

MINNESOTA DEPARTMENT OF TRANSPORTATION

ANOKA COUNTY

CONSTRUCTION PLAN FOR _____ BITUMINOUS RECLAMATION, AND BITUMINOUS SURFACING
 LOCATED ON CR 75 BETWEEN CSAH 22 (VIKING BLVD.) AND CR 77 (RYAN LAKE DR. NE)

GOVERNING SPECIFICATIONS

THE 2020 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN.
 ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING THE LATEST FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS.

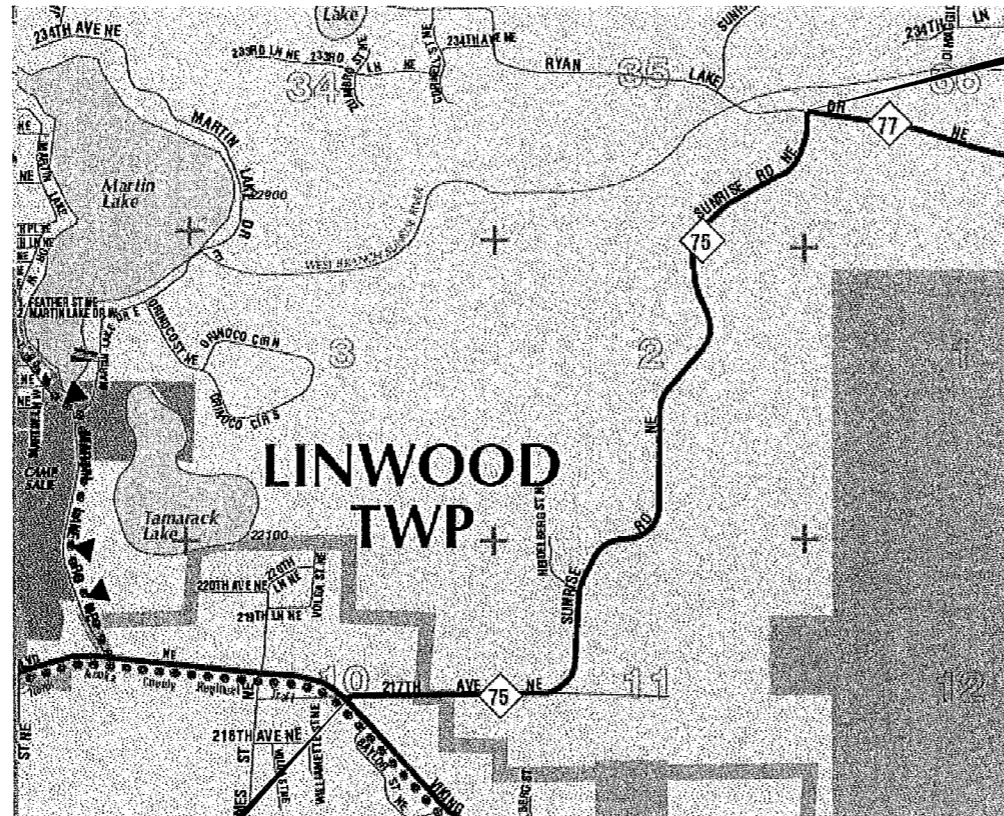
THIS PLAN CONTAINS 20 SHEETS

INDEX

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	STATEMENT OF ESTIMATED QUANTITIES
3	TYPICAL SECTIONS
4-5	DETAILS
6-7	SUPERELEVATION TABULATIONS
8-13	CONSTRUCTION PLAN
14	PERMANENT PAVEMENT MARKING GUIDELINES AND TABS
15-20	PERMANENT STRIPING

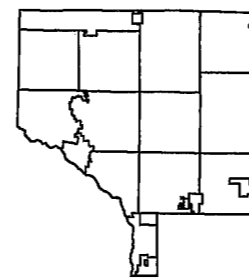
GROSS LENGTH 15455 FEET 2.927 MILES
 EXCEPTIONS-LENGTH 0.00 FEET 0.000 MILES
 NET LENGTH 15455 FEET 2.927 MILES

END CP 24-19-75
 CR 75, STA: 165+05.00



BEGIN CP 24-19-75
 CR 75, STA: 10+50.00

PROJECT LOCATION



TOWNSHIP OF LINWOOD
 ANOKA COUNTY
 MN/DOT TRANSPORTATION DISTRICT - METRO
 SECTIONS 35, 36, 2, 10, 11
 TOWNSHIPS 34, 33 NORTH
 RANGE 22 WEST

Approved 4-24, 2024
 ANOKA COUNTY ENGINEER

DESIGN DESIGNATION (CR 75)

ESAL 20	<u>91,202</u>	FUNCTIONAL CLASSIFICATION	<u>MINOR ARTERIAL</u>
R VALUE	<u>70</u>	NO. OF TRAFFIC LANES	<u>2</u> NO. OF PARKING LANES <u>0</u>
ADT (2022)	<u>772</u>	DESIGN SPEED	<u>50</u> MPH
PROJ. ADT (2024)	<u>772</u>	STOPPING SIGHT DISTANCE BASED ON:	
PROJ. HCADT (2024)	<u>46</u>	HEIGHT OF EYE	<u>3.5'</u> HEIGHT OF OBJECT <u>2.0'</u>
SOIL FACTOR	<u>N/A</u>	DESIGN SPEED NOT ACHIEVED AT :	
	<u>10</u> TON DESIGN	STA. _____ TO STA. _____	MPH _____

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: GERALD J. AUGER JR.
 SIGNATURE: 04-01-2024 26511

DRAWN BY DLD DATE 02/06/2024
 DESIGN BY DLD DATE 02/06/2024
 CHECKED BY CSO DATE 03/05/2024



**ANOKA COUNTY
 HIGHWAY DEPT.**

COUNTY PROJECT 24-19-75

TITLE SHEET

Sheet 1 of 20 Sheets

STATEMENT OF ESTIMATED QUANTITIES

Notes	Item Number	Code	ITEM DESCRIPTION	Unit	TOTAL PROJECT QUANTITIES ESTIMATED
	2021.501	00010	MOBILIZATION	LUMP SUM	1
1	2104.503	00195	SAWING CONCRETE PAVEMENT (FULL DEPTH)	LIN FT	30
1	2104.503	00205	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)	LIN FT	294
2	2104.504	00080	REMOVE CONCRETE DRIVEWAY PAVEMENT	SQ YD	103
3	2104.504	00110	REMOVE BITUMINOUS DRIVEWAY PAVEMENT	SQ YD	482
1	2104.504	00120	REMOVE BITUMINOUS PAVEMENT	SQ YD	495
4	2123.510	00020	MOTOR GRADER	HOUR	24
5	2130.523	00010	WATER	M GALLON	20
6	2211.509	00070	AGGREGATE BASE CLASS 5	TON	847
7	2215.504	00010	FULL DEPTH RECLAMATION	SQ YD	49177
8	2215.507	00010	HAUL FULL DEPTH RECLAMATION (LV)	CU YD	74
9	2221.509	00080	SHOULDER BASE AGGREGATE CLASS 5	TON	2473
10	2232.604	00470	MILL BITUMINOUS PAVEMENT (SPECIAL)	SQ YD	285
	2357.506	00010	BITUMINOUS MATERIAL FOR TACK COAT	GALLON	2512
11,12	2360.509	13200	TYPE SP 9.5 WEARING COURSE MIXTURE (3;B)	TON	59
12	2360.509	24300	TYPE SP 12.5 WEARING COURSE MIXTURE (4;C)	TON	226
13	2360.509	24300	TYPE SP 12.5 WEARING COURSE MIXTURE (4;C)	TON	11553
	2531.504	00060	6" CONCRETE DRIVEWAY PAVEMENT	SQ YD	103
14	2540.602	00150	MAIL BOX SUPPORT	EACH	40
	2563.601	00001	TRAFFIC CONTROL SUPERVISOR	LUMP SUM	1
15,16	2563.601	00010	TRAFFIC CONTROL	LUMP SUM	1
17	2563.613	01100	PORTABLE CHANGEABLE MESSAGE SIGN	UNIT DAY	20
	2573.501	00030	EROSION CONTROL SUPERVISOR	LUMP SUM	1
	2574.507	00100	COMMON TOPSOIL BORROW	CU YD	2405
18	2575.508	40003	HYDRAULIC REINFORCED FIBER MATRIX	POUND	546
19	2581.503	00010	REMOVABLE PREFORMED PAVEMENT MARKING TAPE	LIN FT	618
20	2582.503	30104	4" SOLID LINE MULTI-COMPONENT	LIN FT	33740
20	2582.503	30204	4" BROKEN LINE MULTI-COMPONENT	LIN FT	1120
20	2582.503	30404	4" DOUBLE SOLID LINE MULTI-COMPONENT	LIN FT	9717

CONSTRUCTION NOTES

1	REFERENCE DETAILS (PAGE 4-5) FOR REMOVAL DETAILS
2	ITEM FOR CONCRETE DRIVEWAYS. CONTRACTOR IS RESPONSIBLE FOR CONTACTING PROPERTY OWNER 48 HOURS BEFORE STARTING OPERATION.
3	ITEM FOR BITUMINOUS DRIVEWAYS AND STREET APPROACHES. CONTRACTOR IS RESPONSIBLE FOR CONTACTING PROPERTY OWNER 48 HOURS BEFORE STARTING OPERATION.
4	ITEM USED TO MOVE EXCESS RECLAIM MATERIAL AT THE RECLAIM AREA LIMITS TO CREATE A SMOOTH TRANSITION BETWEEN THE PROPOSED AND EXISTING PAVEMENT AND FOR SUPER ELEVATION/GRADE CORRECTION AREAS.
5	WATER TO BE USE FOR DUST CONTROL AS DIRECTED BY THE ENGINEER.
6	ITEM FOR SUPERELEVATION / GRADE CORRECTION.
7	THIS WORK INCLUDES SPREADING, WATERING, COMPACTING, SHAPING, AND MAINTAINING THE BLENDED RECLAIMED MATERIAL TO THE SPECIFIC PROFILE AND CROSS SECTION.
8	ITEM USED TO HAUL EXCESS RECLAIM FROM TIE-IN POINTS AND REUSED ON SITE.
9	ITEM INCLUDES 7 TONS FOR EACH GRAVEL ENTRANCE AND GRAVEL STREET APPROACH.
10	TO BE USED FOR MILLING STREET APPROACHES AND/OR DETAIL MILLING AREAS AS IDENTIFIED IN THE PLAN.
11	ITEM FOR BITUMINOUS DRIVEWAYS. DRIVEWAYS SHALL BE PAVED AFTER MAINLINE AND BEFORE FINAL STRIPING.
12	CONTRACTOR IS RESPONSIBLE FOR CONTACTING PROPERTY OWNER 48 HOURS BEFORE STARTING OPERATION.
13	STREET APPROACHES SHALL BE PAVED AFTER MAINLINE, AND BEFORE FINAL STRIPING
14	MAILBOXES ARE TO BE INSTALLED AT THE EXISTING MAILBOX LOCATION OR AS DIRECTED BY THE LOCAL POSTAL AUTHORITY, CONTRACTOR IS RESPONSIBLE FOR CONTACTING. MAILBOX REMOVAL AND ALL MATERIALS ARE INCIDENTAL TO INSTALLATION.
15	CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN TEMPORARY SIGNAGE WHENEVER EXISTING SIGNAGE IS REMOVED. TEMPORARY SIGNAGE SHALL BE INCIDENTAL TO TRAFFIC CONTROL.
16	ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO, AND BE INSTALLED IN ACCORDANCE WITH, THE MOST CURRENT REVISION OF THE "MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES". "DO NOT PASS, PASS WITH CARE, NO CENTER STRIPE, AND STOP HERE ON RED SIGNS SHALL BE INPLACE WHENEVER PERMANENT PAVEMENT MARKINGS ARE NOT PRESENT.
17	2 MESSAGE BOARDS, ONE ON THE EACH END OF PROJECT, SHALL BE INSTALLED 10 DAYS PRIOR TO ANY CONSTRUCTION; REFERENCE STRIPING PLAN FOR DETAILS.
18	TYPE 3 FERTILIZER AND TYPE 25-121 SEED ARE INCIDENTAL TO THIS ITEM. SEE "BASIS OF PLANNED QUANTITIES" FOR APPLICATION RATES.
19	CENTERLINE AND LANE DESIGNATION SKIPS TO BE APPLIED AS SOON AS POSSIBLE ON MILLED SURFACE AND EACH NEW LIFT OF PAVEMENT; SKIPS MUST BE IN PLACE BEFORE THE CONTRACTOR LEAVES FOR THE DAY. CONTRACTOR IS TO REMOVE PRIOR TO FINAL STRIPING.
20	FINAL STRIPING SHALL BE INSTALLED WITHIN 72 HOURS OF COMPLETION OF MAINLINE WEAR COURSE PAVING. CANNOT BE INSTALLED SOONER THAN 48 HOURS.

THE FOLLOWING STANDARD PLATES, APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION, SHALL APPLY ON THIS PROJECT

MNDOT STANDARD PLATES

PLATE NO.	DESCRIPTION
8000K	TEMPORARY CHANNELIZERS (3 SHEETS)
9350B	MAILBOX SUPPORT - SWING-AWAY TYPE

BASIS OF PLANNED QUANTITIES

2357	BITUMINOUS MATERIAL FOR TACK COAT	0.05 GAL / SQ YD
2211	AGGREGATE BASE CLASS 5	1.8 TONS / CU YD
2360	ALL BITUMINOUS PAVEMENT	115 LBS / SQ YD / IN THICKNESS
2581	REMOVABLE PREFORM PAVEMENT MARKING TAPE	2' AT 50' INTERVALS
2575	SEED MIXTURE 25-121	61 LBS./ ACRE
2574	FERTILIZER TYPE 3	350 LBS./ ACRE
2575	HYDRAULIC REINFORCED FIBER MATRIX	3900 LBS./ ACRE

BITUMINOUS STREET SUMMARY

LOCATION	BITUMINOUS	
	2360 TYPE SP 12.5 WEAR (4,C)	NOTES
	TON	
HEIDELBERG ST	42	[1]
217TH AVE	66	[1]
PROJECT TOTAL	216	

BITUMINOUS SUMMARY NOTES:

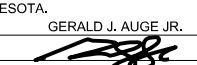
[1] QUANTITY ESTIMATED FOR 2 LIFTS

NO	DATE	BY	CKD	APPR	REVISION	
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NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_SEQ.dgn

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ANOKA COUNTY
HIGHWAY DEPT.

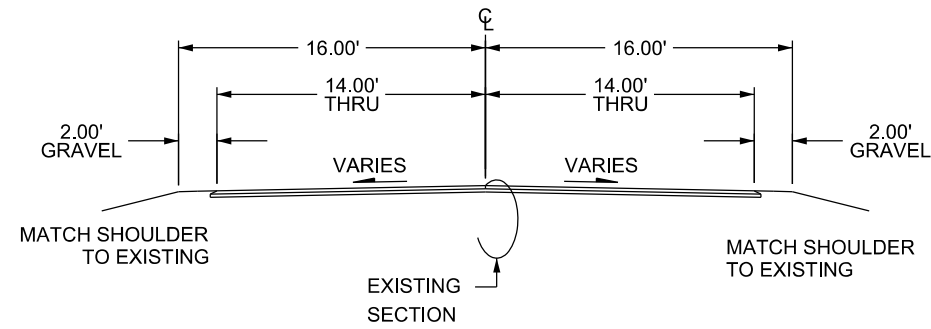
COUNTY PROJECT 24-19-75

STATEMENT OF ESTIMATED QUANTITIES

Sheet 2 of 20 Sheets

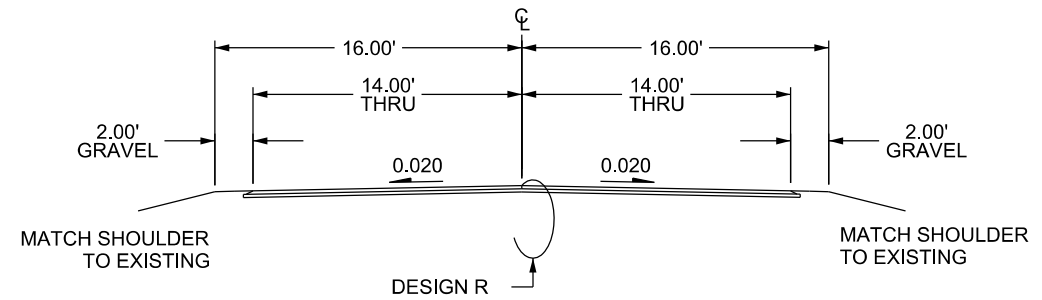
CR 75 - 217TH AVE. / SUNRISE RD. (EXISTING SECTION)

10+50.00 - 165+05.00



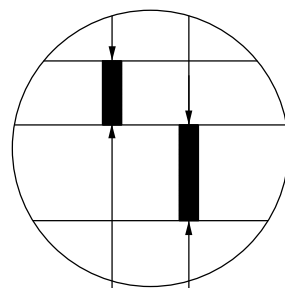
CR 75 - 217TH AVE. / SUNRISE RD. (PROPOSED) SECTION

10+50.00 - 165+05.00



* BUMP OUT 1.0' ON LOW SIDE OF ALL SUPERS *

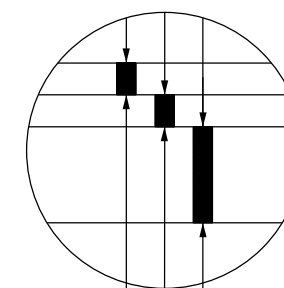
EXISTING SECTION



4" EXISTING BITUMINOUS

EXISTING GRAVEL BASE

DESIGN R RECLAIM SECTION



2.0" BITUMINOUS WEAR(SPWEB440C)

2.0" BITUMINOUS WEAR(SPWEB440C)

RECLAIMED BITUMINOUS

NO	DATE	BY	CKD	APPR	REVISION	TIME
	04/23/2024					2:12:51 PM

NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_TYPICALS.dgn

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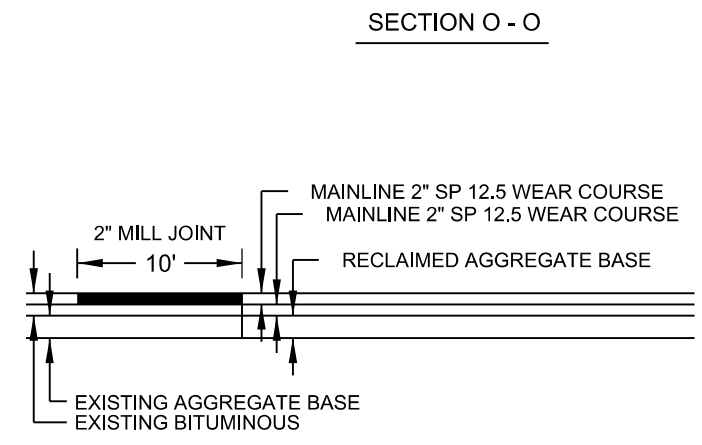
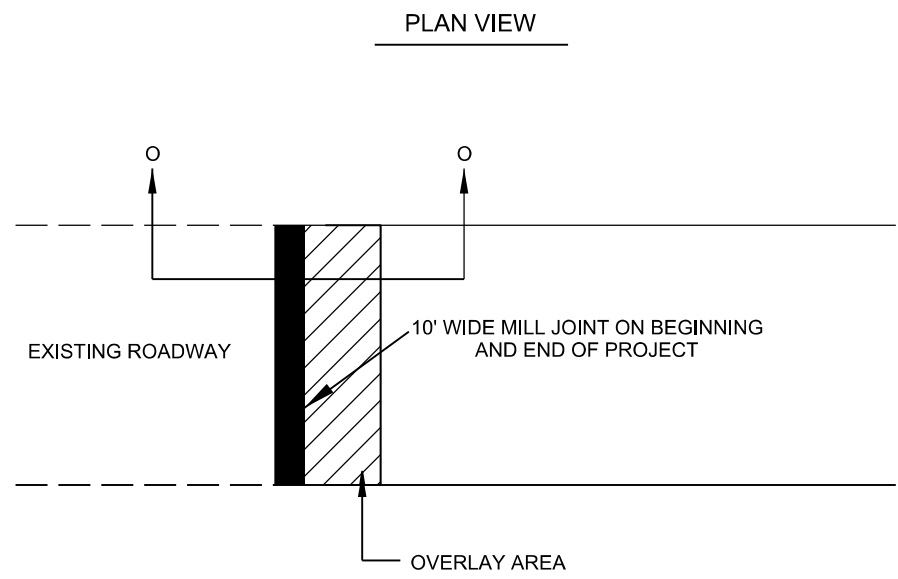
**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 24-19-75

TYPICAL SECTIONS

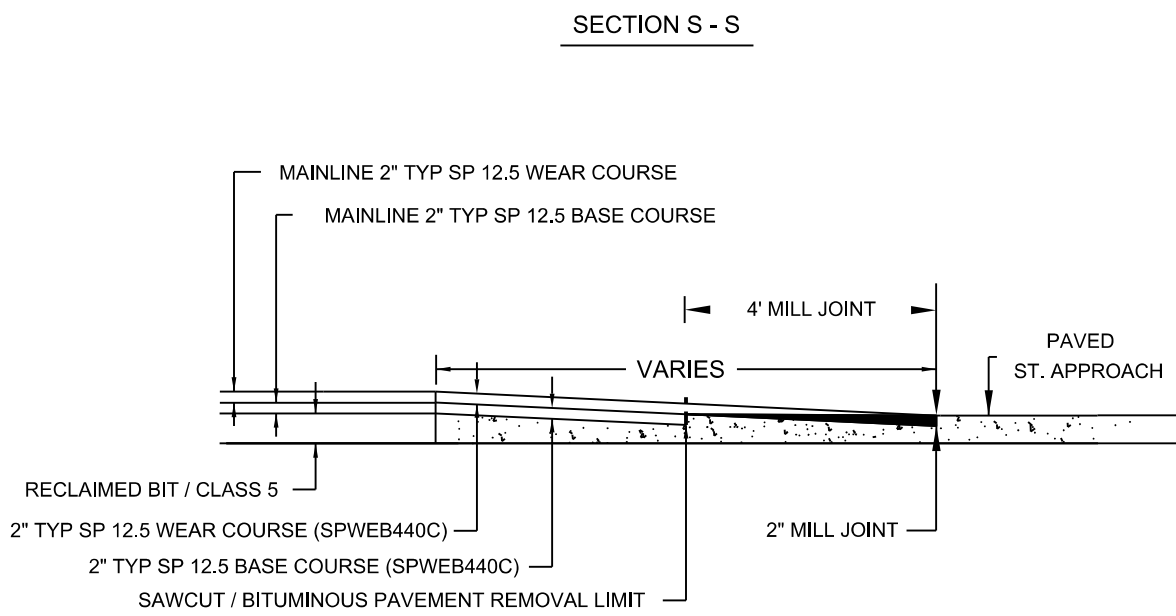
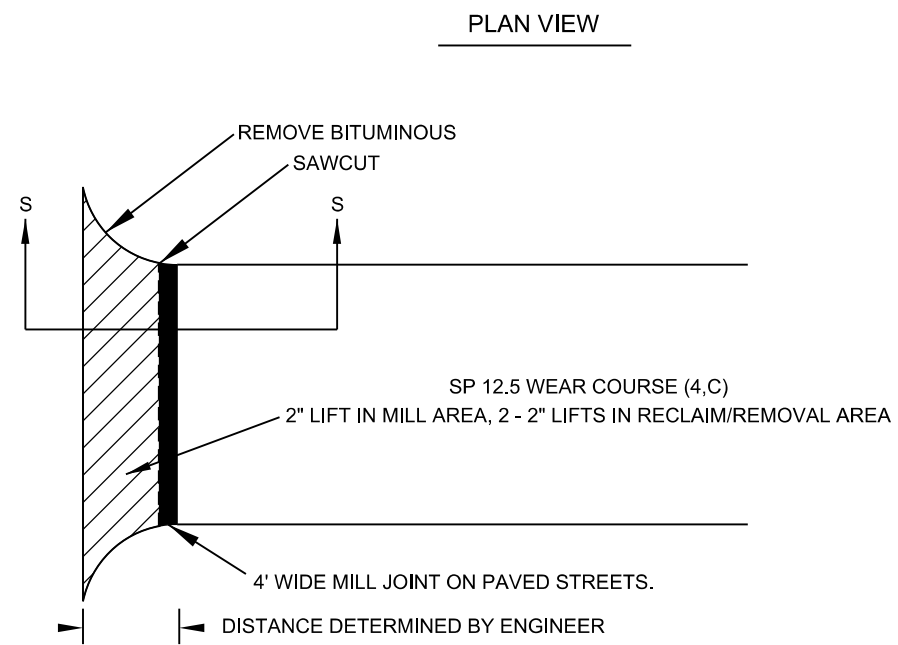
Sheet 3 of 20 Sheets

MAINLINE JOINT DETAIL (RECLAIM)



STREET APPROACH DETAIL (RECLAIM)

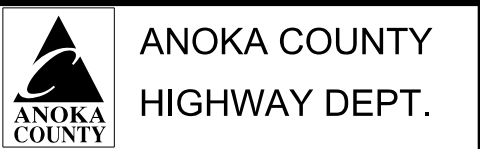
BITUMINOUS STREET



NO	DATE	BY	CKD	APPR	REVISION	04/23/2024	2:12:51 PM
NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_DETAILS.dgn							

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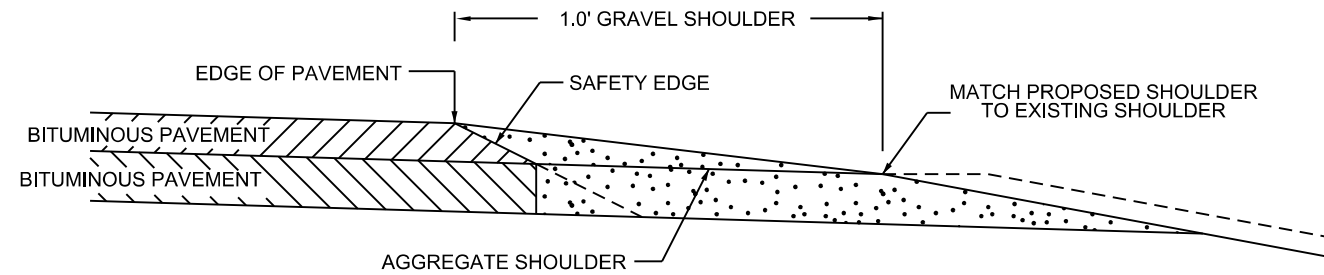


COUNTY PROJECT 24-19-75

DETAILS
 Sheet 4 of 20 Sheets

SHOULDER DETAIL

BITUMINOUS SAFETY EDGE
GRAVEL SHOULDER



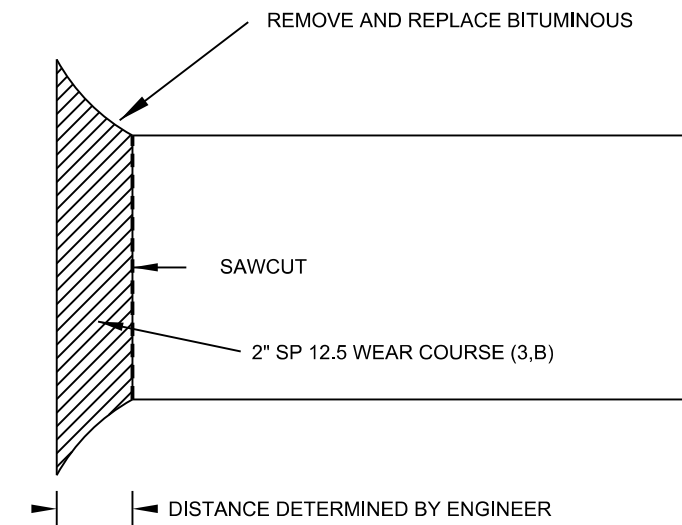
SAFETY EDGE TO BE USED IN ALL NON-CURB AREAS ON SHOULDER.

OPTIONAL DESIGN EXTENDS SAFETY EDGE DEEPER THAN 6" AND WIDER THAN 10.5". SEE SPECIAL PROVISIONS.

RECLAIM AREA - DRIVEWAY DETAIL

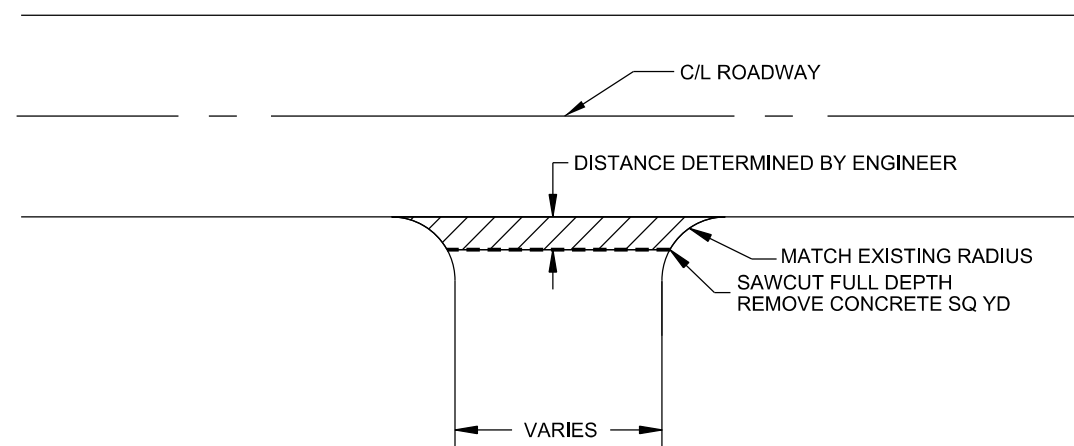
BITUMINOUS

PLAN VIEW



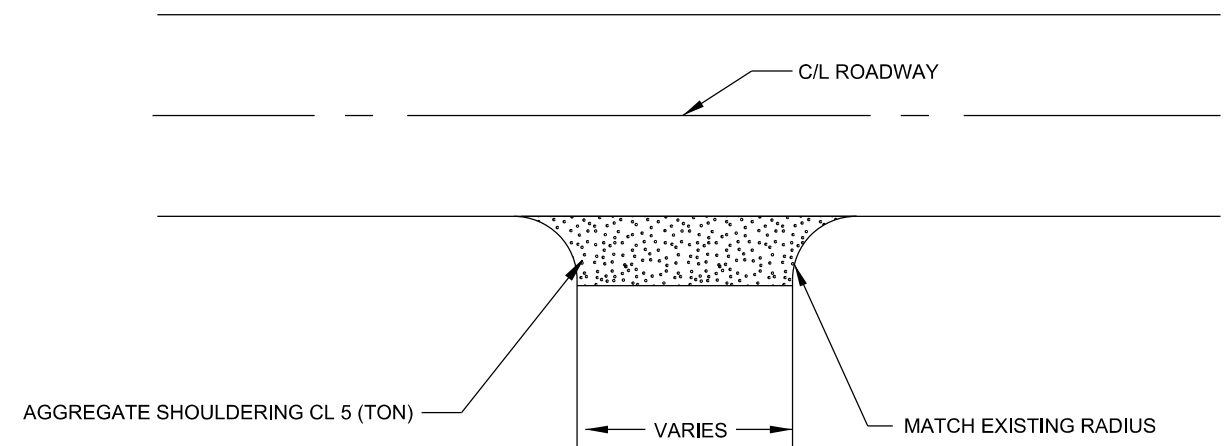
DRIVEWAY DETAIL

CONCRETE DRIVEWAY



DRIVEWAY DETAIL

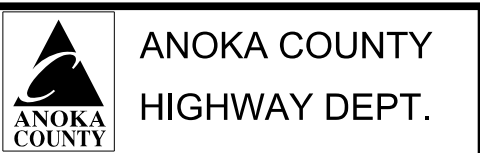
GRAVEL / FIELD ENTRANCE



NO	DATE	BY	CKD	APPR	REVISION	04/23/2024	2:12:52 PM
NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_DETAILS.dgn							

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COUNTY PROJECT 24-19-75

DETAILS
 Sheet 5 of 20 Sheets

DLDiers

SUPERELEVATION CORRECTION TAB FOR CURVE 1			
39+50	BEG TRANS RT	-2.0%	-2.0%
40+00		-2.0%	-1.0%
40+50		-2.0%	0.0%
41+00		-2.0%	1.0%
41+50	BEG TRANS LT	-2.0%	2.0%
42+00		-3.0%	3.0%
42+50		-4.0%	4.0%
43+00		-5.0%	5.0%
43+55	PC / BEG FULL SUPER	-6.0%	6.0%
44+00		-6.0%	6.0%
44+50		-6.0%	6.0%
45+00		-6.0%	6.0%
45+50		-6.0%	6.0%
45+95	PCC	-6.0%	6.0%
46+50		-6.0%	6.0%
47+00		-6.0%	6.0%
47+66	PCC	-6.0%	6.0%
48+00		-6.0%	6.0%
48+50		-6.0%	6.0%
49+00		-6.0%	6.0%
49+50		-6.0%	6.0%
50+00		-6.0%	6.0%
50+50		-6.0%	6.0%
51+00		-6.0%	6.0%
51+50		-6.0%	6.0%
52+00		-6.0%	6.0%
52+39	PT / END FULL SUPER	-6.0%	6.0%
53+00		-5.0%	5.0%
53+50		-4.0%	4.0%
54+00		-3.0%	3.0%
54+50	END TRANS LT	-2.0%	2.0%
55+00		-2.0%	1.0%
55+50		-2.0%	0.0%
56+00		-2.0%	-1.0%
56+50	END TRANS RT	-2.0%	-2.0%
57+00		-2.0%	-2.0%

CURVE 1

SUPERELEVATION CORRECTION TAB CURVE 2			
STATION	POINT	LEFT GRADE	RIGHT GRADE
58+00		-2.0%	-2.0%
58+50	BEG TRANS LT	-2.0%	-2.0%
59+00		-1.0%	-2.0%
59+50		0.0%	-2.0%
60+00		1.0%	-2.0%
60+50	BEG TRANS RT	2.0%	-2.0%
61+00		3.0%	-3.0%
61+50		4.0%	-4.0%
61+82	PC / BEG FULL SUPER	5.0%	-5.0%
62+50		5.0%	-5.0%
63+00		5.0%	-5.0%
63+50		5.0%	-5.0%
64+00		5.0%	-5.0%
64+50		5.0%	-5.0%
65+00		5.0%	-5.0%
65+50		5.0%	-5.0%
66+00		5.0%	-5.0%
66+50		5.0%	-5.0%
67+00		5.0%	-5.0%
67+50		5.0%	-5.0%
68+10	PT	5.0%	-5.0%
68+50		5.0%	-5.0%

CURVE 2

SUPERELEVATION CORRECTION TAB CURVE 3 AND 4			
69+00		5.0%	-5.0%
69+50		5.0%	-5.0%
70+04	PC	5.0%	-5.0%
70+50		5.0%	-5.0%
71+00		5.0%	-5.0%
71+41	PCC	5.0%	-5.0%
72+00		5.0%	-5.0%
72+50		5.0%	-5.0%
73+00		5.0%	-5.0%
73+63	PCC/ END FULL / BEG TRANS	5.0%	-5.0%
74+00		4.3%	-4.3%
74+50		3.3%	-3.3%
75+00		2.4%	-2.4%
75+50		1.4%	-1.4%
76+00		0.4%	-0.4%
76+22		0.0%	0.0%
76+50		-0.6%	0.6%
76+65	PT	-1.0%	1.0%
77+00		-1.8%	1.8%
77+55	PC	-3.1%	3.1%
78+00		-4.1%	4.1%
78+50		-5.3%	5.3%
78+81	PCC / BEG FULL / END TRANS	-6.0%	6.0%
79+50		-6.0%	6.0%
80+00		-6.0%	6.0%
80+50		-6.0%	6.0%
81+00		-6.0%	6.0%
81+50		-6.0%	6.0%
82+00		-6.0%	6.0%
82+50		-6.0%	6.0%
83+00		-6.0%	6.0%
83+50		-6.0%	6.0%
84+00	PCC	-6.0%	6.0%
84+50		-6.0%	6.0%
85+00		-6.0%	6.0%
85+50		-6.0%	6.0%
85+92	PT / END FULL SUPER	-6.0%	6.0%
86+50		-5.0%	5.0%
87+00		-4.0%	4.0%
87+50		-3.0%	3.0%
88+00	END TRANS LT	-2.0%	2.0%
88+50		-2.0%	1.0%
89+00		-2.0%	0.0%
89+50		-2.0%	-1.0%
90+00	END TRANS RT	-2.0%	-2.0%
90+50		-2.0%	-2.0%
91+00		-2.0%	-2.0%
91+50		-2.0%	-2.0%
92+00		-2.0%	-2.0%
92+50		-2.0%	-2.0%

CURVE 3

CURVE 4

SUPERELEVATION CORRECTION TAB CURVE 5 AND 6			
STATION	POINT	LEFT GRADE	RIGHT GRADE
97+00		-2.0%	-2.0%
97+50		-2.0%	-2.0%
98+00	BEG TRANS LT	-2.0%	-2.0%
98+50		-1.0%	-2.0%
99+00		0.0%	-2.0%
99+50		1.0%	-2.0%
100+00	BEG TRANS RT	2.0%	-2.0%
100+50		3.0%	-3.0%
101+00		4.0%	-4.0%
101+33	PC / BEG FULL SUPER	5.0%	-5.0%
102+00		5.0%	-5.0%
102+69	PCC	5.0%	-5.0%
103+00		5.0%	-5.0%
103+50		5.0%	-5.0%
104+00		5.0%	-5.0%
104+50		5.0%	-5.0%
105+00		5.0%	-5.0%
105+63	PCC/ TRANS TO	5.0%	-5.0%
106+00		5.0%	-5.0%
106+50		5.0%	-5.0%
106+94	PT / END FULL SUPER	5.0%	-5.0%
107+50		4.0%	-4.0%
108+00		3.0%	-3.0%
108+50	END TRANS RT	2.0%	-2.0%
109+00		1.0%	-2.0%
109+50		0.0%	-2.0%
110+00		-1.0%	-2.0%
110+50	END TRANS LT	-2.0%	-2.0%
111+00	BEG TRANS RT	-2.0%	-2.0%
111+50		-2.0%	-1.0%
112+00		-2.0%	0.0%
112+50		-2.0%	1.0%
113+00	BEG TRANS LT	-2.0%	2.0%
113+50		-3.0%	3.0%
114+22	PC	-4.0%	4.0%
114+50		-5.0%	5.0%
115+18	PCC / BEG FULL SUPER	-6.0%	6.0%
115+50		-6.0%	6.0%
116+00		-6.0%	6.0%
116+50		-6.0%	6.0%
117+00		-6.0%	6.0%
117+50		-6.0%	6.0%
118+00		-6.0%	6.0%
118+43	PCC	-6.0%	6.0%
119+00		-6.0%	6.0%
119+50		-6.0%	6.0%
120+00		-6.0%	6.0%
120+50		-6.0%	6.0%
121+00		-6.0%	6.0%
121+33	PT	-6.0%	6.0%
122+00		-3.8%	3.8%
122+50		-2.1%	2.1%
123+00		-0.5%	0.5%

CURVE 5


CURVE 6

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_DETAILS.dgn

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SIGNATURE: 

04-01-2024 26511

DRAWN BY: DLD DATE: 02/06/2024

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CHECKED BY: CSO DATE: 03/05/2024



ANOKA COUNTY
HIGHWAY DEPT.

COUNTY PROJECT 24-19-75

SUPERELEVATION TABULATIONS

Sheet 6 of 20 Sheets

DLDiets

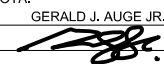
SUPERELEVATION CORRECTION TAB CURVE 7, 8 AND 9			
123+15		0.0%	0.0%
123+50		1.0%	-1.0%
124+00		2.3%	-2.3%
124+50		3.7%	-3.7%
124+96	PC	5.0%	-5.0%
125+50	BEG FULL SUPER	6.0%	-6.0%
126+00		6.0%	-6.0%
126+50		6.0%	-6.0%
127+00		6.0%	-6.0%
127+69	PCC	6.0%	-6.0%
128+00		5.5%	-5.5%
128+50		5.0%	-5.0%
129+00		4.5%	-4.5%
129+50		4.0%	-4.0%
130+00		4.0%	-4.0%
130+50		4.0%	-4.0%
131+00		4.0%	-4.0%
131+50		4.0%	-4.0%
132+15	PCC	4.5%	-4.5%
132+78	PCC BEG FULL SUPER	5.0%	-5.0%
133+00		5.0%	-5.0%
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134+00		5.0%	-5.0%
134+50		5.0%	-5.0%
135+00		5.0%	-5.0%
135+50		5.0%	-5.0%
135+63	PCC / TRANS TO 5%	5.0%	-5.0%
136+00		5.0%	-5.0%
136+50		5.0%	-5.0%
137+00		5.0%	-5.0%
137+33	PT	5.0%	-5.0%
138+00		5.0%	-5.0%
138+50		5.0%	-5.0%
139+00		5.0%	-5.0%
139+50		5.0%	-5.0%
140+06	PC	5.0%	-5.0%
140+50		5.0%	-5.0%
141+00		5.0%	-5.0%
141+50		5.0%	-5.0%
142+00		5.0%	-5.0%
142+50		5.0%	-5.0%
143+00		5.0%	-5.0%
143+50		5.0%	-5.0%
143+75	PT END FULL SUPER	5.0%	-5.0%
144+50		4.0%	-4.0%
145+00		3.0%	-3.0%
145+50	END TRANS RT	2.0%	-2.0%
146+00		1.0%	-2.0%
146+50		0.0%	-2.0%
147+00		-1.0%	-2.0%
147+50	END TRANS LT	-2.0%	-2.0%

SUPERELEVATION CORRECTION TAB CURVE 10 AND 11			
STATION	POINT	LEFT GRADE	RIGHT GRADE
149+50		-2.0%	-2.0%
150+00		-2.0%	-1.0%
150+50		-2.0%	0.0%
151+00		-2.0%	1.0%
151+50		-2.0%	2.0%
152+00		-3.0%	3.0%
152+50		-4.0%	4.0%
153+00		-5.0%	5.0%
153+50	PC / BEG FULL SUPER	-5.0%	5.0%
154+00		-5.0%	5.0%
154+50		-5.0%	5.0%
155+00		-5.0%	5.0%
155+50		-5.0%	5.0%
156+00		-5.0%	5.0%
156+50		-5.0%	5.0%
157+00		-5.0%	5.0%
157+67	PT/ TRANS	-5.0%	5.0%
158+30	PC	-5.0%	5.0%
159+00		-5.0%	5.0%
159+50		-5.0%	5.0%
160+16	PCC / END TRANS	-5.0%	5.0%
160+50		-5.0%	5.0%
161+00		-5.0%	5.0%
161+50		-5.0%	5.0%
162+00		-5.0%	5.0%
162+50		-4.0%	4.0%
162+76	PT	-3.0%	3.0%
163+00		-2.0%	2.0%
163+69	PC	-2.0%	1.0%
164+00		-2.0%	0.0%
164+41	PT	-2.0%	-1.0%
164+50		-2.0%	-2.0%
165+00		-2.0%	-2.0%
165+50		-2.0%	-2.0%

NO	DATE	BY	CKD	APPR	REVISION	04/23/2024	2:12:57 PM
NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_DETAILS.dgn							

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

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SIGNATURE: 

04-01-2024 26511

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CHECKED BY: CSO DATE: 03/05/2024



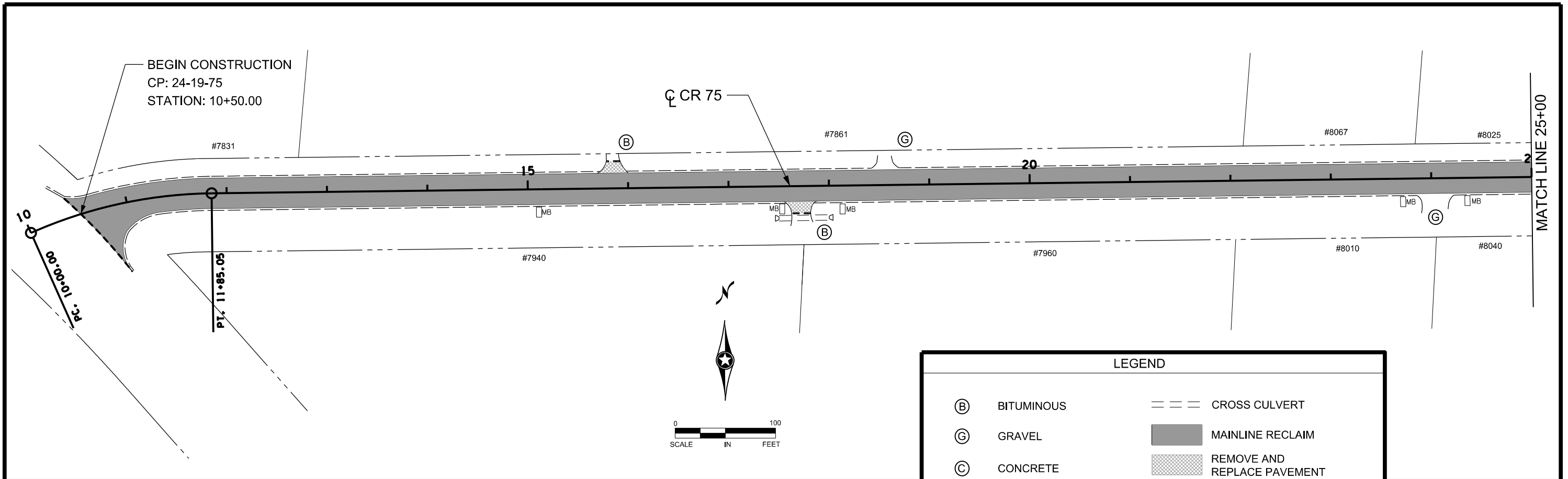
**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 24-19-75

**SUPERELEVATION
TABULATIONS**

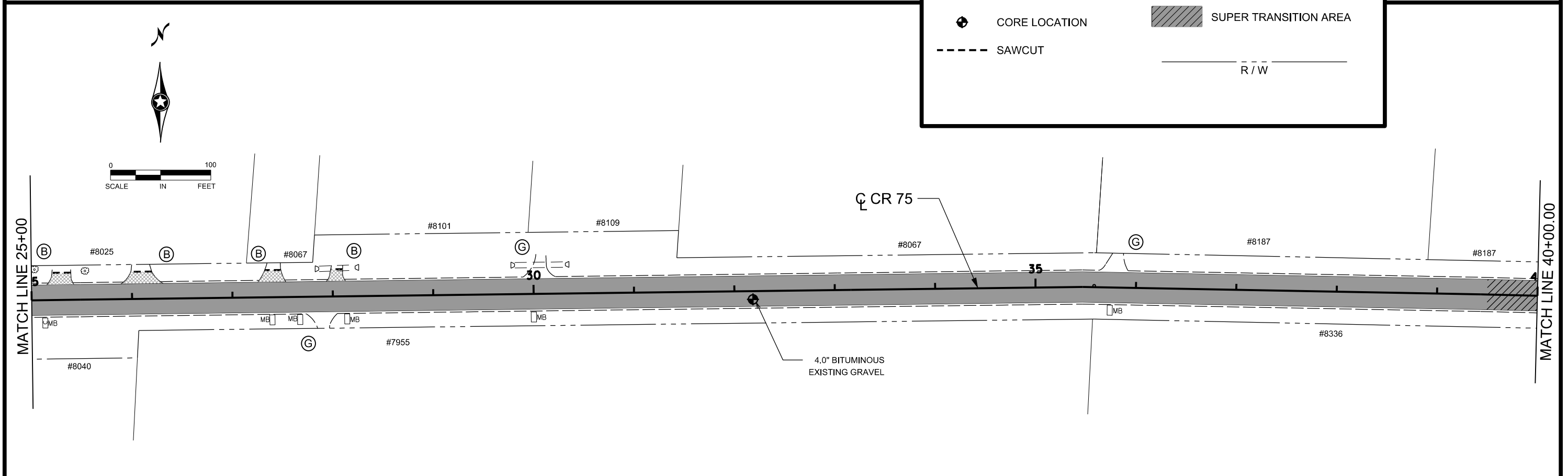
Sheet 7 of 20 Sheets

DLDiers



LEGEND

(B)	BITUMINOUS	---	CROSS CULVERT
(G)	GRAVEL	[Solid Grey]	MAINLINE RECLAIM
(C)	CONCRETE	[Cross-hatched]	REMOVE AND REPLACE PAVEMENT
+	CORE LOCATION	[Diagonal Hatched]	SUPER TRANSITION AREA
---	SAWCUT	---	R/W



NO	DATE	BY	CKD	APPR	REVISION	
	04/23/2024					2:13:01 PM

NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_CP.dgn

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04-01-2024 26511

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CHECKED BY: CSO DATE: 03/05/2024

**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 24-19-75

CONSTRUCTION PLAN

STA 10+50.00 TO 40+00.00

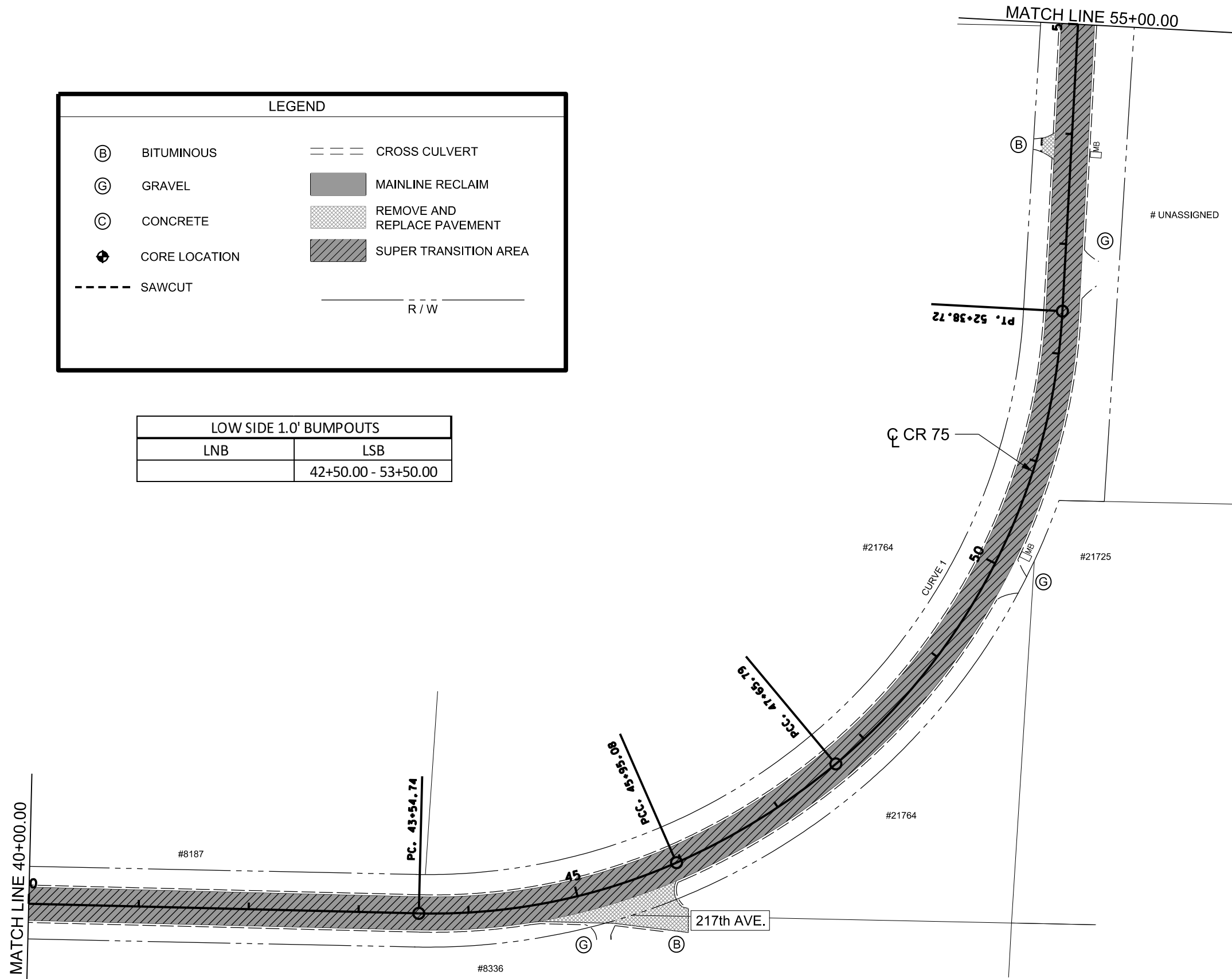
Sheet 8 of 20 Sheets

DLDiers

LEGEND

(B) BITUMINOUS	--- CROSS CULVERT
(G) GRAVEL	█ MAINLINE RECLAIM
(C) CONCRETE	▨ REMOVE AND REPLACE PAVEMENT
⊕ CORE LOCATION	▩ SUPER TRANSITION AREA
- - - SAWCUT	— R / W

LOW SIDE 1.0' BUMPOUTS	
LNB	LSB
	42+50.00 - 53+50.00



NO	DATE	BY	CKD	APPR	REVISION	
	04/23/2024					2:13:01 PM

NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_CP2.dgn

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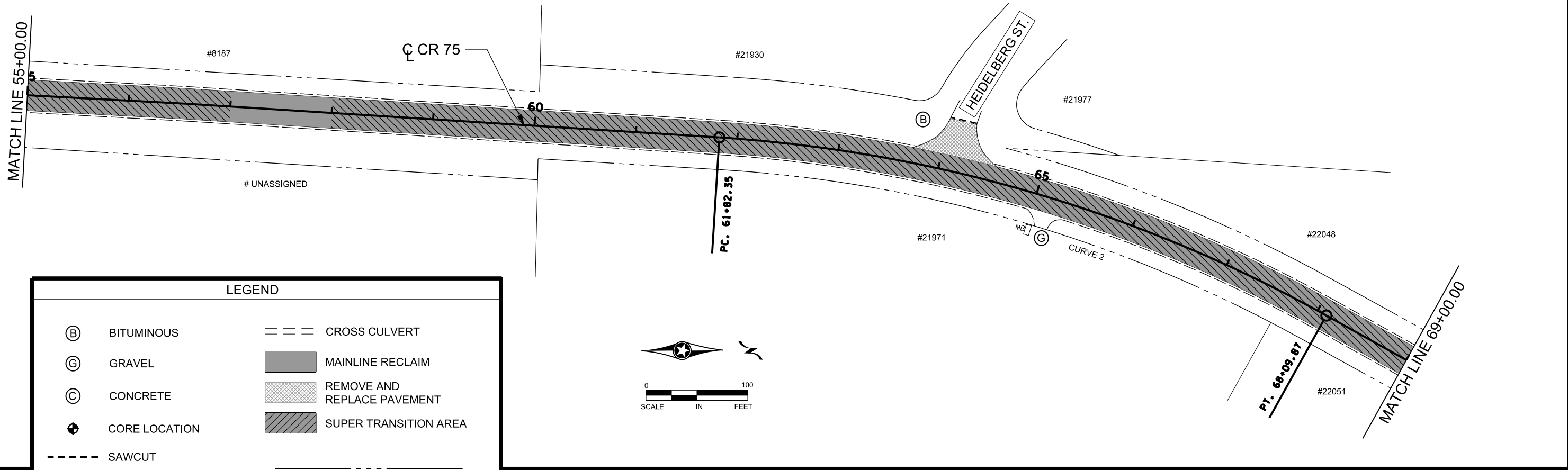
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ANOKA COUNTY
HIGHWAY DEPT.

COUNTY PROJECT 24-19-75

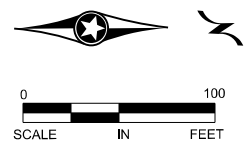
CONSTRUCTION PLAN
STA 40+00.00 TO 55+00.00
Sheet 9 of 20 Sheets

DLDiers

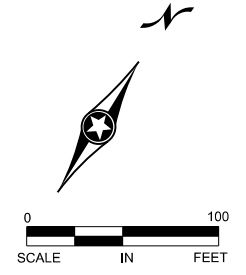
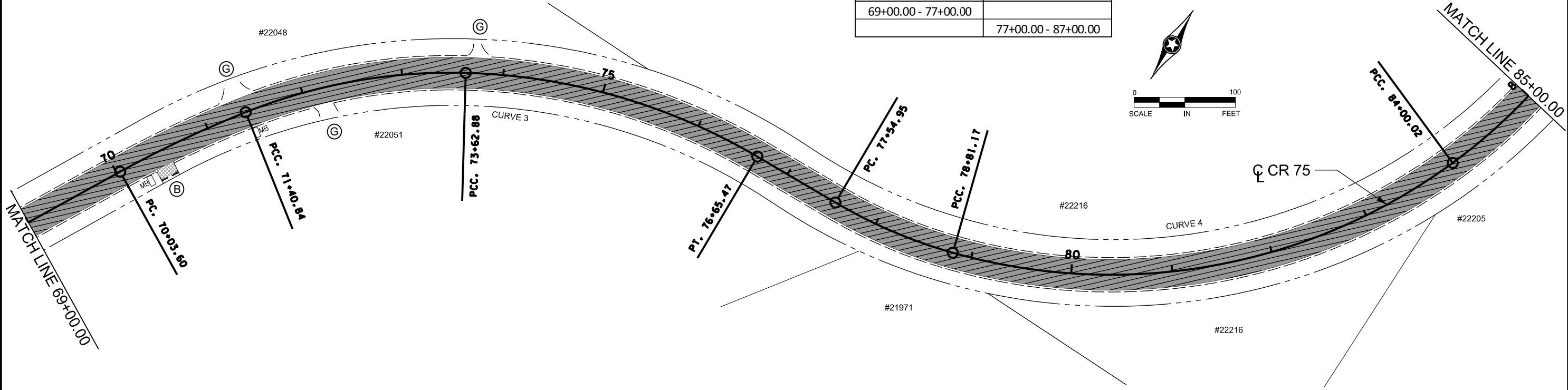


LEGEND

(B)	BITUMINOUS	— — —	CROSS CULVERT
(G)	GRAVEL	[Hatched Box]	MAINLINE RECLAIM
(C)	CONCRETE	[Cross-hatched Box]	REMOVE AND REPLACE PAVEMENT
⊕	CORE LOCATION	[Diagonal Hatched Box]	SUPER TRANSITION AREA
- - -	SAWCUT	— — —	R / W



LOW SIDE 1.0' BUMPOUTS	
LNB	LSB
61+00.00 - 69+00.00	
69+00.00 - 77+00.00	
	77+00.00 - 87+00.00



NO	DATE	BY	CKD	APPR	REVISION	
	04/23/2024					2:13:02 PM

NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_CP3.dgn

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SIGNATURE: *[Signature]*

04-01-2024 26511

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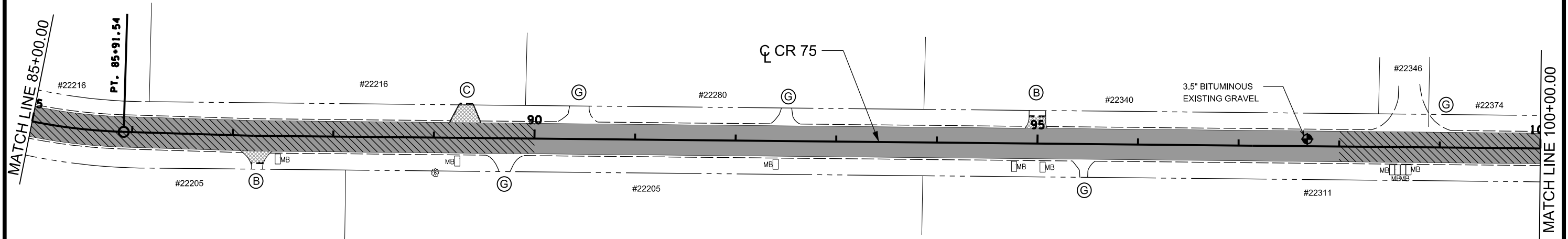
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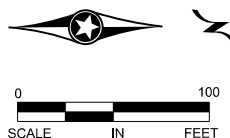
ANOKA COUNTY
HIGHWAY DEPT.

COUNTY PROJECT 24-19-75

CONSTRUCTION PLAN
STA 55+00.00 TO 85+00.00
Sheet 10 of 20 Sheets

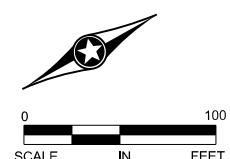
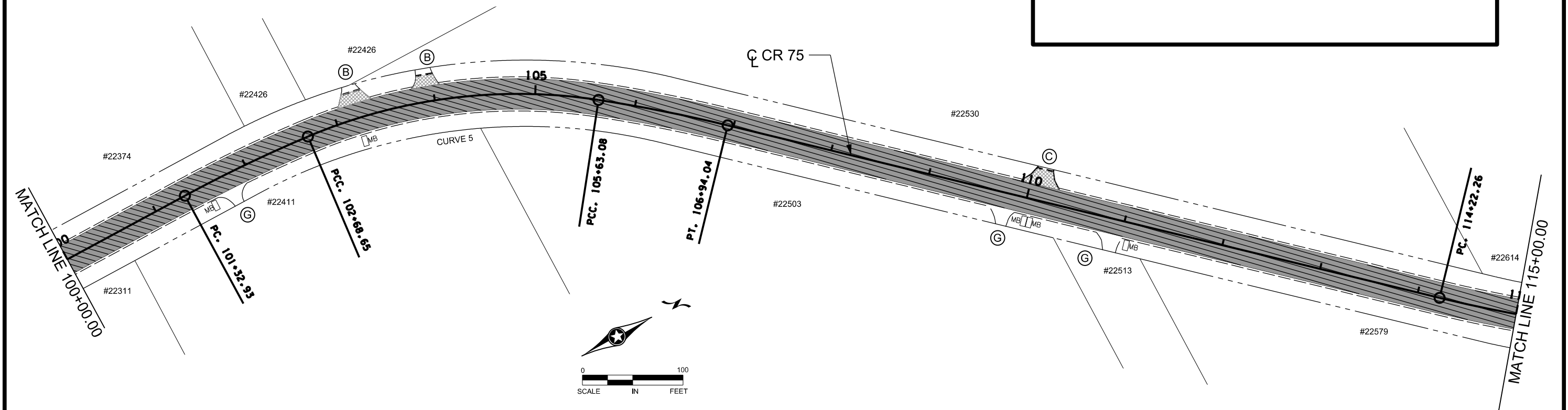


LOW SIDE 1.0' BUMPOUTS	
LNB	LSB
101+00.00 - 107+50.00	



LEGEND

- (B) BITUMINOUS
- (G) GRAVEL
- (C) CONCRETE
- ⊕ CORE LOCATION
- - - SAWCUT
- - - CROSS CULVERT
- █ MAINLINE RECLAIM
- ▨ REMOVE AND REPLACE PAVEMENT
- ▩ SUPER TRANSITION AREA
- R/W



NO	DATE	BY	CKD	APPR	REVISION	
	04/23/2024					2:13:03 PM

NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_CP4.dgn

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SIGNATURE: *[Signature]*

04-01-2024 26511

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DESIGN BY: DLD DATE: 02/06/2024

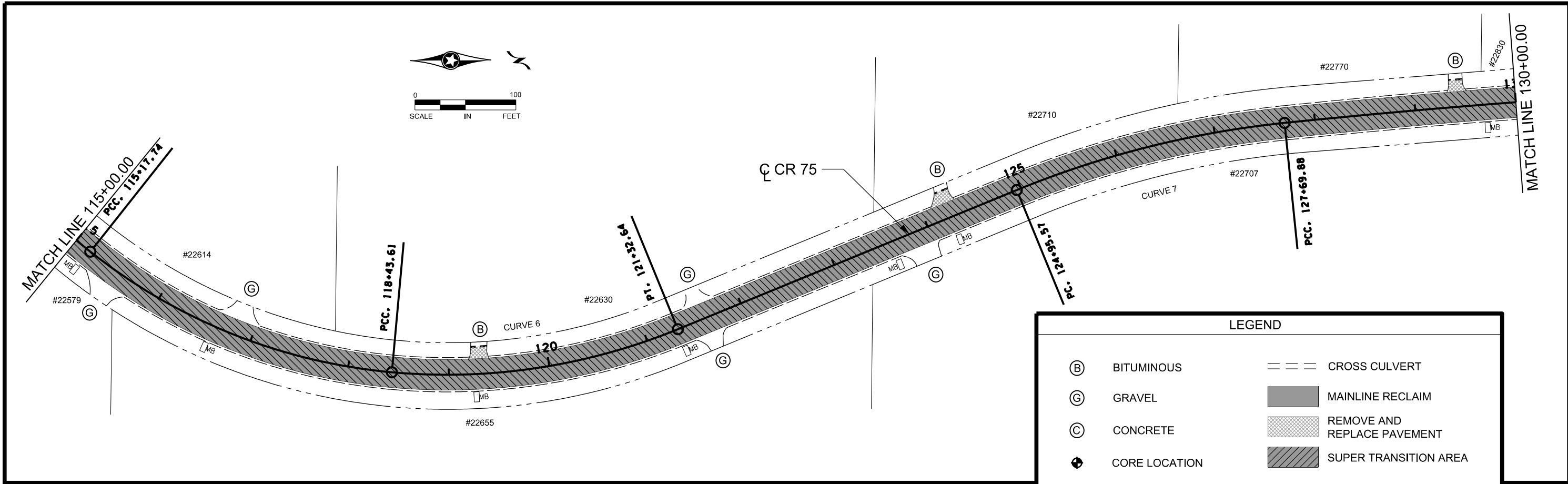
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ANOKA COUNTY
HIGHWAY DEPT.

COUNTY PROJECT 24-19-75

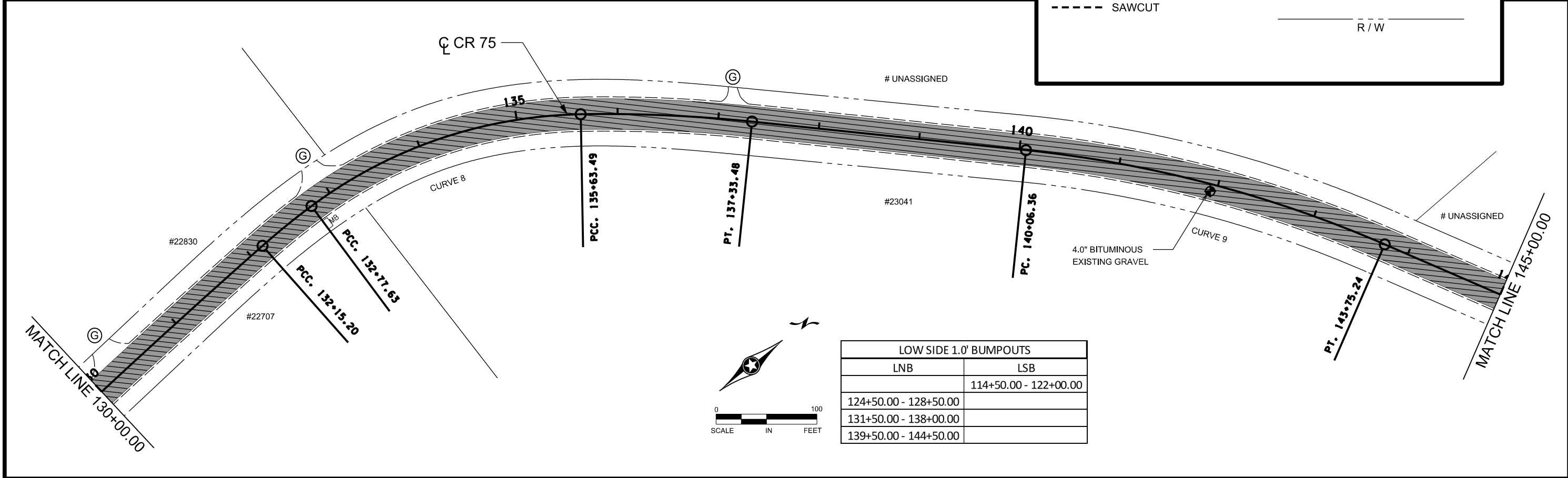
CONSTRUCTION PLAN
STA 85+00.00 TO 115+00.00
Sheet 11 of 20 Sheets

DLDiers



LEGEND

(B)	BITUMINOUS	---	CROSS CULVERT
(G)	GRAVEL	[Hatched Box]	MAINLINE RECLAIM
(C)	CONCRETE	[Cross-hatched Box]	REMOVE AND REPLACE PAVEMENT
⊙	CORE LOCATION	[Diagonal-hatched Box]	SUPER TRANSITION AREA
- - -	SAWCUT	---	R / W



LOW SIDE 1.0' BUMPOUTS

LNB	LSB
	114+50.00 - 122+00.00
124+50.00 - 128+50.00	
131+50.00 - 138+00.00	
139+50.00 - 144+50.00	

NO	DATE	BY	CKD	APPR	REVISION	
	04/23/2024					2:13:04 PM

NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_CP5.dgn

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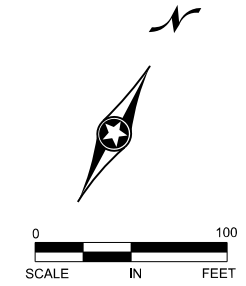
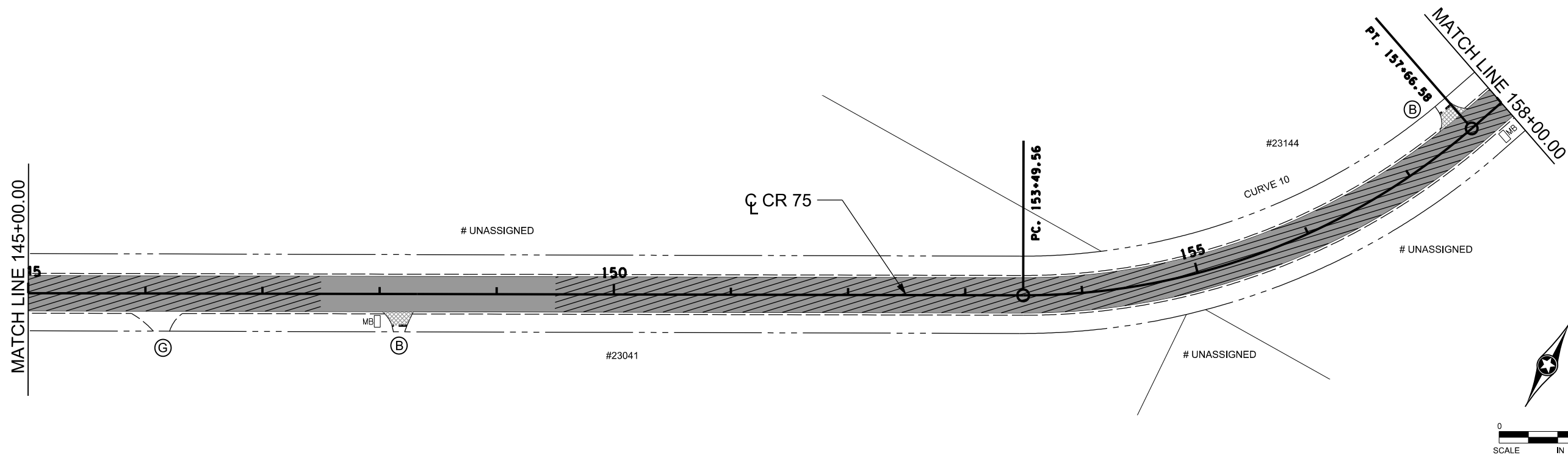
ANOKA COUNTY HIGHWAY DEPT.

COUNTY PROJECT 24-19-75

CONSTRUCTION PLAN

STA 115+00.00 TO 145+00.00

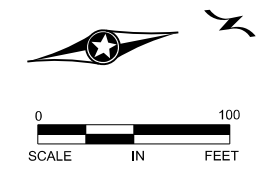
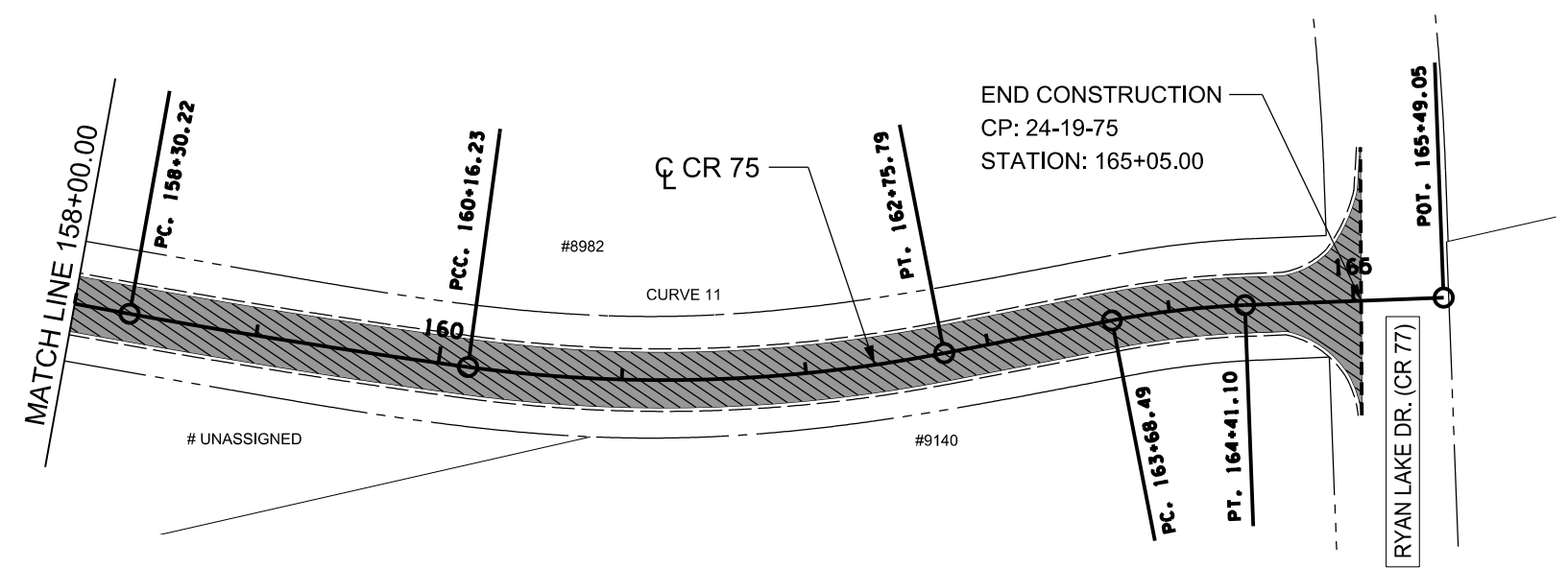
Sheet 12 of 20 Sheets



LOW SIDE 1.0' BUMPOUTS	
LNB	LSB
	153+00.00 - 164+00.00

LEGEND

(B)	BITUMINOUS	--- --	CROSS CULVERT
(G)	GRAVEL	[Solid Grey Box]	MAINLINE RECLAIM
(C)	CONCRETE	[Cross-hatched Box]	REMOVE AND REPLACE PAVEMENT
⊕	CORE LOCATION	[Diagonal-hatched Box]	SUPER TRANSITION AREA
- - - -	SAWCUT		
		— — — —	R/W



NO	DATE	BY	CKD	APPR	REVISION	
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NAME: P:\25-01-00\CR_75_(CSAH 22-CR 77)\Base\Proposed\CR_75_CP6.dgn

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**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 24-19-75

CONSTRUCTION PLAN

STA 145+00.00 TO 165+05.00

Sheet 13 of 20 Sheets

**PERMANENT PAVEMENT MARKING PLAN
NOTES & GUIDELINES**

GENERAL INFORMATION:

1. THE ENGINEER'S INVOLVEMENT IN THE APPLICATION OF THE MATERIAL SHALL BE LIMITED TO FIELD CONSULTATION AND INSPECTION. ANOKA COUNTY HIGHWAY DEPARTMENT WILL PLACE NECESSARY "SPOTTING" AT APPROPRIATE POINTS TO PROVIDE HORIZONTAL CONTROL FOR STRIPING AND TO DETERMINE NECESSARY STARTING AND CUTOFF POINTS. LONGITUDINAL JOINTS, PAVEMENT EDGES AND EXISTING MARKINGS MAY SERVE AS HORIZONTAL CONTROL WHEN SO DIRECTED.
2. EDGE LINES AND LANE LINES ARE TO BE BROKEN ONLY AT INTERSECTIONS WITH PUBLIC ROADS AND AT PRIVATE ENTRANCES IF THEY ARE CONTROLLED BY A YIELD SIGN, STOP SIGN OR TRAFFIC SIGNAL. THE BREAK POINT IS TO BE AT THE START OF THE RADIUS FOR THE INTERSECTION OR AT MARKED STOP LINES OR CROSSWALKS.
3. A TOLERANCE OF 1/4 INCH UNDER OR 1/4 INCH OVER THE SPECIFIED WIDTH WILL BE ALLOWED FOR STRIPING PROVIDED THE VARIATION IS GRADUAL AND DOES NOT DETRACT FROM THE GENERAL APPEARANCE. BROKEN LINE SEGMENTS MAY VARY UP TO ONE-HALF FOOT FROM THE SPECIFIED LENGTHS PROVIDED THE OVER AND UNDER VARIATIONS ARE REASONABLY COMPENSATORY. ALIGNMENT DEVIATIONS FROM THE CONTROL GUIDE SHALL NOT EXCEED 1 INCH. MATERIAL SHALL NOT BE APPLIED OVER LONGITUDINAL JOINTS. ESTABLISHMENT OF APPLICATION TOLERANCES SHALL NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO COMPLY AS CLOSELY AS PRACTICABLE WITH THE PLANNED DIMENSIONS.
4. PERMANENT PAVEMENT MARKINGS SHALL NOT BE PLACED OVER TEMPORARY TAPE MARKINGS.
5. THE FILLING OF TANKS, POURING OF MATERIALS OR CLEANING OF EQUIPMENT SHALL NOT BE PERFORMED ON UNPROTECTED PAVEMENT SURFACES UNLESS ADEQUATE PROVISIONS ARE MADE TO PREVENT SPILLAGE OF MATERIAL.

MULTI-COMPONENT (MULTI-COMP):

1. THE ROAD SURFACE SHALL BE CLEANED AT THE DIRECTION OF THE ENGINEER JUST PRIOR TO APPLICATION. PAVEMENT CLEANING SHALL CONSIST OF AT LEAST BRUSHING WITH A ROTARY BROOM (NON-METALLIC) OR AS RECOMMENDED BY THE MATERIAL MANUFACTURER AND ACCEPTABLE TO THE ENGINEER. NEW PORTLAND CEMENT CONCRETE SURFACES SHALL BE SANDBLAST CLEANED TO REMOVE ANY SURFACE TREATMENT AND/OR LAITANCE ON LOW SPEED (SPEED LIMIT 35 MPH OR LESS) URBAN PORTLAND CEMENT CONCRETE ROADWAYS. SANDBLAST CLEANING SHALL BE USED FOR ALL MULTI-COMP PAVEMENT MARKINGS.
2. THE MULTI-COMP MARKING APPLICATION SHALL IMMEDIATELY FOLLOW THE PAVEMENT CLEANING. GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE MULTI-COMP LINE TO PROVIDE AN IMMEDIATE NO-TRACK SYSTEM.
3. A MULTI-COMP LINE SHALL BE APPLIED WITH A MINIMUM THICKNESS OF 20 MILS (WET) AND 4" WIDE. GLASS BEADS SHALL BE APPLIED AT A MINIMUM RATE OF 25 LBS POUNDS PER GALLON RATE SUFFICIENT TO ACHIEVE AN ACCEPTABLE NO-TRACK SYSTEM.
4. PAVEMENT MARKINGS SHALL ONLY BE APPLIED IN SEASONABLE WEATHER WHEN AIR AND PAVEMENT SURFACE TEMPERATURES ARE 40° OR HIGHER AND SHALL NOT BE APPLIED WHEN THE WIND OR OTHER CONDITIONS CAUSE A FILM OR DUST TO BE DEPOSITED ON THE PAVEMENT SURFACE AFTER CLEANING AND BEFORE THE MARKING MATERIAL CAN BE APPLIED.

PAVEMENT MARKING TABULATION			
ITEM DESCRIPTION	UNIT	TOTAL QUANTITY	
		WHITE	YELLOW
4" SOLID LINE MULTI-COMP	LIN FT	30,517	3,223
4" SOLID DOUBLE LINE MULTI-COMP	LIN FT		9,717
4" BROKEN LINE MULTI-COMP	LIN FT		1,120

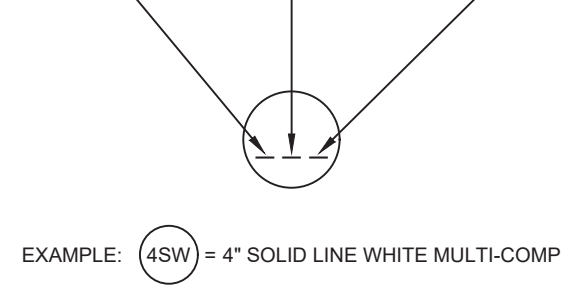
PAVEMENT MARKING SYMBOLS & MATERIALS LEGEND

— — BROKEN LINE - 50' CYCLE (10' LINE, 40' GAP)

STRIPING KEY

○ CIRCLE - MULTI-COMP

<u>1ST DIGIT WIDTH</u>	<u>2ND DIGIT PATTERN</u>	<u>3RD DIGIT COLOR</u>
4", 8", ETC.	S - SOLID B - BROKEN T - DOTTED D - DOUBLE SOLID	W - WHITE Y - YELLOW B - BLACK

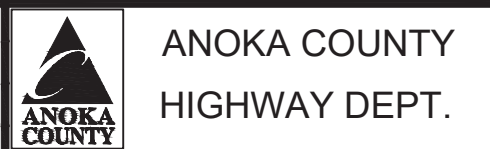


NO	DATE	BY	CKD	APPR	REVISION

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: SEAN R. THIEL
SIGNATURE: *Sean R. Thiel*
DATE: 03/08/2024 LICENSE NO. 45129

DRAWN BY: FL DATE: 03/05/24
DESIGN BY: FL DATE: 03/05/24
CHECKED BY: SRT DATE: _____



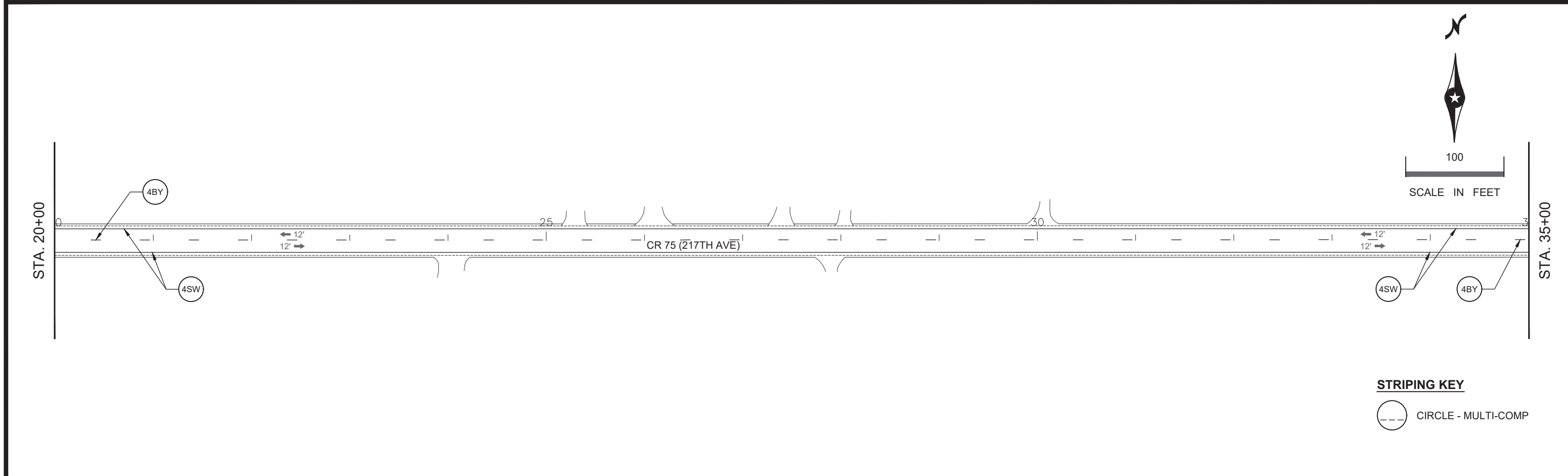
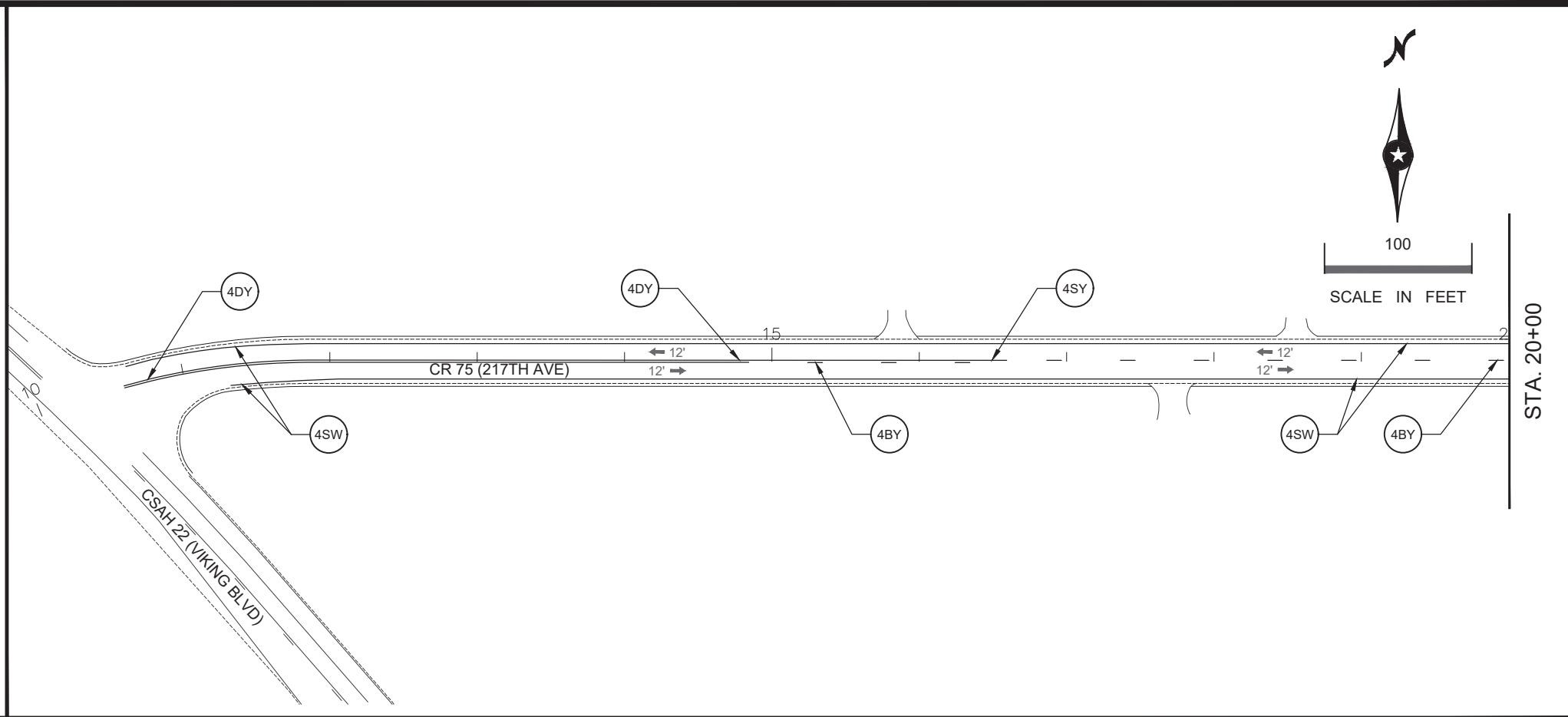
CP 24-19-75

PERMANENT PAVEMENT MARKING GUIDELINES AND TABULATIONS

Sheet 14 of 20 Sheets

NOTES: (TYP.)

- LOCATIONS OF PAVEMENT MARKINGS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- ALL PERMANENT STRIPING SHALL BE PLACED WITHIN 72 HOURS OF MAINLINE PAVING.



STRIPING KEY

○ --- CIRCLE - MULTI-COMP

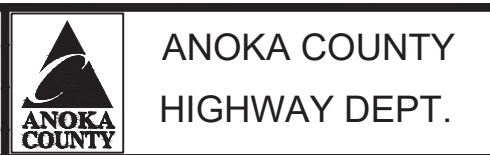
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NAME: P:25-01-00CR_75_(CSAH 22-CR 77)BaseTrafficPermanent Striping.dwg

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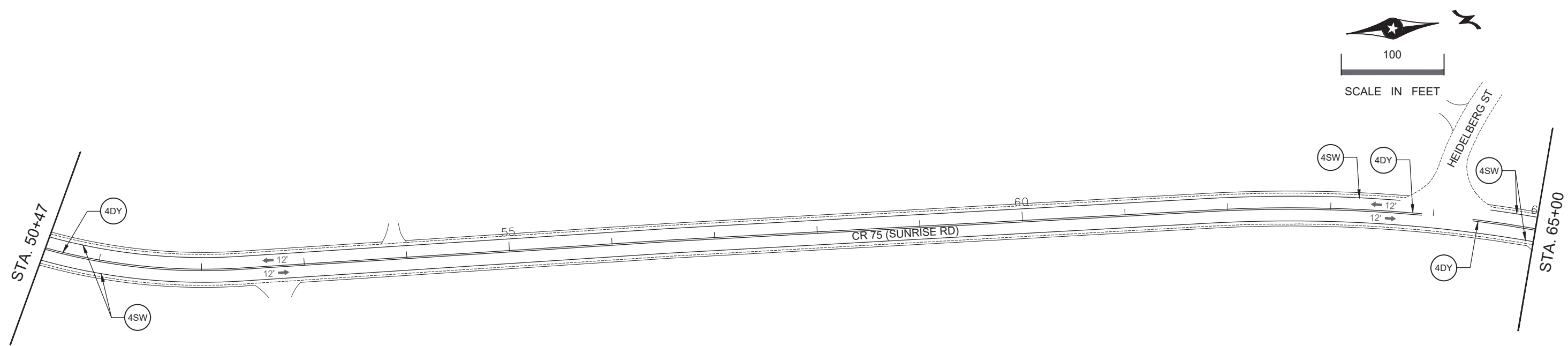
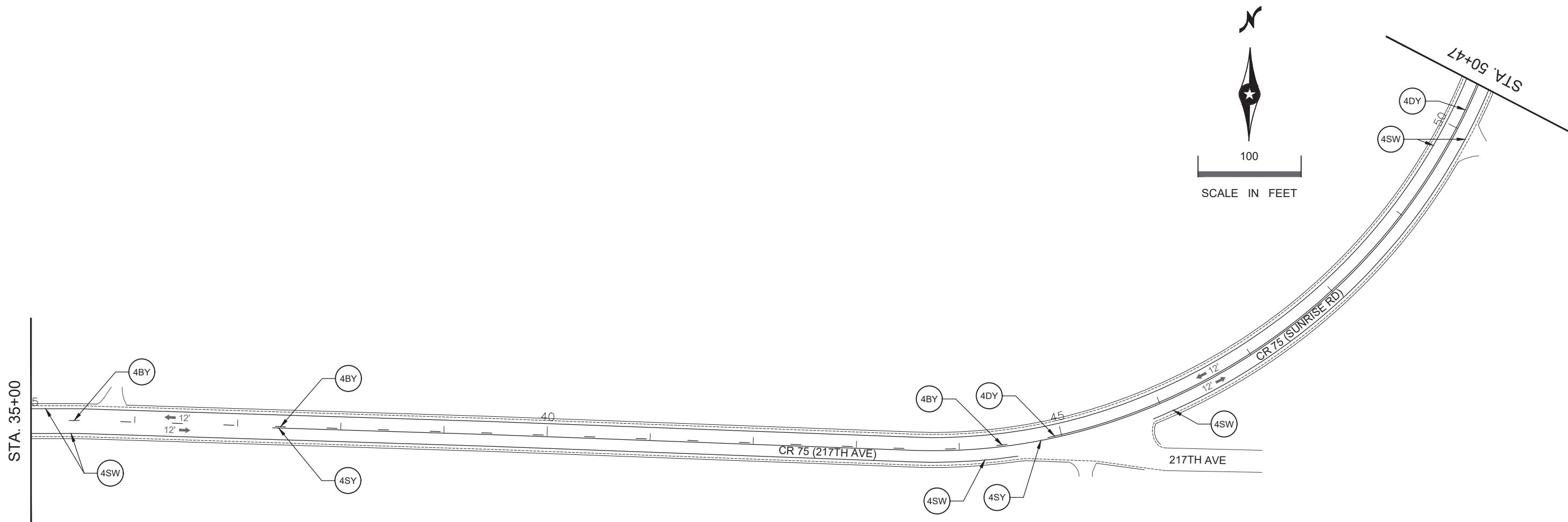
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PERMANENT STRIPING
 Sheet 15 of 20 Sheets



STRIPING KEY
 ○ CIRCLE - MULTI-COMP

NO	DATE	BY	CHKD	APPR	REVISION

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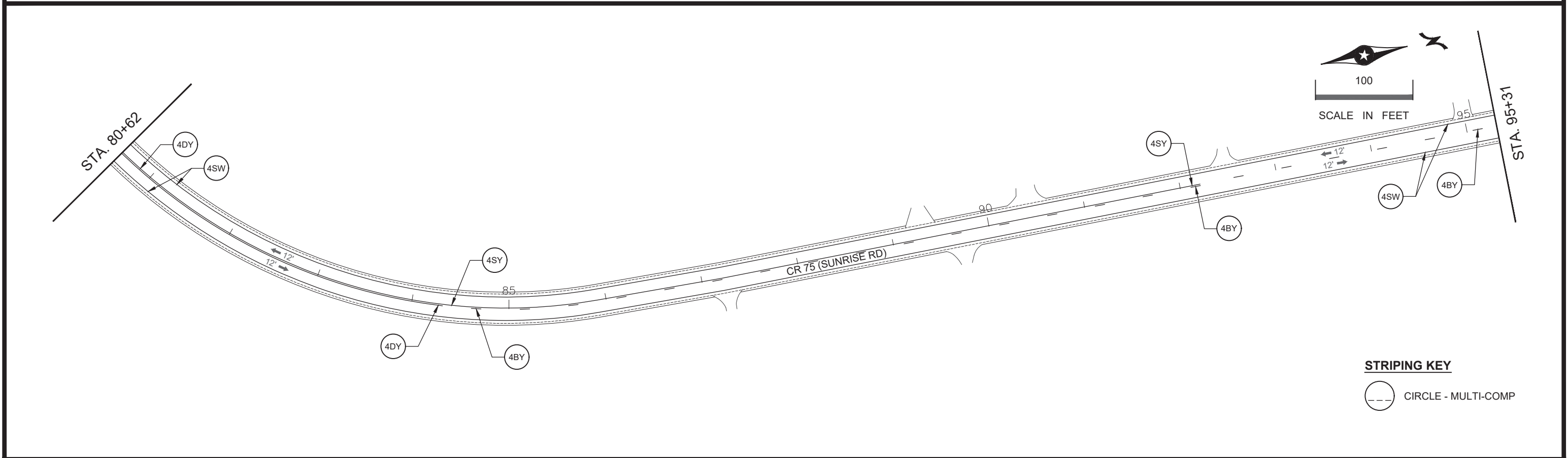
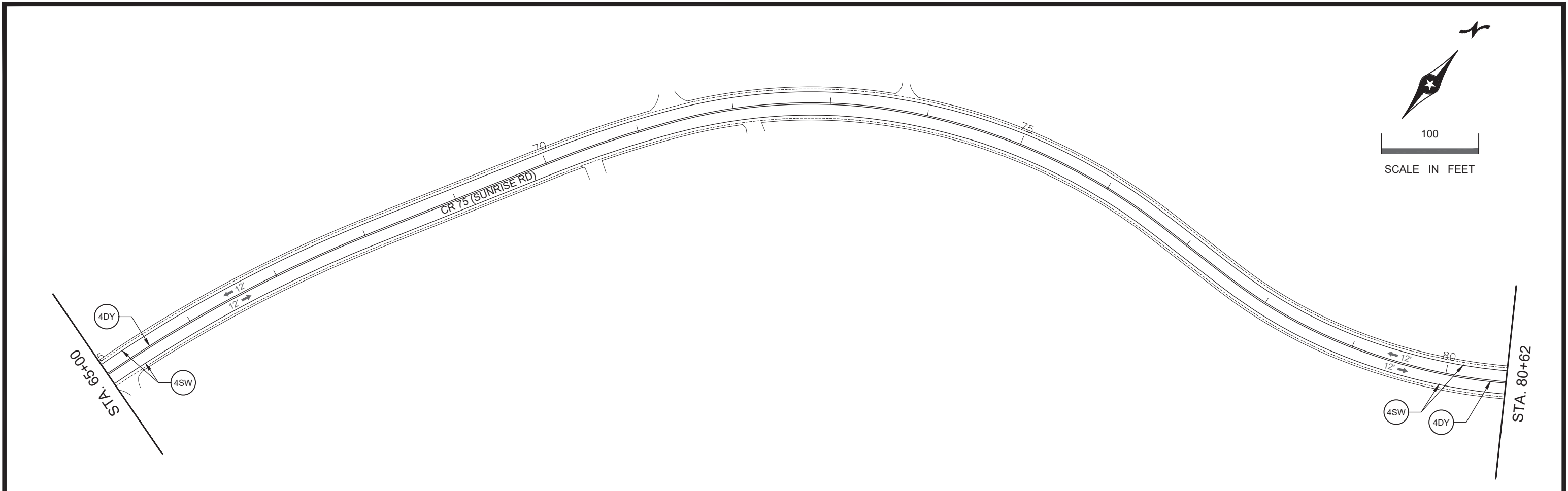
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ANOKA COUNTY
HIGHWAY DEPT.

CP 24-19-75

PERMANENT STRIPING
 Sheet 16 of 20 Sheets



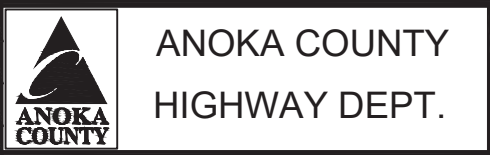
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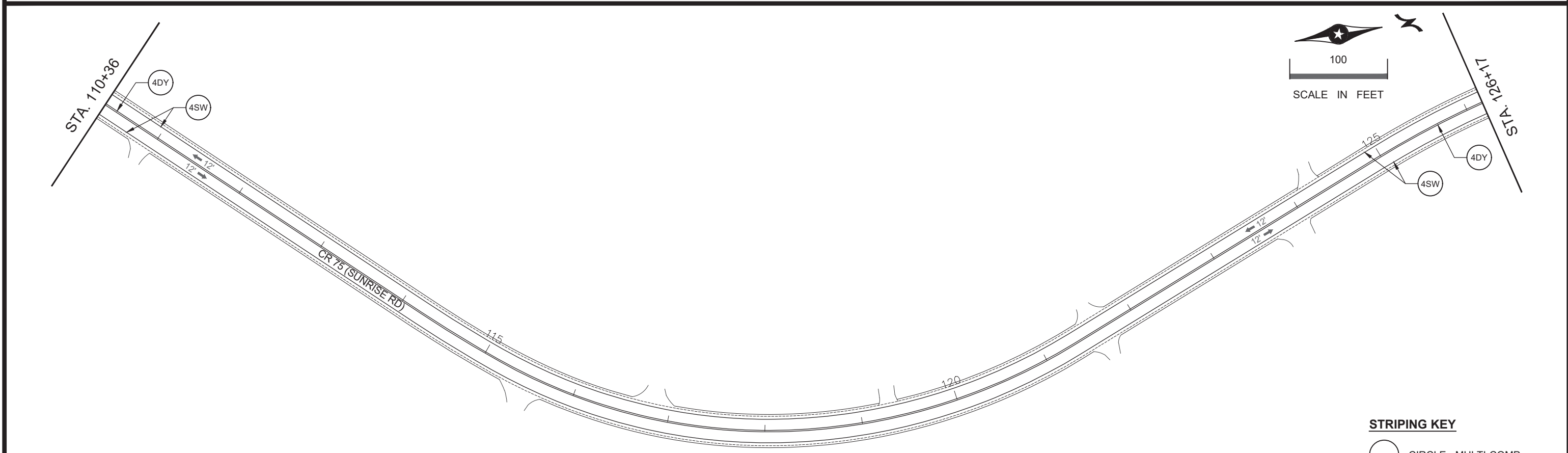
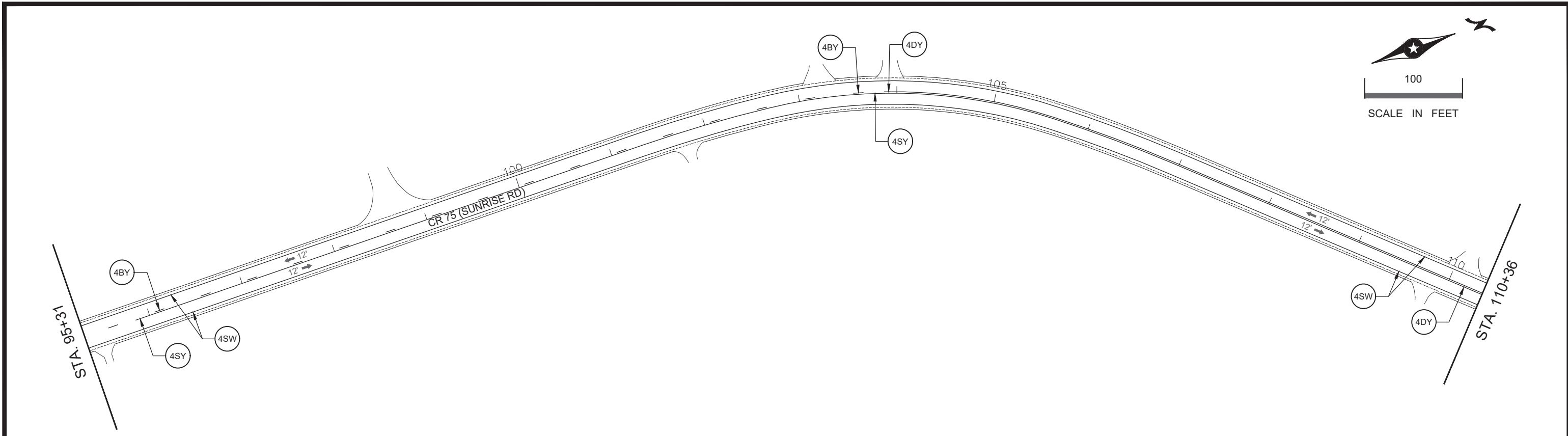
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PERMANENT STRIPING
 Sheet 17 of 20 Sheets

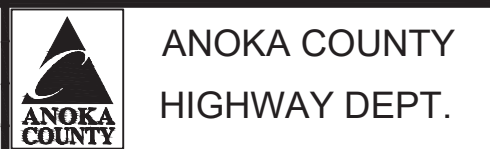


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 ○ CIRCLE - MULTI-COMP

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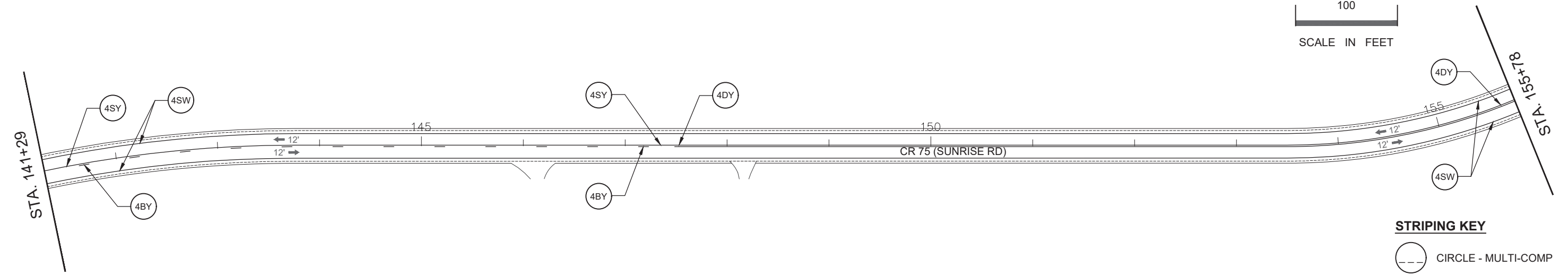
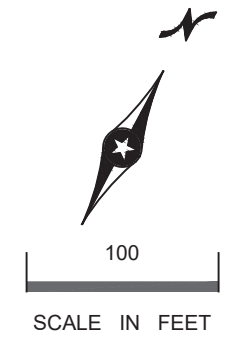
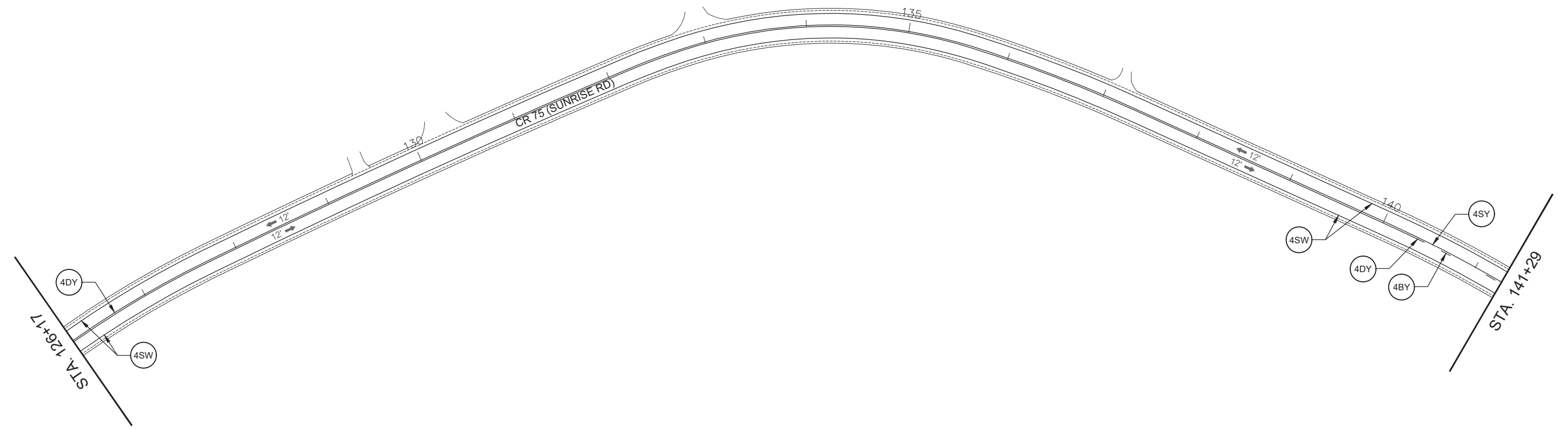
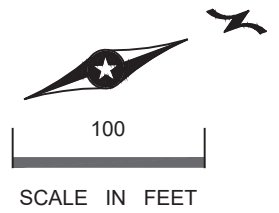
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PERMANENT STRIPING
 Sheet 18 of 20 Sheets

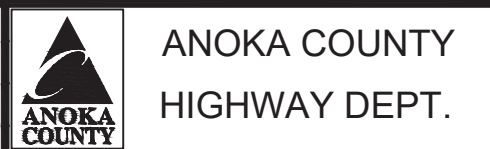


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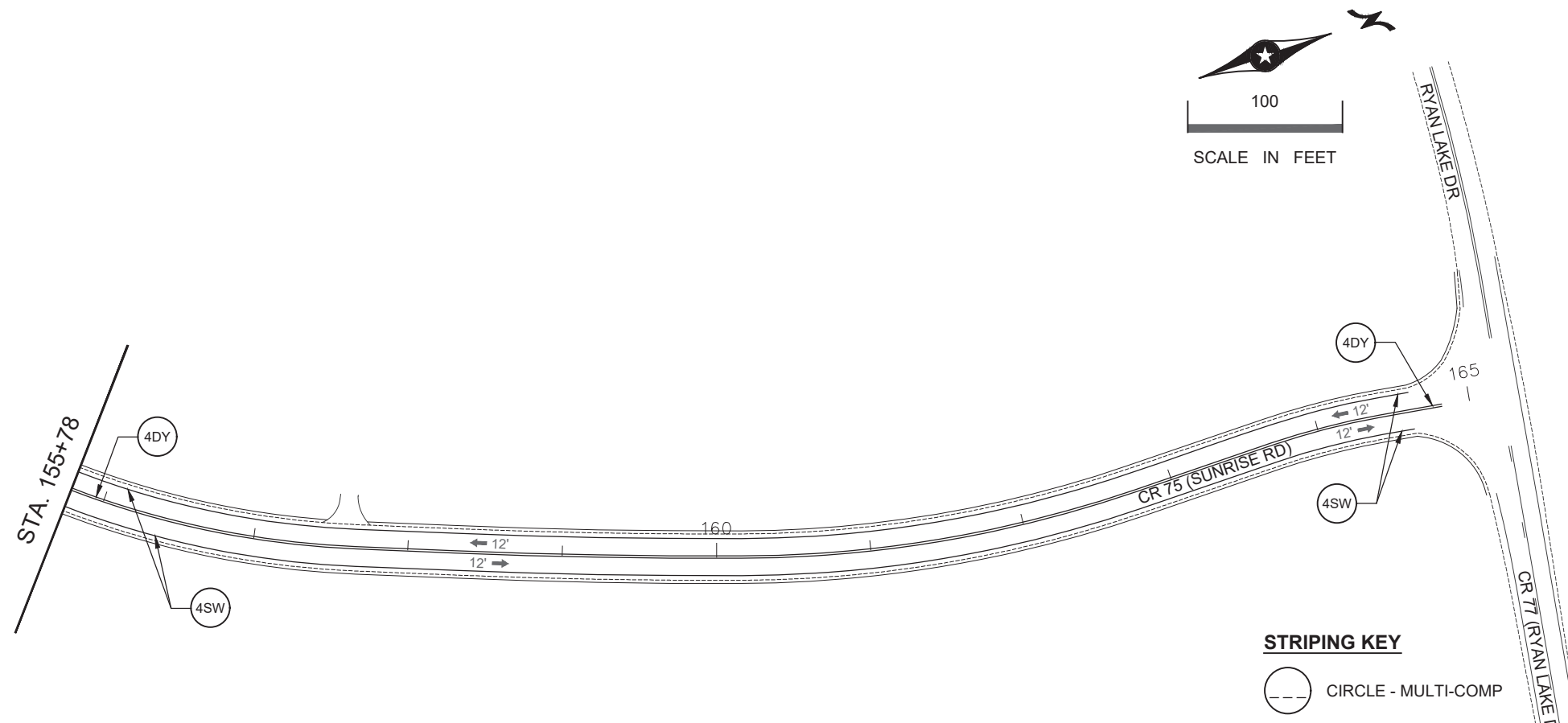
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 Sheet 19 of 20 Sheets



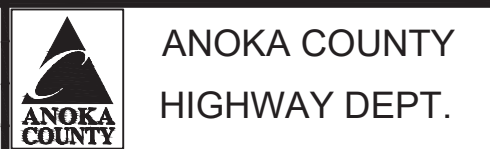
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PERMANENT STRIPING

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