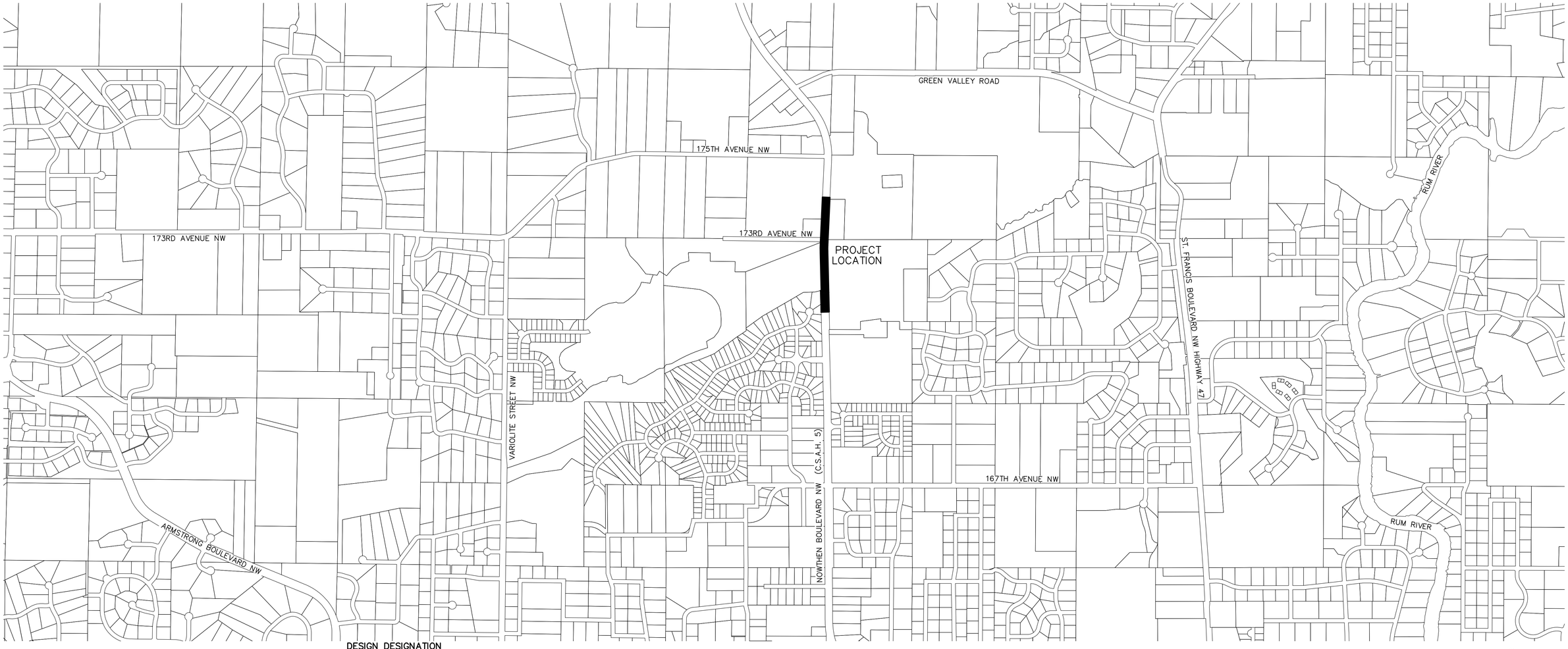
TROTT BROOK CROSSING THIRD ADDITION

CONSTRUCTION PLANS FOR TURN LANES INCLUDING; GRADING, STORM SEWER, STREETS, SIGNING AND STRIPING

RAMSEY, MINNESOTA



DESIGN DESIGNATION

~ NOTE: PROPOSED TURN LANE MATCHES THE EXISTING ROAD SECTION ~

$ESAL_{20}$ =	MATCH EXISTING TRAFFIC
R VALUE =	MATCH EXISTING
CURRENT ADT (2023) =	7177
PROJ. ADT (2043) =	
PROJ. HCADT (2035) =	
SOIL FACTOR =	N/A
____ TON DESIGN	
FUNCTIONAL CLASSIFICATION	MINOR ARTERIAL

NO. OF TRAFFIC LANES 1 NO. OF PARKING LANES 0
DESIGN SPEED 55 MPH
BASED ON STOPPING SIGHT DISTANCE _____
HEIGHT OF EYE ____ HEIGHT OF OBJECT ____
DESIGN SPEED NOT ACHIEVED AT:
STA. ___–___ TO STA. ___–___ MPH ___–___

CALL BEFORE YOU DIG

811 Know what's below. Call before you dig.

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APPROVED: ANOKA COUNTY ENGINEER _____ DATE _____

SHEET INDEX

N

0 400 800 1600

(IN FEET)

BENCHMARKS

1. Anoka County Benchmark No. 3077 – Elevation 901.84 ft. (NAVD88)
2. Anoka County Benchmark No. 3078 – Elevation 867.97 ft. (NAVD88)

CARLSON ENGINEERING
ENGINEERING SURVEYING PLANNING
3890 PHEASANT RIDGE DR NE
SUITE 100
BLAINE, MN 55449
TEL 763.489.7900
FAX 763.489.7959
CARLSON-ENGINEERING.COM

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Print Name: Aaron D. Briski, P.E.
Signature: Aaron Briski
Date: 6/6/2025 License #: 57811

Drawn: GJS
Designed: ADB
Date: 6/6/2025

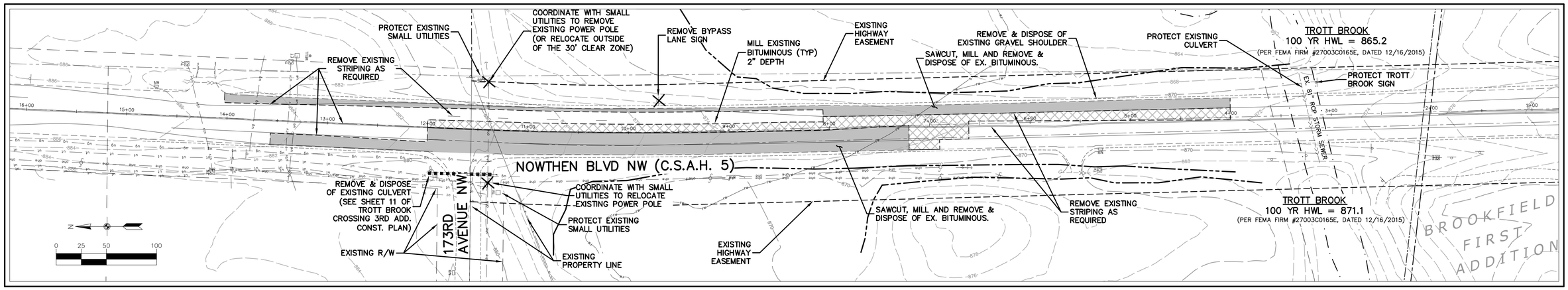
Revisions:
1. 7/17/25 per ACHD Comments
2. 9/12/25 per ACHD Comments
3. 10/29/25 per ACHD Comments
4. 12/31/25 per ACHD Comments

TWIN CITIES LAND DEVELOPMENT
4800 Olson Memorial Highway, Suite 100
Golden Valley, Minnesota 55422

TROTT BROOK CROSSING
Ramsey, Minnesota

COVER SHEET

T1 of T7



REMOVALS LEGEND

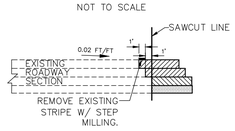
	EXISTING	REMOVALS
PROPERTY LINE		
EASEMENT LINE		
SAWCUT		
GRAVEL EDGE		FULL DEPTH / MILL
BITUMINOUS		
CONCRETE		
SANITARY SEWER		
STORM SEWER		
WATER MAIN		
OVERHEAD UTILITY		
UNDERGROUND TELEPHONE		
UNDERGROUND FIBEROPTIC		
UNDERGROUND GAS		
SIGN		
STORM CATCH BASIN		
MANHOLE		
HYDRANT		
GATE VALVE		
TELEPHONE BOX		
UTILITY POLE		
FENCE		
STRIPING		
10' CONTOUR		
2' CONTOUR		
WETLAND LINE		
SPOT ELEVATION		
TREE		

* CONTRACTOR TO VERIFY UNDERGROUND UTILITIES PRIOR TO COMMENCEMENT OF ANY EXCAVATION.

COORDINATE WITH UTILITY COMPANIES FOR RELOCATION OF ANY UNDERGROUND UTILITIES UNDER THE PROPOSED ROAD.

SAWCUT & MILLING DETAIL

NOT TO SCALE



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TROTT BROOK CROSSING
Ramsey, Minnesota

REMOVALS PLAN

T2 of T7

Save Date: 12/17/25 | F:\Jobs\9461 - 9480\9461 - trott brook\cad c3d\engineering\final plans\utility\turn lane\9461_turn lanes.dwg

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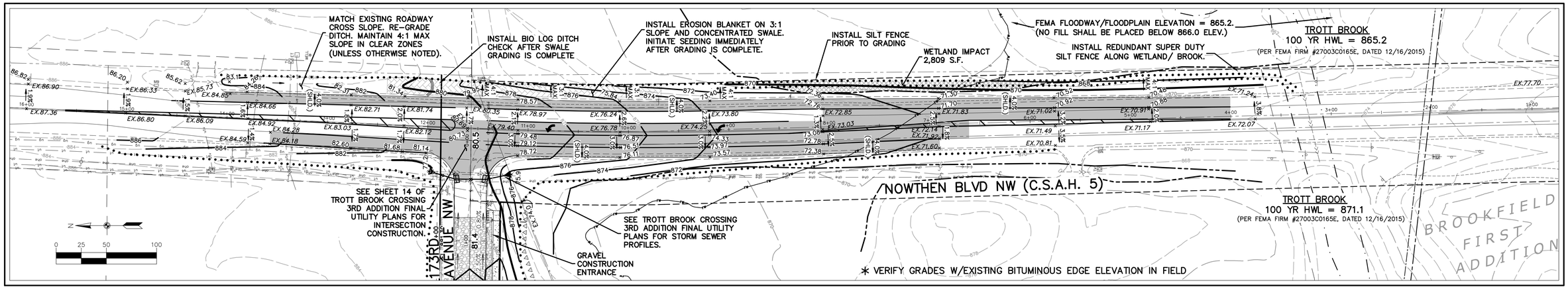


TURF ESTABLISHMENT

TURF ESTABLISHMENT SHALL APPLY TO ALL DISTURBED AREAS AND SHALL BE ACCORDING TO MnDOT STANDARD SPECIFICATIONS FOR CONSTRUCTION (LATEST EDITION). EXCEPT AS MODIFIED BELOW.

TURF ESTABLISHMENT SHALL OCCUR AS SOON AS POSSIBLE BUT IN NO CASE MORE THAN 7 DAYS.

SEED: MnDOT MIXTURE 25-141 AT 59 POUNDS PER ACRE.

DORMANT SEED: SHALL BE APPLIED AT TWICE THE NORMAL RATE AFTER NOVEMBER 1ST.

*MULCH: TYPE 1 AT 2 TONS PER ACRE (DISK ANCHORED).
*EROSION BLANKET TO BE USED ON STEEPER SLOPES AND CONCENTRATED SWALES.

FERTILIZER: TYPE 1 10-10-10 AT 200 POUNDS PER ACRE.

WETLAND SUMMARY

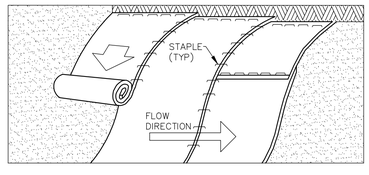
WETLAND FILL = 2,809 SF

NOTE: WETLAND FILL TOTAL IS LESS THAN THE DEMINIMUS AMOUNT; NO WETLAND REPLACEMENT WILL BE REQUIRED

GRADING LEGEND

	EXISTING	PROPOSED
PROPERTY LINE		
EASEMENT LINE		
CURB LINE		
GRAVEL EDGE		
BITUMINOUS		
CONCRETE		
SANITARY SEWER		
STORM SEWER		
WATER MAIN		
OVERHEAD UTILITY	ohe	
UNDERGROUND TELEPHONE	ut	ut
UNDERGROUND FIBEROPTIC	ufo	
UNDERGROUND GAS	ug	ug
STORM CATCH BASIN		
MANHOLE	SAN MH	
HYDRANT	HYD	
GATE VALVE	GV	
TELEPHONE BOX		
UTILITY POLE		PP
FENCE	x x	
10' CONTOUR		
2' CONTOUR		
WETLAND LINE		
SPOT ELEVATION	00.0	00.0
SIGN		
SILT FENCE		
BIO LOG (DITCH CHECK)		
TREELINE		
SOIL BORING	TH	
EROSION CONTROL BLANKET		

EROSION CONTROL BLANKET



NOTES:

1. PREPARE SOIL BEFORE INSTALLING BLANKETS, INCLUDING APPLICATION OF LIME, FERTILIZER AND/OR SEED.
2. BEGIN AT THE TOP OF THE SLOPE (OR CHANNEL) BY ANCHORING THE BLANKET IN A 6" DEEP X 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.
3. ROLL THE BLANKETS DOWN (STARTING DOWNSTREAM PROCEEDING UPSTREAM) HORIZONTALLY ACROSS THE SLOPE.
4. THE EDGES OF PARALLEL BLANKETS MUST BE STAPLED WITH A MINIMUM 4" OVERLAP.
5. WHEN BLANKETS MUST BE SPLICED DOWN THE SLOPE, PLACE BLANKETS END OVER END (SHINGLE STYLE) WITH MINIMUM 6" OVERLAP. USE A DOUBLE ROW OF STAGGERED STAPLES 4" APART TO SECURE BLANKETS.
6. IN HIGH FLOW CHANNEL APPLICATIONS, A STAPLE CHECK SLOT IS RECOMMENDED AT 30 TO 40 FOOT INTERVALS. USE A ROW OF STAPLES 4" APART OVER ENTIRE WIDTH OF THE CHANNEL. PLACE A SECOND ROW 4" BELOW THE FIRST ROW IN A STAGGERED PATTERN.
7. THE TERMINAL ENDS OF THE BLANKETS MUST BE ANCHORED IN A 6" DEEP X 6" WIDE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING.

GRADING & EROSION CONTROL NOTES

1. ALL GRADES ARE TO FINISHED PAVEMENT UNLESS OTHERWISE NOTED.
2. THE LATEST EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION 'STANDARD SPECIFICATIONS FOR CONSTRUCTION' SHALL GOVERN AS WELL AS THE CITY OF RAMSEY SPECIFICATIONS.
3. ALL TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNING SHALL CONFORM TO THE MMUTCD, INCLUDING "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS" LATEST EDITION
4. CONTRACTOR TO OBTAIN A PERMIT FROM ANOKA COUNTY HIGHWAY DEPARTMENT PRIOR TO PERFORMING ANY WORK WITHIN THE COUNTY RIGHT-OF-WAY.
5. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY UP GRADIENT LAND DISTURBING ACTIVITY.
6. MAX SLOPE SHALL BE 4:1 UNLESS OTHERWISE NOTED. EROSION CONTROL BLANKET TO BE INSTALLED ON 3:1 SLOPES (AS SHOWN).
7. TROTT BROOK IS AN IMPAIRED WATER FOR AN EPA-IMPAIRMENT FOR BENTHIC MACROINVERTEBRATES BIOASSESSMENTS; DISSOLVED OXYGEN; FISH BIOASSESSMENTS; SULFATE.
8. FEMA FLOODPLAIN/FLOOD WAY IS AT AN ELEVATION OF 865.2 ON THE EAST SIDE OF NOWTHEN BLVD (C.S.A.H. 5). NO FILL SHALL BE PLACED BELOW THE 866.0 ELEVATION.
9. INITIATE STABILIZATION ON ALL EXPOSED SOILS IMMEDIATELY AFTER CONSTRUCTION COMPLETE. STABILIZATION MUST BE COMPLETED WITHIN SEVEN (7) CALENDAR DAYS AFTER GRADING IN THAT PORTION IS COMPLETE.
10. CONTRACTOR TO OBTAIN A PERMIT FROM ANOKA COUNTY HIGHWAY DEPARTMENT PRIOR TO PERFORMING ANY WORK WITHIN THE COUNTY RIGHT-OF-WAY.
11. CONTRACTOR SHALL COORDINATE WORK AROUND AND RELOCATION OF SMALL UTILITIES.
12. CONTRACTOR TO REMOVE ANY TREES WITHIN THE 30' CLEAR ZONE WITHIN THE PROJECT BOUNDARY. FIELD VERIFY WITH COUNTY INSPECTOR.

BENCHMARKS

1. Anoka County Benchmark No. 3077 – Elevation 901.84 ft. (NAVD88)
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TROTT BROOK CROSSING
Ramsey, Minnesota

GRADING, DRAINAGE & EROSION CONTROL PLAN

T3 of T7

Save Date: 12/17/25 | F:\Jobs\9461 - 9480\9461 - trott brook\cad c3d\engineering\final plans\utility\turn lane\9461_turn lanes.dwg

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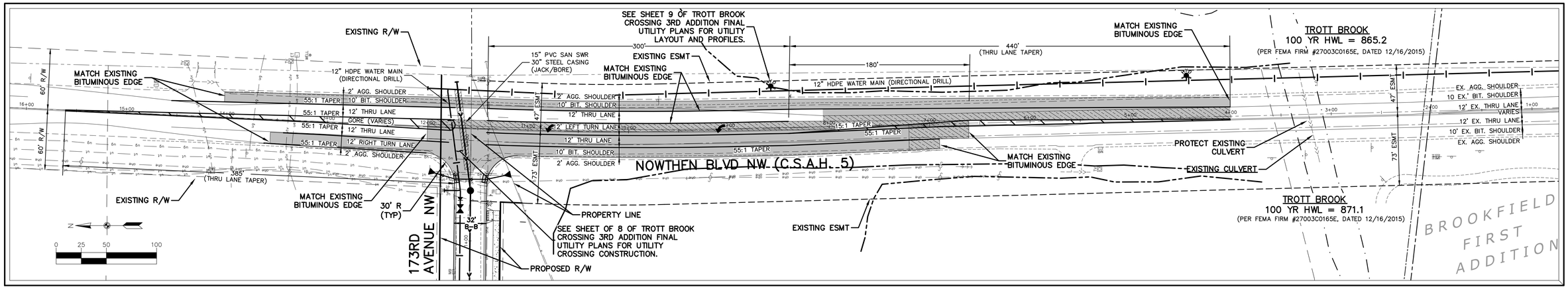

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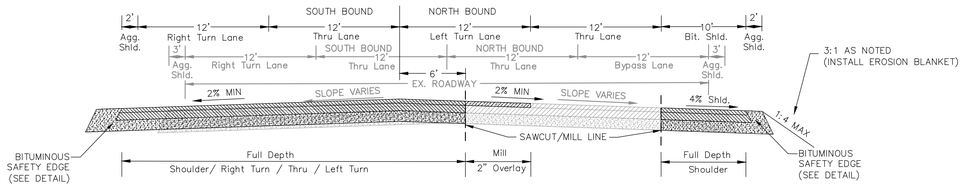
4. CONTRACTOR TO OBTAIN A PERMIT FROM ANOKA COUNTY HIGHWAY DEPARTMENT PRIOR TO PERFORMING ANY WORK WITHIN THE COUNTY RIGHT-OF-WAY.
5. ALL TURN LANE STRIPING SHALL BE COMPLETED USING EPOXY PAINT.
6. CONTRACTOR SHALL REMOVE ALL CONFLICTING PAVEMENT MARKINGS VIA SANDBLASTING OR WATERBLASTING (OR APPROVED EQUAL).
7. CONTRACTOR SHALL COORDINATE WORK AROUND AND RELOCATION OF SMALL UTILITIES.
8. CONTRACTOR SHALL COORDINATE ANY RELOCATION OF SMALL UTILITIES W/ ANOKA COUNTY HIGHWAY DEPARTMENT.
9. CONTRACTOR TO CONTACT ANOKA COUNTY HIGHWAY DEPARTMENT 3 BUSINESS DAYS PRIOR TO ANY CONSTRUCTION WITHIN THE RIGHT-OF-WAY.
10. TURN LANE CONSTRUCTION IS TO BE COORDINATED THROUGH ANOKA COUNTY HIGHWAY DEPARTMENT'S PERMITTING SECTION



CURB TYPES LEGEND

- BITUMINOUS EDGE W/ GRAVEL SHOULDER
- B618 CONCRETE CURB & GUTTER

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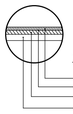
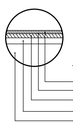


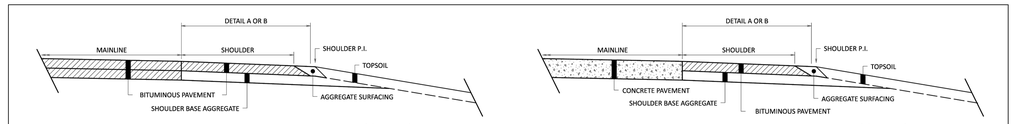
*EX. PAVEMENT SECTION
2"–TYPE 12.5 SPWEB440F BITUMINOUS WEAR COURSE
BITUMINOUS TACK COAT
2"–TYPE 12.5 SPNW440B BITUMINOUS BASE COURSE
RECLAIMED BASE

* CONTRACTOR TO VERIFY EXISTING STREET SECTION. MATCH EXISTING SECTION.



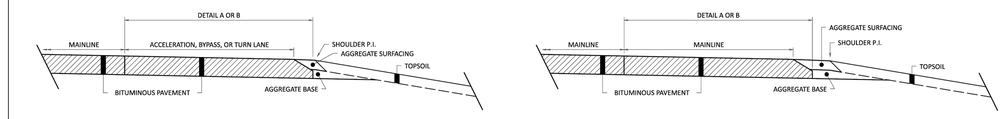
*PAVEMENT SECTION
2"–TYPE 12.5 SPWEB440F BITUMINOUS WEAR COURSE
BITUMINOUS TACK COAT
2"–TYPE 12.5 SPNW440B BITUMINOUS BASE COURSE
6"–CLASS 5 AGGREGATE BASE
APPROVED SUBGRADE

* CONTRACTOR TO VERIFY EXISTING STREET SECTION. MATCH EXISTING SECTION.



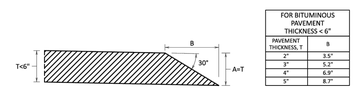
BITUMINOUS PAVEMENT WITH BITUMINOUS SHOULDERS (8' OR LESS)

CONCRETE PAVEMENT WITH BITUMINOUS SHOULDERS (8' OR LESS)



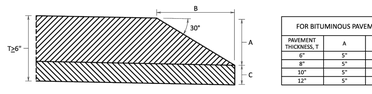
BITUMINOUS PAVEMENT WITH ACCELERATION LANES, BYPASS LANES OR TURN LANES

BITUMINOUS PAVEMENT WITH AGGREGATE SHOULDERING



DETAIL A
PAVEMENT THICKNESS <6"

FOR BITUMINOUS PAVEMENT THICKNESS < 6"	
PAVEMENT THICKNESS, T	B
2"	3.5"
3"	5.2"
4"	6.9"
5"	8.7"



DETAIL B
PAVEMENT THICKNESS ≥ 6"

FOR BITUMINOUS PAVEMENT THICKNESS ≥ 6"			
PAVEMENT THICKNESS, T	A	B	C
6"	5"	8.7"	1"
8"	5"	8.7"	3"
10"	5"	8.7"	5"
12"	5"	8.7"	7"

NOTES:

CONSTRUCT THE SAFETY EDGE ALONG ALL BITUMINOUS PAVEMENT EDGES ADJACENT TO AGGREGATE SURFACING. THIS INCLUDES:
- MAINLINE ROADWAYS
- SHOULDERS 8' WIDE OR LESS
- RAMPS AND LOOPS
- ACCELERATION, BYPASS AND TURN LANES

CONSTRUCT THE SAFETY EDGE USING A MANUFACTURED SHOE DEVICE ATTACHED TO THE PAVING MACHINE. A SINGLE-PLATE STRIKE-OFF METHOD IS NOT ALLOWED.

PROVIDE THE SAFETY EDGE ALONG THE ROADWAY THROUGH UNPAVED ENTRANCES SUCH AS FARM ACCESSES, UNPAVED DRIVEWAYS, AND GRAVEL ROAD ACCESSES. FOR PAVED PUBLIC ENTRANCES AND PAVED DRIVEWAYS, STOP THE SAFETY EDGE AND MATCH THE PROPOSED CONSTRUCTED PAVEMENT TO THE EXISTING CONDITIONS OR FOLLOW THE DESIGN PLANS. SHORT SECTIONS OF HANDWORK MAY BE NECESSARY FOR TRANSITIONS AND TURNOUTS.

SAFETY EDGE IS OPTIONAL FOR PAVED SHOULDER WIDTHS GREATER THAN 8'.

SEE TYPICAL SECTIONS FOR SAFETY EDGE PLACEMENT LOCATIONS.

THE SAFETY EDGE IS ADDED TO THE OUTSIDE OF THE REQUIRED BITUMINOUS TOP SURFACE WIDTH SHOWN IN THE PLANS.

LEAD EXPERT OFFICE	CURT TURGEON DIRECTOR OFFICE OF MATERIALS & ROAD RESEARCH	SAFETY EDGE BITUMINOUS PAVEMENT	APPROVED: 02-21-2024 REVISED:	THOMAS STYBICKI STATE DESIGN ENGINEER	STANDARD PLAN 5-297.220	2 OF 2
DEPARTMENT OF TRANSPORTATION		STANDARD PLAN		STATE PROJ. NO.	SHEET NO.	
				TRUNK HWY.	TOTAL SHEETS	

LEGEND

	EXISTING	PROPOSED
PROPERTY LINE		
EASEMENT LINE		
CURB LINE		
GRAVEL EDGE		
		FULL SECTION / OVERLAY
BITUMINOUS		
CONCRETE		
SANITARY SEWER		
STORM SEWER		
WATER MAIN		
OVERHEAD UTILITY		
UNDERGROUND TELEPHONE		
UNDERGROUND FIBEROPTIC		
UNDERGROUND GAS		
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WETLAND LINE		
SIGN		

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Ramsey, Minnesota

SITE & UTILITY PLAN

T4 of T7

Save Date: 12/17/25 | F:\Jobs\9461 - 9480\9461 - trott brook\cad c3d\engineering\final plans\utility\turn lane\9461_turn lanes.dwg

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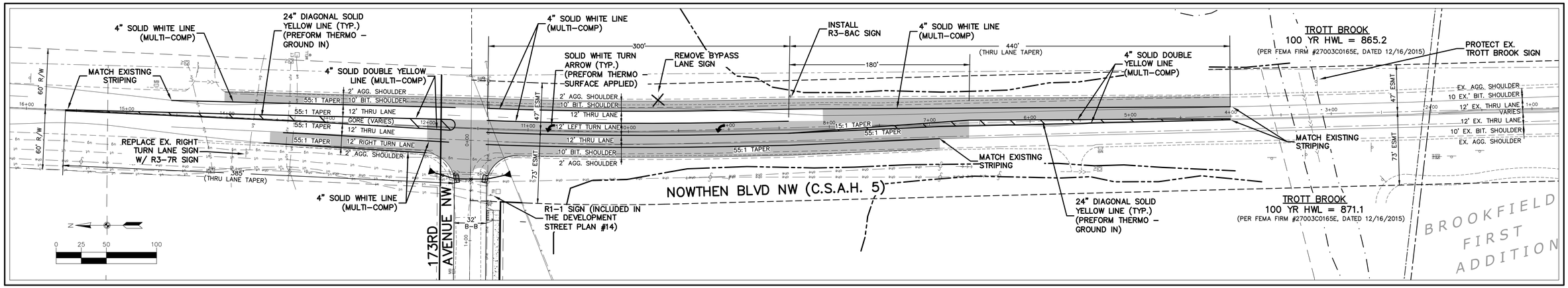

LEGEND

	EXISTING	PROPOSED
PROPERTY LINE		
EASEMENT LINE		
CURB LINE		
GRAVEL EDGE		
BITUMINOUS		
CONCRETE		
SANITARY SEWER		
STORM SEWER		
WATER MAIN		
OVERHEAD UTILITY		
UNDERGROUND TELEPHONE		
UNDERGROUND FIBEROPTIC		
UNDERGROUND GAS		
STORM CATCH BASIN		
MANHOLE		
HYDRANT		
GATE VALVE		
TELEPHONE BOX		
UTILITY POLE		
FENCE		
STRIPING		
WETLAND LINE		
SIGN		

SIGN SCHEDULE

SIGN	SIGN NO.	SIZE	QUANTITY
STOP	R1-1	30" X 30" (WHITE ON RED)	(INCLUDED IN SITE PLANS)
ONLY ONLY	R3-8AC	36" X 30" (BLACK ON WHITE)	1
RIGHT LANE MUST TURN RIGHT	R3-7R	30" X 30" (BLACK ON WHITE)	1

TURN LANE NOTES

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3. ALL TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNING SHALL CONFORM TO THE MMUTCD, INCLUDING "FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS" LATEST EDITION
4. CONTRACTOR TO OBTAIN A PERMIT FROM ANOKA COUNTY HIGHWAY DEPARTMENT PRIOR TO PERFORMING ANY WORK WITHIN THE COUNTY RIGHT-OF-WAY.
5. ALL TURN LANE STRIPING SHALL BE COMPLETED USING MULTI-COMP.
6. CONTRACTOR SHALL REMOVE ALL CONFLICTING PAVEMENT MARKINGS.
7. CONTRACTOR SHALL COORDINATE WORK AROUND AND RELOCATION OF SMALL UTILITIES.
8. CONTRACTOR SHALL COORDINATE RELOCATION OF FIBER INTERCONNECT/CONDUIT & HAND HOLES W/ ANOKA COUNTY HIGHWAY DEPARTMENT.
9. CONTRACTOR TO CONTACT ANOKA COUNTY HIGHWAY DEPARTMENT 3 BUSINESS DAYS PRIOR TO ANY CONSTRUCTION WITHIN THE RIGHT-OF-WAY.
10. APPLICABLE MNDOT STANDARD PLATES: 3022C, 4006L, 9000E, 4129G, 4154B, 4160D, 9102E

BENCHMARKS

1. Anoka County Benchmark No. 3077 – Elevation 901.84 ft. (NAVD88)
2. Anoka County Benchmark No. 3078 – Elevation 867.97 ft. (NAVD88)

CALL BEFORE YOU DIG

811 Know what's **below.** **Call** before you dig.

The subsurface utility information shown on this plan is utility Quality Level D. This quality level was determined according to the guidelines of CI/ASCE 38-02, entitled "Standard Guideline for the Collection and Depiction of Existing Subsurface Utility Data."

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I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota

Print Name: Aaron D. Briski, P.E.
Signature: Aaron Briski
Date: 6/6/2025 License #: 57811

Drawn: GJS
Designed: ADB
Date: 6/6/2025

Revisions:
1. 7/17/25 per ACHD Comments
2. 9/12/25 per ACHD Comments
3. 10/29/25 per ACHD Comments
4. 12/31/25 per ACHD Comments

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Golden Valley, Minnesota 55422

TROTT BROOK CROSSING
Ramsey, Minnesota

SIGNING & STRIPING PLAN

T5 of T7

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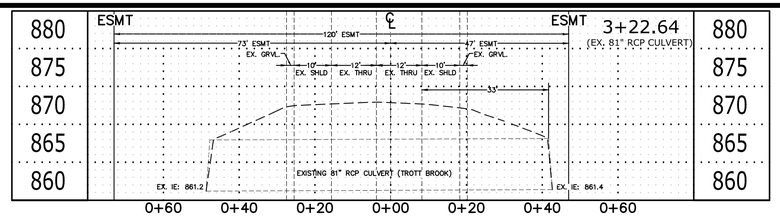

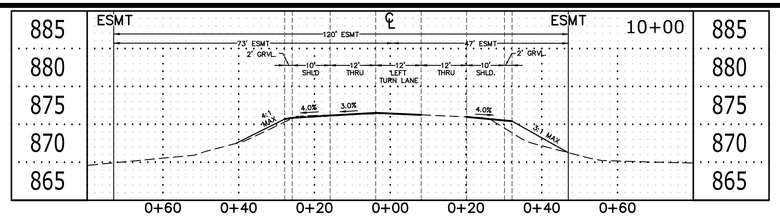

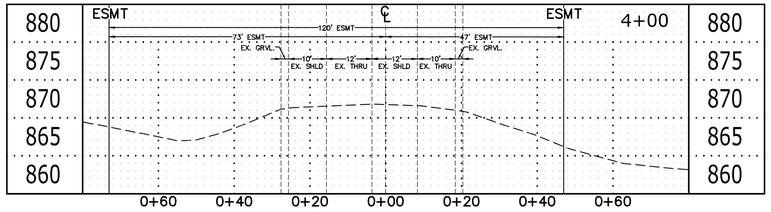

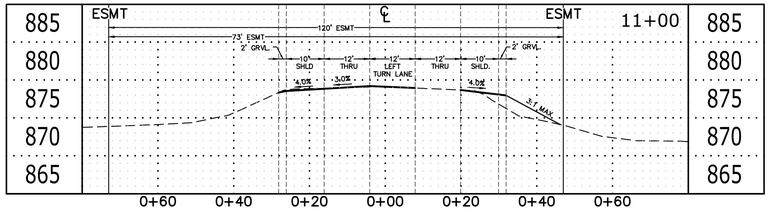

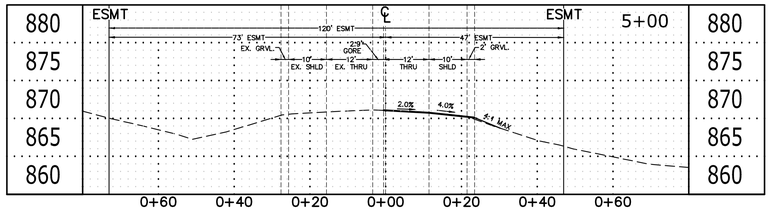

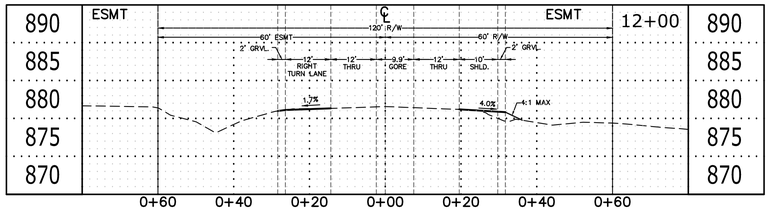

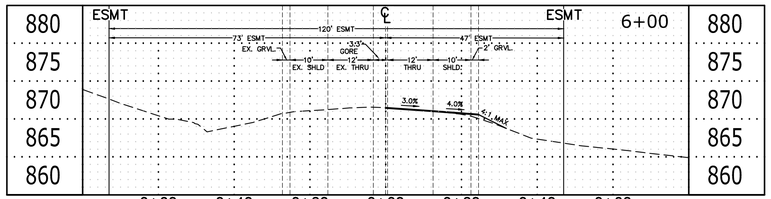

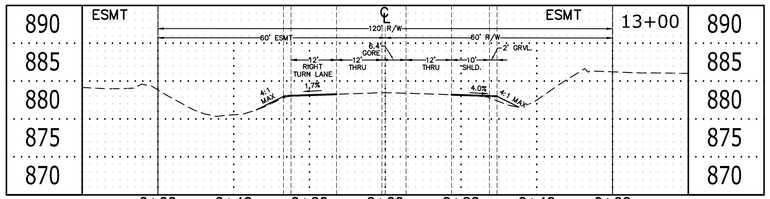

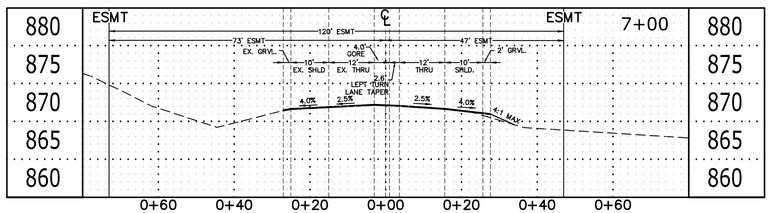

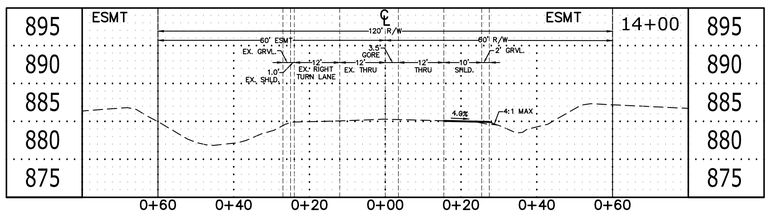

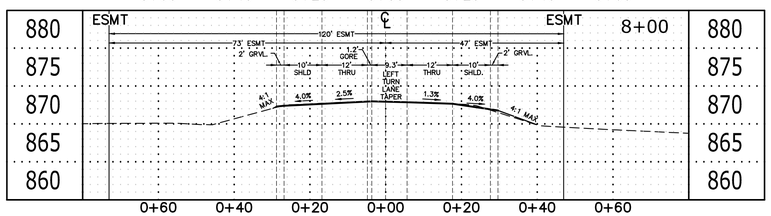

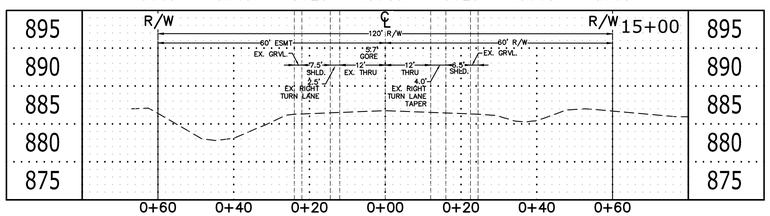

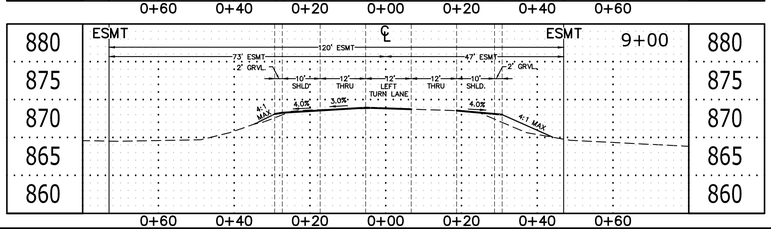

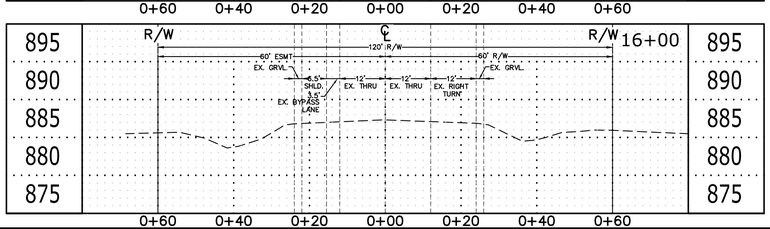

1" = 20'

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Signature: Aaron Briski
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Ramsey, Minnesota

CROSS SECTIONS

T6 of T7

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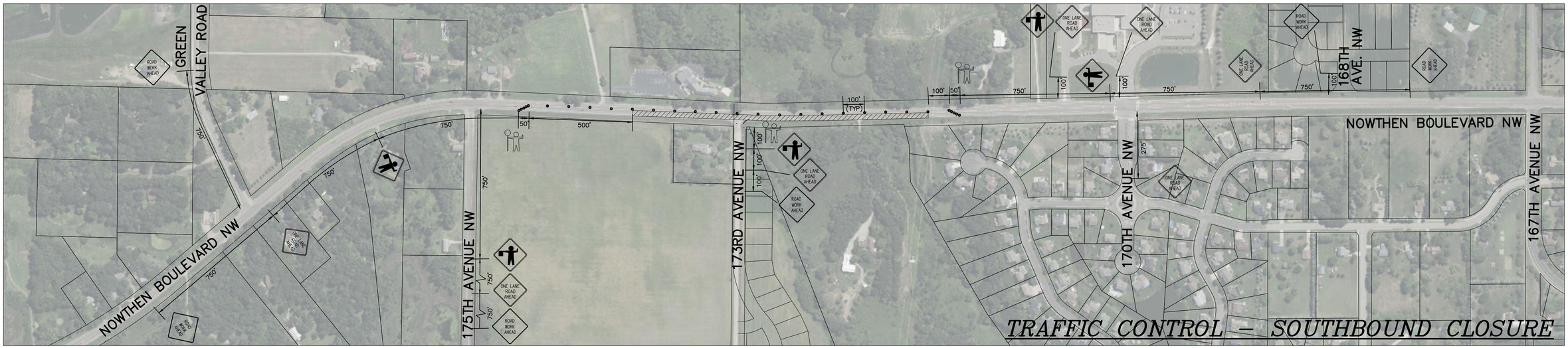
TRAFFIC CONTROL – NORTHBOUND CLOSURE



TRAFFIC CONTROL – SOUTHBOUND CLOSURE

SIGN SCHEDULE (NORTHBOUND CLOSURE)	
SIGN	QUANTITY
ROAD WORK AHEAD	5
ONE LANE ROAD AHEAD	8
	7

SIGN SCHEDULE (SOUTHBOUND CLOSURE)	
SIGN	QUANTITY
ROAD WORK AHEAD	6
ONE LANE ROAD AHEAD	7
	5

TRAFFIC CONTROL PLAN NOTES

1. BOTH LANES SHALL BE OPEN AT THE END OF DAY TO ALLOW TRAFFIC TO FLOW NORMALLY.
2. INTERMITTENT SIGNS SHALL BE USED FOR LOOSE GRAVEL, UNEVEN LANES, BUMP, ETC.
3. ONE LANE ROAD AHEAD, BE PREPARED TO STOP AND FLAGGER SIGN SHALL BE COVERED OR LAID DOWN WHEN FLAGGING OPERATION IS NOT IN PROGRESS.
4. FLAGGER OPERATIONS FOR ONE LANE ROAD ARE LIMITED TO TIMES OF 9AM TO 3PM MONDAY THROUGH FRIDAY.
5. ALL EDGE DROP OFFS MUST BE BACKFILLED OR RAMPED MEETING LONDITUDINAL DROP-OFF GUIDELINES MNDOT FIELD MANUAL FIGURE 6K-7 WHEN FLAGGERS ARE NOT PRESENT (J-BARRIER MIGHT BE NEEDED).
6. IF BARRIER IS REQUIRED CRASH SLEDS MUST BE USED. ANOKA COUNTY DOES NOT PERMIT SAND/WATER CRASH DRUMS/BARRELS.
7. IF ANY TRAFFIC CONTROL WILL BE LEFT UP FOR 72 HOURS OR MORE, AN OFFICIAL TRAFFIC CONTROL PLAN SIGNED BY A MN LICENSED PE WILL BE REQUIRED. IF THE CONTRACTOR DOING THE WORK WILL USE SOMETHING OTHER THAN THIS PLAN A NEW TRAFFIC CONTROL PLAN MUST BE SUBMITTED TO ACHD PRIOR TO ANY WORK TAKING PLACE.

LEGEND

	EXISTING	PROPOSED
PROPERTY LINE		
EASEMENT LINE		
CURB LINE		
BITUMINOUS		
CONCRETE		
CONSTRUCTION AREA		
TRAFFIC BARRELS		
SIGN		

N

0 125 250 500

(IN FEET)

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TRAFFIC CONTROL PLAN

T7 of T7

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