

PLAN SYMBOLS

- COUNTY LINE _____
- TOWNSHIP OR RANGE LINE _____
- SECTION LINE _____
- QUARTER LINE _____
- SIXTEENTH LINE _____
- RIGHT-OF-WAY LINE _____
- SLOPE EASEMENT _____
- EXISTING RIGHT OF WAY _____
- PROPERTY LINE _____
- CORPORATE OR CITY LIMITS _____
- RETAINING WALL _____
- RAILROAD _____
- RAILROAD RIGHT OF WAY _____
- RIVER OR CREEK _____
- DRAINAGE DITCH _____
- CULVERT _____
- DROP INLET _____
- GUARD RAIL _____
- BARBED WIRE FENCE _____
- WOVEN WIRE FENCE _____
- CHAIN LINK FENCE _____
- WOOD FENCE _____
- STONE WALL OR FENCE _____
- HEDGE _____

- LOWLAND _____
- TIMBER _____
- ORCHARD _____
- BRUSH _____
- NURSERY _____

- CATTLE GUARD _____
- OVERPASS (Highway Over) _____
- UNDERPASS (Highway Under) _____
- BRIDGE _____

- BUILDING (One Story Frame) _____
- F-FRAME C-CONCRETE _____
- S-STONE T-TILE _____
- B-BRICK ST-STUCCO _____

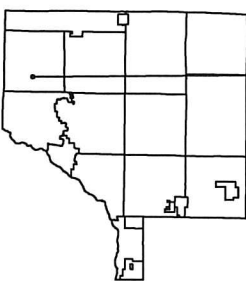
- RAILROAD CROSSING BELL _____
- RAILROAD CROSSING GATE _____
- MANHOLE _____
- CATCH BASIN _____
- FIRE HYDRANT _____
- CAST IRON MONUMENT _____
- IRON PIN _____
- GRAVEL PIT _____
- SAND PIT _____
- BORROW PIT _____
- ROCK QUARRY _____

UTILITY SYMBOLS

- POWER POLE LINE _____
- TELEPHONE OR TELEGRAPH POLE LINE _____
- JOINT TELEPHONE & POWER ON POWER POLES _____
- ON TELEPHONE POLES _____
- ANCHOR _____
- STEEL TOWER _____
- STREET LIGHT _____
- PEDESTAL (Cable Terminal) _____
- GAS MAIN _____
- WATERMAIN _____
- TELEPHONE CABLE IN CONDUIT _____
- ELECTRIC CABLE IN CONDUIT _____
- TELEPHONE MANHOLE _____
- ELECTRIC MANHOLE _____
- BURIED TELEPHONE CABLE _____
- BURIED ELECTRIC CABLE _____
- SEWER (Sanitary or Storm) _____
- SEWER MANHOLE _____

SCALES

- PLAN _____ 0' 100'
- PROFILE HORIZONTAL _____ 0' 100'
- VERTICAL _____ 0' 10'
- X-SECTIONS HORIZONTAL _____ 0' 10'
- VERTICAL _____ 0' 10'



PROJECT LOCATION

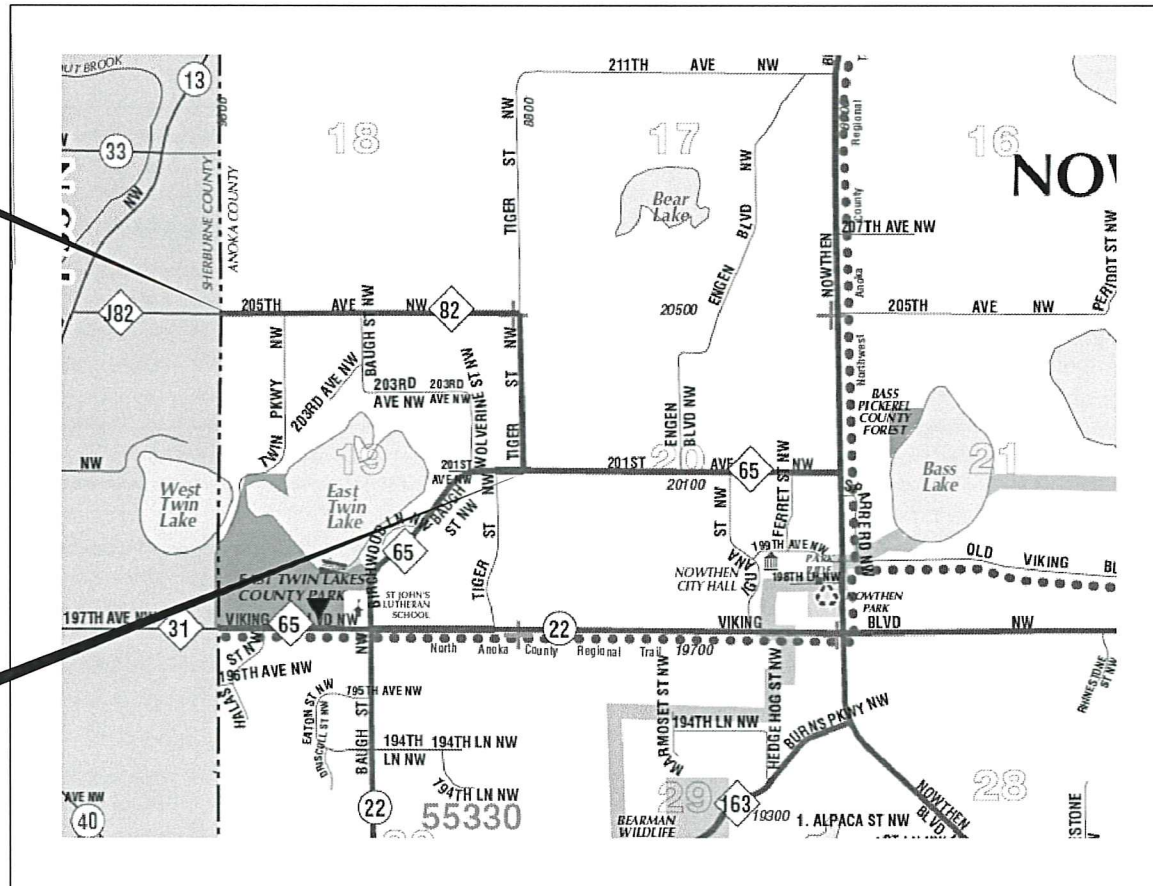
CITY OF NOWTHEN
 ANOKA COUNTY
 SECTION 17 & 18 & 19 & 20
 TOWNSHIP 33 NORTH
 RANGE 25 WEST

MINNESOTA DEPARTMENT OF TRANSPORTATION ANOKA COUNTY

CONSTRUCTION PLAN FOR _____ BITUMINOUS RECLAMATION, BITUMINOUS OVERLAY
 LOCATED ON _____ C.R. 82 BETWEEN WEST COUNTY LINE ON C.R. 82 AND _____ C.R. 65

C.R. 82

GROSS LENGTH	7,572 FEET	1.43 MILES
EXCEPTIONS-LENGTH	0.00 FEET	0.000 MILES
NET LENGTH	7,572 FEET	1.43 MILES



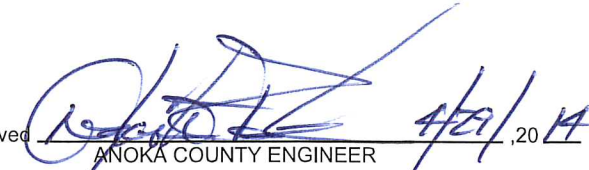
GOVERNING SPECIFICATIONS

THE 2014 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM AND BE INSTALLED IN ACCORDANCE TO THE "MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MNMUTCD), AND PART VI, "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS."

INDEX


SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	STATEMENT OF ESTIMATED QUANTITIES
3	TYPICAL SECTIONS
4 - 5	MISCELLANEOUS DETAILS
6 - 8	CONSTRUCTION PLAN
9 - 13	TRAFFIC PLAN

THIS PLAN CONTAINS 13 SHEETS

Approved:  4/29/14
 ANOKA COUNTY ENGINEER

NO	DATE	BY	CKD	APPR	REVISION	
	04/10/2014					7:15:45 AM

NAME: P:\14-01-00\CR_82_(WestCL-CR65)\Base\PROPOSED\NO RTL\1_TITLE_NO_RTL.dgn

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
 PRINT NAME: ANDREW WITTER
 SIGNATURE: 
 DATE: 4/29/14 LICENSE NO. 42757

DRAWN BY RT DATE 04/10/2014
 DESIGN BY RT DATE 04/10/2014
 CHECKED BY DF DATE 04/10/2014

 ANOKA COUNTY
 HIGHWAY DEPT.

COUNTY PROJECT 14-56-82

TITLE SHEET
 Sheet 1 of 13 Sheets

STATEMENT OF ESTIMATED QUANTITIES				
ITEM NO.	ITEM	NOTES	UNIT	TOTAL PROJECT QUANTITIES ESTIMATED
2021.501	MOBILIZATION		LUMP SUM	1
2104.503	REMOVE BITUMINOUS DRIVEWAY PAVEMENT	8	SQ YD	401
2104.513	SAWING BITUMINOUS PAVEMENT (FULL DEPTH)		LIN FT	607
2123.503	MOTOR GRADER		HOUR	30
2130.501	WATER		M-GAL	194
2211.501	AGGREGATE BASE CLASS 5		TON	154
2215.501	FULL DEPTH RECLAMATION		SQ YD	20601
2221.501	SHOULDER BASE AGGREGATE CLASS 5		TON	643
2232.501	MILL BITUMINOUS SURFACE (2.0")	3	SQ YD	112
2331.607	HAUL BITUMINOUS PAVEMENT RECLAMATION (LV)		CU YD	50
2357.502	BITUMINOUS MATERIAL FOR TACK COAT		GALLON	1042
2360.501	TYPE SP 12.5 WEARING COURSE MIXTURE (4,B)		TON	74
2360.501	TYPE SP 12.5 WEARING COURSE MIXTURE (4,E)		TON	4848
2540.602	INSTALL MAIL BOX SUPPORT	1	EACH	11
2563.601	TRAFFIC CONTROL	4,6	LUMP SUM	1
2563.613	PORTABLE CHANGEABLE MESSAGE SIGN	5	UNIT DAY	14
2573.560	CULVERT END CONTROLS		EACH	2
2581.501	REMOVABLE PREFORME PAVEMENT MARKING TAPE	2	LIN FT	203
2582.502	4" SOLID LINE WHITE - EPOXY	7	LIN FT	15420
2582.502	4" SOLID LINE YELLOW - EPOXY	7	LIN FT	3990
2582.502	4" BROKEN LINE YELLOW-EPOXY	7	LIN FT	1530

CONSTRUCTION NOTES	
1	MAILBOXES ARE TO BE INSTALLED AT THE EXISTING MAILBOX LOCATION OR AS DIRECTED BY THE LOCAL POSTAL AUTHORITY, CONTRACTOR IS RESPONSIBLE FOR CONTACTING. MAILBOX REMOVAL AND ALL MATERIALS ARE INCIDENTAL TO INSTALLATION.
2	CENTERLINE SKIPS TO BE APPLIED AS SOON AS POSSIBLE ON EACH NEW LIFT OF PAVEMENT; SKIPS MUST BE INPLACE BEFORE CONTRACTOR LEAVES FOR THE DAY. CONTRACTOR IS TO REMOVE PRIOR TO FINAL PAINT STRIPING.
3	ITEM INCLUDES 2" DEEP MILL AT BEGINNING AND END OF PROJECT, ALL PAVED ST APPROACHES, AND DRIVEWAYS; SEE DETAILS FOR DIMENSIONS
4	ALL TRAFFIC CONTROL METHODS SHALL CONFORM TO THE CURRENT "MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL
5	2 MESSAGE BOARDS, ONE ON THE EACH END OF PROJECT WILL BE INSTALLED 7 DAYS PRIOR TO ANY CONSTRUCTION; REFERENCE STRIPING PLAN FOR DETAILS.
6	DO NOT PASS, PASS WITH CARE AND NO CENTER STRIPE SIGNS TO BE INPLACE DURING MILLING, RECLAIMING OR PAVING
7	FINAL STRIPING SHALL BE INSTALLED WITHIN 72 HOURS OF COMPLETION OF MAINLINE WEAR COURSE PAVING.
8	CONTRACTOR IS RESPONSIBLE FOR CONTACTING PROPERTY OWNER 48 HOURS BEFORE STARTING OPERATION.

BASIS OF PLANNED QUANTITIES		
2357.502	BITUMINOUS MATERIAL FOR TACK COAT	.05 GAL / SQ YD
2211.501	AGGREGATE BASE CLASS 5	1.8 TON / CU YD
2360.501	ALL BITUMINOUS PAVEMENT	115 LB / SQ YD / IN. THICKNESS

NO	DATE	BY	CKD	APPR	REVISION	04/10/2014	7:15:54 AM
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 PRINT NAME: ANDREW WITEN
 SIGNATURE: *[Signature]*
 DATE: 4/20/14 LICENSE NO. 42757

DRAWN BY RT DATE 04/10/2014
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ANOKA COUNTY
 HIGHWAY DEPT.

COUNTY PROJECT 14-56-82

STATEMENT OF ESTIMATED QUANTITIES / NOTES

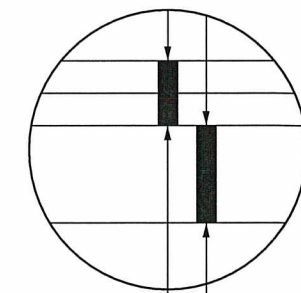
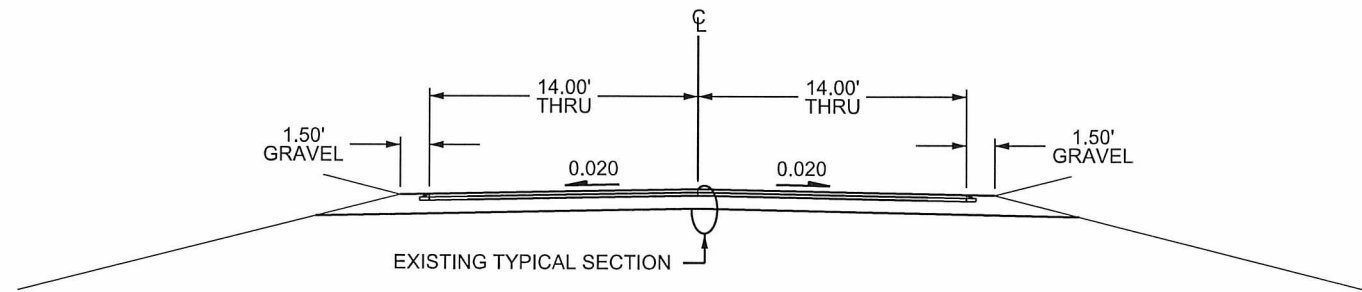
Sheet 2 of 13 Sheets

EXISTING SECTION

TIGER STREET & 205TH AVE (CR 82)

EXISTING

10+53 - 86+25

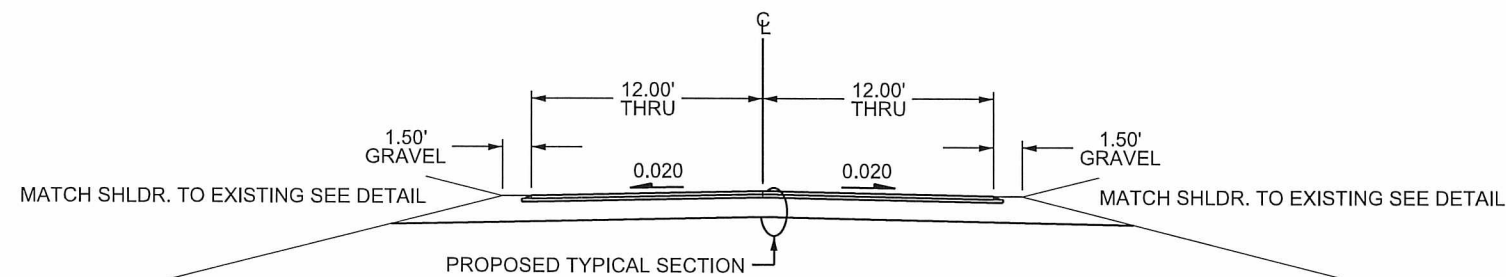


6-8" EXISTING BITUMINOUS
EXISTING BASE

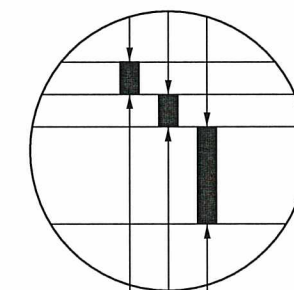
TIGER ST & 205TH AVE (CR 82)

PROPOSED

10+53 - 86+25



PROPOSED RECLAIM SECTION



2.0" BITUMINOUS WEAR(SPWEB440E)
2.0" BITUMINOUS WEAR(SPWEB440E)
RECLAIMED BITUMINOUS

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PRINT NAME: ANDREW WITTER
SIGNATURE: *Andrew Witter*
DATE: 4/28/14 LICENSE NO. 42757

DRAWN BY RT DATE 04/28/2014
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CHECKED BY DF DATE 04/28/2014



ANOKA COUNTY
HIGHWAY DEPT.

COUNTY PROJECT 14-56-82

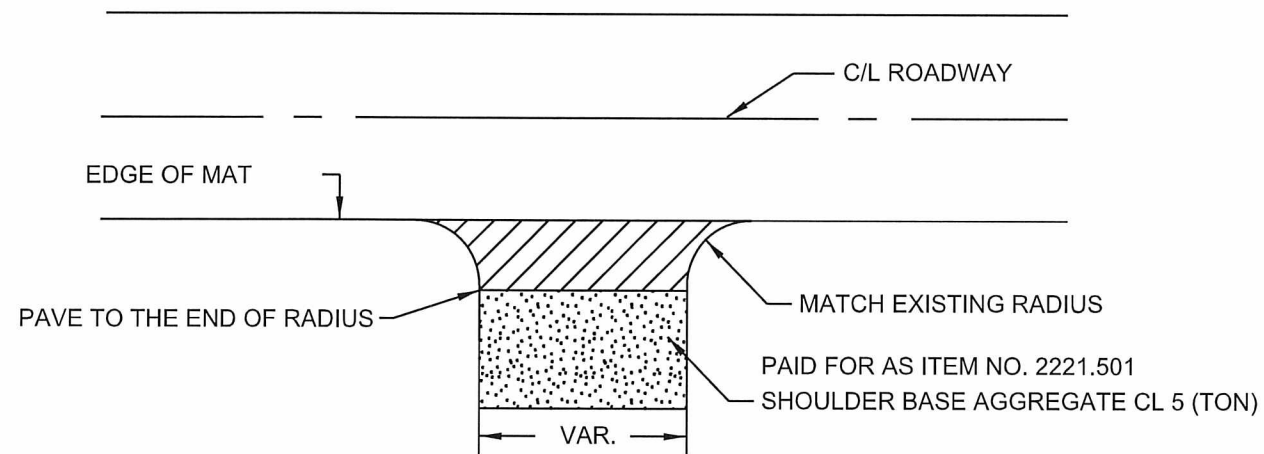
TYPICAL SECTIONS

Sheet 3 of 13 Sheets

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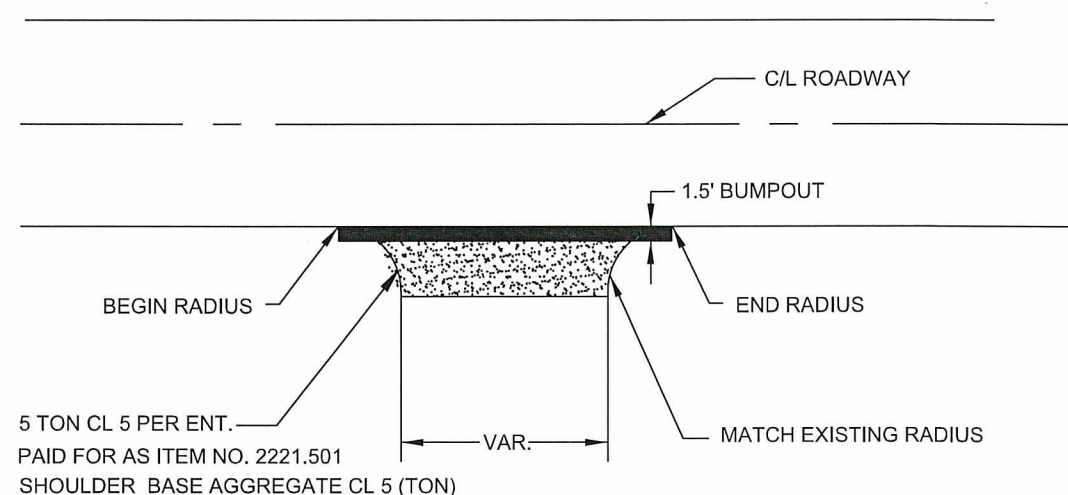
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UNPAVED STREET APPROACH

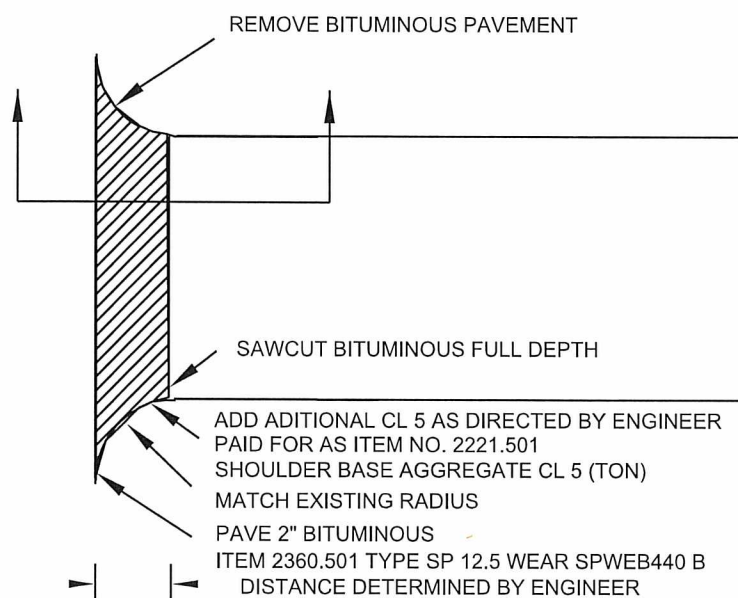


UNPAVED STREET APPROACHES, PAVED SEPERATE FROM MAINLINE, PAID AS STREET APPROACHES.

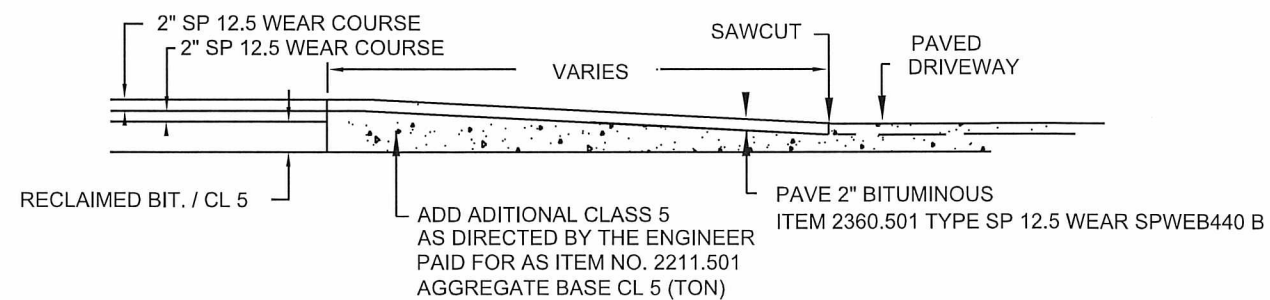
GRAVEL DRIVEWAY AND FIELD ENTERANCE



PAVED STREET APPROACH / DRIVEWAYS DETAIL



PAVED DRIVEWAY DETAIL



NOT TO SCALE

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ANOKA COUNTY
 HIGHWAY DEPT.

COUNTY PROJECT 14-56-82

MISCELLANEOUS DETAILS

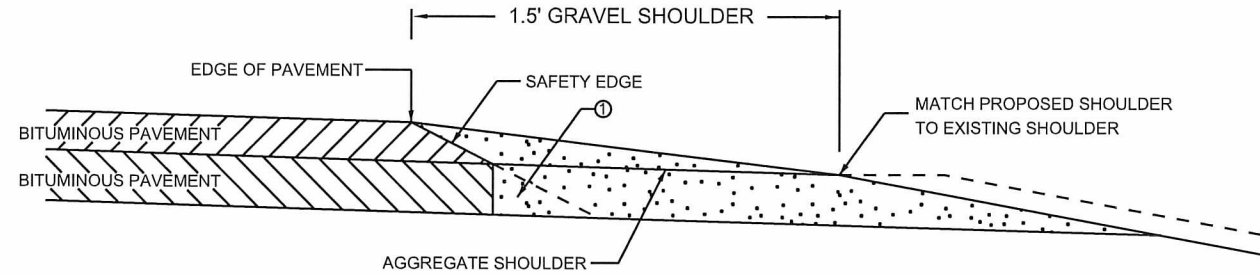
Sheet 4 of 13 Sheets

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SHOULDER DETAIL

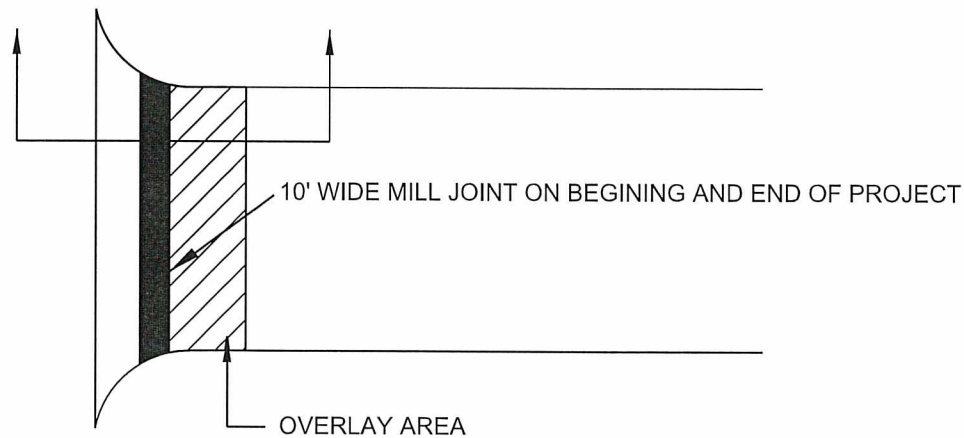
BITUMINOUS SAFETY EDGE
GRAVEL SHOULDER



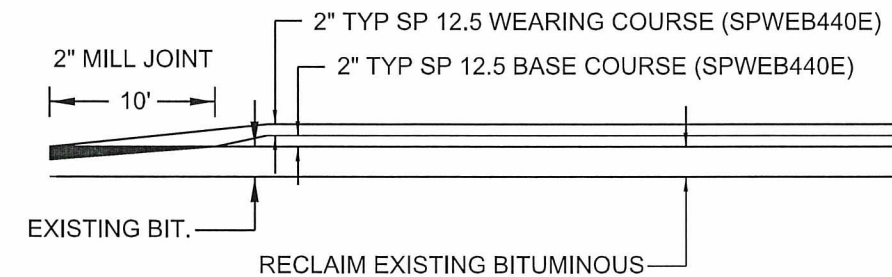
SAFETY EDGE TO BE USED IN ALL NON-CURB AREAS ON SHOULDER.

① OPTIONAL DESIGN EXTENDS SAFETY EDGE DEEPER THAN 6" AND WIDER THAN 10.5".

MAINLINE JOINT DETAIL



MAINLINE JOINT DETAIL



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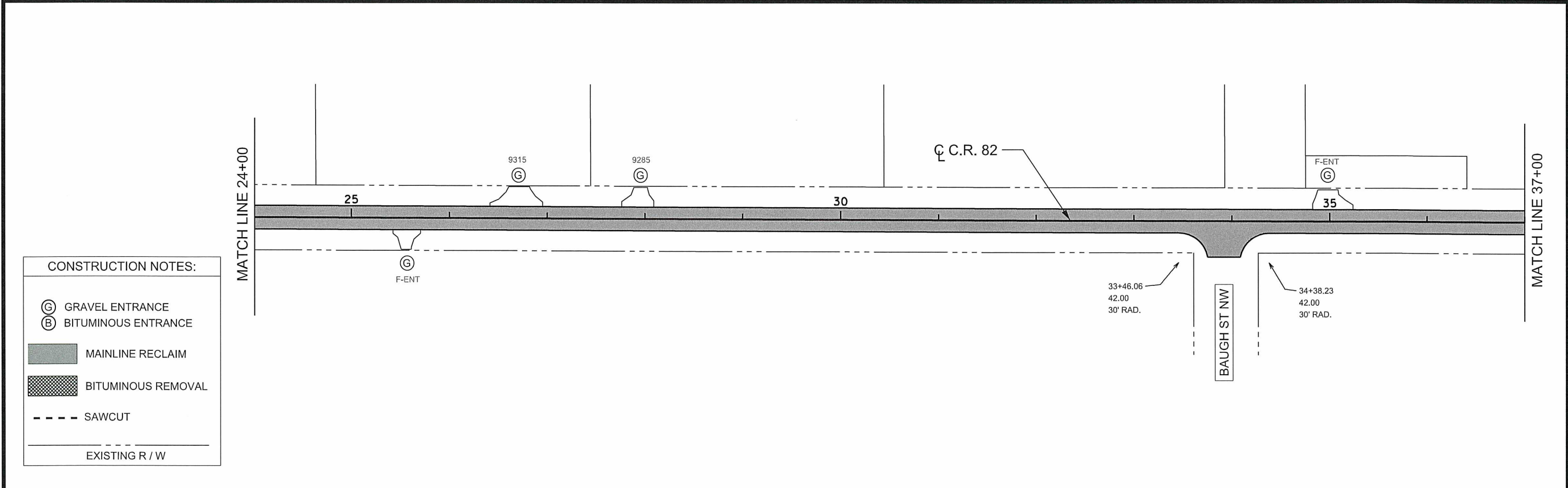
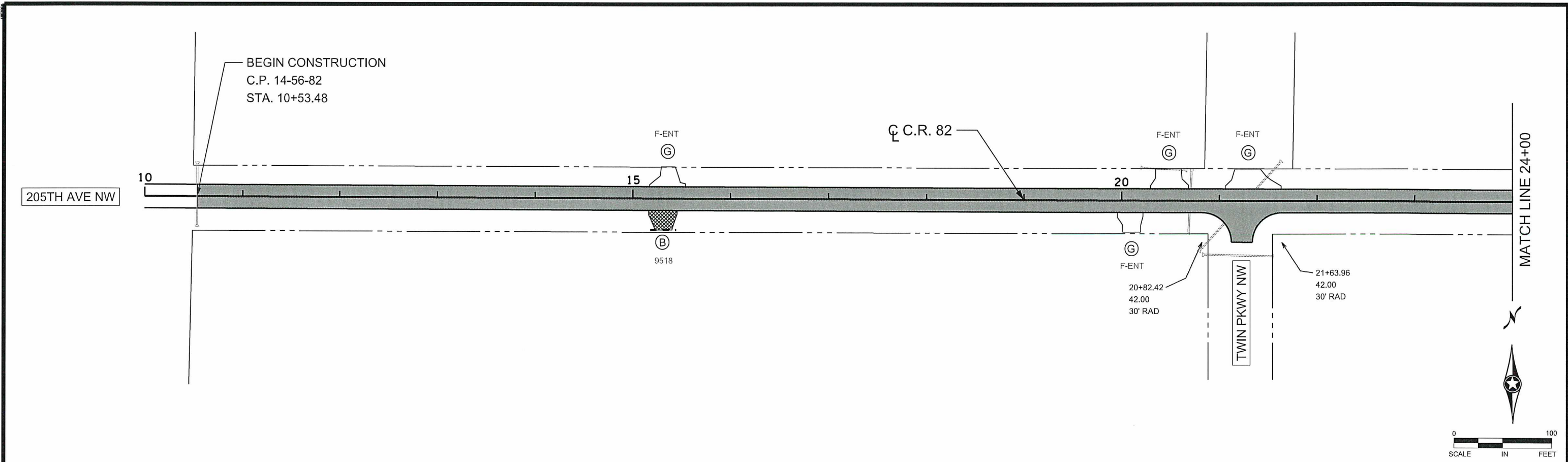


**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 14-56-82

MISCELLANEOUS DETAILS
 Sheet 5 of 13 Sheets

NO	DATE	BY	CKD	APPR	REVISION	
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- CONSTRUCTION NOTES:**
- (G) GRAVEL ENTRANCE
 - (B) BITUMINOUS ENTRANCE
 - MAINLINE RECLAIM
 - BITUMINOUS REMOVAL
 - SAWCUT
 - EXISTING R / W

NO	DATE	BY	CKD	APPR	REVISION	TIME
	04/10/2014					7:16:05 AM

NAME: P:\14-01-00\CR_82_(WestCL-CR65)\Base\PROPOSED\NO RTL\2_PLAN LAYOUT_NO_RTL.dgn

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PRINT NAME: ANDREW WITTEK

SIGNATURE: *Andrew Wittek*

DATE: 4/20/14 LICENSE NO. 42757

DRAWN BY RT DATE 04/10/2014

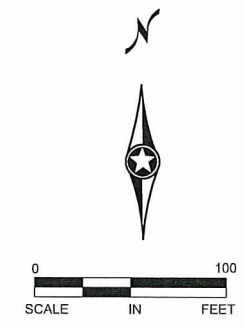
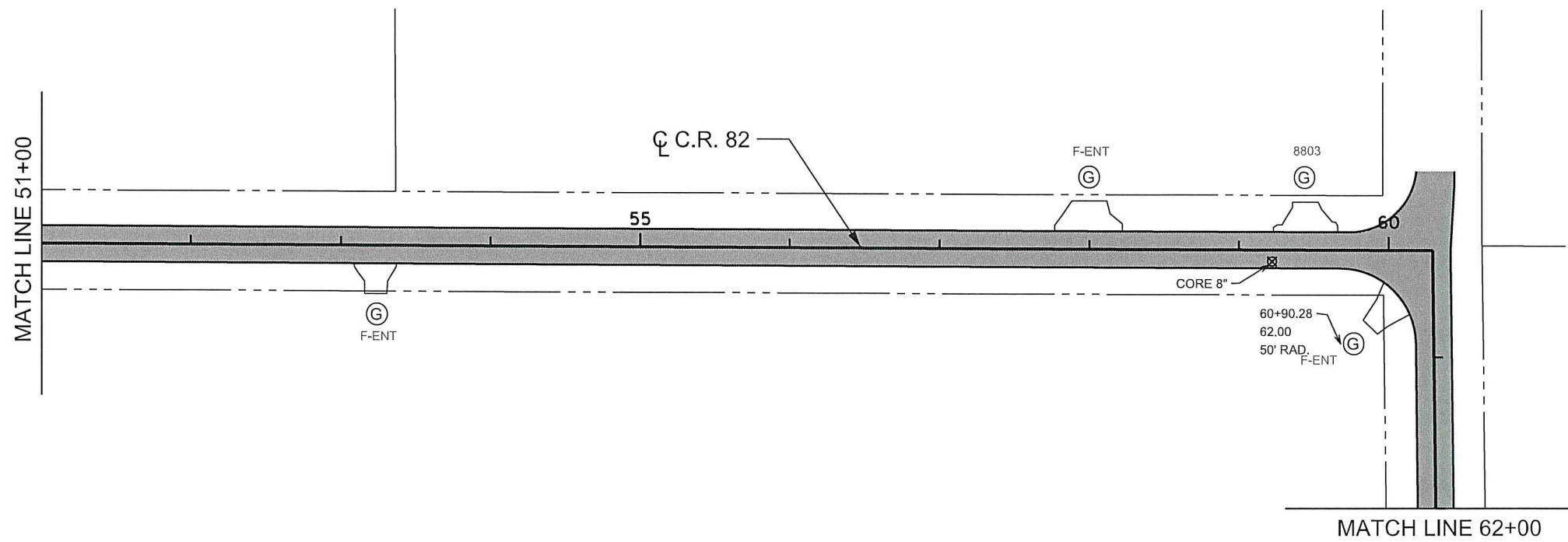
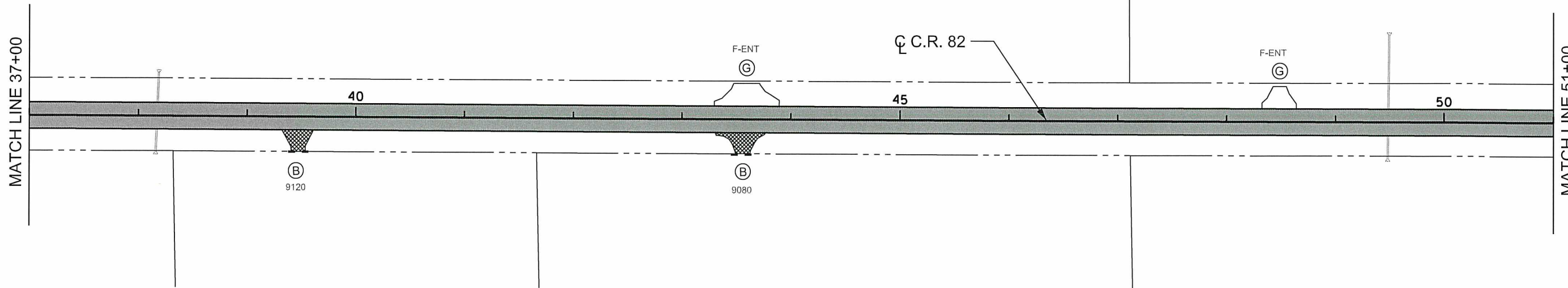
DESIGN BY RT DATE 04/10/2014

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ANOKA COUNTY
HIGHWAY DEPT.

COUNTY PROJECT 14-56-82

CONSTRUCTION PLAN
STA 10+53.48 TO 37+00
Sheet 6 of 13 Sheets



- CONSTRUCTION NOTES:**
- (G) GRAVEL ENTRANCE
 - (B) BITUMINOUS ENTRANCE
 - MAINLINE RECLAIM
 - BITUMINOUS REMOVAL
 - SAWCUT
 - EXISTING R / W

NO	DATE	BY	CKD	APPR	REVISION	DATE	TIME
						04/10/2014	7:16:06 AM

NAME: P:\14-01-00\CR_82_(WestCL-CR65)\Base\PROPOSED\NO RTL\2_PLAN LAYOUT_NO_RTL.dgn

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SIGNATURE: *Andrew Witter*

DATE: 4/23/14 LICENSE NO. 42757

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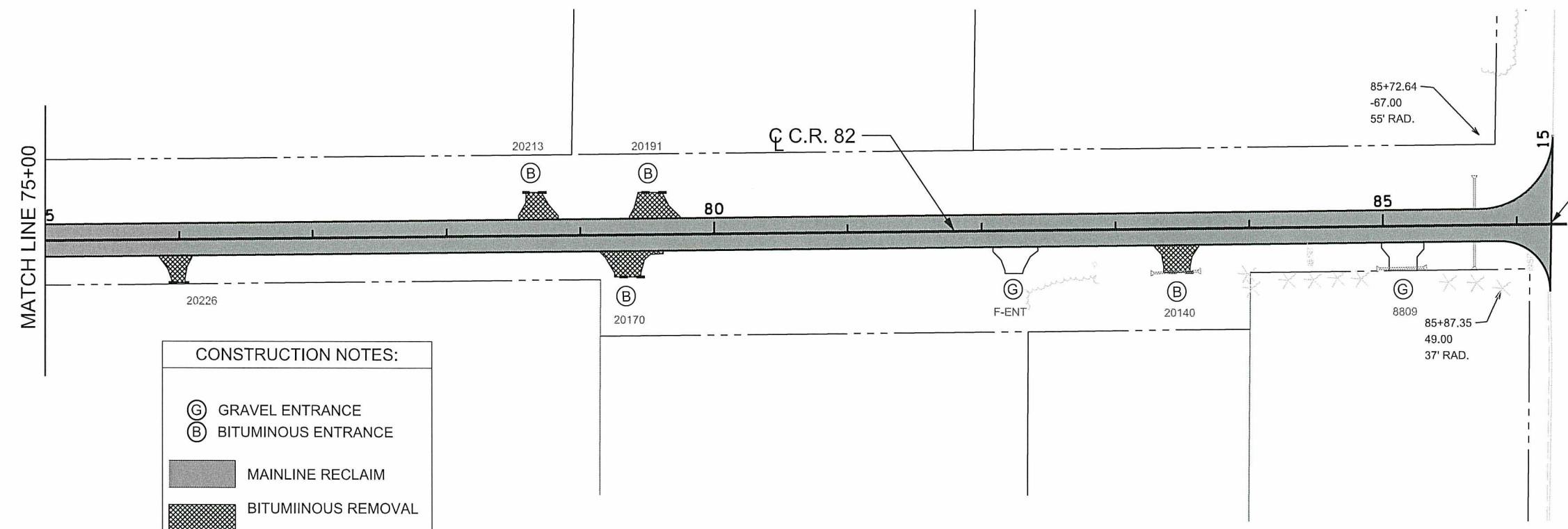
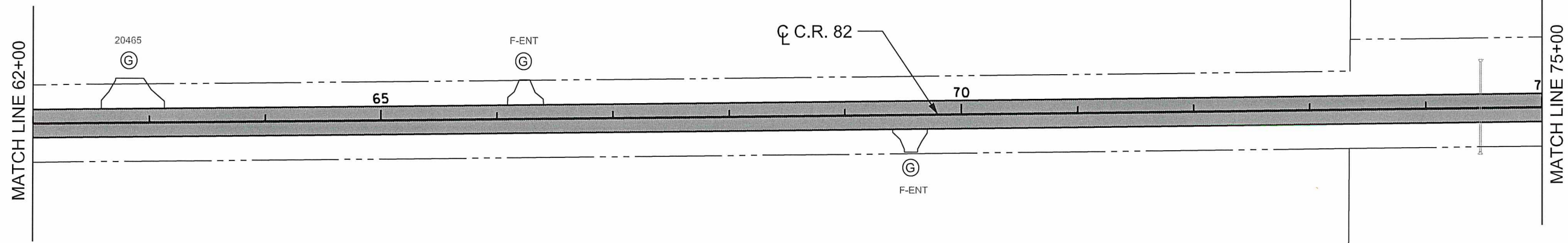
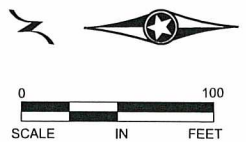
**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 14-56-82

CONSTRUCTION PLAN

STA 37+00 TO 62+00

Sheet 7 of 13 Sheets



CONSTRUCTION NOTES:

- (G) GRAVEL ENTRANCE
- (B) BITUMINOUS ENTRANCE
- MAINLINE RECLAIM
- BITUMINOUS REMOVAL
- SAWCUT
- EXISTING R / W

END CONSTRUCTION
C.P. 14-56-82
STA. 86+25.28

NO	DATE	BY	CKD	APPR	REVISION	04/10/2014	7:16:06 AM

NAME: P:\114-01-00\CR_82_(West\CL-CR65)\Base\PROPOSED\NO RTL\2_PLAN LAYOUT_NO_RTL.dgn

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**ANOKA COUNTY
HIGHWAY DEPT.**

COUNTY PROJECT 14-56-82

CONSTRUCTION PLAN
STA 62+00 TO 86+25.28
Sheet 8 of 13 Sheets

PERMANENT PAVEMENT MARKING PLAN
NOTES AND GUIDELINES

GENERAL INFORMATION:

THE ENGINEER'S INVOLVEMENT IN THE APPLICATION OF THE MATERIAL SHALL BE LIMITED TO FIELD CONSULTATION AND INSPECTION. ANOKA COUNTY HIGHWAY DEPARTMENT WILL PLACE NECESSARY "SPOTTING" AT APPROPRIATE POINTS TO PROVIDE HORIZONTAL CONTROL FOR STRIPING AND TO DETERMINE NECESSARY STARTING AND CUTOFF POINTS, LONGITUDINAL JOINTS, PAVEMENT EDGES AND EXISTING MARKINGS MAY SERVE AS HORIZONTAL CONTROL WHEN SO DIRECTED.

EDGE LINES AND LANE LINES ARE TO BE BROKEN ONLY AT INTERSECTIONS WITH PUBLIC ROADS AND AT PRIVATE ENTRANCES IF THEY ARE CONTROLLED BY A YIELD SIGN, STOP SIGN OR TRAFFIC SIGNAL. THE BREAK POINT IS TO BE AT THE START OF THE RADIUS FOR THE INTERSECTION OR AT MARKED STOP LINES OR CROSSWALKS.

A TOLERANCE OF 1/4 INCH UNDER OR 1/4 INCH OVER THE SPECIFIED WIDTH WILL BE ALLOWED FOR STRIPING PROVIDED THE VARIATION IS GRADUAL AND DOES NOT DETRACT FROM THE GENERAL APPEARANCE. BROKEN LINE SEGMENTS MAY VARY UP TO ONE-HALF FOOT FROM THE SPECIFIED LENGTHS PROVIDED THE OVER AND UNDER VARIATIONS ARE REASONABLY COMPENSATORY. ALIGNMENT DEVIATIONS FROM THE CONTROL GUIDE SHALL NOT EXCEED 1 INCH. MATERIAL SHALL NOT BE APPLIED OVER LONGITUDINAL JOINTS, ESTABLISHMENT OF APPLICATION TOLERANCES SHALL NOT RELIEVE THE CONTRACTOR OF THEIR RESPONSIBILITY TO COMPLY AS CLOSELY AS PRACTICABLE WITH THE PLANNED DIMENSIONS.

EPOXY:

THE ROAD SURFACE SHALL BE CLEANED AT THE DIRECTION OF THE ENGINEER JUST PRIOR TO APPLICATION. PAVEMENT CLEANING SHALL CONSIST OF AT LEAST BRUSHING WITH A ROTARY BROOM (NON-METALLIC) OR AS RECOMMENDED BY THE MATERIAL MANUFACTURER AND ACCEPTABLE TO THE ENGINEER. NEW PORTLAND CEMENT CONCRETE SURFACES SHALL BE SANDBLAST CLEANED TO REMOVE ANY SURFACE TREATMENT AND/OR LAITANCE ON LOW SPEED (SPEED LIMIT 35 MPH OR LESS) URBAN PORTLAND CEMENT CONCRETE ROADWAYS. SANDBLAST CLEANING SHALL BE USED FOR ALL EPOXY PAVEMENT MARKINGS.

THE EPOXY MARKING APPLICATION SHALL IMMEDIATELY FOLLOW THE PAVEMENT CLEANING. GLASS BEANS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE EPOXY RESIN LINE TO PROVIDE AN IMMEDIATE NO-TRACK SYSTEM.

AN EPOXY RESIN LINE 4" WIDE AND 15 MILL THICKNESS (WET), REQUIRES AN APPLICATION RATE OF ONE (1) GALLON OF COMPONENTS FOR 320 FEET OF LINE. GLASS BEADS SHALL BE APPLIED AT A POUND PER GALLON RATE SUFFICIENT TO ACHIEVE AN ACCEPTABLE NO-TRACK SYSTEM.

OPERATIONS SHALL BE CONDUCTED ONLY WHEN THE ROAD PAVEMENT SURFACE TEMPERATURES ARE 50 DEGREES FAHRENHEIT OR GREATER.

PERMANENT PAVEMENT MARKINGS SHALL NOT BE PLACED OVER TEMPORARY TAPE MARKINGS.

PREFORMED THERMOPLASTIC:

THE PREFORMED THERMOPLASTIC MARKINGS SHALL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS ON CLEAN AND DRY SURFACES. SEE SPECIAL PROVISIONS FOR PREFORMED THERMOPLASTIC MARKING SPECIFICATIONS.

PAINT:

AT THE TIME OF APPLYING THE MARKING MATERIAL, THE APPLICATION AREA SHALL BE FREE OF CONTAMINATION. THE CONTRACTOR SHALL CLEAN THE ROADWAY SURFACE PRIOR TO THE LINE APPLICATION IN A MANNER AND TO THE EXTENT REQUIRED BY THE ENGINEER.

GLASS BEADS SHALL BE APPLIED IMMEDIATELY AFTER APPLICATION OF THE PAINT LINE.

EXCEPT WHEN USED AS A TEMPORARY MARKING, PAVEMENT MARKINGS SHALL ONLY BE APPLIED IN SEASONABLE WEATHER WHEN AIR TEMPERATURE IS 50 DEGREES FARHENHEIT OR HIGHER AND SHALL NOT BE APPLIED WHEN THE WIND OR OTHER CONDITIONS CAUSE A FILM OR DUST TO BE DEPOSITED ON THE PAVEMENT SURFACE AFTER CLEANING AND BEFORE THE MARKING MATERIAL CAN BE APPLIED.

THE FILLING OF TANKS, POURING OF MATERIALS OR CLEANING OF EQUIPMENT SHALL NOT BE PERFORMED ON UNPROTECTED PAVEMENT SURFACES UNLESS ADEQUATE PROVISIONS ARE MADE TO PREVENT SPILLAGE OF MATERIAL.

PAVEMENT MARKING TABULATION		
ITEM	UNIT	TOTAL QUANTITY
4" SOLID LINE WHITE -EPOXY PAINT	LIN FT	15420
4" SOLID LINE YELLOW -EPOXY PAINT	LIN FT	3990
4" BROKEN LINE YELLOW - EPOXY PAINT	LIN FT	1530
4" DOUBLE YELLOW -EPOXY PAINT	LIN FT	0

SYMBOLS & MATERIALS LEGEND

■ CROSSWALK BLOCK WHITE PREFORMED THERMOPLASTIC

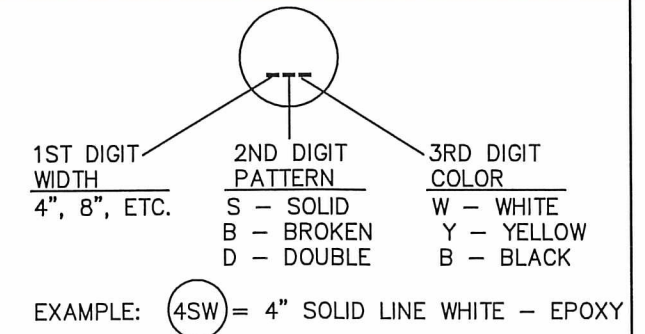
↩ PAVEMENT MESSAGE (LEFT ARROW) PREFORMED THERMOPLASTIC

STRIPING KEY

○ CIRCLE - EPOXY □ SQUARE PREFORMED THERMOPLASTIC

△ TRIANGLE - PAINT

⬠ PENTAGON - REMOVABLE PREFORMED PLASTIC MARKING



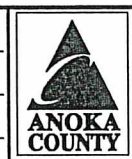
NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\13-01-00\CSAH 18\Bases\TRAFFIC\Perm pvmt mrkg guide notes_guidelines.dwg

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: ANDREW WITTER
 SIGNATURE: *[Signature]*
 DATE: 4/28/14 LICENSE NO. 42757

DRAWN BY: RLB DATE: 2/25/14
 DESIGN BY: RLB DATE: 2/25/14
 CHECKED BY: JR DATE: 2/25/14

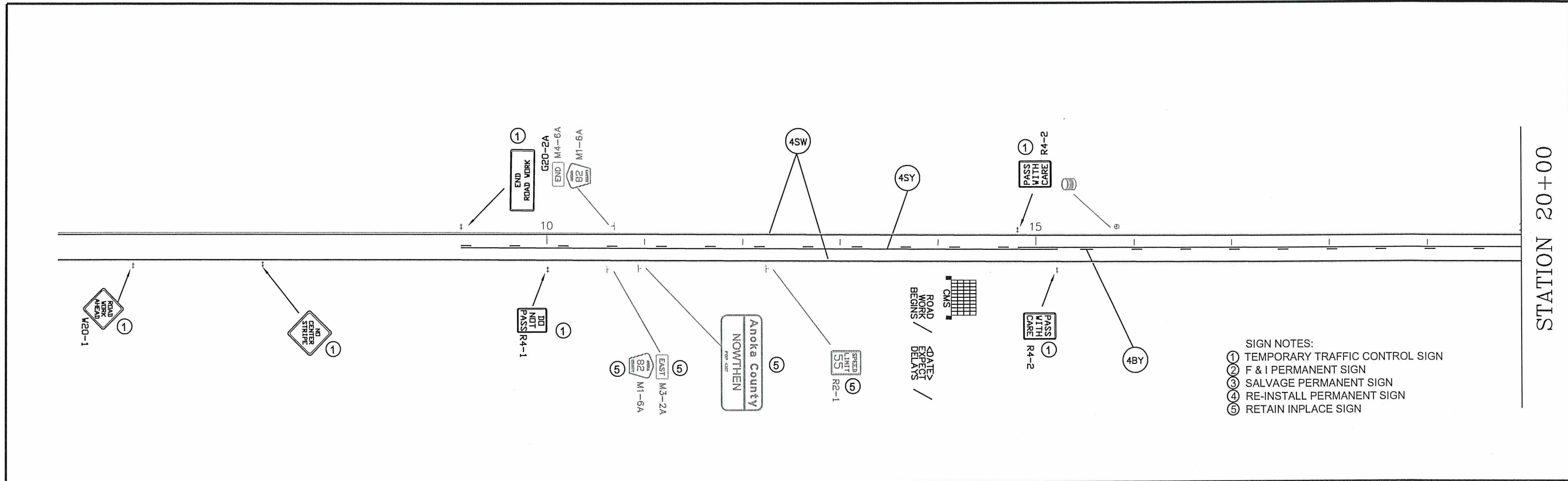


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 STATE AID PROJECT NO. _____
 COUNTY PROJECT NO. 14-56-82

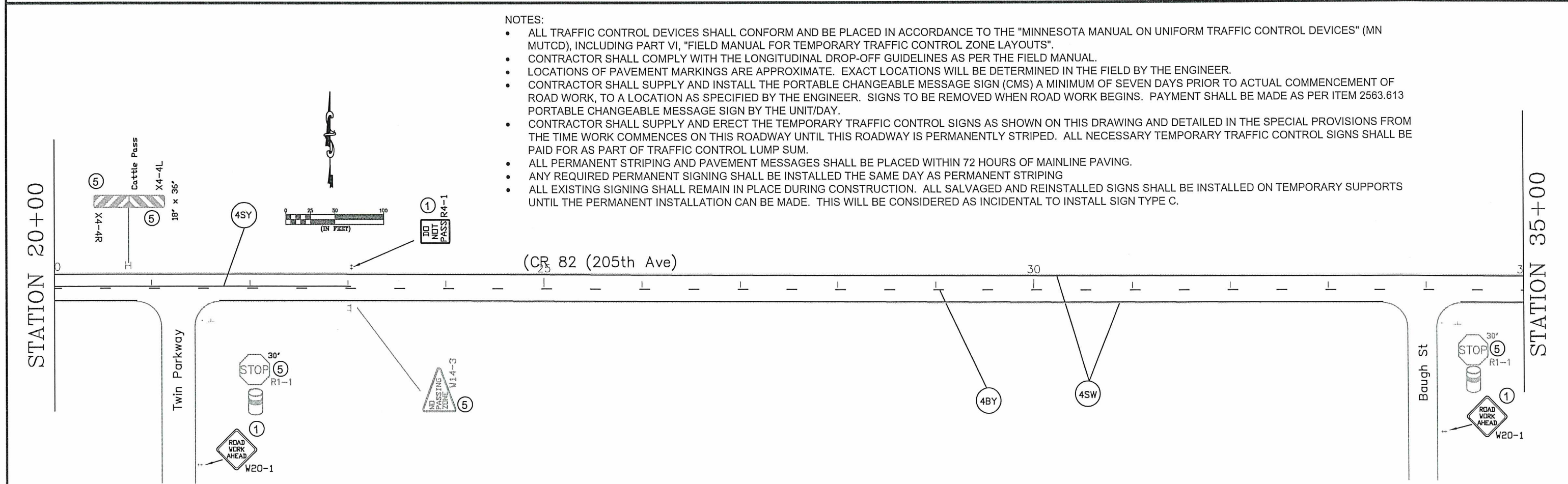
PERMANENT MARKING TABULATION

Sheet 9 of 13 Sheets



STATION 20+00

- SIGN NOTES:
- ① TEMPORARY TRAFFIC CONTROL SIGN
 - ② F & I PERMANENT SIGN
 - ③ SALVAGE PERMANENT SIGN
 - ④ RE-INSTALL PERMANENT SIGN
 - ⑤ RETAIN INPLACE SIGN



STATION 20+00

STATION 35+00

- NOTES:
- ALL TRAFFIC CONTROL DEVICES SHALL CONFORM AND BE PLACED IN ACCORDANCE TO THE "MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MN MUTCD), INCLUDING PART VI, "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS".
 - CONTRACTOR SHALL COMPLY WITH THE LONGITUDINAL DROP-OFF GUIDELINES AS PER THE FIELD MANUAL.
 - LOCATIONS OF PAVEMENT MARKINGS ARE APPROXIMATE. EXACT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
 - CONTRACTOR SHALL SUPPLY AND INSTALL THE PORTABLE CHANGEABLE MESSAGE SIGN (CMS) A MINIMUM OF SEVEN DAYS PRIOR TO ACTUAL COMMENCEMENT OF ROAD WORK, TO A LOCATION AS SPECIFIED BY THE ENGINEER. SIGNS TO BE REMOVED WHEN ROAD WORK BEGINS. PAYMENT SHALL BE MADE AS PER ITEM 2563.613 PORTABLE CHANGEABLE MESSAGE SIGN BY THE UNIT/DAY.
 - CONTRACTOR SHALL SUPPLY AND ERECT THE TEMPORARY TRAFFIC CONTROL SIGNS AS SHOWN ON THIS DRAWING AND DETAILED IN THE SPECIAL PROVISIONS FROM THE TIME WORK COMMENCES ON THIS ROADWAY UNTIL THIS ROADWAY IS PERMANENTLY STRIPED. ALL NECESSARY TEMPORARY TRAFFIC CONTROL SIGNS SHALL BE PAID FOR AS PART OF TRAFFIC CONTROL LUMP SUM.
 - ALL PERMANENT STRIPING AND PAVEMENT MESSAGES SHALL BE PLACED WITHIN 72 HOURS OF MAINLINE PAVING.
 - ANY REQUIRED PERMANENT SIGNING SHALL BE INSTALLED THE SAME DAY AS PERMANENT STRIPING
 - ALL EXISTING SIGNING SHALL REMAIN IN PLACE DURING CONSTRUCTION. ALL SALVAGED AND REINSTALLED SIGNS SHALL BE INSTALLED ON TEMPORARY SUPPORTS UNTIL THE PERMANENT INSTALLATION CAN BE MADE. THIS WILL BE CONSIDERED AS INCIDENTAL TO INSTALL SIGN TYPE C.

NO	DATE	BY	CKD	APPR	REVISION

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 PRINT NAME: ANDREW WITTER
 SIGNATURE: *Andrew Witter*
 DATE: 4/28/14 REG. NO. 42757

DRAWN BY: RLB DATE: 4/7/14
 DESIGN BY: RLB DATE: 4/7/14
 CHECKED BY: JR DATE: 4/7/14

ANOKA COUNTY
HIGHWAY DEPT.

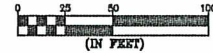
STATE PROJECT NO. _____
 STATE AID PROJECT NO. _____
 STATE AID PROJECT NO. _____
 COUNTY PROJECT NO. 14-56-82

TEMPORARY SIGNING,
 PERMANENT SIGNING
 AND STRIPING
 Sheet 10 of 13 Sheets

- SIGN NOTES:
- ① TEMPORARY TRAFFIC CONTROL SIGN
 - ② F & I PERMANENT SIGN
 - ③ SALVAGE PERMANENT SIGN
 - ④ RE-INSTALL PERMANENT SIGN
 - ⑤ RETAIN INPLACE SIGN

STATION 35+00

(CR 82 (205th Ave))



40

45

5

4BY

4SW

NOTES:

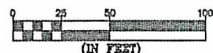
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STATION 50+00

STATION 50+00

(CR 82 (205th Ave))

(CR 82 (205th Ave))



55

60

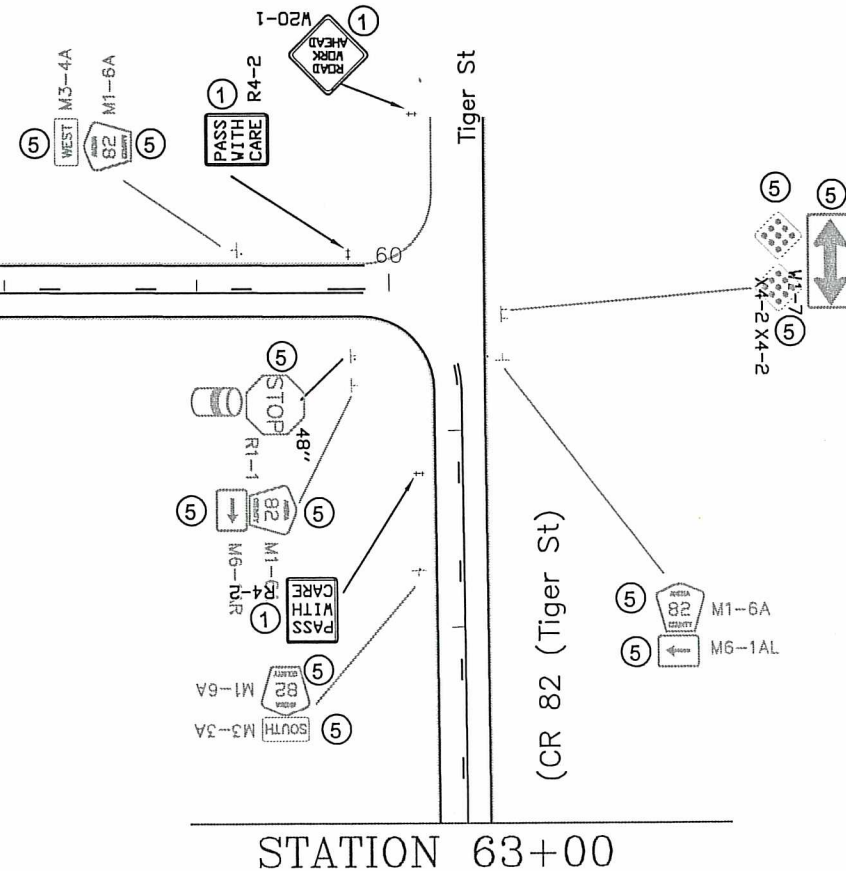
48"

4SY

X4-13

4BY

4SW



STATION 63+00

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
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 SIGNATURE: *Andrew Witter*
 DATE: 4/28/14 REG. NO. 42757

DRAWN BY: RLB DATE: 4/7/14
 DESIGN BY: RLB DATE: 4/7/14
 CHECKED BY: JR DATE: 4/7/14



ANOKA COUNTY
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 COUNTY PROJECT NO. 14-56-82

TEMPORARY SIGNING,
 PERMANENT SIGNING
 AND STRIPING

Sheet 11 of 13 Sheets

TEMPORARY TRAFFIC CONTROL SIGNS						
M.U.T.C.D. CODE	SIZE	PANEL AREA FT. ²	INSERT	QUANTITY		MOUNTING HEIGHT TO PAVEMENT (EXC) FT.
					No. POST	
W8-12	48" x 48"	16.00		2	2	7.0'
R4-1	24" x 30"	5.00		4	1	7.0'
R4-2	24" x 30"	5.00		5	1	7.0'
G20-2A	48" x 24"	8.00		3	2	7.0'
W8-1A	48" x 48"	16.00		AS NEEDED		
W8-1A	48" x 48"	16.00		AS NEEDED		
W8-8	48" x 48"	16.00		AS NEEDED		
W8-9	48" x 48"	16.00		AS NEEDED		
	48" x 48"	16.00		AS NEEDED		
W8-11	48" x 48"	16.00		AS NEEDED		
W20-1	48" x 48"	16.00		AS NEEDED (ESTIMATED 6)		
CMS sign to be installed a minimum of seven days prior to actual commencement of road work. Signs to be removed when road work begins.				2		

NOTES:

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CHANGEABLE MESSAGE BOARD - MESSAGE SEQUENCE LAYOUT

		R	O	A	D		
		W	O	R	K		
		B	E	G	I	N	S

	<	D	A	T	E	>	
		E	X	P	E	C	T
		D	E	L	A	Y	S

CMS sign to be installed a minimum of seven days prior to actual commencement of road work. Signs to be removed when road work begins.

NO	DATE	BY	CKD	APPR	REVISION

NAME: P:\02-617-19\base\Traffic\Curves from CSAH 18 to CSAH 22.dwg

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TRAFFIC CONTROL
 QUANTITY
 Sheet 13 of 13 Sheets