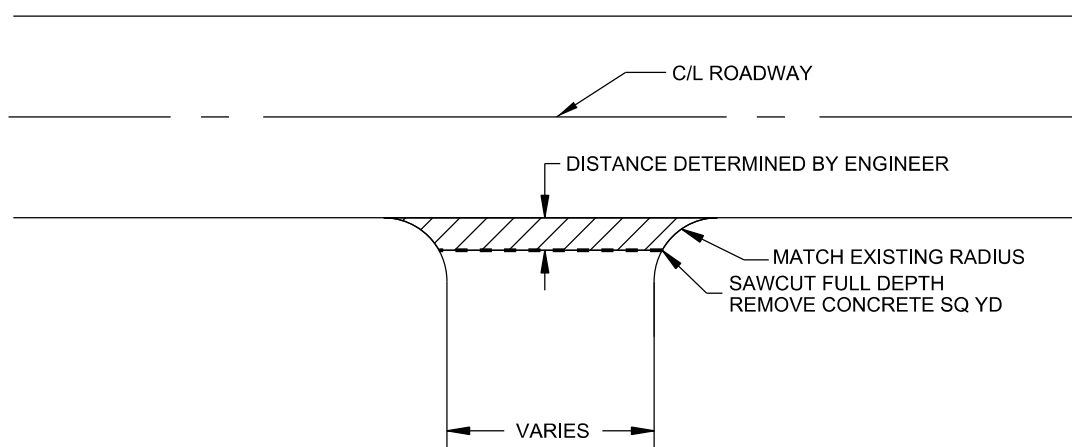
DRIVEWAY DETAIL
GRAVEL / FIELD ENTRANCE



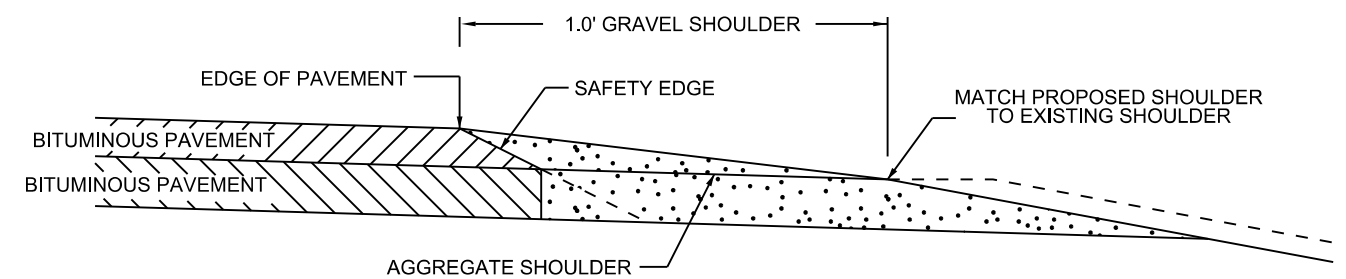
RECLAIM AREA - DRIVEWAY DETAIL
BITUMINOUS
PLAN VIEW



DRIVEWAY DETAIL
CONCRETE DRIVEWAY



SHOULDER DETAIL
BITUMINOUS SAFETY EDGE
GRAVEL SHOULDER



SAFETY EDGE TO BE USED IN ALL NON-CURB AREAS ON SHOULDER.

OPTIONAL DESIGN EXTENDS SAFETY EDGE DEEPER THAN 6" AND WIDER THAN 10.5". SEE SPECIAL PROVISIONS.

NO	DATE	BY	CKD	APPR	REVISION	TIME
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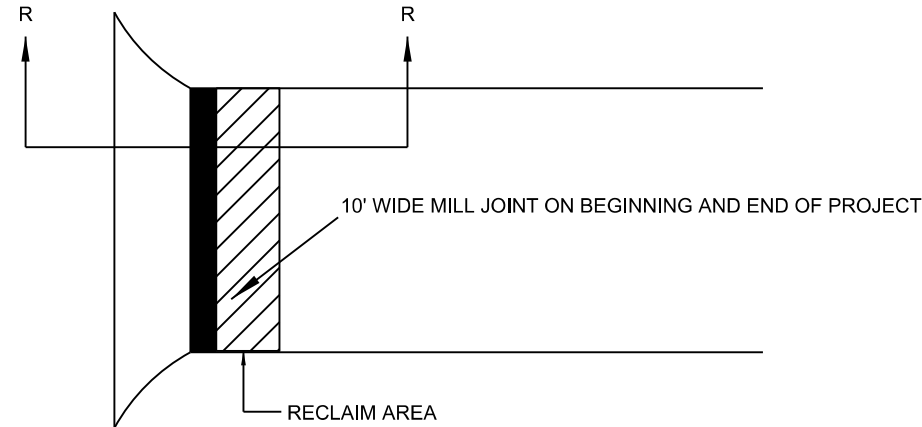
COUNTY PROJECT 21-36-64

DETAILS
 Sheet 4 of 18 Sheets

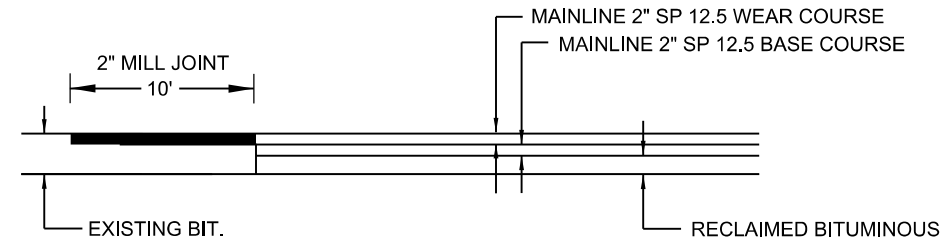
STREET APPROACH DETAIL (RECLAIM)

BITUMINOUS STREET

PLAN VIEW



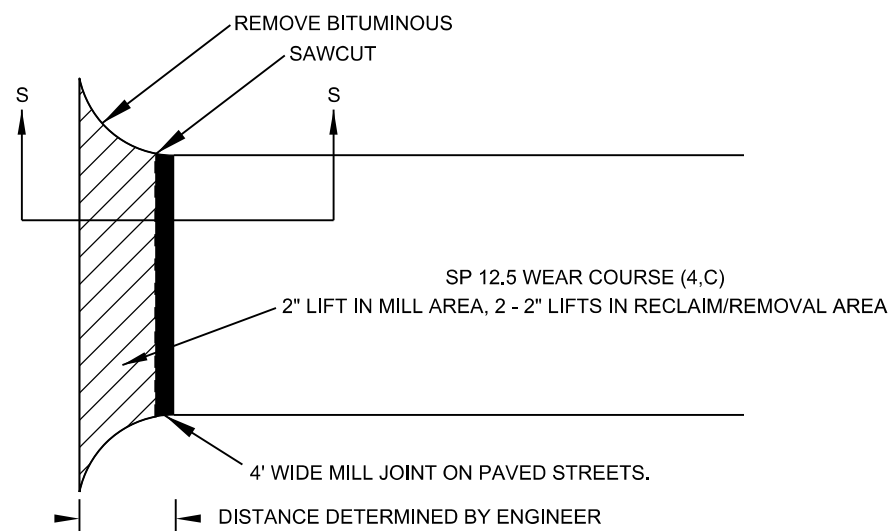
SECTION R - R



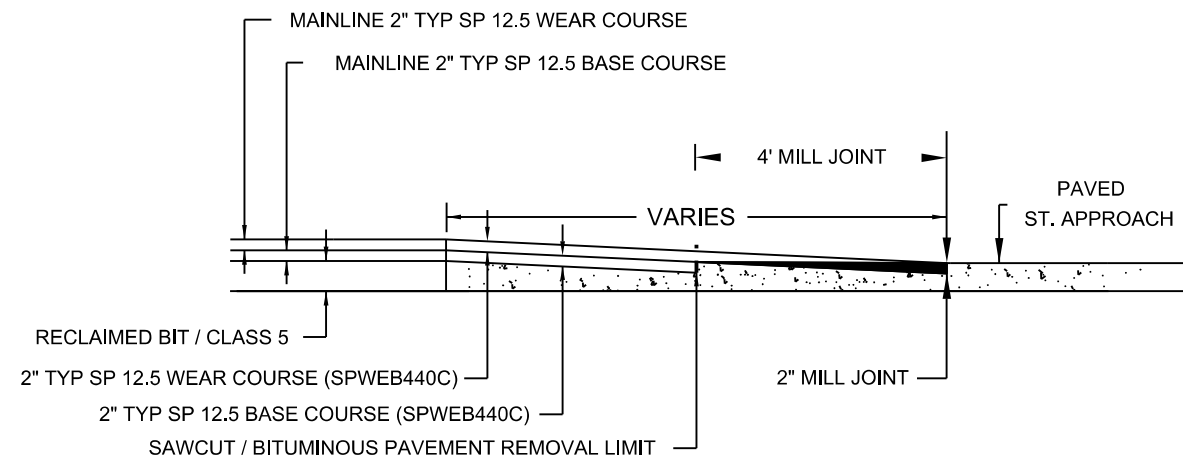
STREET APPROACH DETAIL (RECLAIM)

BITUMINOUS STREET

PLAN VIEW



SECTION S - S



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**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 21-36-64

DETAILS

Sheet 5 of 18 Sheets

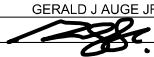
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STATION	POINT	LEFT GRADE	RIGHT GRADE
106+00		-2.0%	-2.0%
106+50	BEGIN LT/RT TRANS	-2.0%	-2.0%
107+00		-2.5%	-1.2%
107+50		-3.0%	-0.4%
108+00		-3.5%	0.4%
108+50	PC & BEGIN FULL SUPER LT	-4.0%	1.2%
109+00	BEGIN FULL SUPER RT	-4.0%	2.0%
109+50		-4.0%	2.0%
110+00		-4.0%	2.0%
110+50	END FULL SUPER LT/RT	-4.0%	2.0%
111+00		-3.0%	1.0%
111+50		-2.0%	0.0%
112+00		-1.0%	-1.0%
112+50	PC	0.0%	-2.0%
113+00		1.0%	-3.0%
113+50	BEGIN FULL SUPER LT/RT	2.0%	-4.0%
114+00		2.0%	-4.0%
114+50		2.0%	-4.0%
115+00	END FULL SUPER LT	2.0%	-4.0%
115+50	PT & END FULL SUPER RT	1.3%	-4.0%
116+00		0.7%	-3.7%
116+50		0.0%	-3.3%
117+00		-0.7%	-3.0%
117+50		-1.3%	-2.7%
118+00	END LT TRANS	-2.0%	-2.3%
118+50	END RT TRANS	-2.0%	-2.0%
119+00		-2.0%	-2.0%

SUPERELEVATION CORRECTION TAB CURVE 3&4			
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119+50	BEGIN LT TRANS	-2.0%	-2.0%
120+00	BEGIN RT TRANS	-1.0%	-2.0%
120+50		0.0%	-2.8%
121+00		1.0%	-3.5%
121+50		2.0%	-4.3%
122+00	PC & BEGIN FULL SUPER LT/RT	3.0%	-5.0%
122+50		3.0%	-5.0%
123+00		3.0%	-5.0%
123+50		3.0%	-5.0%
124+00		3.0%	-5.0%
124+50		3.0%	-5.0%
125+00	END FULL SUPER	3.0%	-5.0%
125+50	PT & END FULL SUPER RT	1.6%	-5.0%
126+00		0.2%	-3.6%
126+50		-1.2%	-2.2%
127+00	PC	-2.6%	-0.8%
127+50	BEGIN FULL SUPER LT	-4.0%	0.6%
128+00	BEGIN FULL SUPER RT	-4.0%	2.0%
128+50		-4.0%	2.0%
129+00		-4.0%	2.0%
129+50	END FULL SUPER RT	-4.0%	2.0%
130+00	PT	-4.0%	1.2%
130+50	END FULL SUPER LT	-4.0%	0.4%
131+00		-3.5%	-0.4%
131+50		-3.0%	-1.2%
132+00	END RT TRANS	-2.5%	-2.0%
132+50	END LT TRANS	-2.0%	-2.0%
133+00		-2.0%	-2.0%

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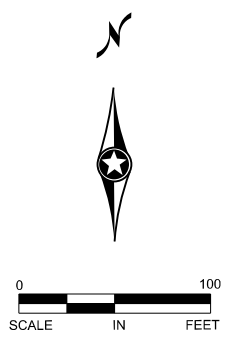
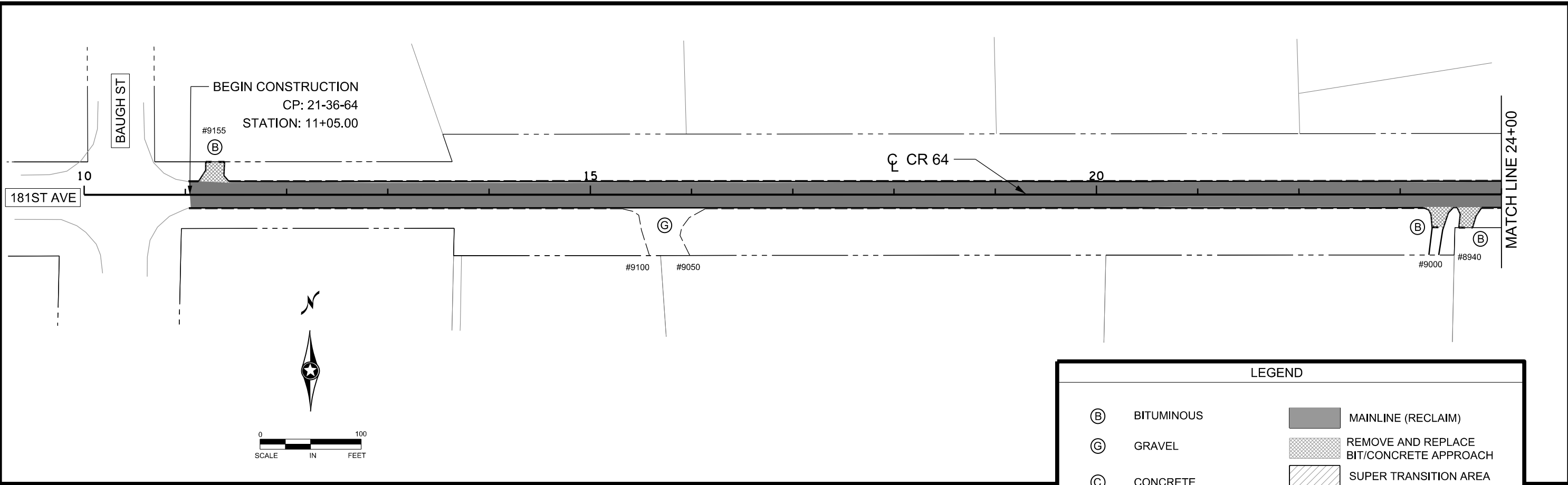


**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 21-36-64

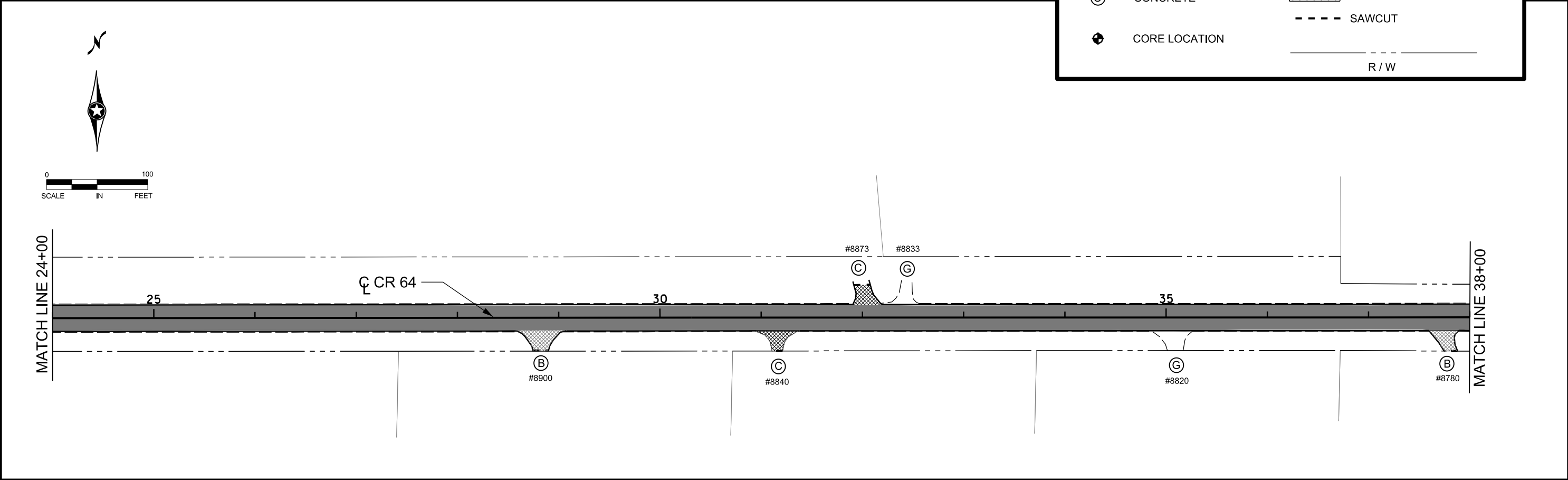
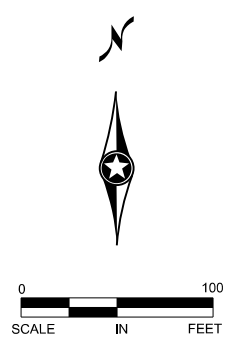
SUPERELEVATION TABULATIONS
 Sheet 6 of 18 Sheets

SPHoots



LEGEND

(B)	BITUMINOUS		MAINLINE (RECLAIM)
(G)	GRAVEL		REMOVE AND REPLACE BIT/CONCRETE APPROACH
(C)	CONCRETE		SUPER TRANSITION AREA
⊕	CORE LOCATION	- - - -	SAWCUT
		— — — —	R / W



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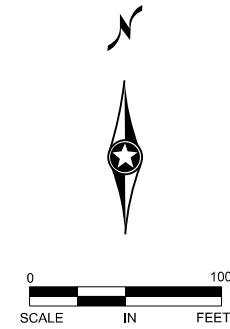
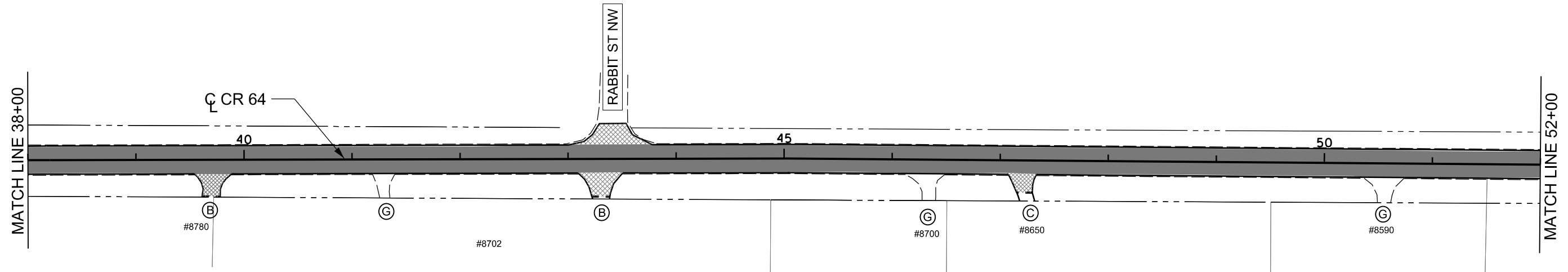
**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 21-36-64

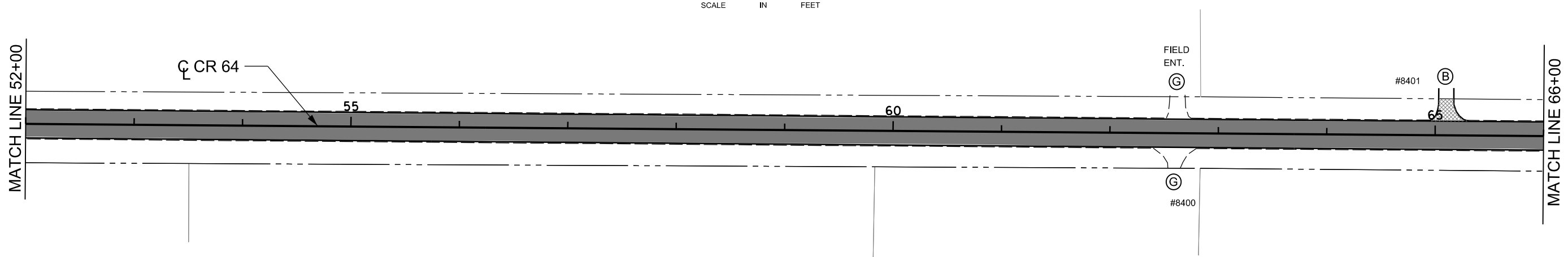
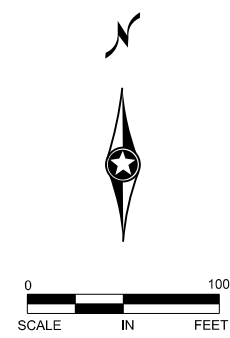
CONSTRUCTION PLAN

STA 11+05 TO 38+00

Sheet 7 of 18 Sheets



LEGEND			
(B)	BITUMINOUS	[Solid Grey Box]	MAINLINE (RECLAIM)
(G)	GRAVEL	[Cross-hatched Box]	REMOVE AND REPLACE BIT/CONCRETE APPROACH
(C)	CONCRETE	[Diagonal Hatched Box]	SUPER TRANSITION AREA
(*)	CORE LOCATION	[Dashed Line]	SAWCUT
		[Solid Line]	R / W



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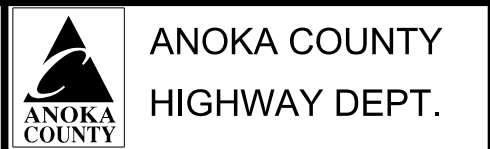
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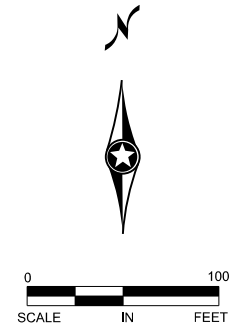
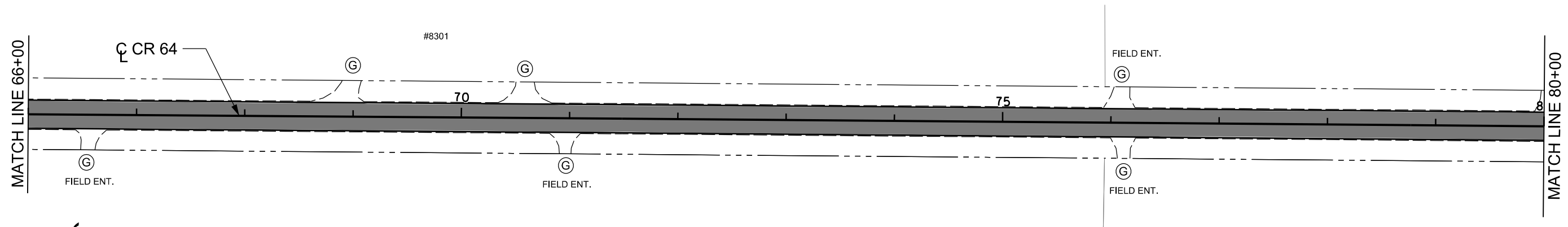


COUNTY PROJECT 21-36-64

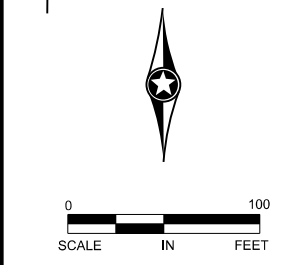
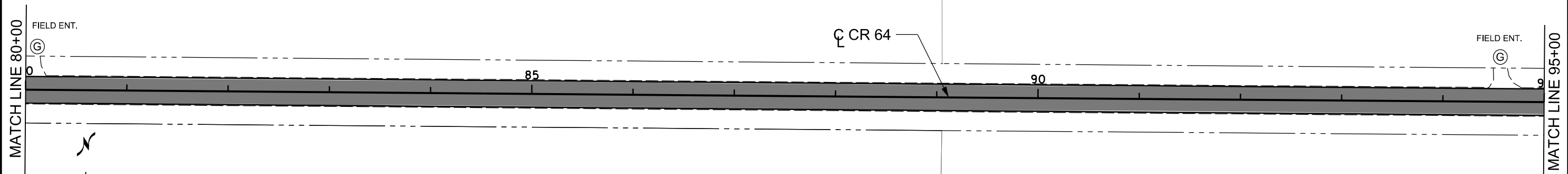
CONSTRUCTION PLAN

STA 38+00 TO 66+00

Sheet 8 of 18 Sheets



LEGEND			
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(G)	GRAVEL		REMOVE AND REPLACE BIT/CONCRETE APPROACH
(C)	CONCRETE		SUPER TRANSITION AREA
(*)	CORE LOCATION	- - - -	SAWCUT
		— — — —	R / W



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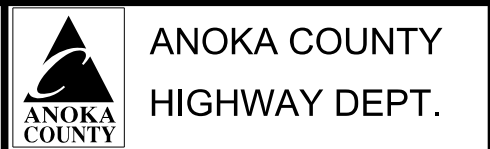
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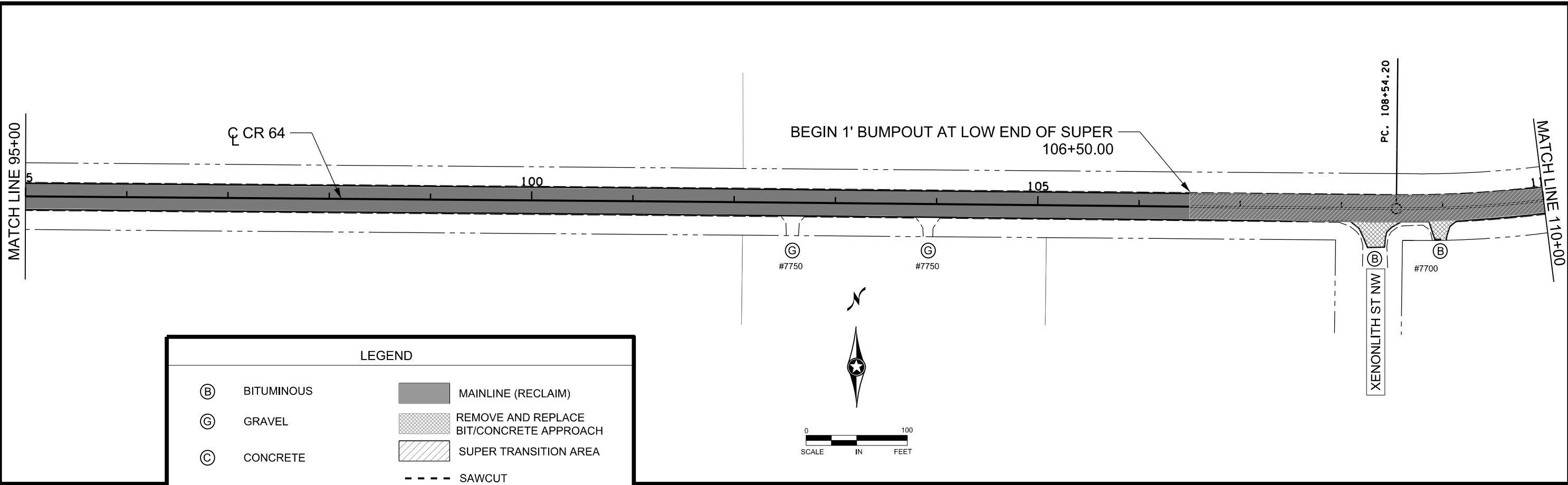
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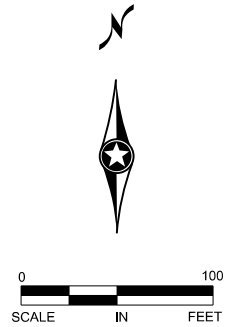
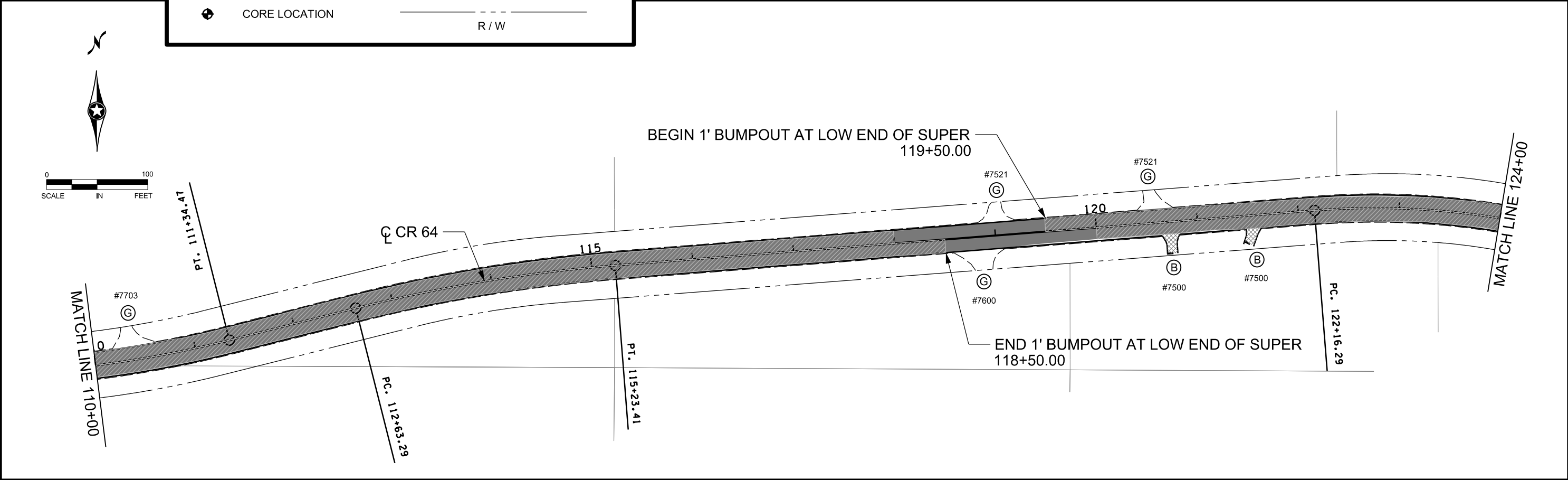
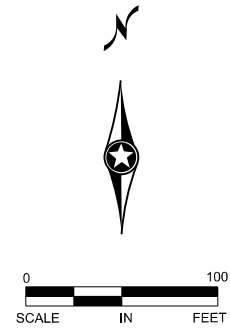
COUNTY PROJECT 21-36-64

CONSTRUCTION PLAN
 STA 66+00 TO 95+00
 Sheet 9 of 18 Sheets



LEGEND

(B)	BITUMINOUS		MAINLINE (RECLAIM)
(G)	GRAVEL		REMOVE AND REPLACE BIT/CONCRETE APPROACH
(C)	CONCRETE		SUPER TRANSITION AREA
⊕	CORE LOCATION	- - - - -	SAWCUT
		- - - - -	R / W



NO	DATE	BY	CKD	APPR	REVISION	
	01/11/2023					11:21:49 AM

NAME: P:\23-01-00\CR_64_(Baugh-Nowthen Blvd)\Base\Proposed\CR64_CP4.dgn

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: GERALD J AUGER JR.

SIGNATURE:

DATE: 12-16-2022 LICENSE NO. 26511

DRAWN BY: MAR DATE: 01/04/2021

DESIGN BY: MAR DATE: 01/04/2021

CHECKED BY: CO DATE: 12/06/2022

**ANOKA COUNTY
HIGHWAY DEPT.**

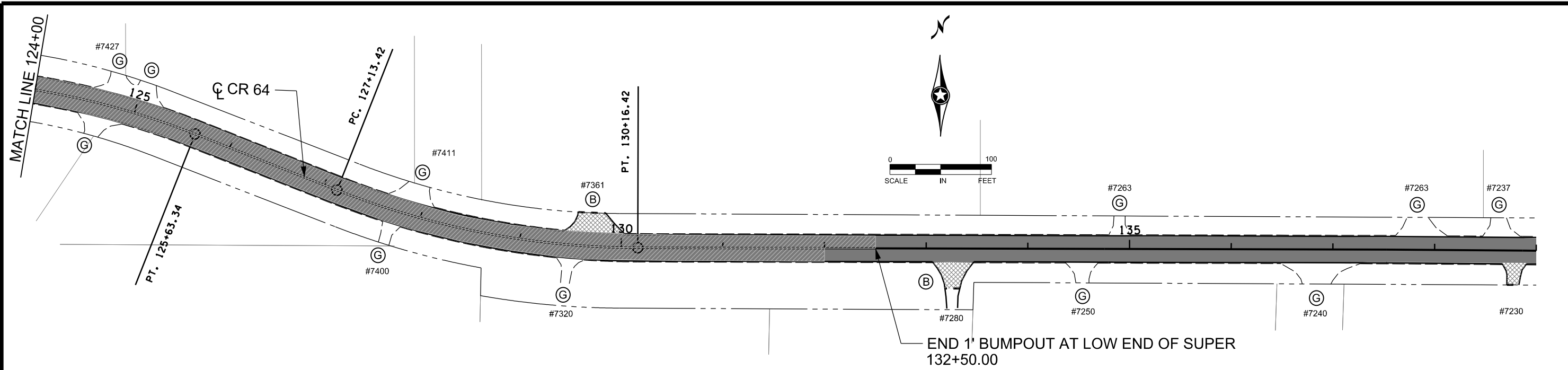
COUNTY PROJECT 21-36-64

CONSTRUCTION PLAN

STA 95+00 TO 124+00

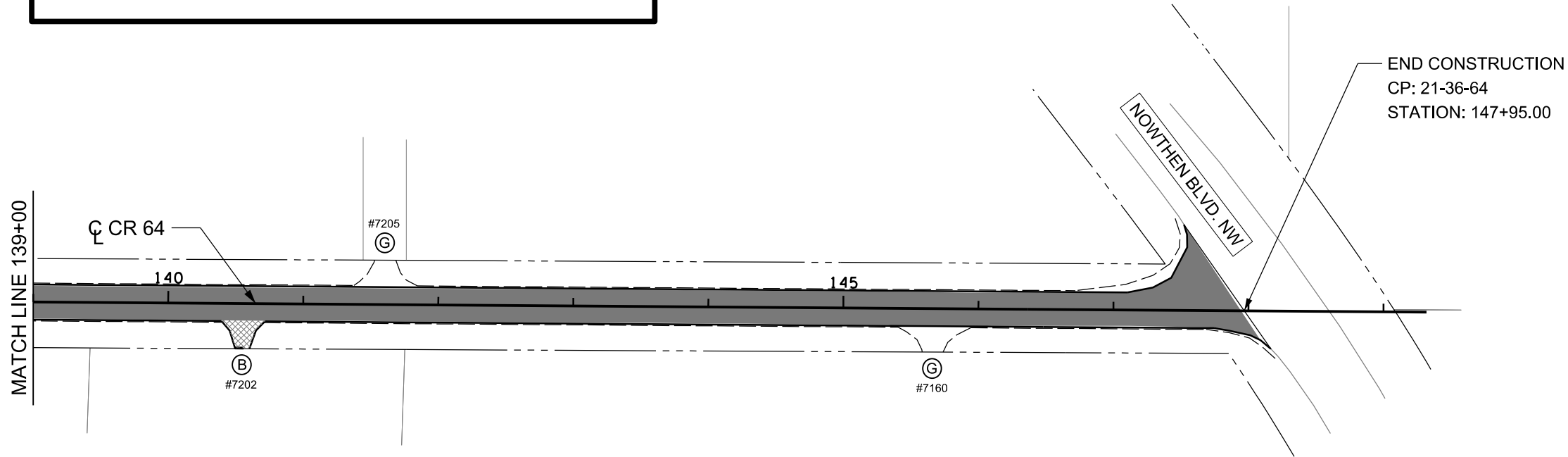
Sheet 10 of 18 Sheets

SPH005



LEGEND

(B) BITUMINOUS	[Hatched Box] MAINLINE (RECLAIM)
(G) GRAVEL	[Dotted Box] REMOVE AND REPLACE BIT/CONCRETE APPROACH
(C) CONCRETE	[Diagonal Lines Box] SUPER TRANSITION AREA
◆ CORE LOCATION	- - - SAWCUT
	— R / W



NO	DATE	BY	CKD	APPR	REVISION	
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NAME: P:\23-01-00\CR_64_(Baugh-Nowthen Blvd)\Base\Proposed\CR64_CP5.dgn

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SIGNATURE: *[Signature]*

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**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 21-36-64

CONSTRUCTION PLAN

STA 124+00 TO 147+95

Sheet 11 of 18 Sheets

**PERMANENT PAVEMENT MARKING PLAN
NOTES AND GUIDELINES**

GENERAL INFORMATION:

THE ENGINEER'S INVOLVEMENT IN THE APPLICATION OF THE MATERIAL SHALL BE LIMITED TO FIELD CONSULTATION AND INSPECTION. ANOKA COUNTY HIGHWAY DEPARTMENT WILL PLACE NECESSARY "SPOTTING" AT APPROPRIATE POINTS TO PROVIDE HORIZONTAL CONTROL FOR STRIPING AND TO DETERMINE NECESSARY STARTING AND CUTOFF POINTS. LONGITUDINAL JOINTS, PAVEMENT EDGES AND EXISTING MARKINGS MAY SERVE AS HORIZONTAL CONTROL WHEN SO DIRECTED.

EDGE LINES AND LANE LINES ARE TO BE BROKEN ONLY AT INTERSECTIONS WITH PUBLIC ROADS AND AT PRIVATE ENTRANCES IF THEY ARE CONTROLLED BY A YIELD SIGN, STOP SIGN OR TRAFFIC SIGNAL. THE BREAK POINT IS TO BE AT THE START OF THE RADIUS FOR THE INTERSECTION OR AT MARKED STOP LINES OR CROSSWALKS.

A TOLERANCE OF 1/4 INCH UNDER OR 1/4 INCH OVER THE SPECIFIED WIDTH WILL BE ALLOWED FOR STRIPING PROVIDED THE VARIATION IS GRADUAL AND DOES NOT DETRACT FROM THE GENERAL APPEARANCE. BROKEN LINE SEGMENTS MAY VARY UP TO ONE-HALF FOOT FROM THE SPECIFIED LENGTHS PROVIDED THE OVER AND UNDER VARIATIONS ARE REASONABLY COMPENSATORY. ALIGNMENT DEVIATIONS FROM THE CONTROL GUIDE SHALL NOT EXCEED 1 INCH. MATERIAL SHALL NOT BE APPLIED OVER LONGITUDINAL JOINTS. ESTABLISHMENT OF APPLICATION TOLERANCES SHALL NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO COMPLY AS CLOSELY AS PRACTICABLE WITH THE PLANNED DIMENSIONS.

MULTI COMPONENT (MULTI COMP):

THE ROAD SURFACE SHALL BE CLEANED AT THE DIRECTION OF THE ENGINEER JUST PRIOR TO APPLICATION. PAVEMENT CLEANING SHALL CONSIST OF AT LEAST BRUSHING WITH A ROTARY BROOM (NON-METALLIC) OR AS RECOMMENDED BY THE MATERIAL MANUFACTURER AND ACCEPTABLE TO THE ENGINEER. NEW PORTLAND CEMENT CONCRETE SURFACES SHALL BE SANDBLAST CLEANED TO REMOVE ANY SURFACE TREATMENT AND/OR LAITANCE ON LOW SPEED (SPEED LIMIT 35 MPH OR LESS) URBAN PORTLAND CEMENT CONCRETE ROADWAYS. SANDBLAST CLEANING SHALL BE USED FOR ALL MULTI COMP PAVEMENT MARKINGS.

THE MULTI COMP MARKING APPLICATION SHALL IMMEDIATELY FOLLOW THE PAVEMENT CLEANING. GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE MULTI COMP LINE TO PROVIDE AN IMMEDIATE NO-TRACK SYSTEM.

A MULTI COMP LINE SHALL BE APPLIED WITH A MINIMUM THICKNESS OF 20 MILS (WET) AND 4" WIDE. GLASS BEADS SHALL BE APPLIED AT A MINIMUM RATE OF 25LBS POUNDS PER GALLON RATE SUFFICIENT TO ACHIEVE AN ACCEPTABLE NO-TRACK SYSTEM.

OPERATIONS SHALL BE CONDUCTED ONLY WHEN THE ROAD PAVEMENT SURFACE TEMPERATURES ARE 50 DEGREES FAHRENHEIT OR GREATER.

PERMANENT PAVEMENT MARKINGS SHALL NOT BE PLACED OVER TEMPORARY TAPE MARKINGS.

PREFORMED THERMOPLASTIC:

THE PREFORMED THERMOPLASTIC MARKINGS SHALL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS ON CLEAN AND DRY SURFACES. SEE SPECIAL PROVISIONS FOR PREFORMED THERMOPLASTIC MARKING SPECIFICATIONS.

PAINT:

AT THE TIME OF APPLYING THE MARKING MATERIAL, THE APPLICATION AREA SHALL BE FREE OF CONTAMINATION. THE CONTRACTOR SHALL CLEAN THE ROADWAY SURFACE PRIOR TO THE LINE APPLICATION IN A MANNER AND TO THE EXTENT REQUIRED BY THE ENGINEER.

GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE PAINT LINE.

EXCEPT WHEN USED AS A TEMPORARY MARKING, PAVEMENT MARKINGS SHALL ONLY BE APPLIED IN SEASONABLE WEATHER WHEN AIR TEMPERATURE IS 50 DEGREES FARHENHEIT OR HIGHER AND SHALL NOT BE APPLIED WHEN THE WIND OR OTHER CONDITIONS CAUSE A FILD OR DUST TO BE DEPOSITED ON THE PAVEMENT SURFACE AFTER CLEANING AND BEFORE THE MARKING MATERIAL CAN BE APPLIED.

THE FILLING OF TANKS, POURING OF MATERIALS OR CLEANING OF EQUIPMENT SHALL NOT BE PERFORMED ON UNPROTECTED PAVEMENT SURFACES UNLESS ADEQUATE PROVISIONS ARE MADE TO PREVENT SPILLAGE OF MATERIAL.

PAVEMENT MARKING TABULATION			
ITEM	UNIT	TOTAL QUANTITY	NOTES
4" SOLID LINE MULTICOMP GROUND IN (WHITE)	LIN FT	27350	
4" SOLID LINE MULTICOMP GROUND IN (YELLOW)	LIN FT	4940	
4" BROKEN LINE MULTI COMP GROUND IN (YELLOW)	LIN FT	1760	1
4" SOLID DBL LINE MULTICOMP GROUND IN (YELLOW)	LIN FT	4600	

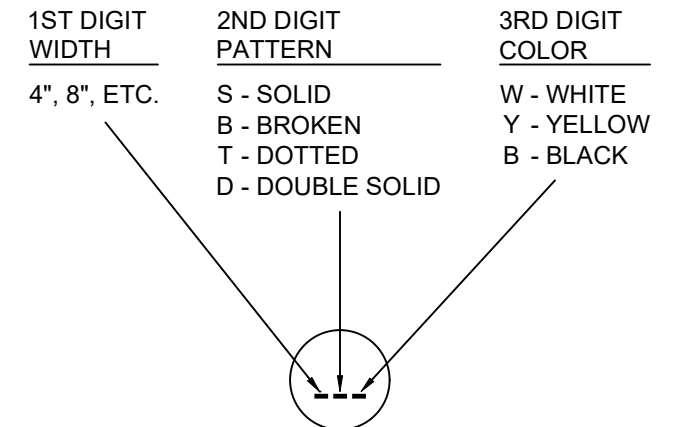
1 10' STRIPE, 40' GAP

SYMBOLS & MATERIALS LEGEND

- CROSSWALK BLOCK WHITE - POLY PREFORM
- ↩ PAVEMENT MESSAGE (LEFT ARROW) POLY PREFORM

STRIPING KEY

- CIRCLE - MULTI COMP GROUND IN
- △ TRIANGLE - PAINT
- SQUARE - POLY PREFORM THERMOPLASTIC
- ⬠ PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING



EXAMPLE: (4SW) = SOLID LINE WHITE - MULTI COMP

- — BROKEN LINE - 50' CYCLE (10' LINE, 40' GAP)
- - - - DOTTED LINE - 15' CYCLE (3' LINE, 12' GAP) UNLESS SHOWN OTHERWISE IN THE PLAN

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PRINT NAME: JORGE R. BERNAL DELGADO DATE: 02-28-23

SIGNATURE: *[Signature]* LICENSE NO. 57216

DRAWN BY ___TMV___ DATE 09/22/22

DESIGN BY ___TMV___ DATE 09/22/22

CHECKED BY ___JRB___ DATE 10/24/22



**ANOKA COUNTY
HIGHWAY DEPT.**

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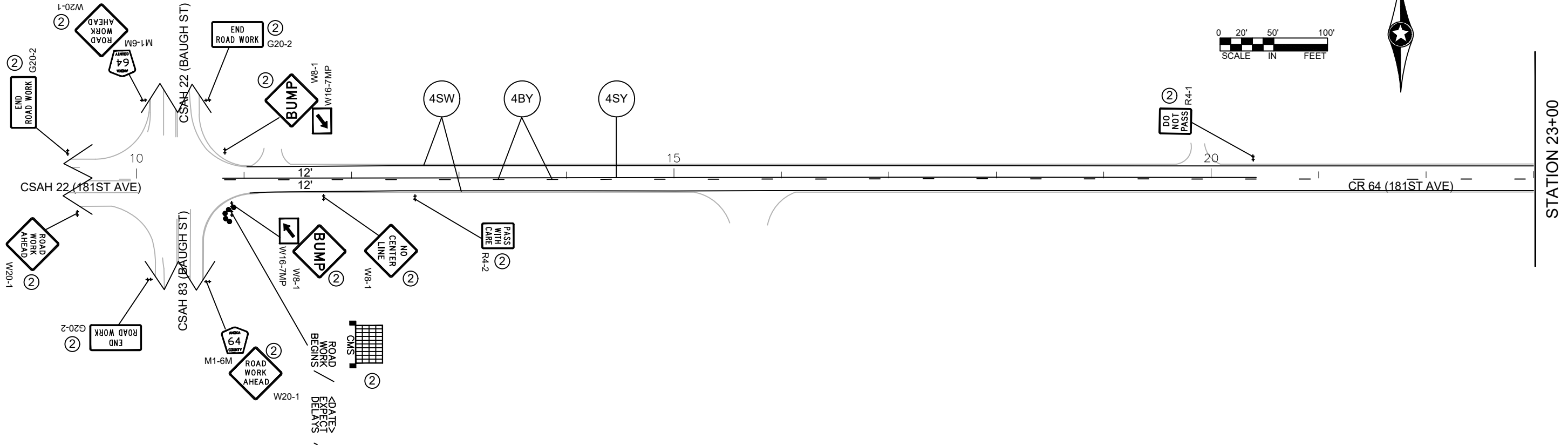
PERMANENT PAVEMENT MARKING PLAN DETAILS

SHEET 12A OF 18 SHEETS

SIGN NOTES:
 ② TEMPORARY TRAFFIC CONTROL SIGN

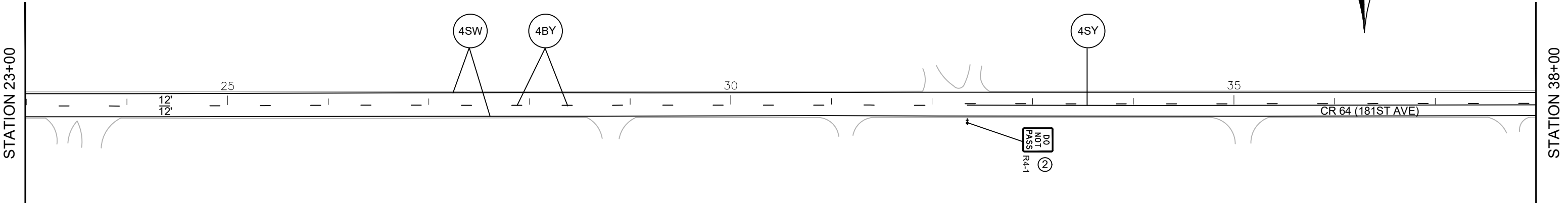
STRIPING KEY

--- CIRCLE - MULTI COMP GROUND IN



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1	02/27/23	TMV	JRB	02/28/23	
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DRAWN BY TMV DATE 06/27/22

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ANOKA COUNTY
 HIGHWAY DEPT.

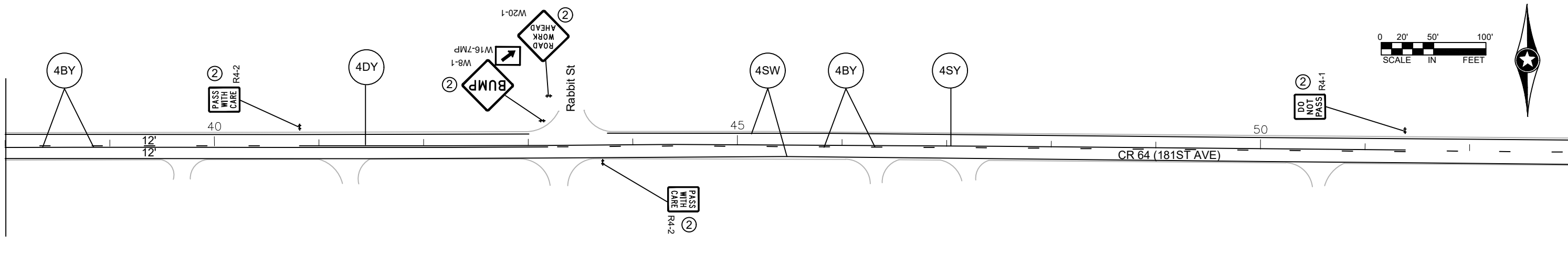
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TEMPORARY SIGNING
 &
 PERMANENT STRIPING

SHEET 13A OF 18 SHEETS

STATION 38+00

STATION 53+00



NOTES:

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STRIPING KEY

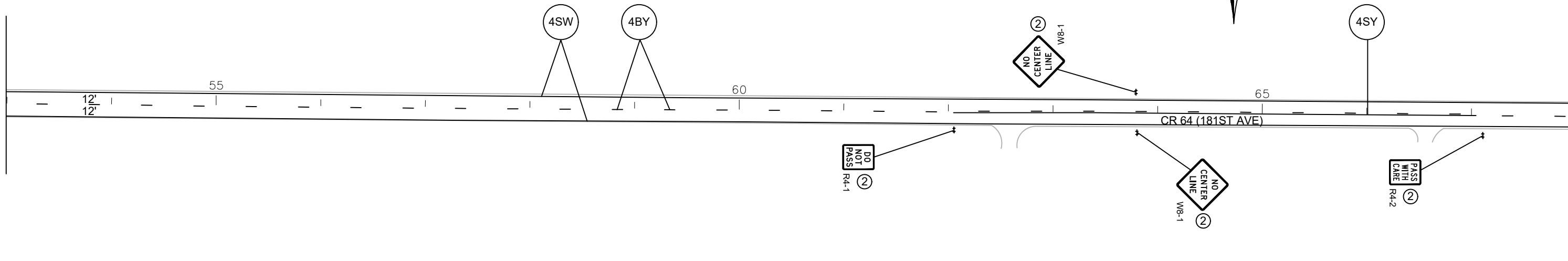
--- CIRCLE - MULTI COMP GROUND IN

SIGN NOTES:

② TEMPORARY TRAFFIC CONTROL SIGN

STATION 53+00

STATION 68+00



1	02/27/23	TMV	JRB	02/28/23	
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SIGNATURE: *[Signature]* LICENSE NO. 57216

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ANOKA COUNTY
HIGHWAY DEPT.

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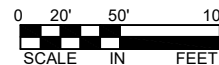
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&
PERMANENT STRIPING

SHEET 14A OF 18 SHEETS

SIGN NOTES:
 ② TEMPORARY TRAFFIC CONTROL SIGN

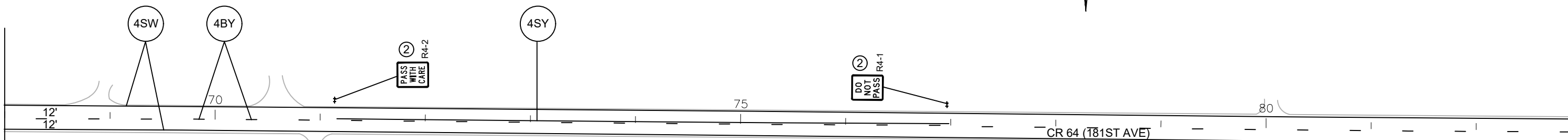
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--- CIRCLE - MULTI COMP GROUND IN



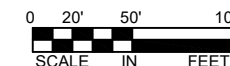
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STATION 83+00



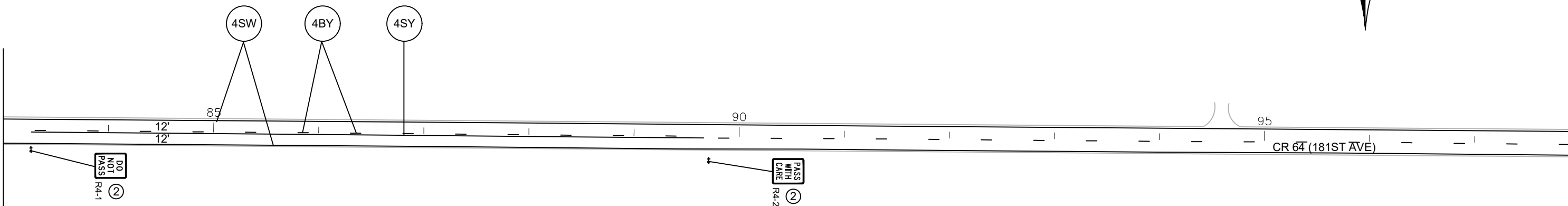
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STATION 83+00

STATION 98+00



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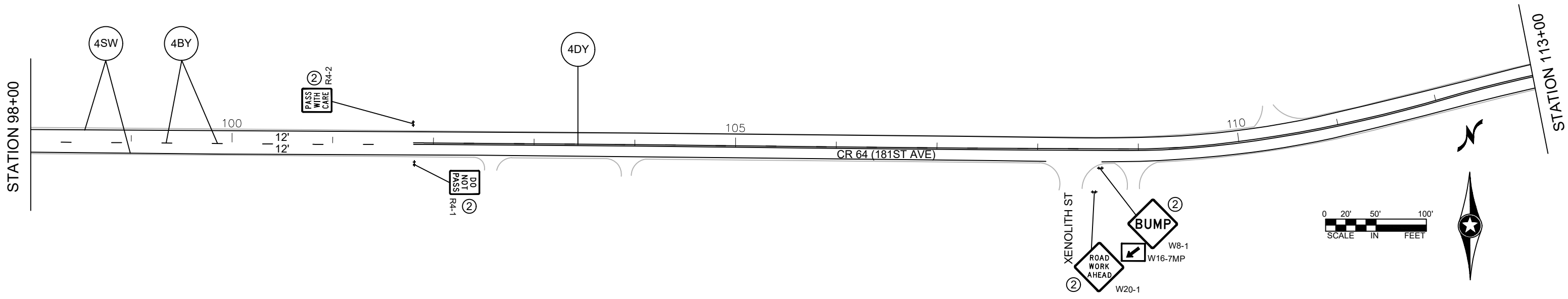


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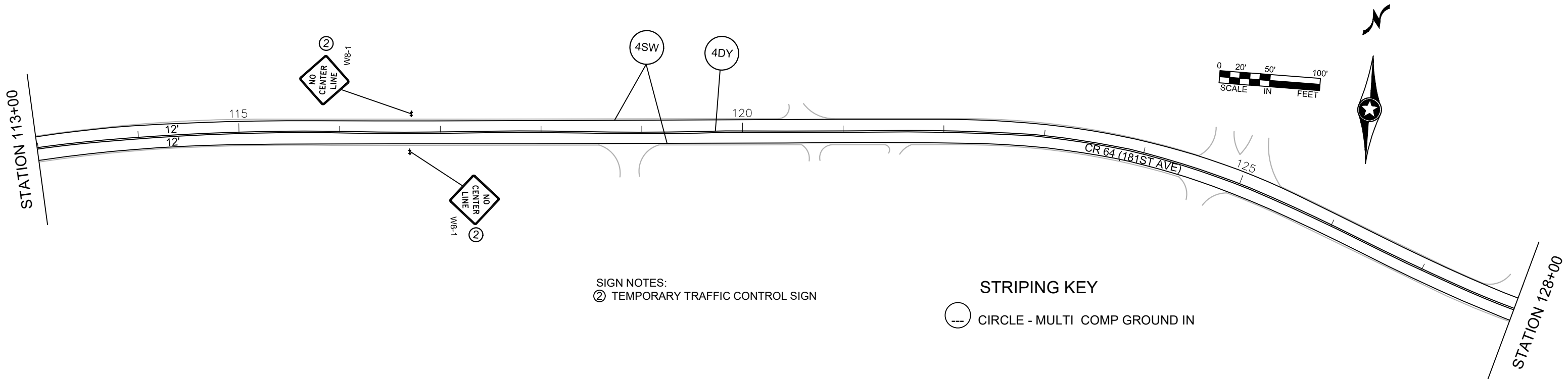
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TEMPORARY SIGNING
 &
 PERMANENT STRIPING

SHEET 15A OF 18 SHEETS



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SIGN NOTES:
 ② TEMPORARY TRAFFIC CONTROL SIGN

STRIPING KEY
 ○ CIRCLE - MULTI COMP GROUND IN

1	02/27/23	TMV	JRB	02/28/23	
NO	DATE	BY	CKD	APPR	REVISION
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ANOKA COUNTY
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CP 21-36-64

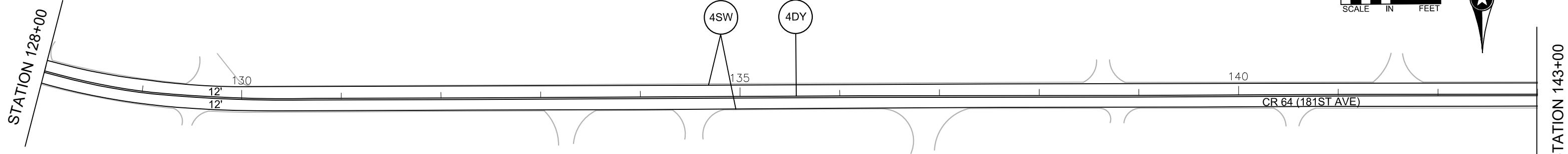
TEMPORARY SIGNING
 &
 PERMANENT STRIPING

SHEET 16A OF 18 SHEETS

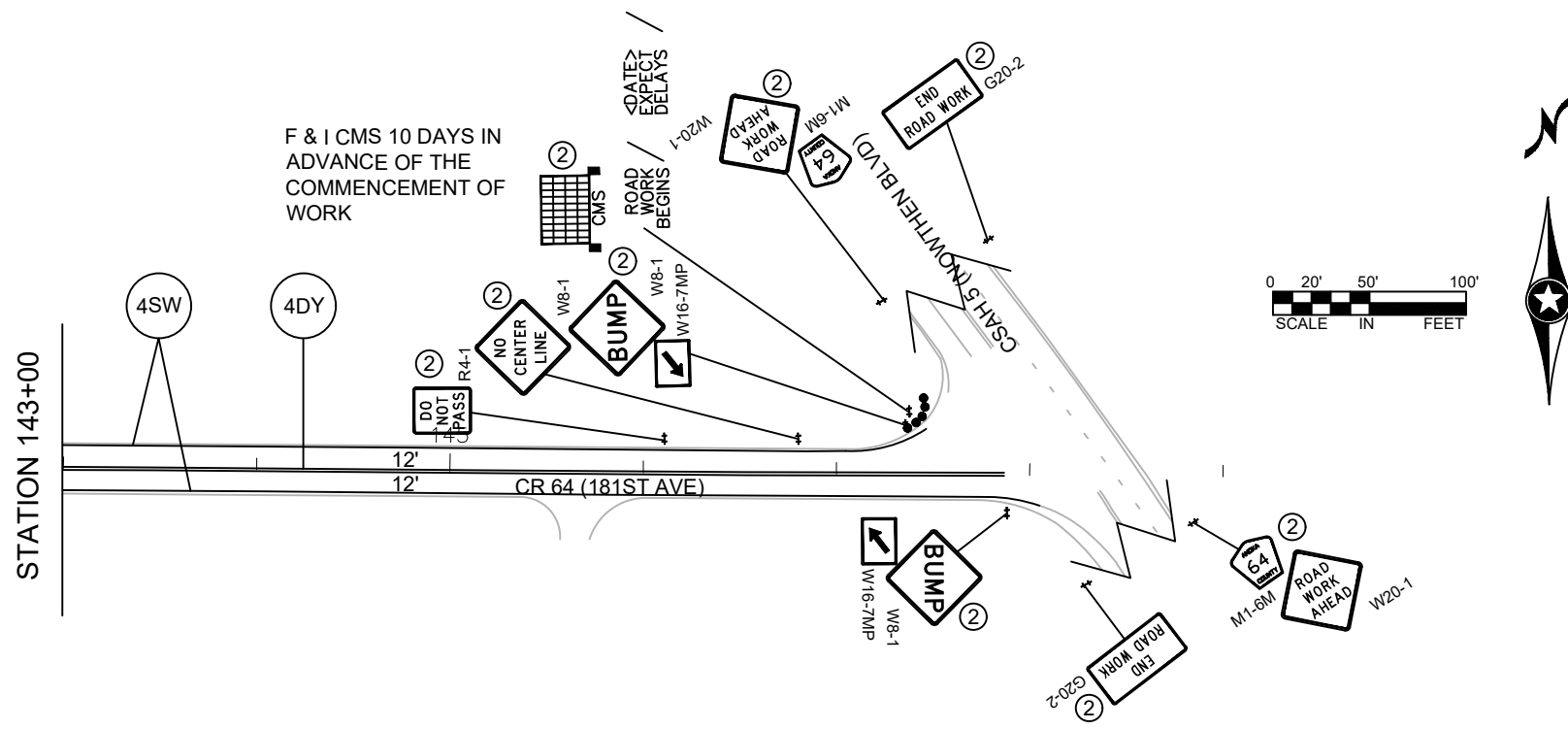
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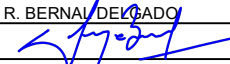
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 - ALL PERMANENT STRIPING AND PAVEMENT MESSAGES SHALL BE PLACED WITHIN 72 HOURS OF MAINLINE PAVING.
 - ANY REQUIRED PERMANENT SIGNING SHALL BE INSTALLED THE SAME DAY AS PERMANENT STRIPING.
 - ALL EXISTING SIGNING SHALL REMAIN IN PLACE DURING CONSTRUCTION. ANY SALVAGED AND REINSTALLED SIGNS SHALL BE INSTALLED ON TEMPORARY SUPPORTS UNTIL THE PERMANENT INSTALLATION CAN BE MADE. THIS WILL BE CONSIDERED AS INCIDENTAL TO INSTALL SIGN TYPE C.



1	02/27/23	TMV	JRB	02/28/23	
NO	DATE	BY	CKD	APPR	REVISION
NAME: P:\22-01-00\CR_64_(Baugh-Nowthen Blvd)\Base\Traffic\Signing & Striping.dwg					

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: JORGE R. BERNAL DELGADO DATE: 02-28-23

SIGNATURE:  LICENSE NO. 57216

DRAWN BY TMV DATE 06/27/22

DESIGN BY TMV DATE 06/27/22

CHECKED BY JRB DATE 10/24/22




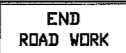
















ANOKA COUNTY
HIGHWAY DEPT.

CP 21-36-64

TEMPORARY SIGNING
 &
 PERMANENT STRIPING

SHEET 17A OF 18 SHEETS

TEMPORARY TRAFFIC CONTROL SIGNS			
M.U.T.C.D. CODE	SIZE	INSERT	QUANTITY
W8-1	48" x 48"		6
R4-1	24" x 30"		8
R4-2	24" x 30"		7
G20-2	36" x 18"		5
W8-1	48" x 48"		6
W16-7P	30" x 18"		6
W3-4	48" x 48"		AS NEEDED
W8-1	48" x 48"		AS NEEDED
W8-8	48" x 48"		AS NEEDED
W8-9	48" x 48"		AS NEEDED

TEMPORARY TRAFFIC CONTROL SIGNS			
M.U.T.C.D. CODE	SIZE	INSERT	QUANTITY
W8-11	48" x 48"		AS NEEDED
W8-23	48" x 48"		AS NEEDED
M1-6M	24" x 24"		4
W20-1	48" x 48"		AS NEEDED (ESTIMATED 6)
W20-4	48" x 48"		AS NEEDED
W20-7	48" x 48"		AS NEEDED
REFLECTORIZED REBOUNDABLE DRUM			AS NEEDED (ESTIMATED 10)
CMS sign to be placed a minimum of ten days prior to actual commencement of road work. Signs to be removed when road work begins.			2 AT 10 DAYS EA

CHANGEABLE MESSAGE BOARD - MESSAGE SEQUENCE LAYOUT

		R	O	A	D		
		W	O	R	K		
		B	E	G	I	N	S

	<	D	A	T	E	>	
	E	X	P	E	C	T	
	D	E	L	A	Y	S	

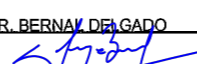
CMS SIGN TO BE PLACED A MINIMUM OF TEN DAYS PRIOR TO ACTUAL COMMENCEMENT OF ROAD WORK. SIGNS TO BE REMOVED WHEN ROAD WORK BEGINS.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\22-01-00\CR_64_(Baugh-Nowthen Blvd)\Base\Traffic\Signing & Striping.dwg

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: JORGE R. BERNAL DELGADO DATE: 11-8-22

SIGNATURE:  LICENSE NO. 57216

DRAWN BY: TMV DATE: 06/27/22

DESIGN BY: TMV DATE: 06/27/22

CHECKED BY: JRB DATE: 10/24/22



ANOKA COUNTY
HIGHWAY DEPT.

CP 21-36-64

TEMPORARY SIGNING
&
PERMANENT STRIPING
SHEET 18 OF 18 SHEETS