



ANOKA COUNTY TRANSPORTATION DIVISION

1440 BUNKER LAKE BLVD NW

ANDOVER, MN 55304

763-324-3176

highwaypermits@anokacountymn.gov

RIGHT OF WAY

COMMERCIAL ACCESS

APPLICATION FOR PERMIT FOR INSTALLATION OF UTILITIES OR PLACING OBSTRUCTIONS ON THE COUNTY HIGHWAY SYSTEM
ALL APPLICANTS MUST BE REGISTERED PRIOR TO PERMIT APPROVAL

APPLICANT NAME Wruck Excavating

CONTACT PERSON Kyle Pedersen

ADDRESS 15920 US HWY 10

CITY Becker

PHONE NUMBER 3203105205

EMAIL kyle@wruckcompanymn.com

COMPANY OR INDIVIDUAL PERFORMING WORK Wruck Excavating

CONTACT PERSON Tony Wruck

EMAIL Tony@wruckcompanymn.com

PHONE NUMBER 320-267-1855

PERMIT WORK TO START 11/01/2023

PERMIT WORK TO BE COMPLETED 11/30/2023

DURATION OF JOB 2 weeks

ARE YOU BEING ASKED TO RELOCATE DUE TO A COUNTY PROJECT? No

ANOKA COUNTY PROJECT NUMBER

WORK SITE ADDRESS 9730 University Ave

CITY Coon Rapids

METHOD OF INSTALLATION/CONSTRUCTION open cut

NATURE OF WORK Bring sewer and water into lot.

SURFACE TO BE DISTURBED

SITE PLAN

IF THE ROADWAY IS ENCROACHED, YOU MUST ATTACH A TRAFFIC CONTROL PLAN AND/OR REFERENCE THE MOST CURRENT VERSION OF THE MN TEMPORARY TRAFFIC CONTROL FIELD MANUAL (3+ DAYS REQUIRES PLANS TO BE SIGNED BY A LICENSED PE).

DITCH/BLVD



GRAVEL

X BITUMINOUS

X CONCRETE

NONE

IS SIGNING AND STRIPING REQUIRED? No

DEPTH FROM SURFACE 8ft

(60" minimum under county roads)

SIZE AND KIND OF PIPE/CABLE 6" PVC Sanitary, 6" Water service

NUMBER OF EXCAVATIONS 2

SIZE OF EXCAVATIONS 12ft x 16ft x 9ft

(Length, width, and depth)

LOCATION OF EXCAVATIONS ALL EXCAVATIONS ARE TO BE PROTECTED AT ALL TIMES AND THEN BACKFILLED WHEN UNATTENDED AND/OR OVERNIGHT

Specific written descriptions of excavations - if shown on attached drawing, drawing shall be specific with depth and distance from centerline, curb, or other distinguishable location. Traffic control plans cannot be approved without specific excavation descriptions.

Excavation #1: (Sanitary Tie in at Inv: 900.80) 12ft L x 16ft W x 8ft depth. MH is 6ft off curb line.

Excavation #2: (water tie in at Inv: 903.1) 12ft L x 16ft W x 8ft depth.

THIS PERMIT COVERS THE RIGHT OF WAY IN ANOKA COUNTY ONLY

ACTD reserves the right to make changes to these special conditions.



Anoka County

MINNESOTA

Respectful. Innovative. Fiscally Responsible

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GENERAL INFORMATION

One permit must be approved for each county road on which work will be performed prior to any work within the right of way by any utility/contractor. Emergency conditions which threaten the safety of the public and require immediate repair are exceptions to this rule. Under those circumstances, the utility/contractor is permitted to begin and/or complete the necessary repairs. The Anoka County Transportation Division (ACTD) shall be notified of emergency repairs as soon as feasible and a written permit is to be completed within two business days of occurrence.

A license-permit bond is generally required of the contractor as part of the registration process, the amount of which will be determined by the nature of the utility work.

A sketch or drawing shall accompany each permit application which will show the location of the proposed work/utility with reference to the county highway center line and right of way line. A complete set of plans is required for all sewer/water projects.

It shall be the responsibility of the applicant to determine which of the special conditions apply to each permit.

ACTD reserves the right to revoke any utility permit and halt work, if, upon inspection of any job site, the special conditions are not met, and/or a hazard exists for the applicant or public safety is threatened. **The failure to comply with the terms and conditions of any applicable Federal, State, Regional, and local laws, rules and regulations, including any provision of Anoka County's Right-of-Way Ordinance shall be cause for immediate revocation of a permit.**

The applicant shall notify ACTD immediately upon completion of project so that the ACTD can inspect the site to determine if restoration has been satisfactorily completed.

The undersigned hereby accepts the terms and conditions of this permit and the regulations of Anoka County, and agrees to fully comply therewith to the satisfaction of the ACTD. The county of Anoka, its officials, employees, and agents, shall be held harmless, by the applicant/permittee, from any demands, claims, lawsuits, or damages relating to the work described in this permit.

APPLICANT'S SIGNATURE

DocuSigned by:

727D4EC46AF449A...

DATE 10/30/2023

AUTHORIZATION OF PERMIT

In consideration of the applicant's agreement to comply in all respects with the regulations of the ACTD covering such operations, permission is hereby granted for the work to be done as described in the above application. Said work to be done in accordance with the general conditions listed above and the special conditions required as hereby stated. It is expressly understood that this permit is conditioned upon replacement or restoration of the county highway and its right of way to their original or to a satisfactory condition. It is further understood that this permit is issued subject to the approval of local city or township authorities having joint supervision over said street or highway.

APPROVED BY:

DATE

TITLE: Traffic Technician

NOT VALID UNLESS SIGNED BY ANOKA COUNTY

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SPECIAL CONDITIONS

TRAFFIC CONTROL

- 1) Detours
 - a) Detailed detour layouts shall be submitted to the traffic engineer for approval.
 - b) No detours shall be permitted without prior approval of the Anoka county traffic engineer.
 - c) A ten day notice must be given prior to the installation of any detour.
 - d) It shall be the responsibility of the applicant to notify Anoka county central communications, local government bodies, and any affected bus companies ten days prior to any road closures/detours.
 - e) Immediately upon completion of work and/or detours, all posts, barricades, and signs shall be removed from the right of way.
- 2) Traffic control devices
 - a) All traffic control devices, barricades, flashers, etc., shall be furnished by the applicant and shall be in accordance with the most recent edition of the Minnesota manual on uniform traffic control devices and temporary traffic control zone layouts – field manual of the same manual.

CONSTRUCTION REQUIREMENTS

- 1) Open cutting of bituminous or concrete surfaced roads will be allowed only at the discretion of the county engineer.
- 2) Neither supplies nor excavation materials shall be placed on the bituminous or concrete surface at any time.
- 3) No trenches will be allowed to remain open overnight.
- 4) Materials removed from the trench shall be used as backfill insofar as they are suitable. All backfill material shall conform to MNDOT specifications for compaction. The use of heavy equipment on top of trench, slapping with backhoe bucket and/or back casting to achieve compaction is prohibited. Any additional material required to back fill to the original grade shall be furnished by the applicant at no expense to the ACTD. All the base and surface courses damaged during construction operations shall be restored to a condition equal to or better than before operations began. The applicant shall be responsible for and restore any settlement.
- 5) All culverts, ditches, shoulders, and backslopes shall be restored to their original condition unless otherwise directed by the ACTD. Shoulders which have been previously constructed or reconstructed with special materials shall be replaced in kind. Restoration of signs, guardrails, guard posts, etc., are the sole responsibility of the applicant and shall be restored to their original condition.
- 6) All roadway maintenance required within the limits of the utility project that is related to the applicant's activities shall be the sole responsibility of the applicant for one year after completion of the project. Upon completion of the restoration work, the applicant shall request a final inspection by the ACTD. The ACTD's approved completion date shall be the starting date of the applicant's one-year responsibility.

HORIZONTAL BORING AND JACKING

- 1) All hard surface roadways shall be jacked or bored.
- 2) All crossings of Anoka County maintained roadbeds, shall be made by boring inside a casing or carrier pipe, or by jacking unless otherwise directed by the Anoka County Engineer. The auger shall lead the casing or carrier pipe by at least six inches whenever possible and never lead the carrier pipe by more than one inch.
- 3) The use of pneumatic devices to facilitate the roadbed crossings will be allowed in most cases with prior approval. In the event approval is not granted and applicant uses a pneumatic device to cross a roadbed and encounters an obstruction and/or unstable subbase material which makes forward or reverse motion of pneumatic device impossible, said pneumatic device then becomes part of the roadway subbase and permission to excavate to retrieve device will not be granted.
- 4) If a pneumatic device is used for the work permitted herein, the installation must be kept to a minimum of four feet below the surface of the roadway if the pneumatic device is less than two inches in diameter, and a minimum of five feet below the surface of the roadway if the pneumatic device is two inches in diameter or larger.

BITUMINOUS RESTORATION

- 1) The locations and dimensions of all openings to be made in the bituminous surface shall be approved by the ACTD prior to any cutting or any surface opening operations.
- 2) All openings in bituminous surfaces shall be cut in a straight line with the sides smooth and vertical. No ragged edges will be permitted. Cutting shall be done with a concrete saw.
- 3) All necessary dust control operations shall be carried out by the applicant at no expense to Anoka County.
- 4) The minimum requirement for subgrade replacement shall be the upper twelve inches of material and shall meet MNDOT specifications for class five placed in six inch layers compacted to one hundred percent of optimum density.
- 5) All manhole casings, gate valves, and other utility structures shall be set one quarter inch below the top of the finished surface.
- 6) Bituminous tack coat materials and application thereof shall conform to MNDOT specification 2357.
- 7) All bituminous surfacing shall be replaced as soon as practicable after the base construction. All bituminous surfacing shall be machine laid. Any exceptions must be approved by the ACTD. Bituminous surfacing shall be replaced to original pavement depth or to a minimum of six inches of bituminous mixture (2360), whichever is greater. Bituminous mixtures must be placed in lifts not exceeding three inches in thickness for base and binder courses and not exceeding two inches for the wear course.
- 8) All surface restoration regardless of size shall conform to existing grades.
- 9) Any unnecessary or negligent damage to bituminous surface in conjunction with the installation and/or repair of a utility shall be cut out and replaced in kind as directed by the ACTD.

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CONCRETE RESTORATION

- 1) Curb and gutter, sidewalks, and driveways shall be restored in accordance with MNDOT specifications 2531 and 2521.

UTILITY LINES

- 1) There shall be only a single pole line on the county right of way on either side of the center line thereof.
- 2) Exact locations of longitudinal installations on county highways shall be located as directed by the ACTD.

SECTION CORNER MONUMENTS

- 1) Utility locations shall not interfere with the location of any section, quarter, witness, or right of way monuments. For assistance in locations, contact the Anoka County Surveyor's Office.
- 2) The applicant shall be responsible for replacement of any existing property irons disturbed during construction.
- 3) The applicant shall notify the Anoka County Surveyor's Office three working days in advance of any anticipated disturbance of any section, quarter, witness, or right of way monuments.
- 4) Any monument disturbed during the course of construction, shall be reset by the Anoka County Surveyor's Office at the expense of the applicant.

ATTACHING TO BRIDGES/STRUCTURES

- 1) No utility is permitted to be hung from, or otherwise attached to, any bridge or structure without having detailed plans approved by the Anoka County Engineer. These plans are to show approaches to the structure, method of installation, type, and dimension of housing for the utility.

ADDITIONAL PROVISIONS

- 1) All subcontractors, installers, and crew shall possess a physical or electronic copy of all documents in relation to the approved permit prior to the commencement of work and be kept on site. This includes, but it not limited to the following:
 - a) Approved permit
 - b) Any/all traffic control plans and/or layouts
- 2) Shall notify Anoka County Permits at 763-324-3176 or highwaypermits@anokacountymn.gov
 - a) At least 36 hours prior to the commencement of work
 - b) With time frame of proposed of work
 - c) Anticipated traffic control
 - d) When work is complete - including restorations - to request a final inspection
- 3) No work during inclement weather or when plows are out in any capacity
- 4) All traffic control shall be in accordance with the most current version of the MnDOT Temporary Traffic Control Field Manual

INITIAL
^{DS}
 kp

SITE DEVELOPMENT PLANS

FOR

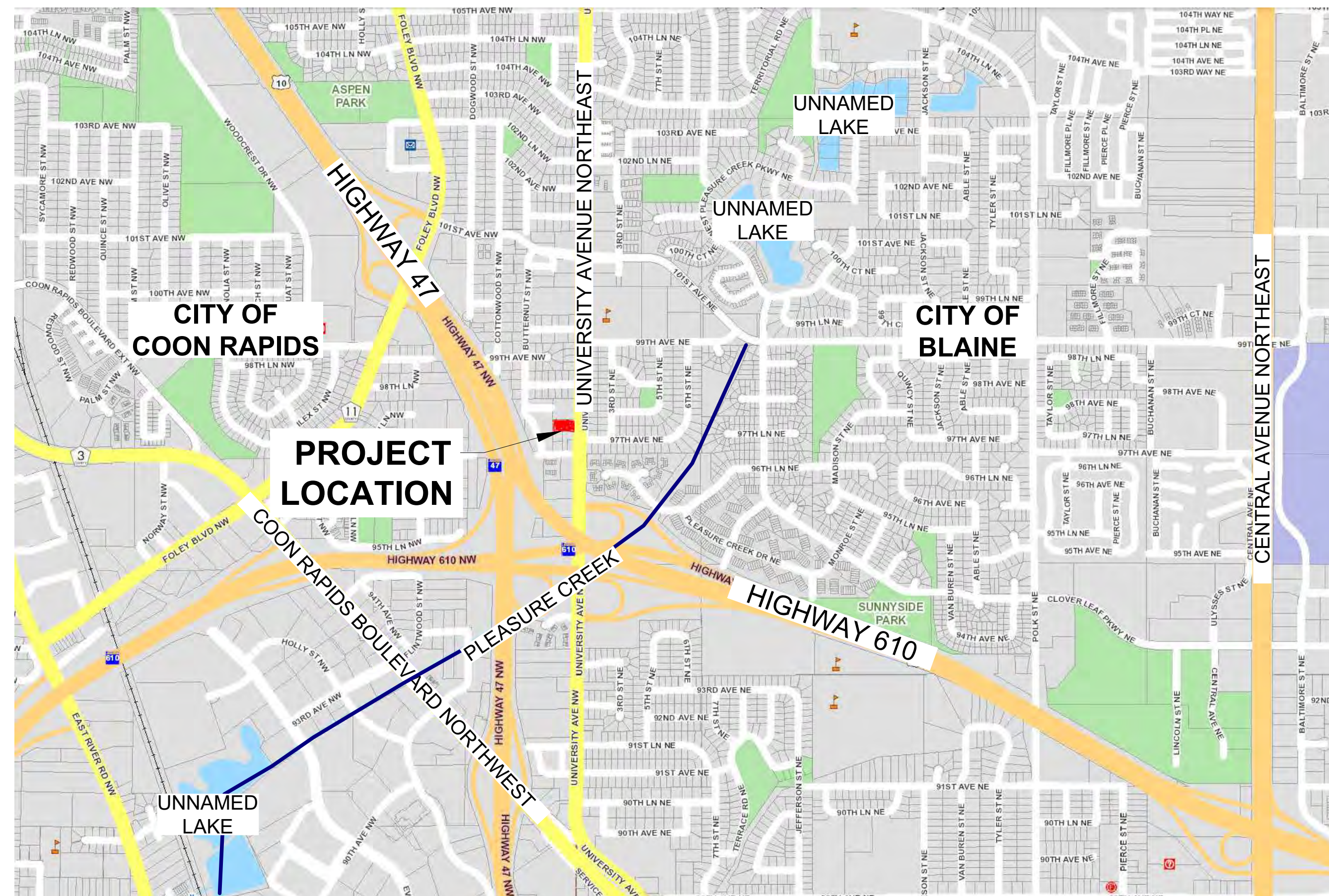
CEDARWOOD

9730 UNIVERSITY AVE. NW
COON RAPIDS, MINNESOTA

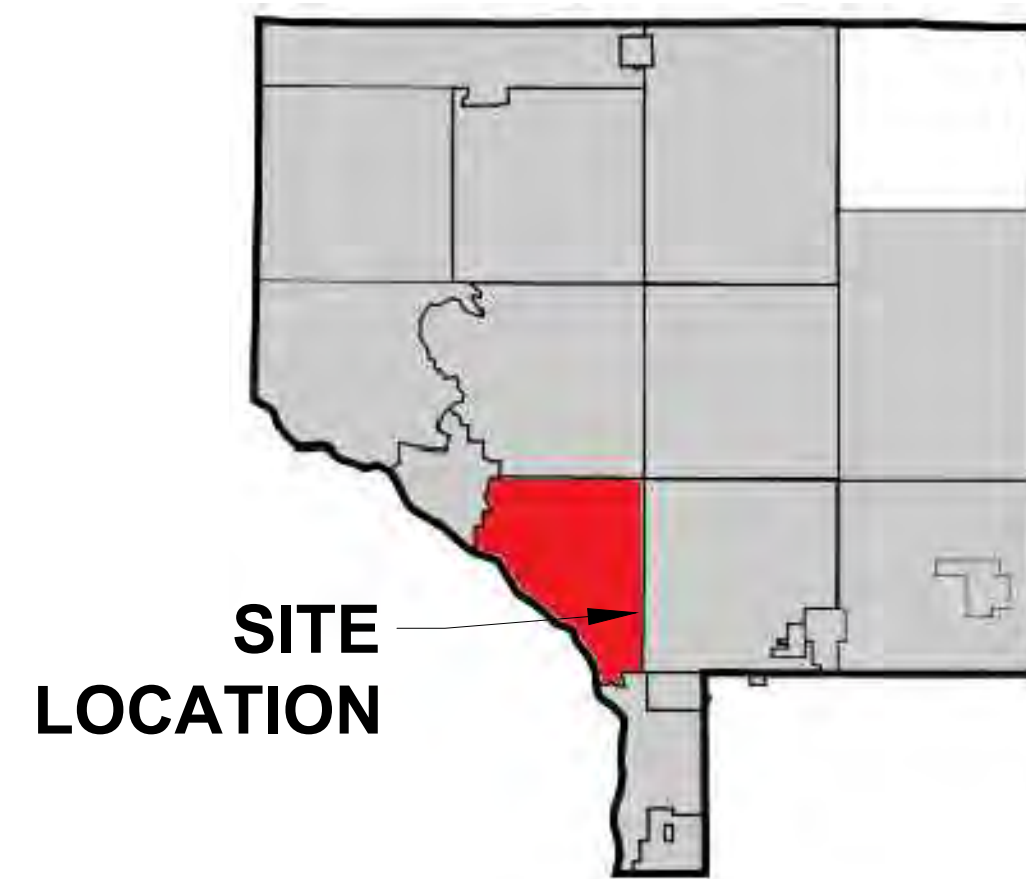


SITE LOCATION

ANOKA COUNTY



PROJECT LOCATION



SITE LOCATION

CITY OF COON RAPIDS

SHEET INDEX

THIS PLAN CONTAINS 9 SHEETS

SHEET NO.	DESCRIPTION
C1	TITLE SHEET
C2-C4	DETAILS
C5	EXISTING CONDITIONS & REMOVALS PLAN
C6	SITE PLAN
C7	GRADING & EROSION CONTROL PLAN
C8	UTILITY PLAN
L1	LANDSCAPE PLAN

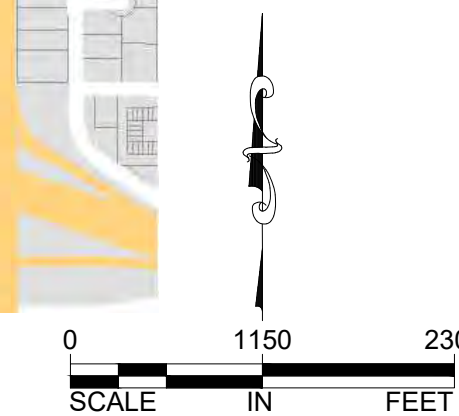


CALL BEFORE YOU DIG!
Gopher State One Call
Twin City Area: 651-454-0002
Toll Free: 1-800-252-1166

THE SUBSURFACE UTILITY INFORMATION DEPICTED IN THESE PLANS ARE SHOWN ACCORDING TO CII/ASCE 38-02, "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA," LEVEL D STANDARDS.

LEGEND

IMPAIRED WATERS



F:\survey\auditors subdivision - anoka\105\9730 university ave\02 Engineering - 1111101 CAD\05 Sheet Files\01 Title Sheet.dwg

DESIGNED BY: JAP	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. PRELIMINARY JEFFREY A. PRASCH, P.E.	REVISIONS		
DRAWN BY: JPF		04.13.23	CLIENT COMMENTS	
CHECKED BY: GRP		DATE: 2.28.23	LIC. NO.: 52706	

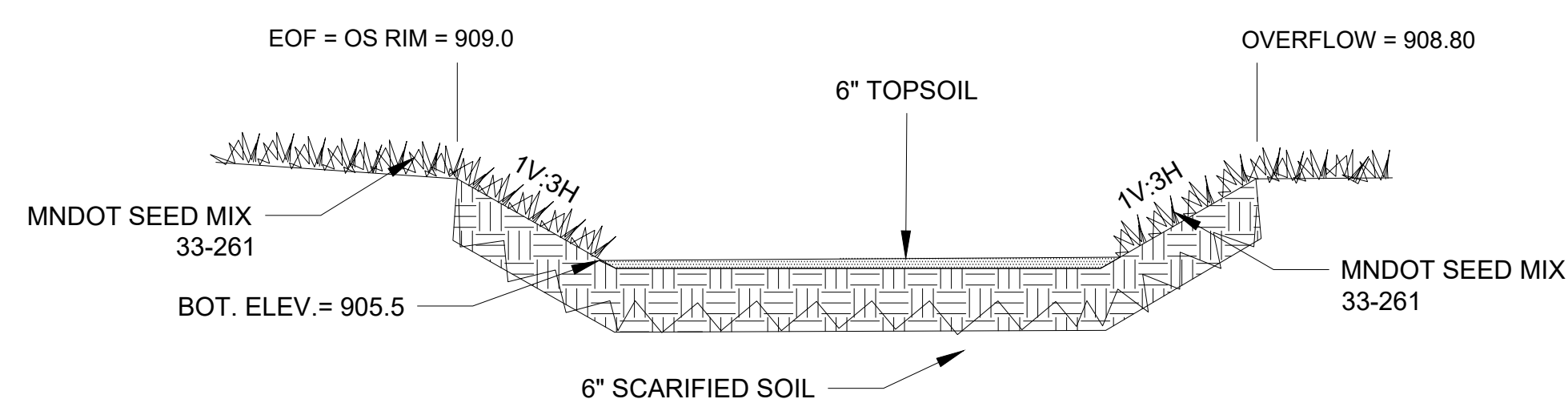
DEMARC
LAND SURVEYING & ENGINEERING
7601 73RD AVENUE N, BROOKLYN PARK, MN 55428
PHONE: 763.560.3093 FAX: 763.560.3522
www.DemarcInc.com

VALUE HOMES
12301 CENTRAL AVE. NE, SUITE 101
BLAINE, MINNESOTA 55434

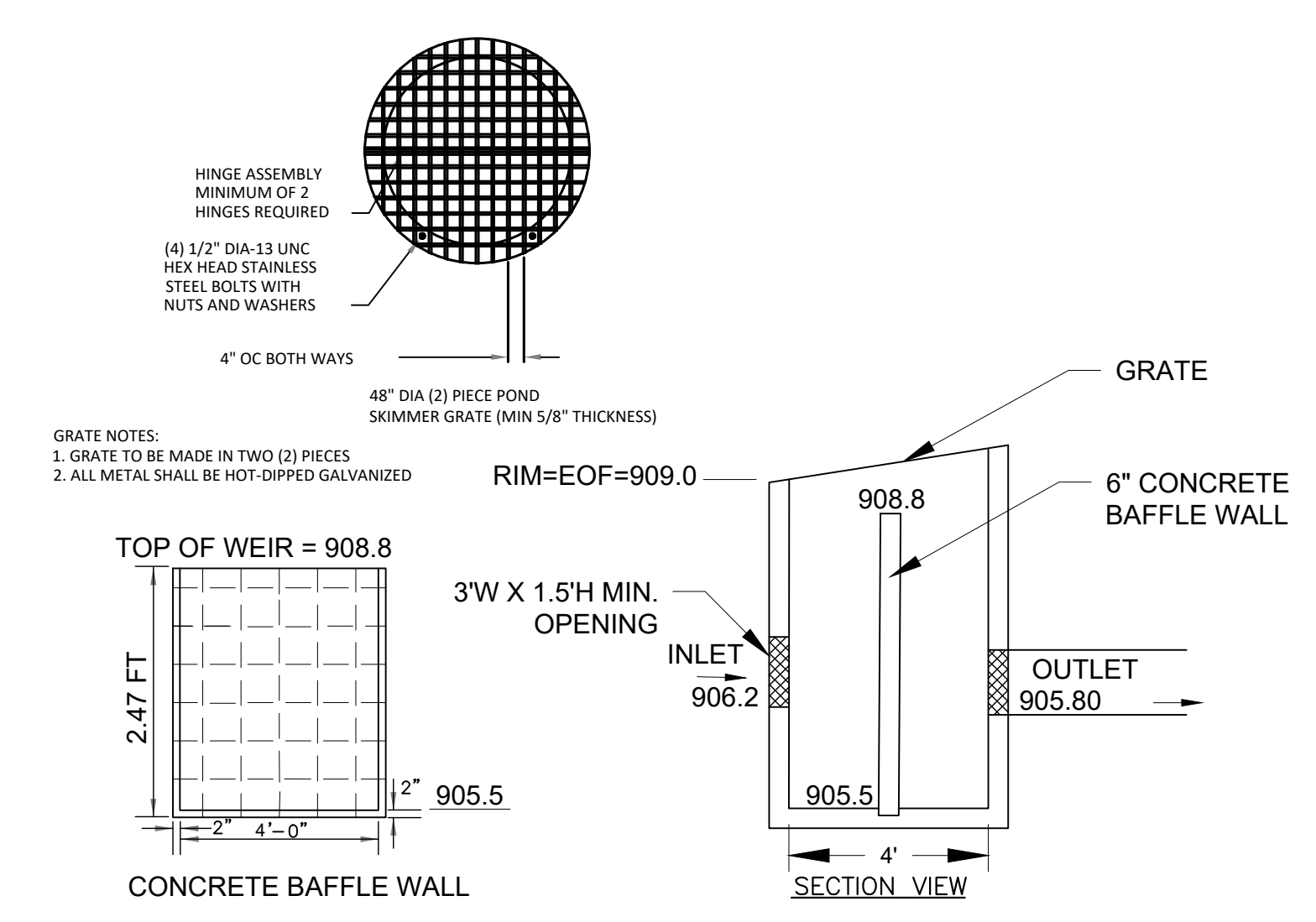
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COON RAPIDS, MINNESOTA

TITLE SHEET

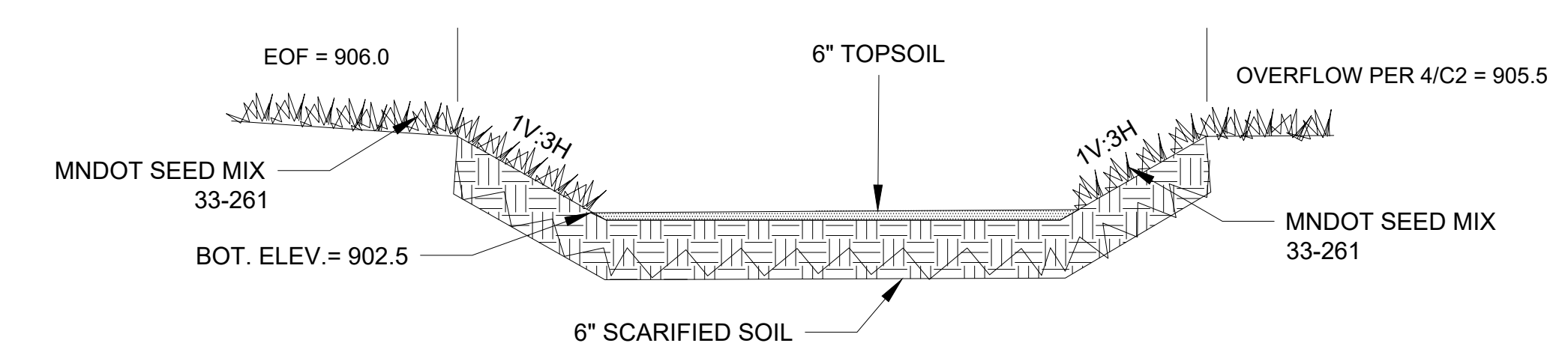
PROJECT: 90259A
SHEET NO.
C1 OF C8



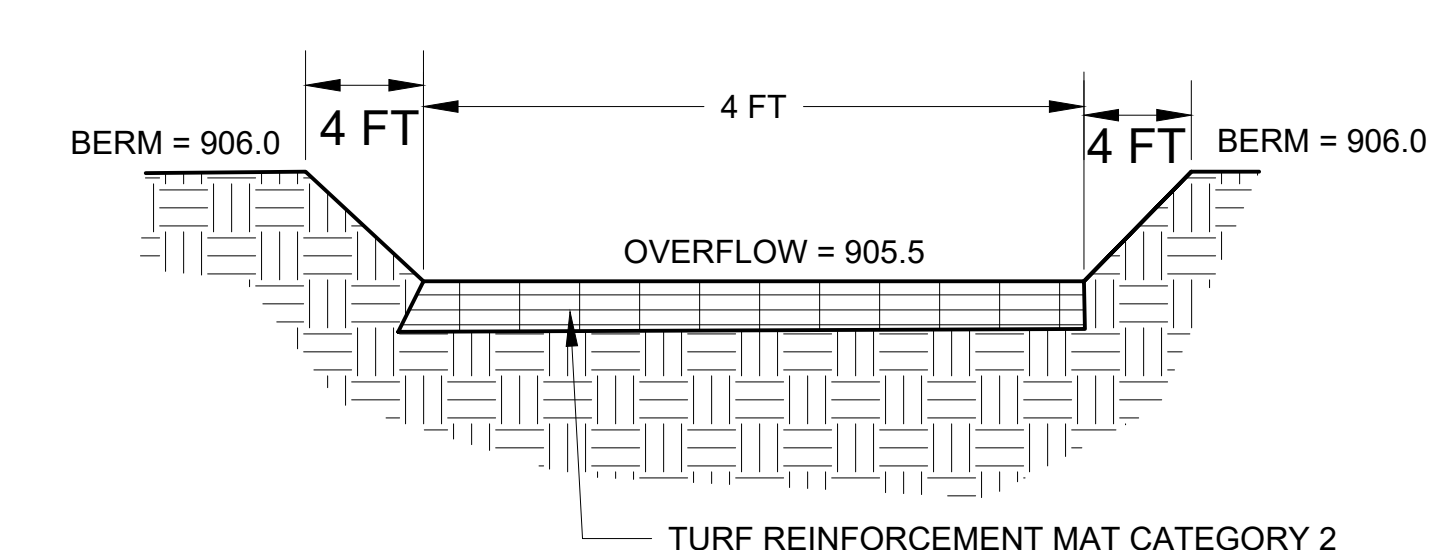
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C2
INFILTRATION BASIN "A"
NOT TO SCALE



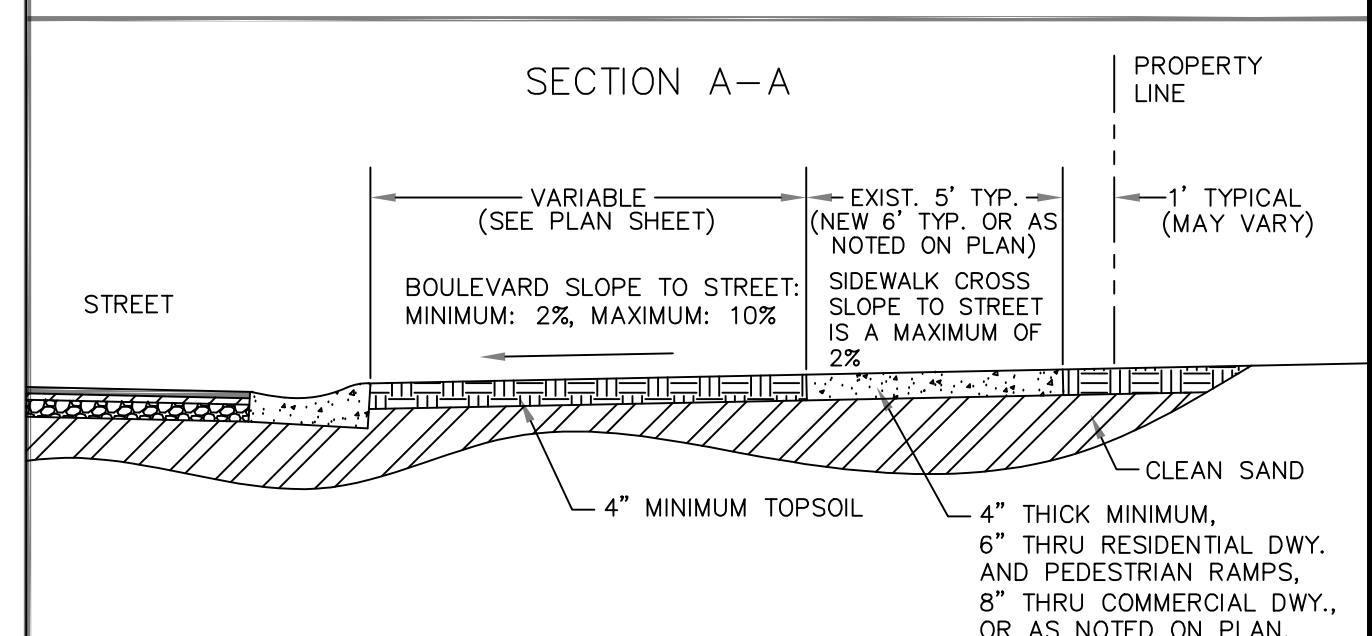
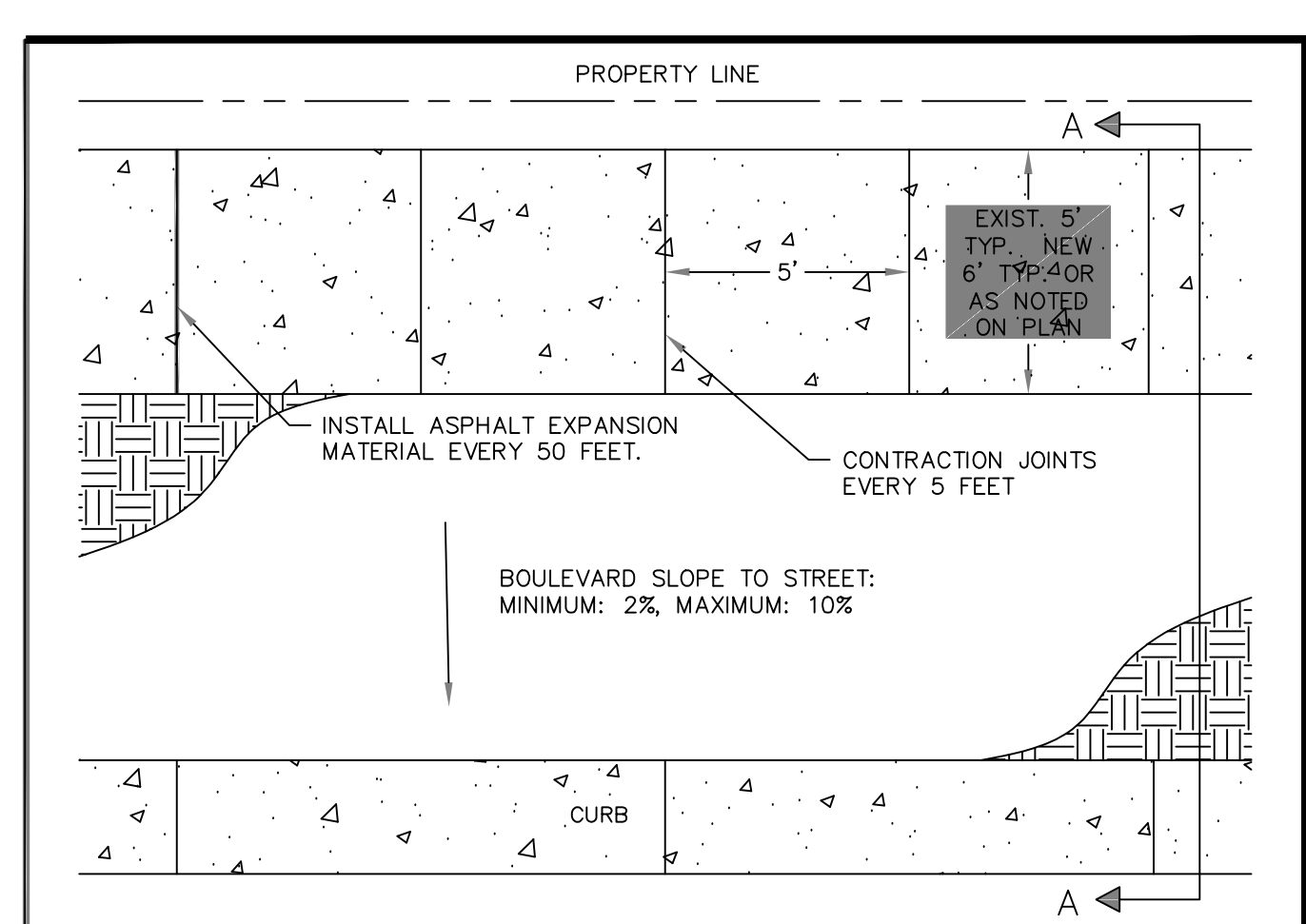
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C2
OUTLET STRUCTURE (OS-1)
NOT TO SCALE



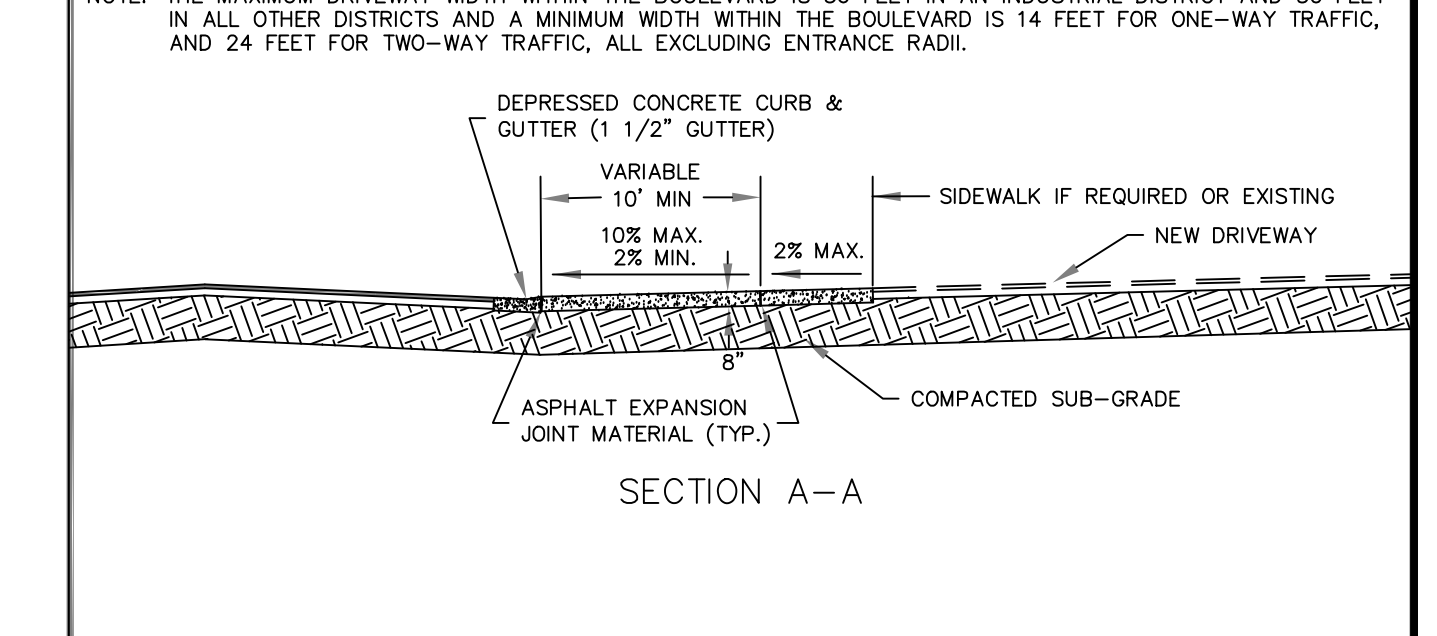
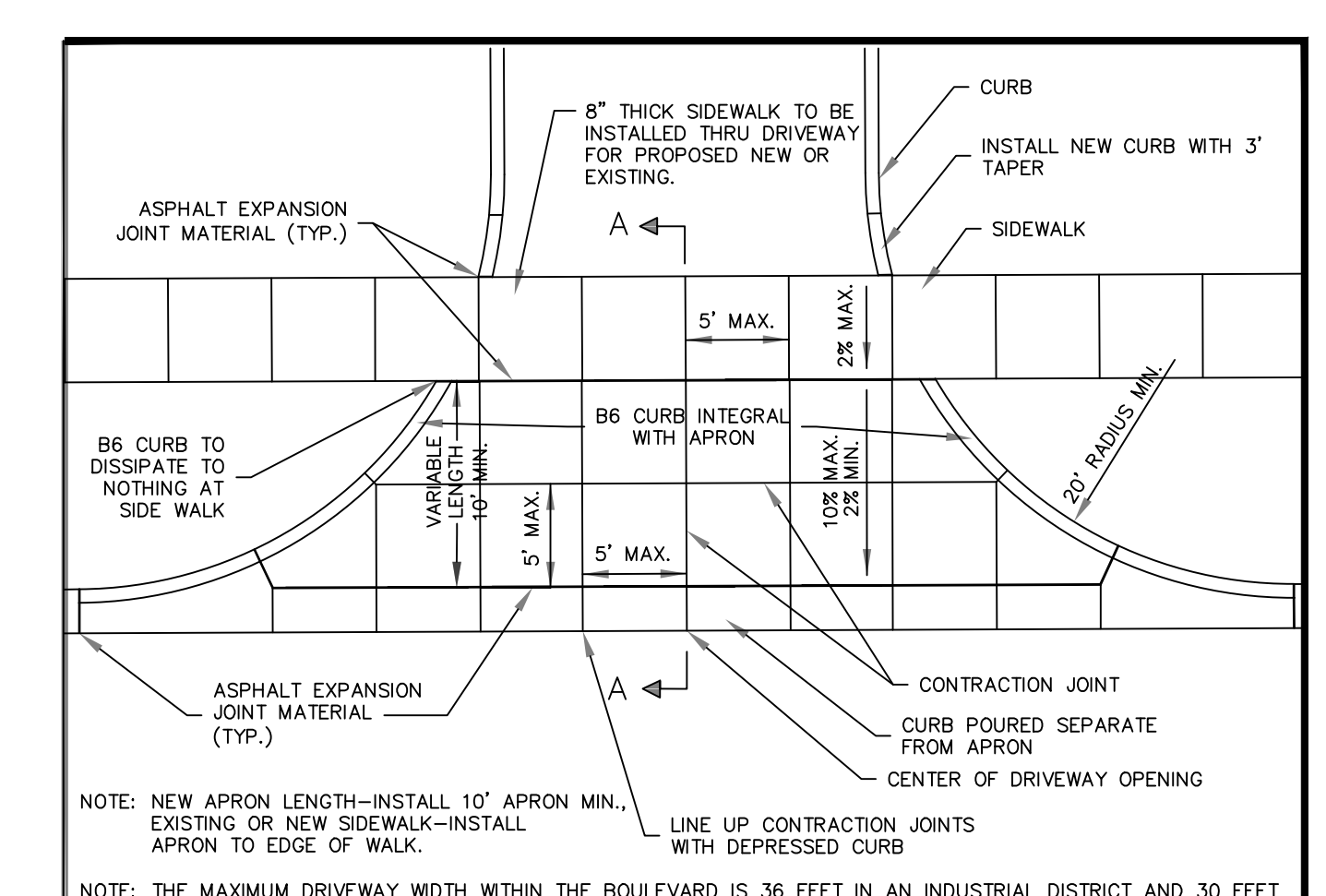
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C2
INFILTRATION BASIN "B"
NOT TO SCALE



4
C2
BASIN "B" OUTFLOW DETAIL
NOT TO SCALE



CONCRETE SIDEWALK SECTION
DRAWN: R.L.S. DATE: 1/3/2019 SCALE: NONE PLATE NO. STR-6



COMMERCIAL DRIVEWAY NEW
DRAWN: R.L.S. DATE: 1/16/2018 SCALE: NONE PLATE NO. STR-10

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DRAWN BY:	ABL		04.13.23	CLIENT COMMENTS
CHECKED BY:	GRP			
DATE: 2.28.23		LIC. NO.: 52706		

PRELIMINARY	
JEFFREY A. PRASCH, P.E.	

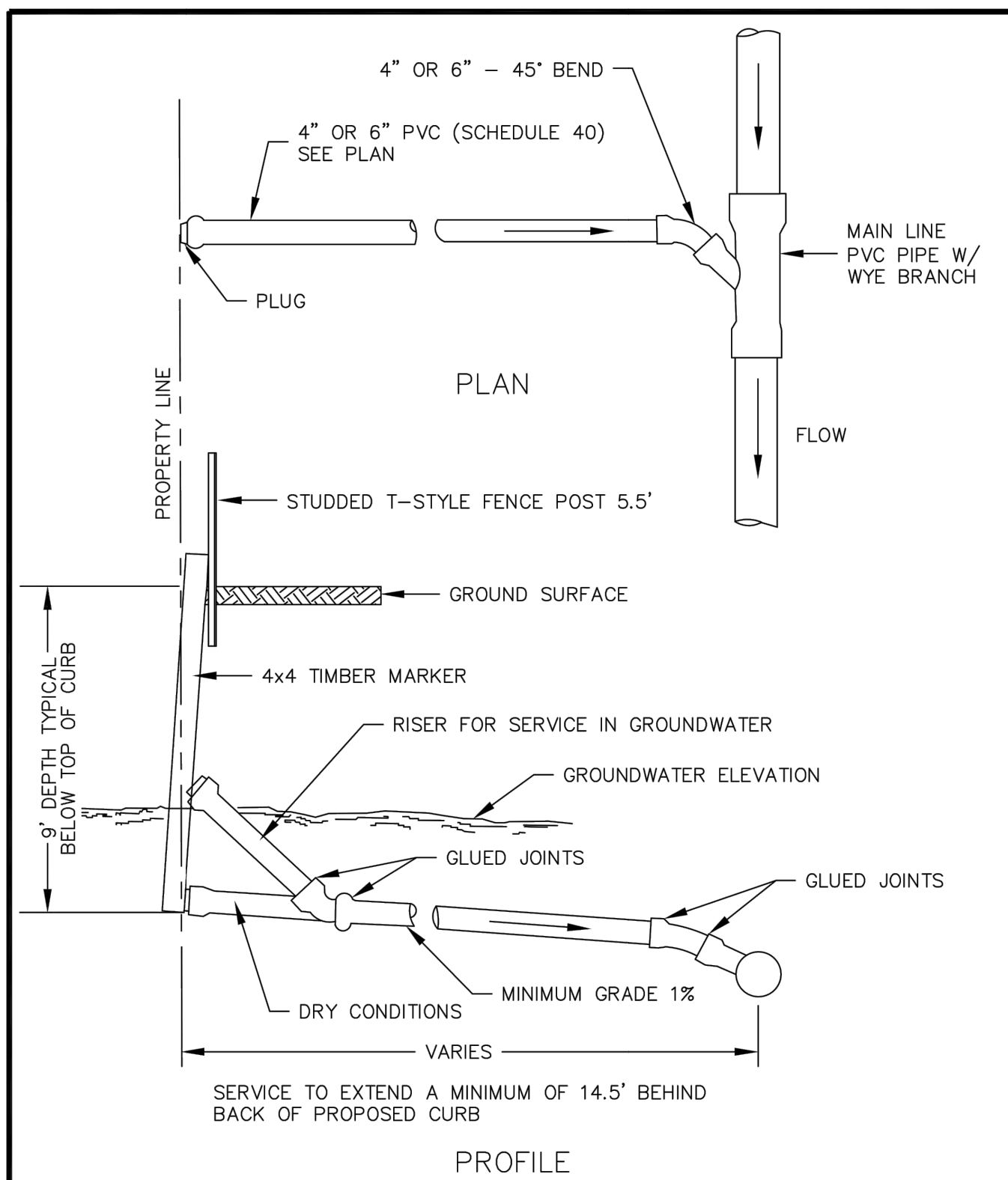
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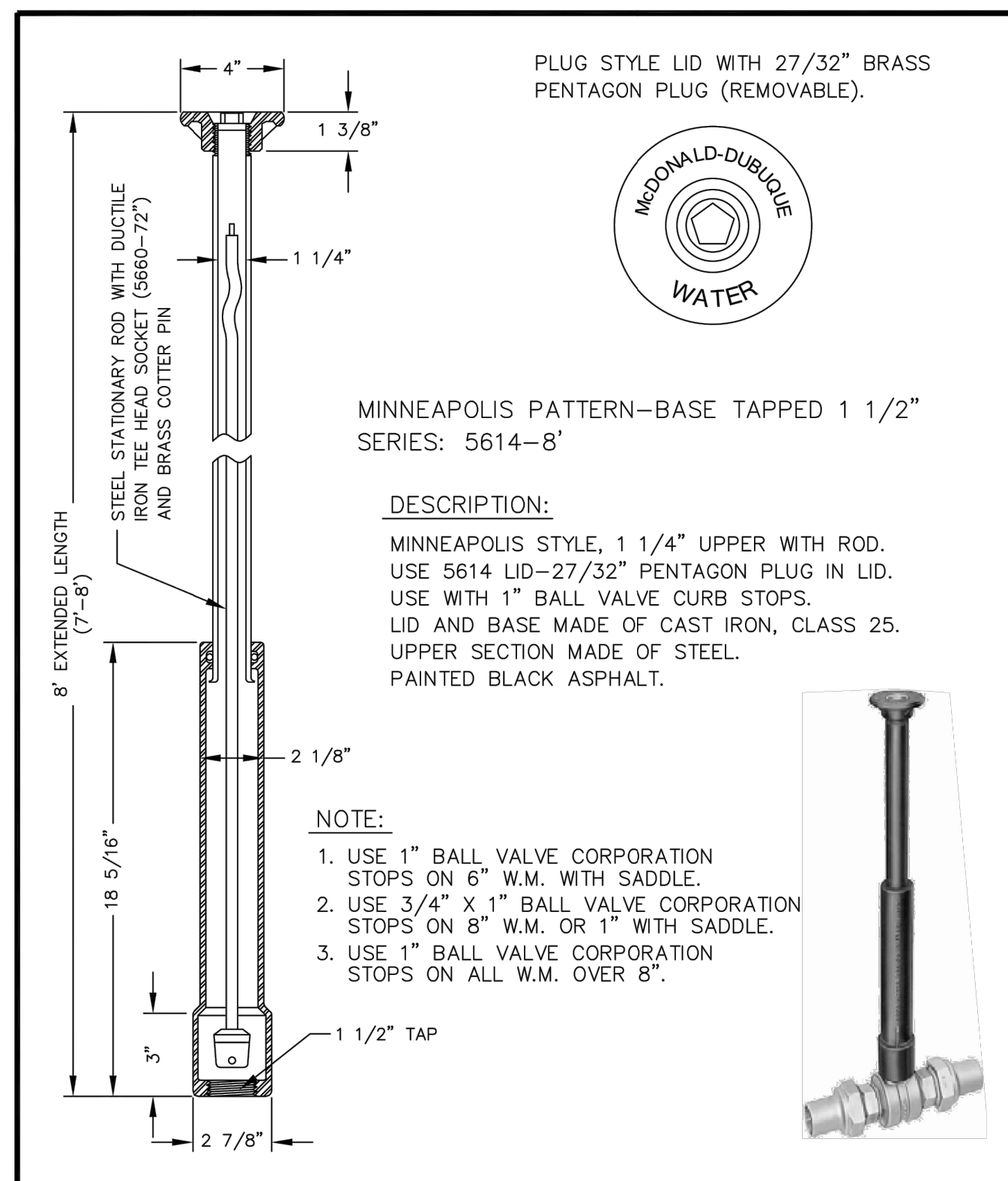
DETAILS

PROJECT: 90259A
SHEET NO. C2 OF C8



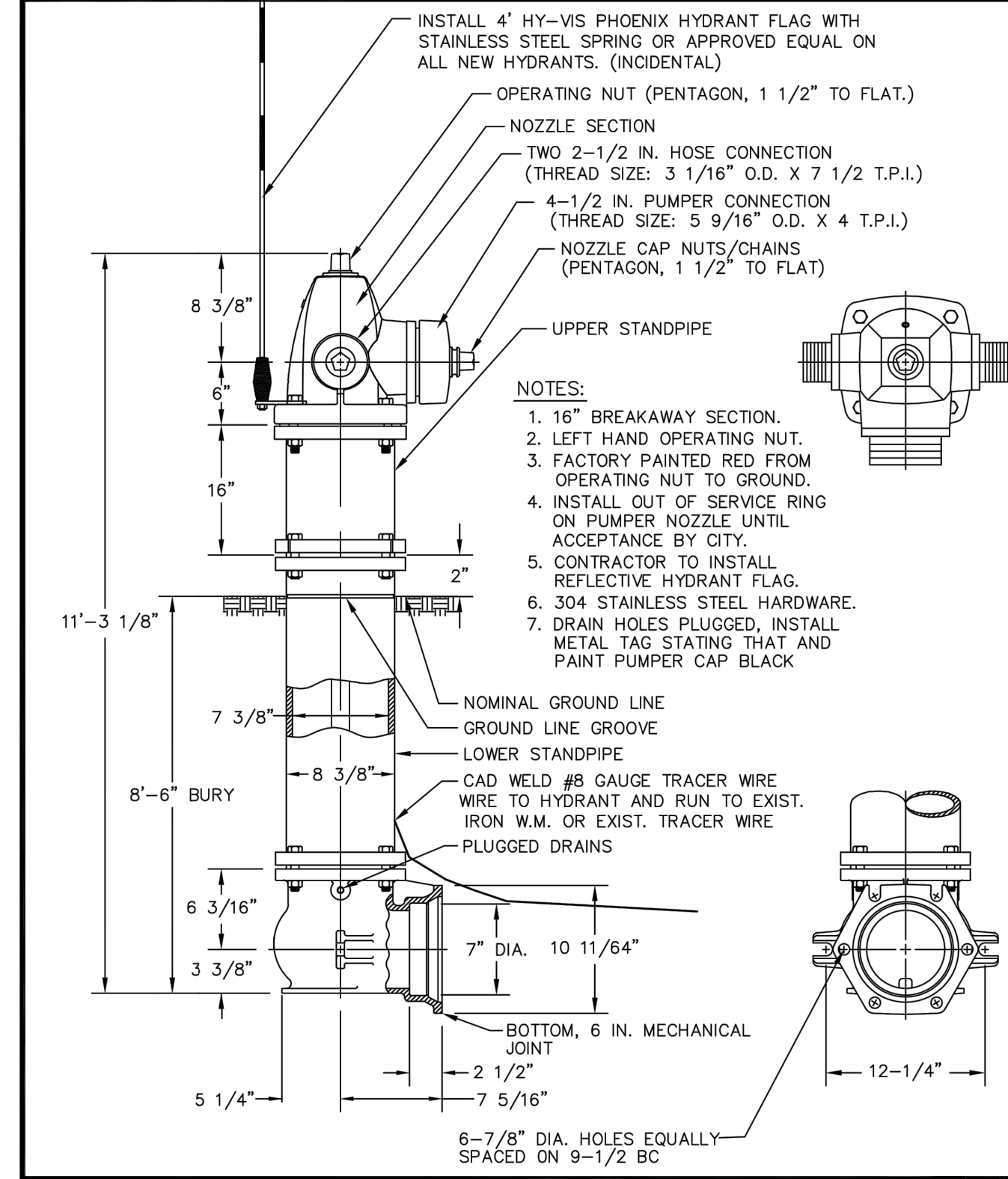
TYPICAL SEWER SERVICE

City of Coon Rapids, Minnesota
 DRAWN: R.L.S. DATE: 1/7/2019 SCALE: NONE PLATE NO. SAN-1



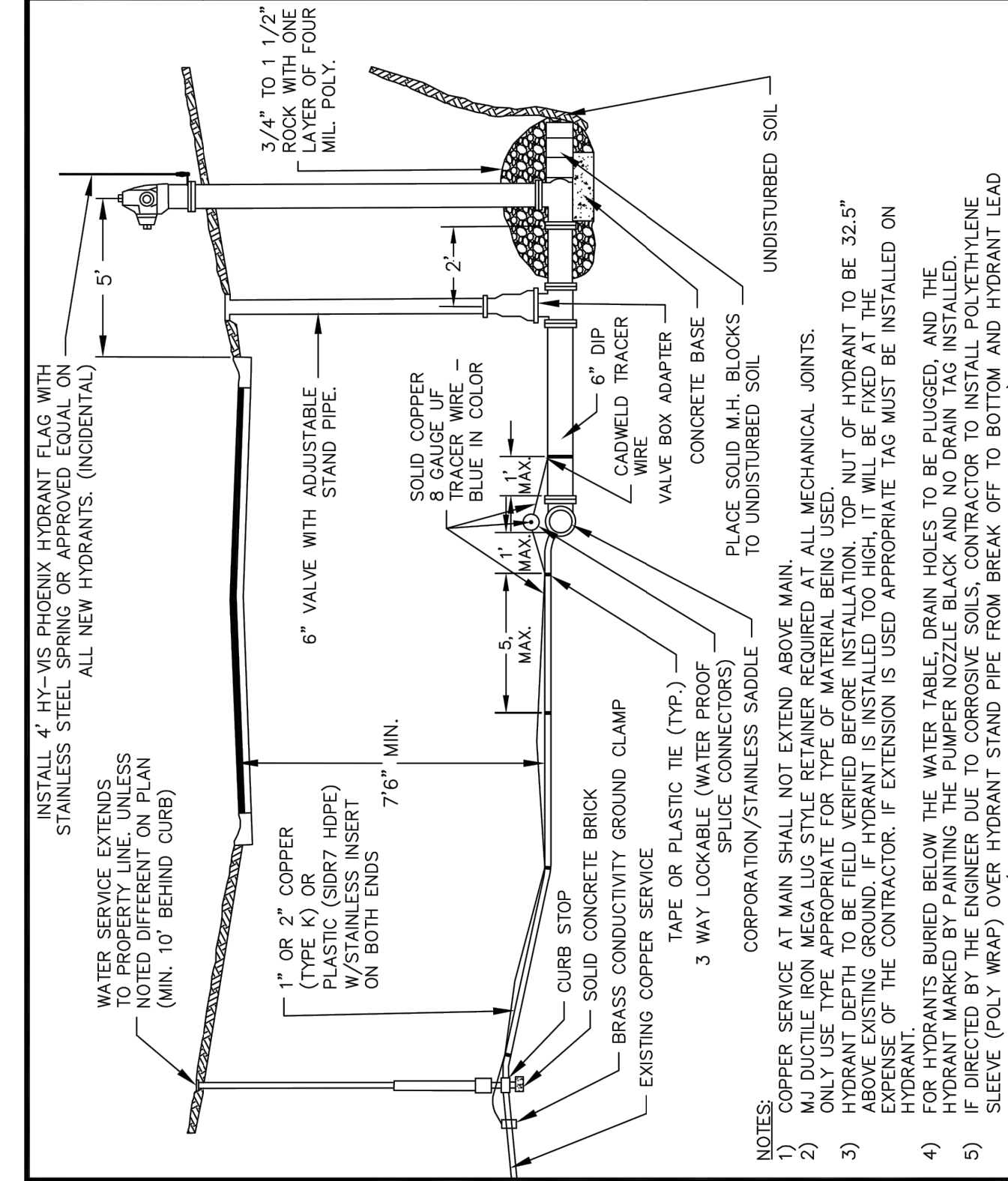
CURB STOP BOX WITH ROD

City of Coon Rapids, Minnesota
 DRAWN: R.L.S. DATE: 1/16/2019 SCALE: NONE PLATE NO. WM-4



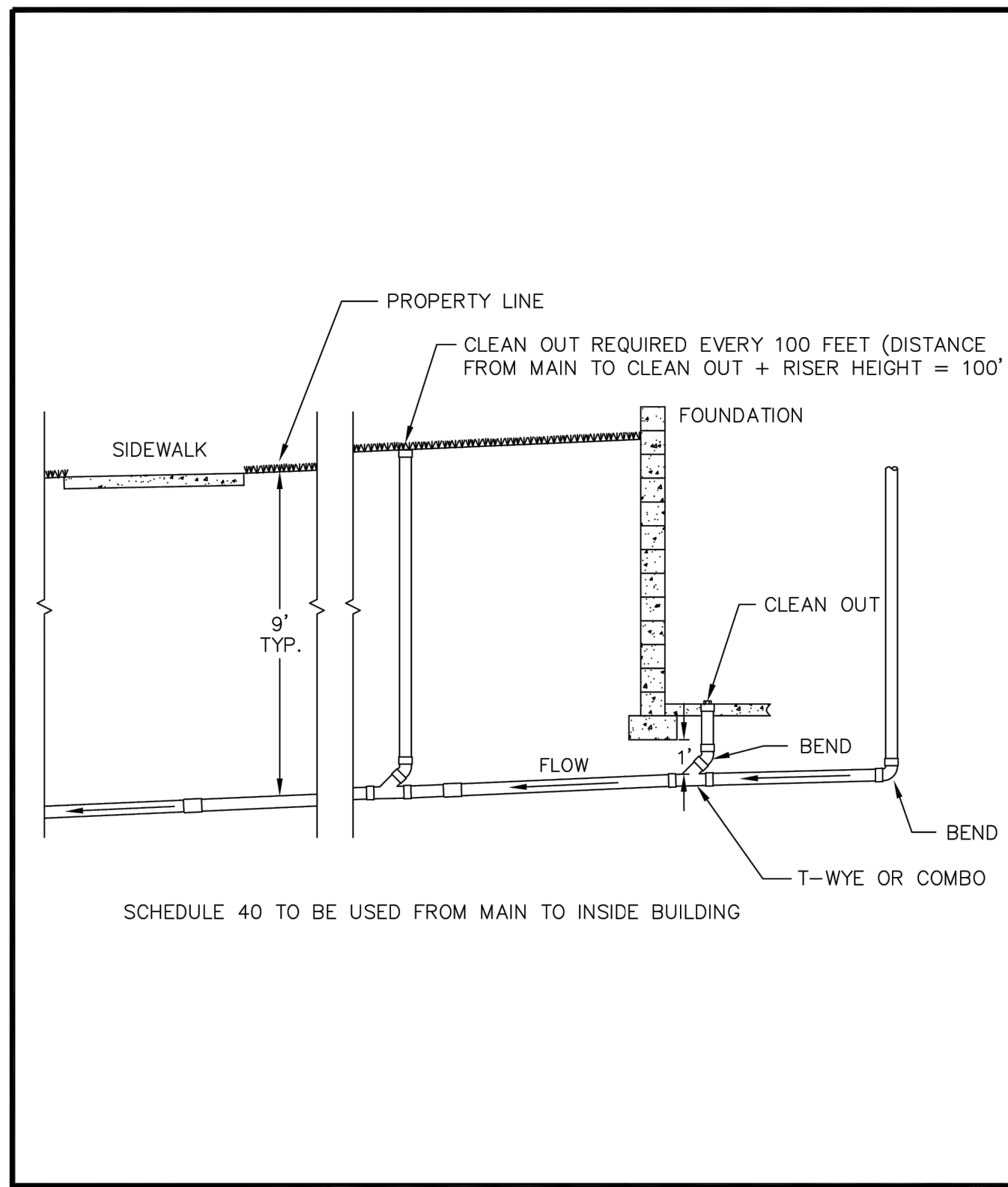
HYDRANT DETAIL WB67 PACER 8'-6" BURY/MECH. JOINT BASE

City of Coon Rapids, Minnesota
 DRAWN: R.L.S. DATE: 1/17/2019 SCALE: NONE PLATE NO. WM-1



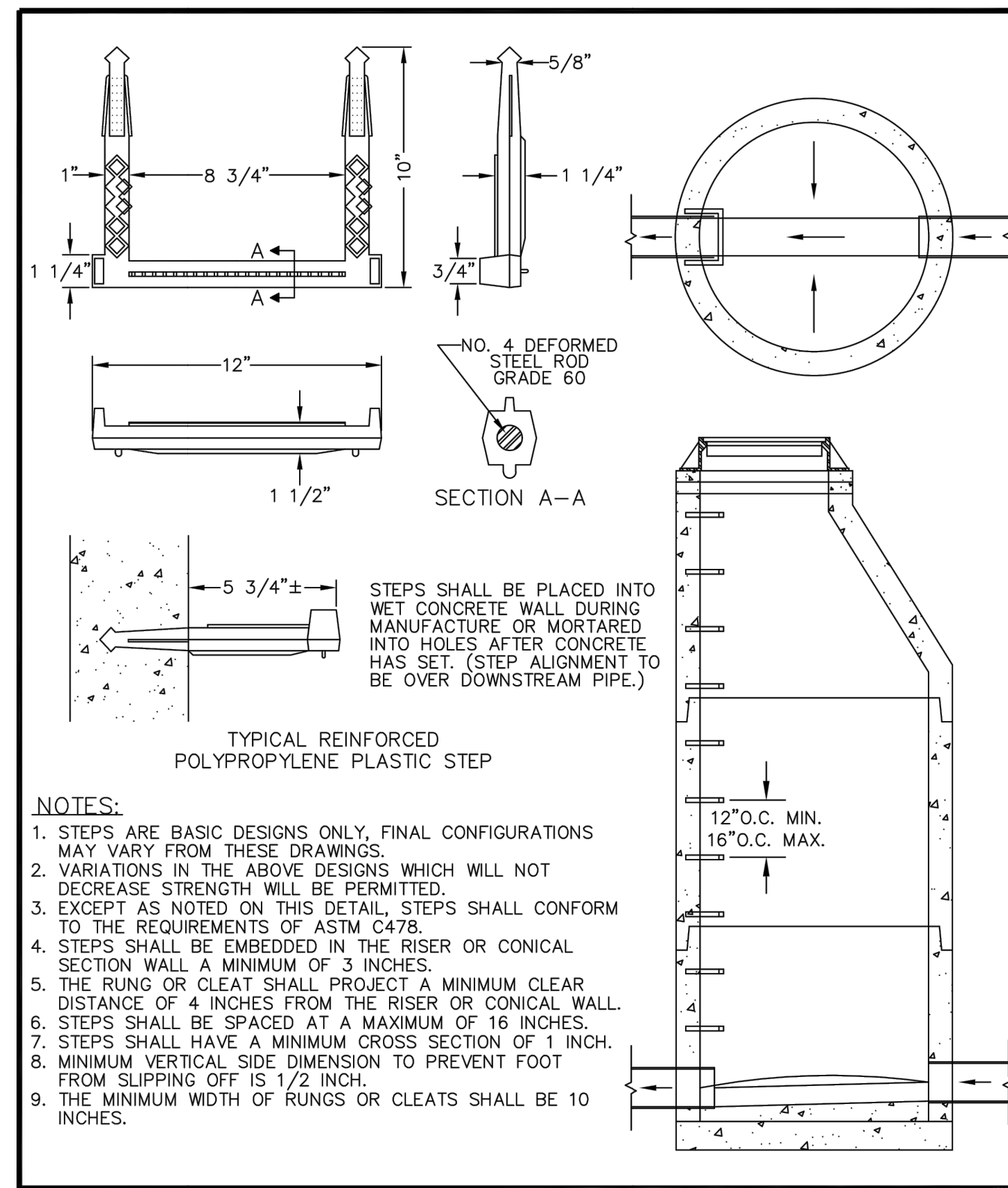
TYPICAL HYDRANT & WATER SERVICE LAYOUT FOR NEW & TOTAL REPLACEMENT OF WATER MAIN

City of Coon Rapids, Minnesota
 DRAWN: R.L.S. DATE: 1/7/2019 SCALE: NONE PLATE NO. WM-3



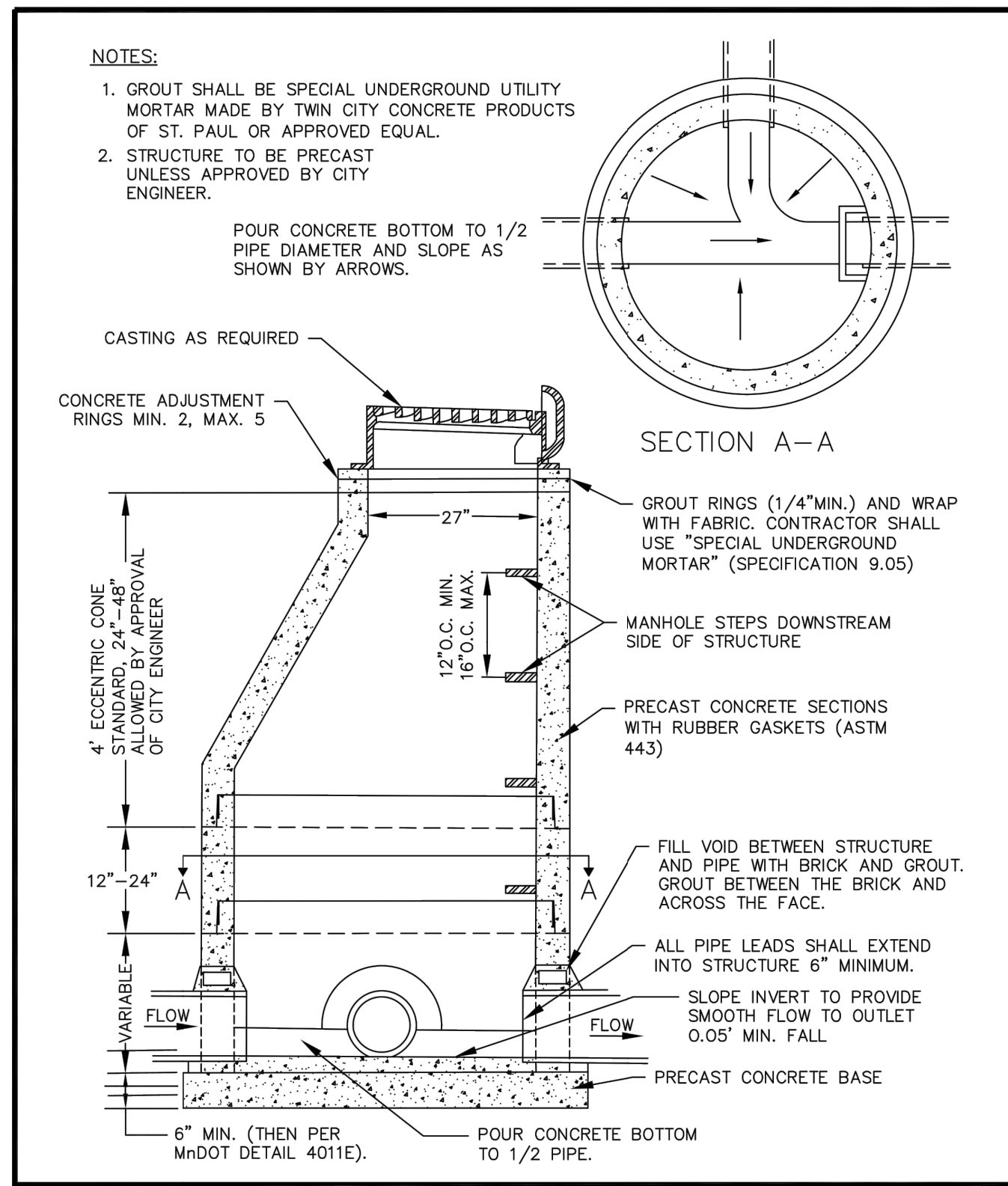
SANITARY SEWER SERVICE FROM PROPERTY LINE TO INSIDE BUILDING

City of Coon Rapids, Minnesota
 DRAWN: R.L.S. DATE: 1/7/2019 SCALE: NONE PLATE NO. SAN-3



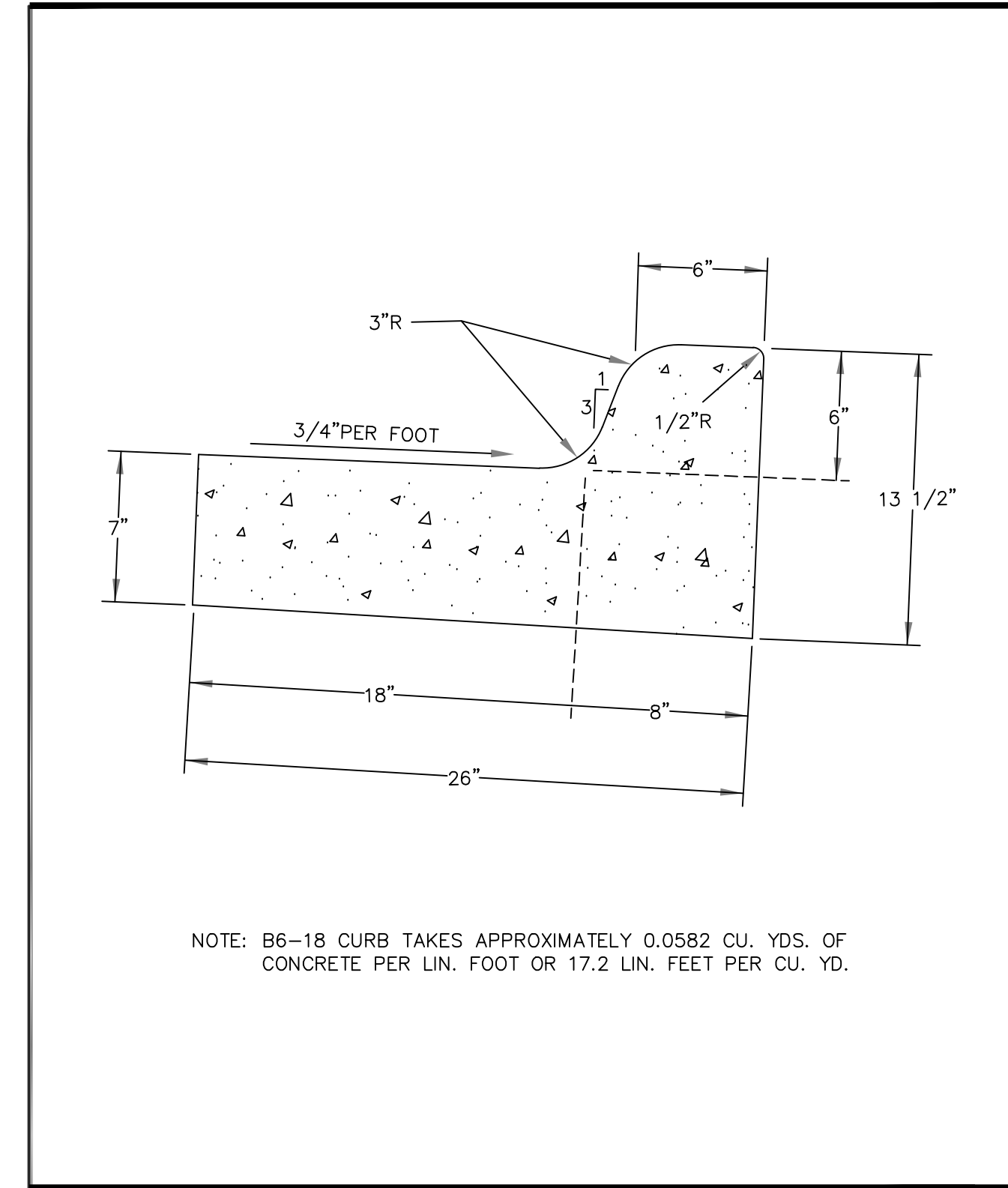
MANHOLE OR CATCH BASIN STEP

City of Coon Rapids, Minnesota
 DRAWN: R.L.S. DATE: 1/7/2019 SCALE: NONE PLATE NO. STM-3



STANDARD STORM DRAIN INLET MANHOLE

City of Coon Rapids, Minnesota
 DRAWN: R.L.S. DATE: 1/29/2019 SCALE: NONE PLATE NO. STM-11



B-618 CONCRETE CURB & GUTTER DETAIL

City of Coon Rapids, Minnesota
 DRAWN: R.L.S. DATE: 1/24/2019 SCALE: NONE PLATE NO. STR-2

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DRAWN BY:	ABL	
CHECKED BY:	GRP	
DATE:	2.28.23	

REVISIONS	DATE	DESCRIPTION
04.13.23	CLIENT COMMENTS	

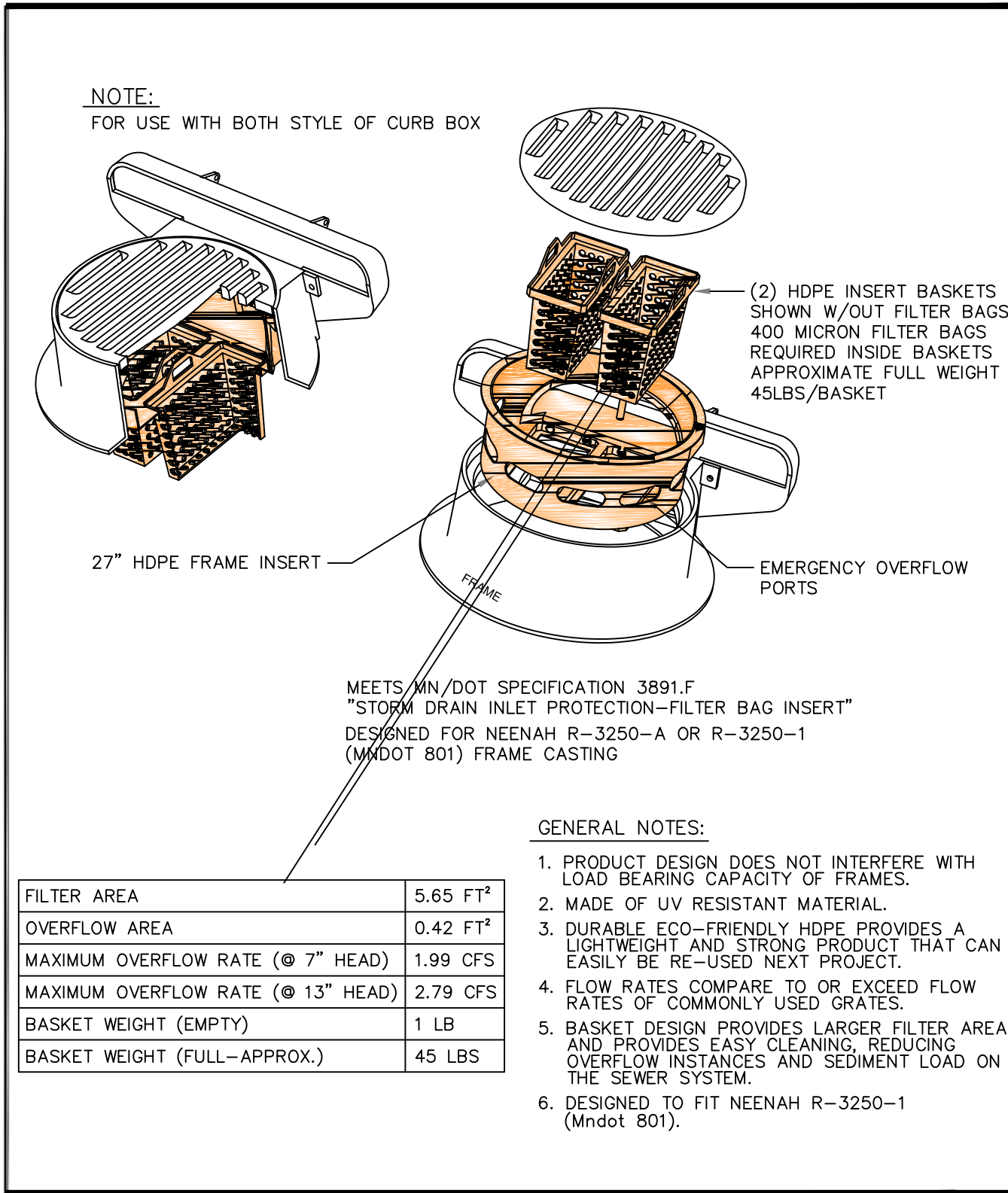
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DETAILS

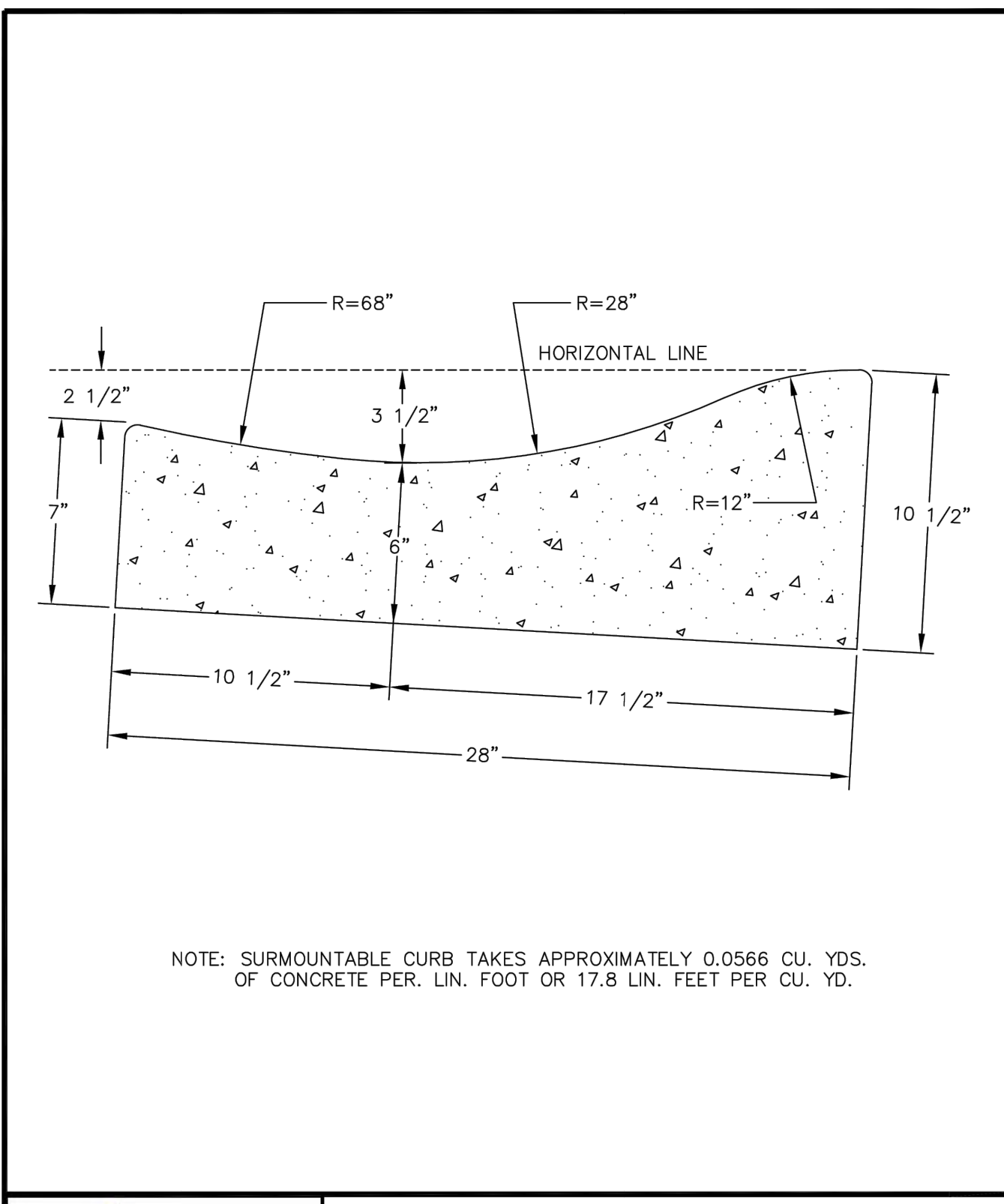
PROJECT:	90259A
SHEET NO.	C3 OF C8



COON RAPIDS Minnesota

INFRASAFE 27" DIA. INLET PROTECTION DEVICE

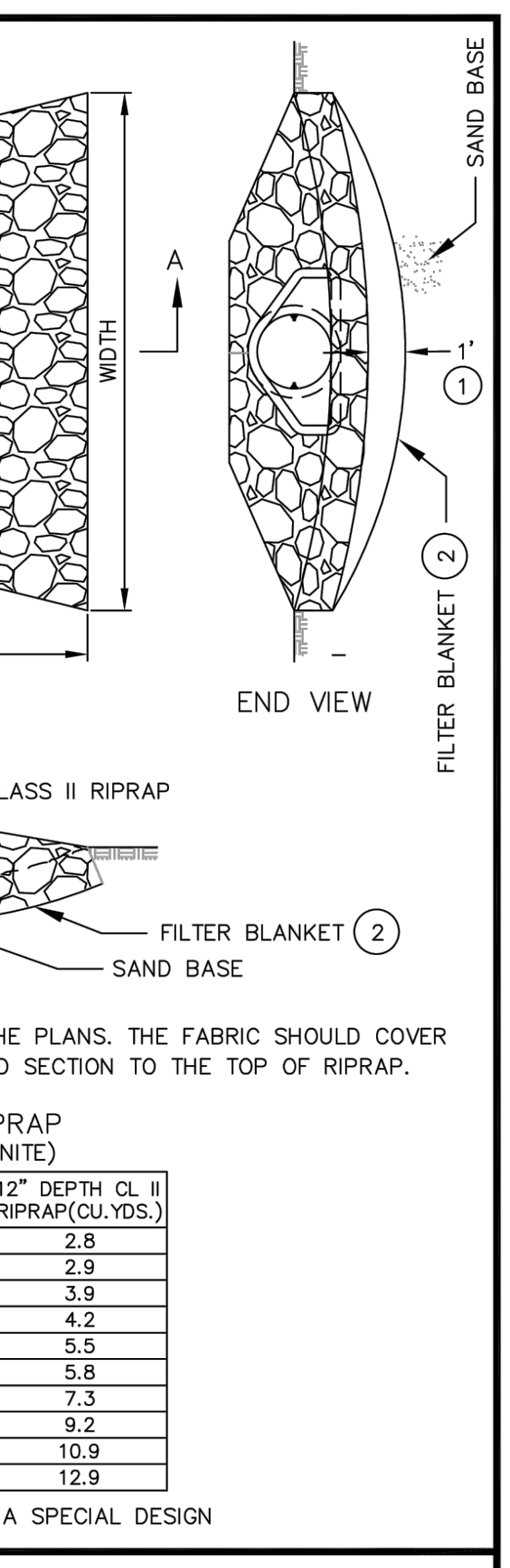
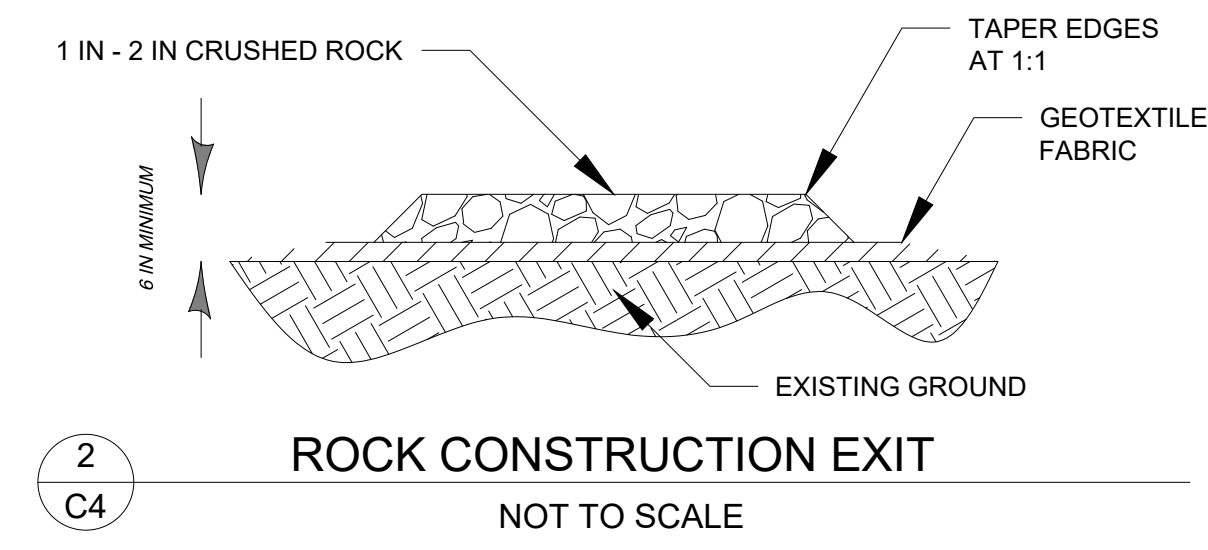
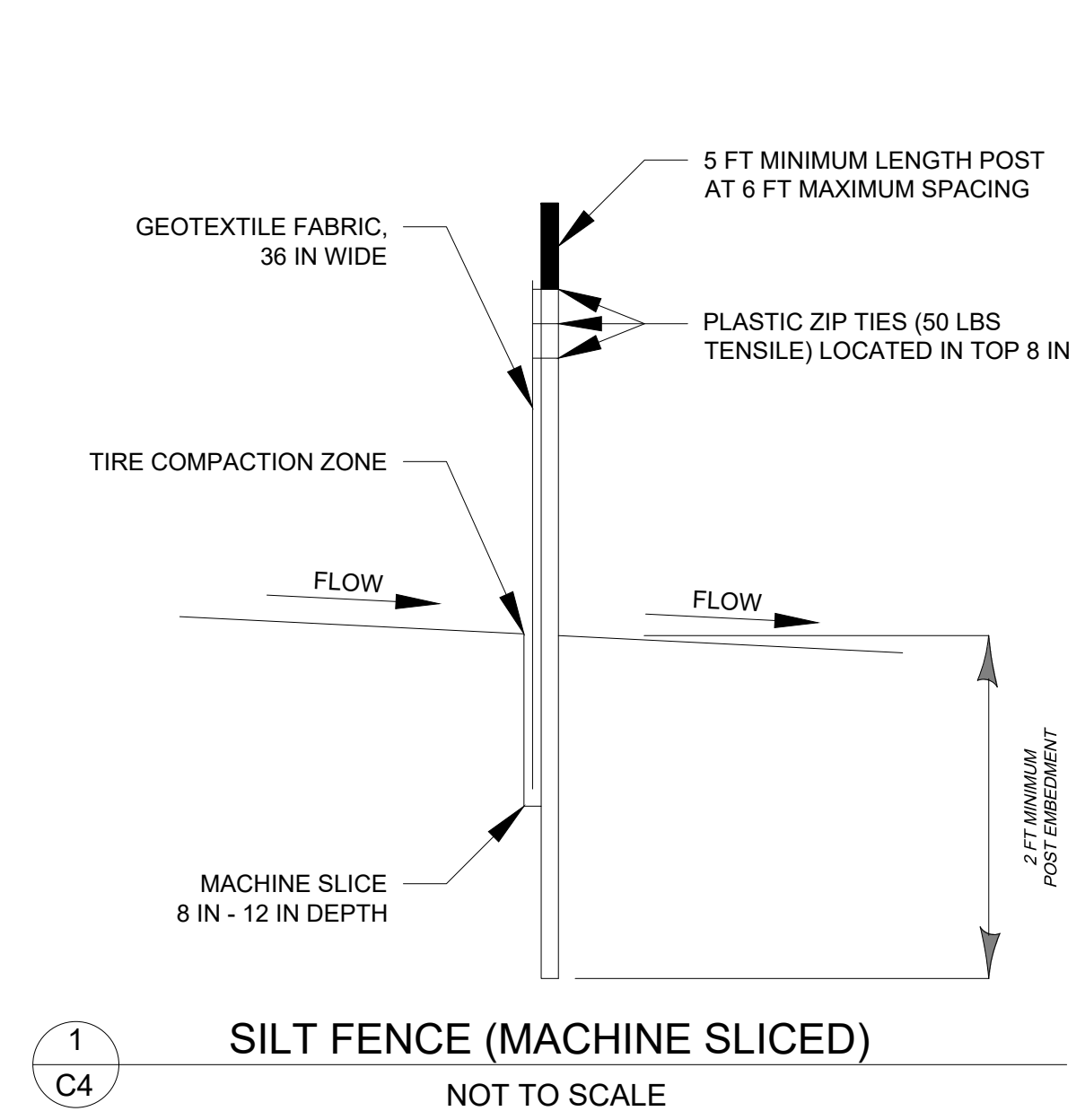
DRAWN: R.L.S. DATE: 3/4/2019 SCALE: NONE PLATE NO. EC-1



COON RAPIDS Minnesota

SURMOUNTABLE CURB DETAIL

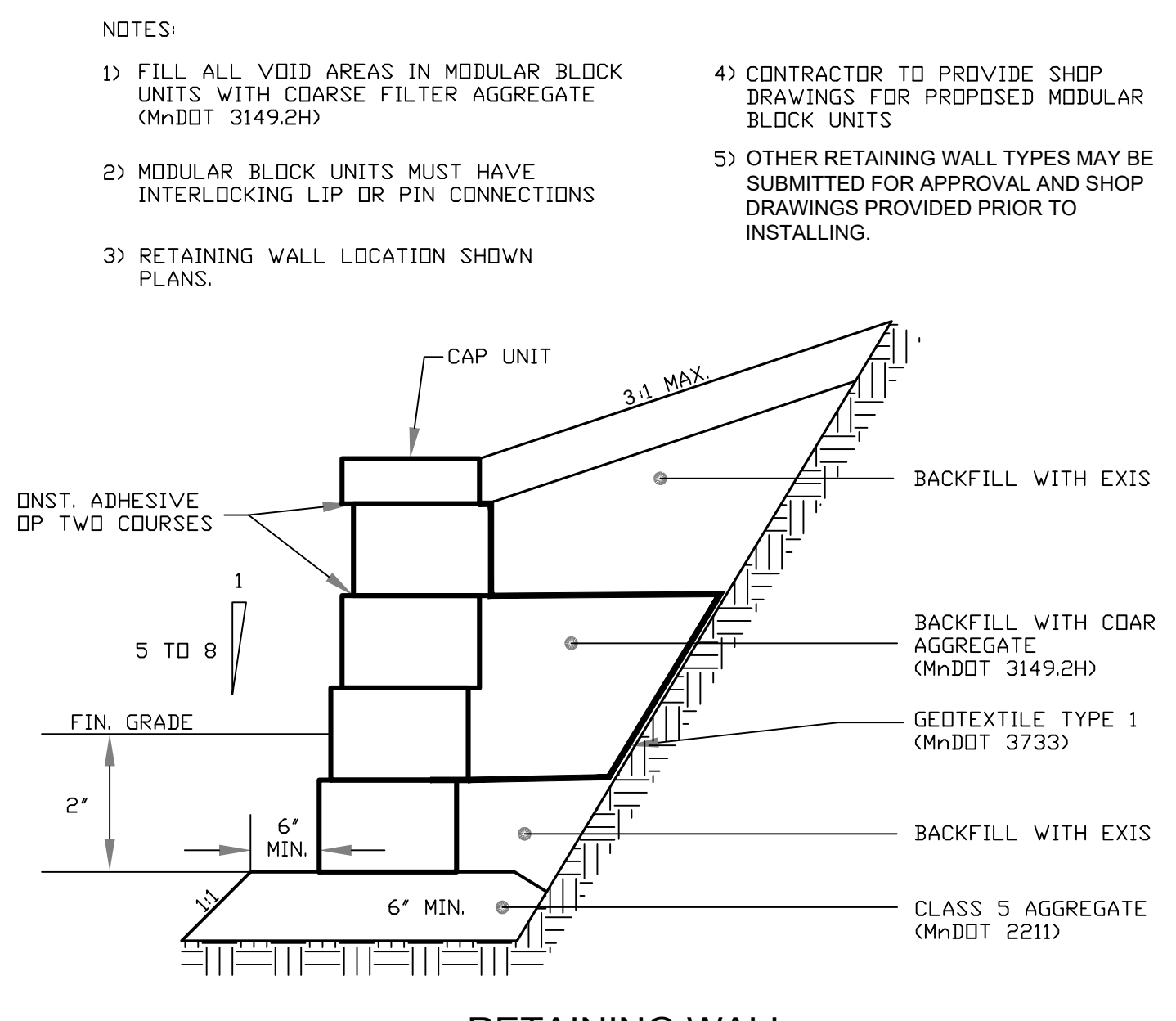
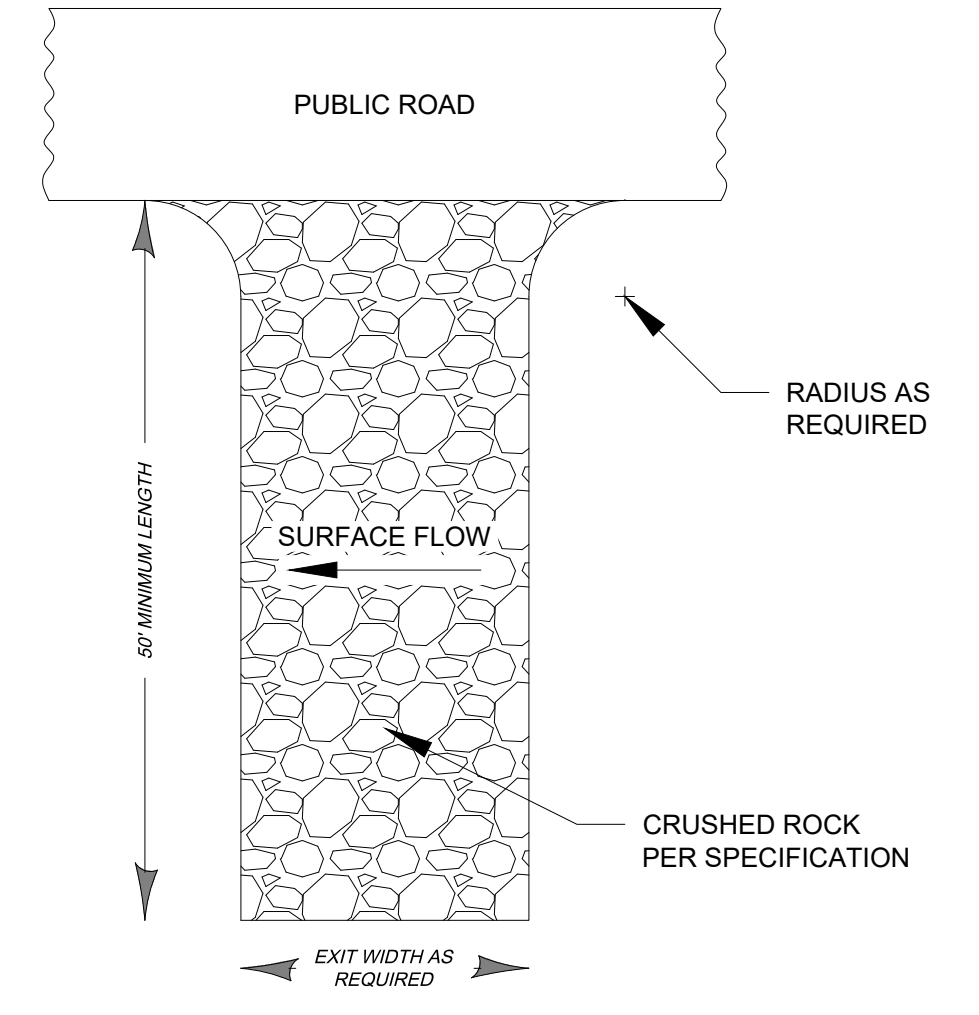
DRAWN: R.L.S. DATE: 1/24/2019 SCALE: NONE PLATE NO. STR-3



COON RAPIDS Minnesota

RIPRAP DETAIL (NO MOW STRIP)

DRAWN: R.L.S. DATE: 11/28/2018 SCALE: NONE PLATE NO. STM-12



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DRAWN BY:	ABL	<p style="text-align: center;">PRELIMINARY</p> <p style="text-align: center;">JEFFREY A. PRASCH, P.E.</p>		
CHECKED BY:	GRP		DATE: 2.28.23	LIC. NO.: 52706

REVISIONS	DATE	DESCRIPTION

DEMARC

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BLAINE, MINNESOTA 55434

CEDARWOOD

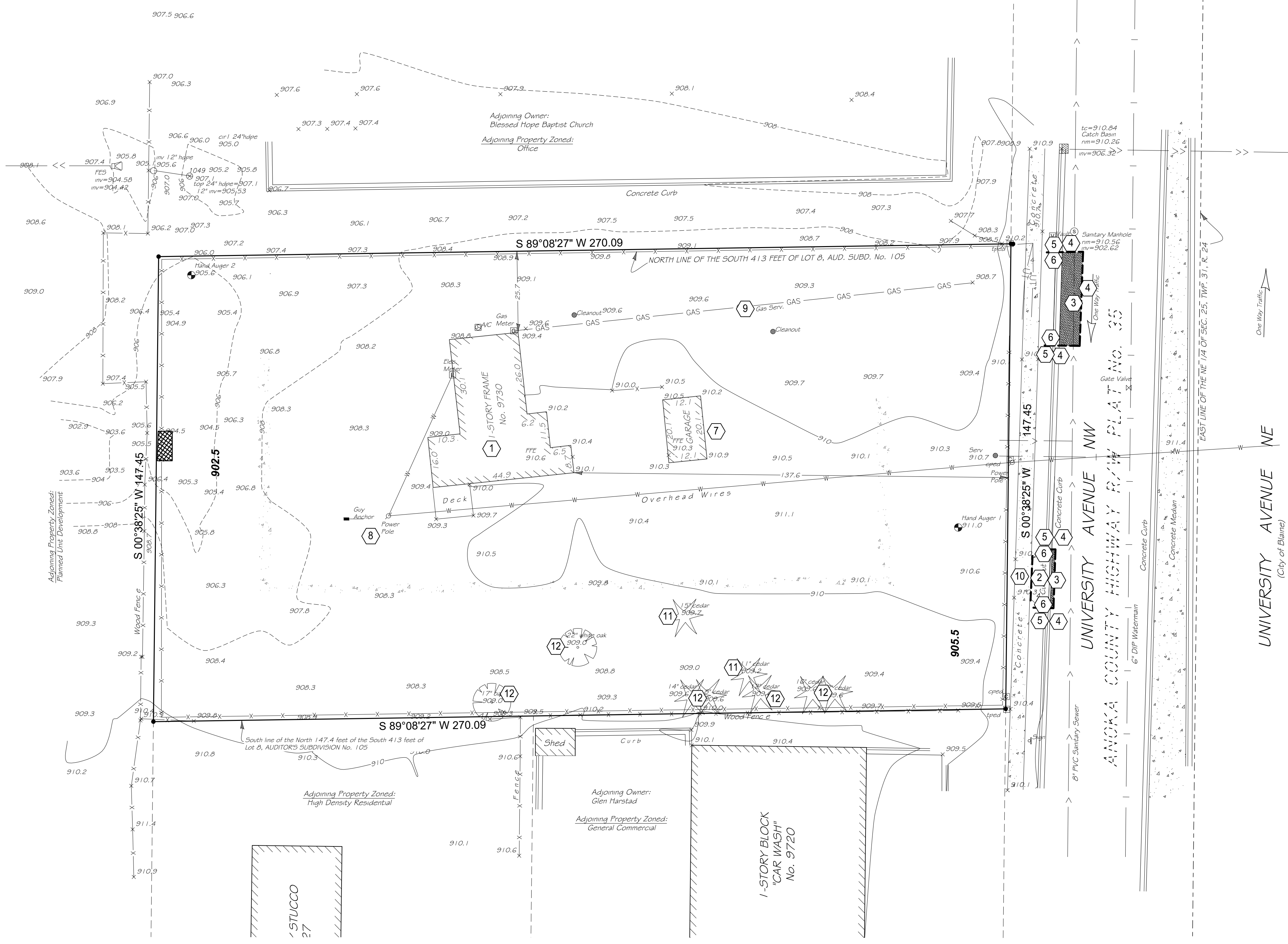
9730 UNIVERSITY AVE. NW
COON RAPIDS, MINNESOTA

DETAILS

PROJECT: 90259A

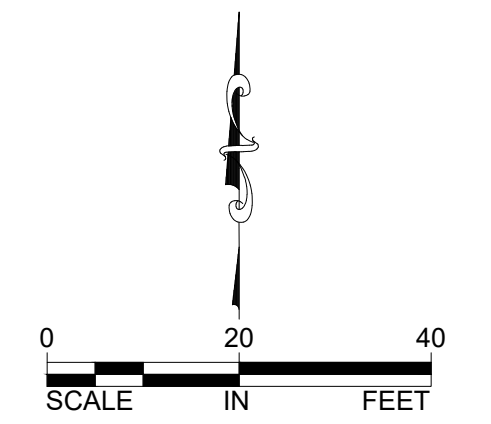
SHEET NO. C4 OF C8

F:\survey\auditors subdivision - anoka\105\9730 university ave\02 Engineering - 1111101 CAD\05 Sheet Files\03 Existing Conditions & Removals Plan.dwg



- REFERENCE NOTES:
- ① REMOVE BUILDING
 - ② REMOVE CONCRETE PAVEMENT (FULL DEPTH)
 - ③ REMOVE AND PATCH BITUMINOUS PAVEMENT (MATCH EXISTING PAVEMENT SECTION)
 - ④ SAWCUT BITUMINOUS PAVEMENT (FULL DEPTH)
 - ⑤ SAWCUT CONCRETE CURB & GUTTER (FULL DEPTH)
 - ⑥ REMOVE CONCRETE CURB & GUTTER
 - ⑦ REMOVE GARAGE
 - ⑧ RELOCATE POWER POLE AND OVERHEAD WIRES (COORDINATE WITH UTILITY COMPANY)
 - ⑨ RELOCATE GAS LINE (COORDINATE WITH UTILITY COMPANY)
 - ⑩ PROTECT SIDEWALK
 - ⑪ REMOVE TREES
 - ⑫ PROTECT TREES

- LEGEND
- 860 — EXISTING CONTOUR
 - EXISTING CURB & GUTTER
 - EXISTING BUILDING
 - REMOVE CURB & GUTTER
 - EXISTING STORM SEWER
 - REMOVE CONCRETE
 - EXISTING WATERMAIN
 - REMOVE BITUMINOUS PAVEMENT
 - EXISTING HYDRANT
 - EXISTING SANITARY MANHOLE
 - SAWCUT PAVEMENT



DESIGNED BY: JAP
 DRAWN BY: ABL
 CHECKED BY: GRP

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRELIMINARY

JEFFREY A. PRASCH, P.E.

DATE: 2.28.23 LIC. NO.: 52706

REVISIONS	
04.13.23	CLIENT COMMENTS

DEMARC
 LAND SURVEYING & ENGINEERING
 7601 73RD AVENUE N, BROOKLYN PARK, MN 55428
 PHONE: 763.560.3093 FAX: 763.560.3522
 www.Demarcinc.com

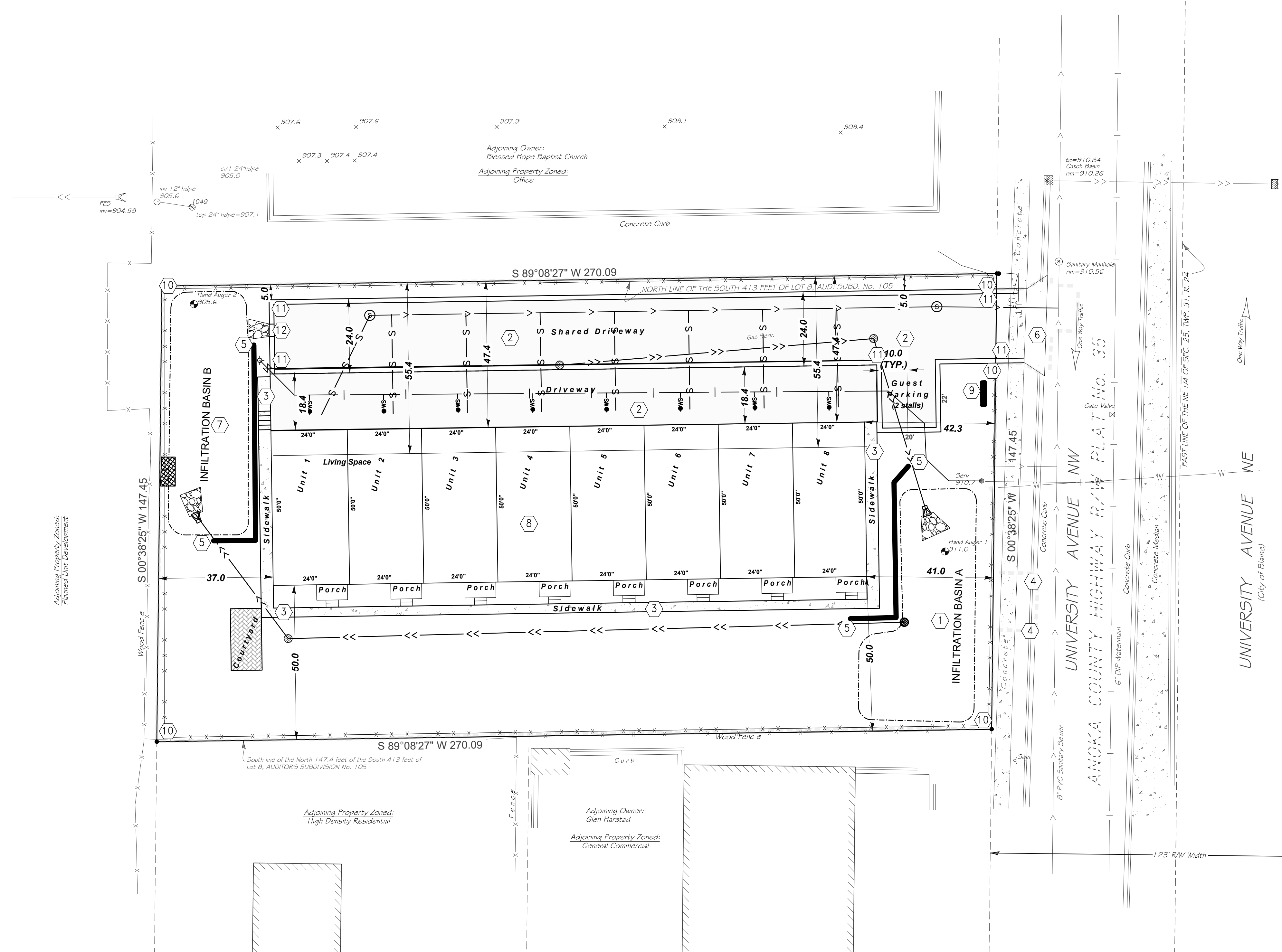
VALUE HOMES
 12301 CENTRAL AVE. NE, SUITE 101
 BLAINE, MINNESOTA 55434
 612.481.7841

CEDARWOOD
 9730 UNIVERSITY AVE. NW
 COON RAPIDS, MINNESOTA

EXISTING CONDITIONS & REMOVALS PLAN

PROJECT: 90259A
 SHEET NO.
 C5 OF C8

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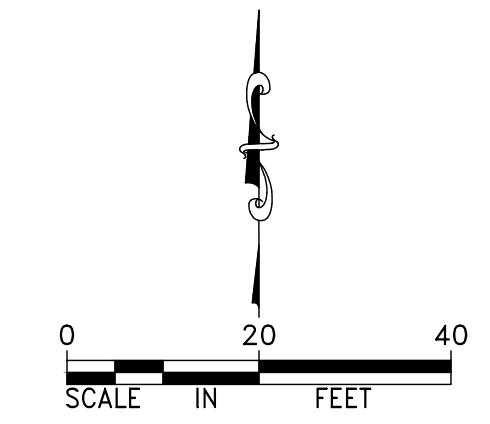


- REFERENCE NOTES:
- ① INFILTRATION BASIN "A" PER 1/C2 AND 2/C2
 - ② BITUMINOUS PAVEMENT
 - ③ CONCRETE SIDEWALK
 - ④ B612 CURB & GUTTER PER STR-2 ON C3 (MATCH EXISTING)
 - ⑤ MAXIMUM 4 FT HIGH WALL PER 4/C4 (BLOCK OR APPROVED EQUAL)
 - ⑥ DRIVEWAY ENTRY PER STR-10 ON C2
 - ⑦ INFILTRATION BASIN "B" PER 3/C2 AND 4/C2
 - ⑧ BUILDING (SEE ARCHITECTURAL PLANS)
 - ⑨ MONUMENT SIGN
 - ⑩ 6' FENCE
 - ⑪ SURMOUNTABLE CURB PER STR-3 ON C4
 - ⑫ 4' WIDE CURB CUT WITH RANDOM RIPRAP CLIFF

SITE DATA:

LOT AREA = 0.91 ACRES
 DISTURBED AREA = 0.93 ACRES
 EXISTING IMPERVIOUS AREA = 0.04 ACRES
 EXISTING PERVIOUS AREA = 0.87 ACRES
 PROPOSED IMPERVIOUS AREA = 0.48 ACRES
 PROPOSED PERVIOUS AREA = 0.43 ACRES
 BUILDING COVERAGE = 0.22 ACRES = 24.11%

- LEGEND**
- EXISTING BUILDING
 - EXISTING CURB & GUTTER
 - PROPOSED CURB & GUTTER
 - EXISTING STORM SEWER
 - PROPOSED STORM SEWER
 - EXISTING CONCRETE
 - PROPOSED CONCRETE
 - BITUMINOUS DRIVEWAY
 - EXISTING FENCE
 - EXISTING SIGN
 - PROPOSED PARKING STRIPE



DESIGNED BY: JAP
 DRAWN BY: ABL
 CHECKED BY: GRP

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PRELIMINARY

JEFFREY A. PRASCH, P.E.
 DATE: 2.28.23 LIC. NO.: 52706

REVISIONS	
04.13.23	CLIENT COMMENTS

DEMARC
 LAND SURVEYING & ENGINEERING
 7601 73RD AVENUE N, BROOKLYN PARK, MN 55428
 PHONE: 763.560.3093 FAX: 763.560.3522
 www.DemarcInc.com

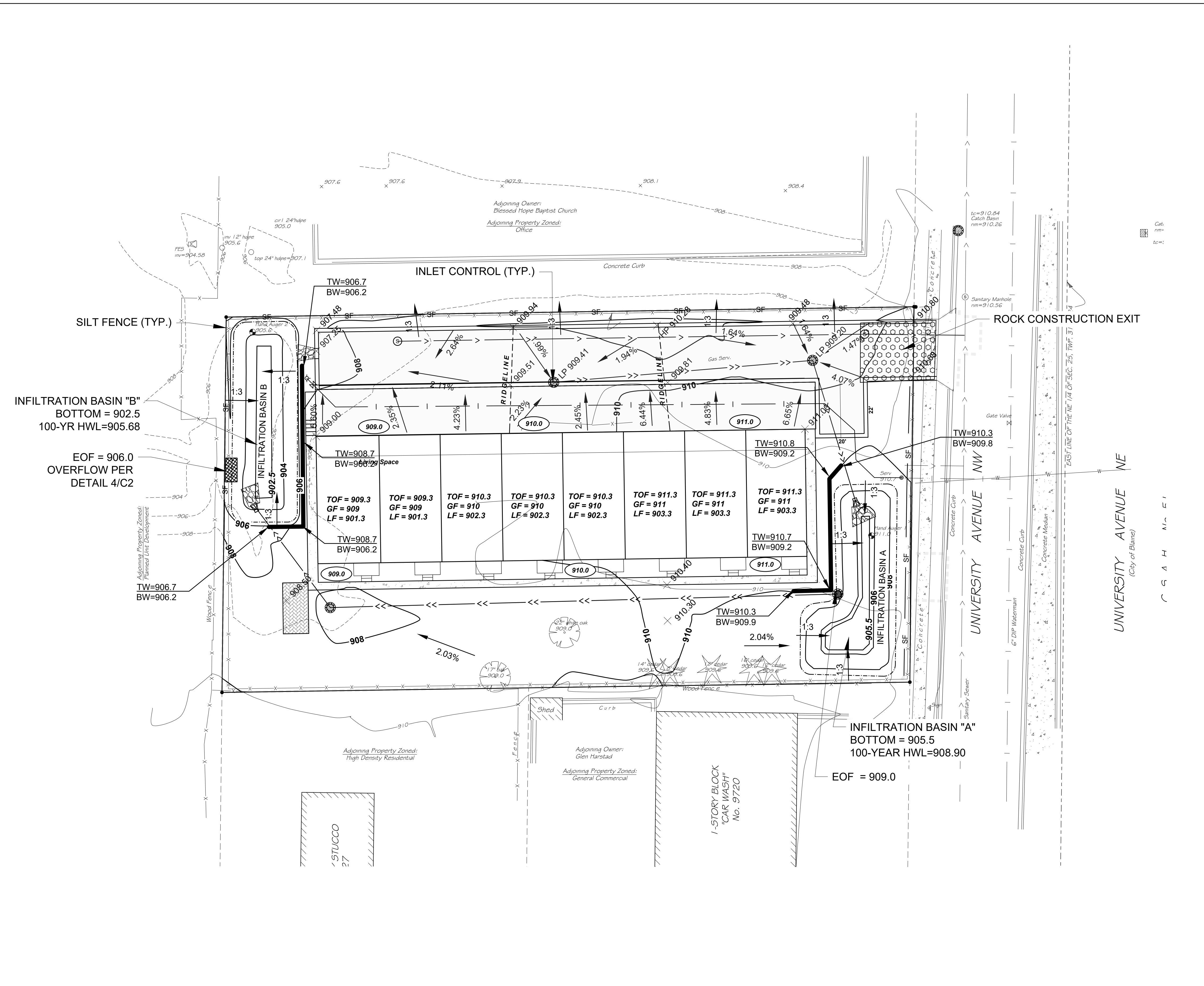
VALUE HOMES
 12301 CENTRAL AVE. NE, SUITE 101
 BLAINE, MINNESOTA 55434
 612.481.7841

CEDARWOOD
 9730 UNIVERSITY AVE. NW
 COON RAPIDS, MINNESOTA

SITE PLAN

PROJECT: 90259A
 SHEET NO.
 C6 OF C8

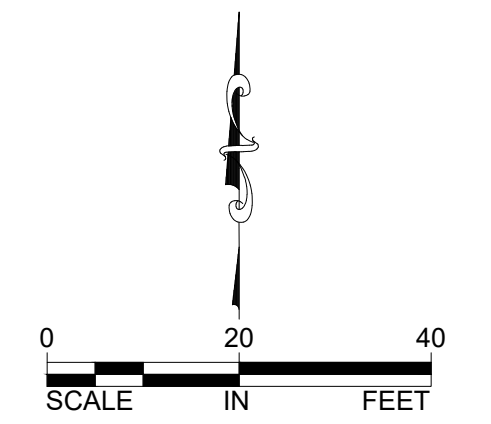
F:\survey\auditors subdivision - anokat108\9730 university ave\02 Engineering - 1111101 CAD\05 Sheet Files\05 Grading & Drainage Plan.dwg



- GENERAL CONSTRUCTION AND SOILS NOTES:**
- CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES, LAWS AND ORDINANCES.
 - THE CITY OF COON RAPID'S STANDARD SPECIFICATIONS FOR STREET AND UTILITY CONSTRUCTION SHALL GOVERN FOR ALL IMPROVEMENTS WITHIN THE PUBLIC RIGHT-OF-WAY.
 - THE 2020 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL APPLY.
 - THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR FOLLOWING ALL APPLICABLE OSHA STANDARDS.
 - IF UTILITY RELOCATION IS NECESSARY, THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE APPROPRIATE UTILITY COMPANIES.
 - MATERIAL UNSUITABLE FOR CONSTRUCTION (E.G. TOPSOIL, SILTS, ORGANIC MATERIALS, PEATS, DEBRIS, ETC.) ENCOUNTERED DURING EXCAVATION WITHIN THE PARKING LOT AND BUILDING PAD AREAS SHALL BE REMOVED AND PROPERLY DISCARDED.
 - GRADING SHALL BE CONSTRUCTED WITH SUITABLE MATERIAL AND IN LOOSE LIFTS NO THICKER THAN TWELVE (12) INCHES. COMPACTION WITHIN THE DRIVEWAY AND BUILDING PAD SHALL BE COMPACTED TO 100% OF THE MAXIMUM STANDARD PROCTOR DENSITY.
 - TOPSOIL SHALL BE SALVAGED AND STOCKPILED FOR USE AS TOPSOIL IN GREEN SPACES DISTURBED DUE TO CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR DISPOSING OF BITUMINOUS AND CONCRETE MATERIALS DISTURBED BY CONSTRUCTION IN ACCORDANCE WITH MNDOT SPEC 2104.3. ANY EXCESS MATERIAL SHALL BE DISPOSED OF OFF-SITE IN A MANNER ACCEPTABLE TO THE OWNER AND ALL GOVERNING REGULATIONS.
 - CONTRACTOR SHALL VERIFY ALL EXISTING UTILITIES AND CALL GOPHER STATE ONE CALL 48 HOURS PRIOR TO CONSTRUCTION WORK. EXISTING UTILITIES SHOWN ON THESE PLANS ARE ONLY APPROXIMATE AND MAY NOT BE RELIED UPON AS EXACT OR COMPLETE.

LEGEND

	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING BUILDING
	PROPOSED CONSTRUCTION/GRADING LIMITS
	EXISTING CURB & GUTTER
	PROPOSED CURB & GUTTER
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	EXISTING WATERMAIN
	PROPOSED WATERMAIN
	EXISTING BITUMINOUS PAVEMENT
	PROPOSED BITUMINOUS PAVEMENT
	EXISTING HYDRANT
	PROPOSED HYDRANT
	PROPOSED GATE VALVE
	EXISTING SANITARY MANHOLE
	PROPOSED SLOPE
	HP 860.00 PROPOSED HIGH POINT
	MP 860.00 PROPOSED MATCH POINT
	LP 860.00 PROPOSED LOW POINT
	860.00 PROPOSED GUTTER LINE OR FINISHED GROUND ELEVATION
	PROPOSED RIDGELINE
	PROPOSED INLET PROTECTION
	PROPOSED SILT FENCE
	PROPOSED ROCK CONSTRUCTION EXIT



DESIGNED BY: JAP	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.	REVISIONS	
DRAWN BY: ABL		04.13.23	CLIENT COMMENTS
CHECKED BY: GRP			
PRELIMINARY			
JEFFREY A. PRASCH, P.E.			
DATE: 2.28.23	LIC. NO.: 52706		

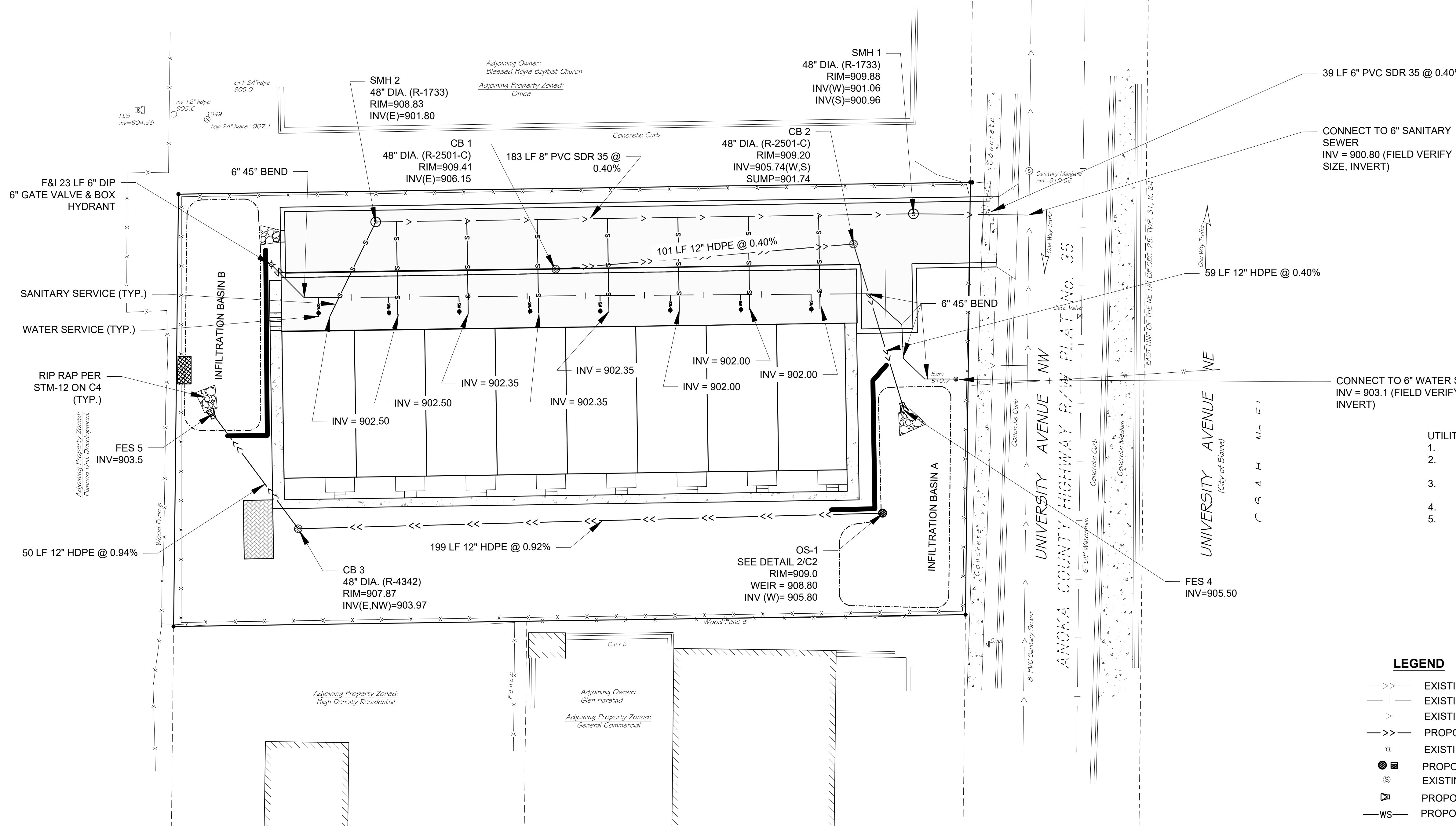
DEMARC
LAND SURVEYING & ENGINEERING
7601 73RD AVENUE N, BROOKLYN PARK, MN 55428
PHONE: 763.560.3093 FAX: 763.560.3522
www.DemarcInc.com

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612.481.7841

CEDARWOOD
9730 UNIVERSITY AVE. NW
COON RAPIDS, MINNESOTA

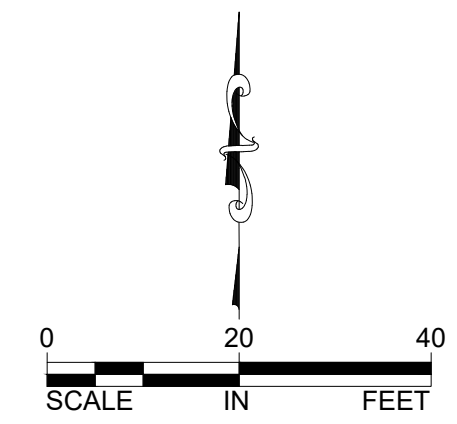
GRADING & EROSION CONTROL PLAN

PROJECT: 90259A
SHEET NO. C7 OF C8



- UTILITY NOTES:**
1. MAINTAIN 7.5 FT COVER OVER ALL WATERMAIN.
 2. ALL WATER SERVICES AND WATERMAIN CROSSING SANITARY SEWER SHALL BE INSULATED.
 3. FIELD VERIFY SANITARY SEWER AND WATERMAIN LOCATION AND ELEVATION.
 4. ALL HDPE PIPE SHALL BE DUAL-WALL PIPE
 5. ALL SAN SERV. SHALL BE 4" SCHEDULE 40 PVC

- LEGEND**
- >>— EXISTING STORM SEWER
 - |— EXISTING WATERMAIN
 - >— EXISTING SANITARY SEWER
 - >>— PROPOSED STORM SEWER
 - ⊕ EXISTING HYDRANT
 - ⊙ PROPOSED CATCH BASIN
 - ⊙ EXISTING SANITARY MANHOLE
 - ⊠ PROPOSED FES
 - WS— PROPOSED WATER SERVICE
 - S— PROPOSED SANITARY SERVICE



F:\survey\auditors subdivision - anoka\1059730 university ave\02 Engineering - 1111101 CAD\05 Sheet Files\06 Utility Plan.dwg

DESIGNED BY:	JAP	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.
DRAWN BY:	ABL	
CHECKED BY:	GRP	
DATE:	2.28.23	

REVISIONS	
04.13.23	CLIENT COMMENTS

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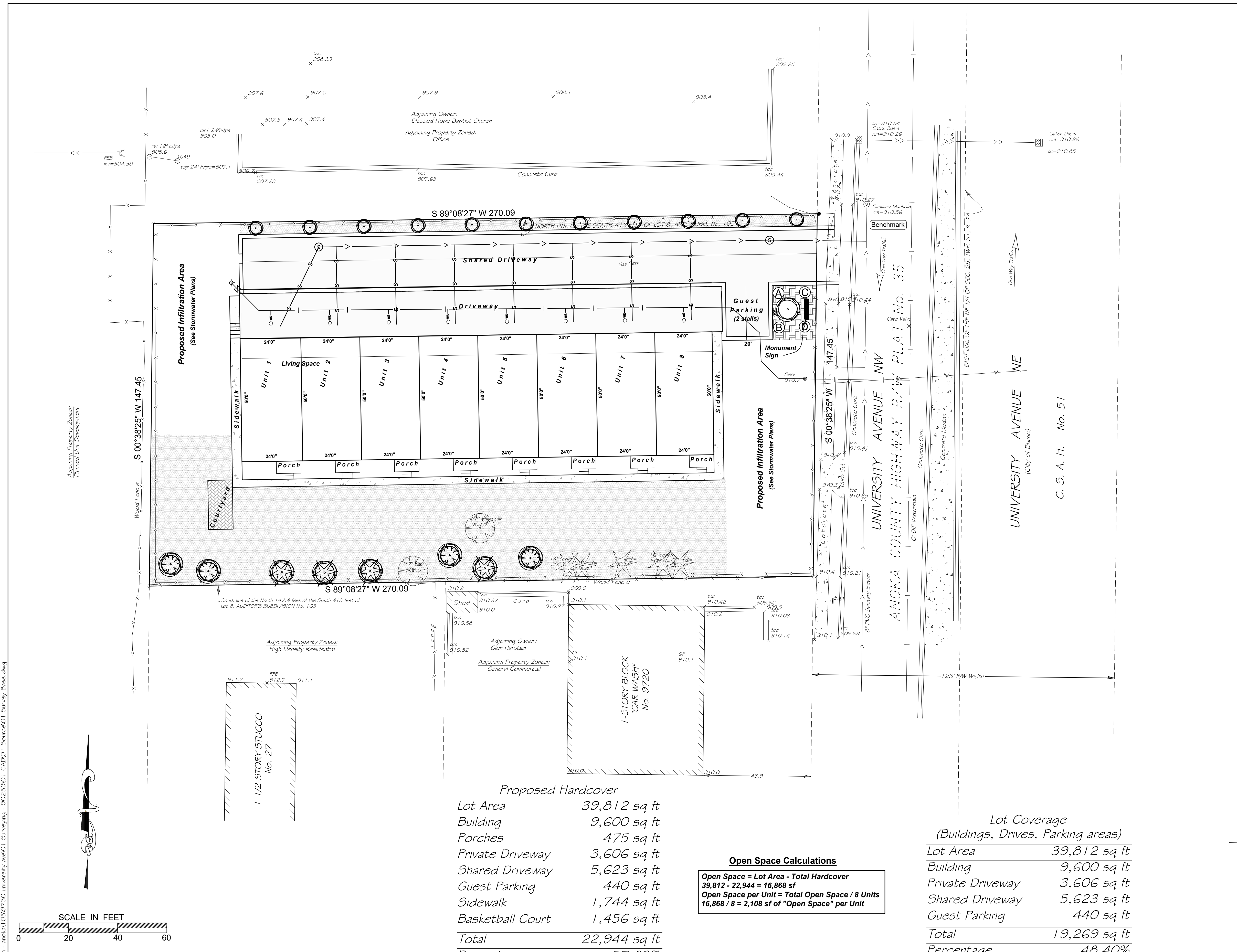
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 12301 CENTRAL AVE. NE, SUITE 101
 BLAINE, MINNESOTA 55434
 612.481.7841

CEDARWOOD
 9730 UNIVERSITY AVE. NW
 COON RAPIDS, MINNESOTA

UTILITY PLAN

PROJECT: 90259A
 SHEET NO.
 C8 OF C8

PRELIMINARY
 JEFFREY A. PRASCH, P.E.
 LIC. NO.: 52706



Legend

- Denotes Found Iron Monument
- Denotes Iron Monument Set
- ⊕ Power Pole
- ⊗ Catch Basin
- ⊕ Water Valve
- ⊗ Proposed Blackhill Spruce (5)
- ⊗ Proposed Sod with Sprinkler System
- GAS — Gas Main
- UT — Underground Communications
- UE — Underground Electric
- SS — Sanitary Sewer
- SWS — Storm Sewer
- W — Watermain
- W — Overhead Wires
- ⊗ Communication Pedestal Riser
- ⊗ Telephone Pedestal Riser
- ⊗ Gas Meter
- ⊗ Flared End Section
- ⊗ Denotes Existing Elevation
- ⊗ Proposed Japanese Lylac (11)
- ⊗ Existing Coniferous Tree to Remain
- ⊗ Existing Deciduous Tree to Remain
- ⊗ Proposed Maple (4)

Zoning Information

Property Currently Zoned: (LDR 1) Low Density Residential District (single family housing)

Zoning Ordinance Requirements:

- Building Setbacks - principal structure
 - Front - 40 feet
 - Side Yard - 15 feet (10 feet for garage)
 - Rear Yard - 50 feet
- See City code for specifics regarding balconies, decks & porches.
- Maximum Building Height - 35 feet
- Minimum Finished Floor Area - 1,200 sq.ft
- Maximum lot area to be covered by impervious surface - 20%
- Minimum number of parking stalls per unit - 3

Refer to City code for additional requirements, exceptions and modifications.

Miscellaneous Notes

- Property Address: 9730 University Ave. NW, Coon Rapids, MN 5448
PID No.: 25-31-24-14-0005
- Area of Parcel = 39,812 sq. ft.
- By graphic interpretation only, this property is in Flood Zone "X" (area of minimal flooding) per FEMA panel map number 27003C0319E, effective on 12/16/2015
- Title insurance commitment showing property description and any legal encumbrances of record not provided, survey subject to change. The only easements shown are from plats of record or information provided by client.
- Property located in Section 25, Township 32, Range 24, Anoka County, Minnesota.
- Benchmark: Invert of manhole on University Ave. at the NE corner of property as shown. Information per maps from City of Coon Rapids
Elevation = 902.62 feet
- Heavy snow cover on site at time of survey, some improvements may not have been visible.

Landscape Detail (no scale)

Typical Unit

- A Dogwood x 9 (1 Per Unit + Sign Area)
- B Hydrangea x 9 (1 Per Unit + Sign Area)
- C Barberry x 9 (1 Per Unit + Sign Area)
- D Spirea x 9 (1 Per Unit + Sign Area)

Decorative Mulch With Irrigation for Plants

Legal Description

The North 147.40 feet of the South 413 feet of Lot 8, Auditor's Subdivision No. 105, Anoka County, Minnesota. Subject to road.

Proposed Hardcover

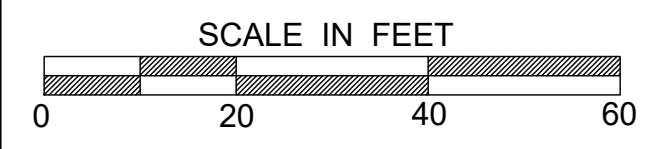
Lot Area	39,812 sq ft
Building	9,600 sq ft
Porches	475 sq ft
Private Driveway	3,606 sq ft
Shared Driveway	5,623 sq ft
Guest Parking	440 sq ft
Sidewalk	1,744 sq ft
Basketball Court	1,456 sq ft
Total	22,944 sq ft
Percentage	57.63%

Open Space Calculations

Open Space = Lot Area - Total Hardcover
 39,812 - 22,944 = 16,868 sf
 Open Space per Unit = Total Open Space / 8 Units
 16,868 / 8 = 2,108 sf of "Open Space" per Unit

Lot Coverage (Buildings, Drives, Parking areas)

Lot Area	39,812 sq ft
Building	9,600 sq ft
Private Driveway	3,606 sq ft
Shared Driveway	5,623 sq ft
Guest Parking	440 sq ft
Total	19,269 sq ft
Percentage	48.40%



FIELD BY: RP
 DRAWN BY: JPF
 CHECKED BY: GRP

I certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.
 Prepared this 28th day of February, 2025.

PRELIMINARY

Signed: *Gregory R. Prasch*
 Gregory R. Prasch Registration No. 24992

REVISIONS

Date	Description
04.12.23	Client Comments

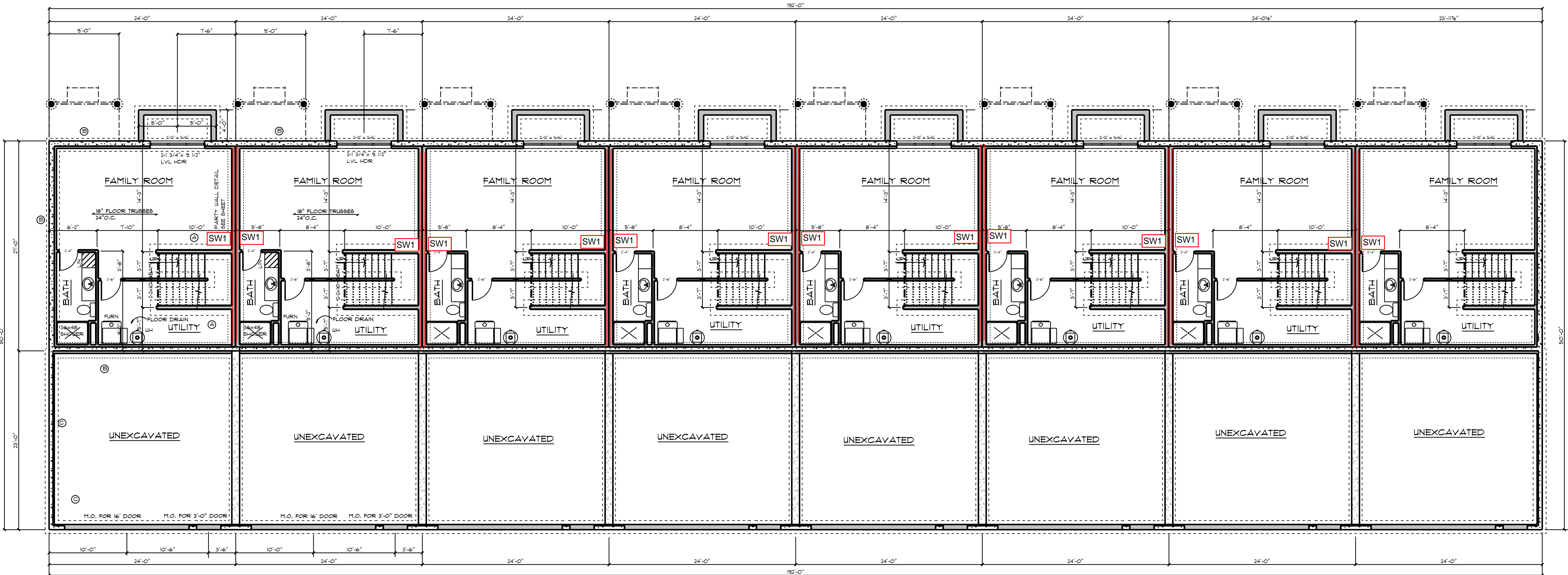
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 LAND SURVEYING & ENGINEERING
 7601 73rd Avenue North (763) 560-3093
 Minneapolis, Minnesota 55428 Demarcinc.com

SURVEY FOR:
VALUE HOMES
 12301 CENTRAL AVE. NE, SUITE 101
 BLAINE, MINNESOTA 55434
 612.481.7841

CEDARWOOD
 9730 UNIVERSITY AVE. NW
 COON RAPIDS, MINNESOTA

LANDSCAPE PLAN

PROJECT: 90259A
 FB No: 1128-54
 SHEET NO.
 L1 OF L1

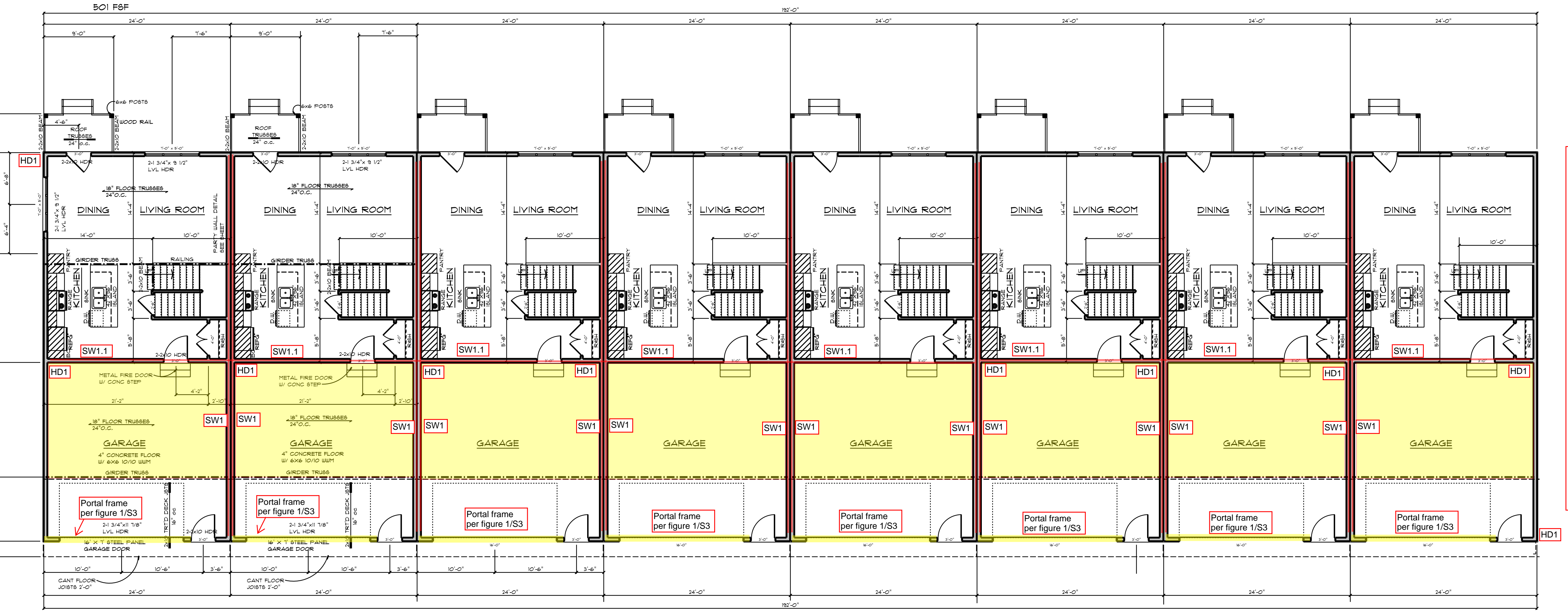


FOOTING NOTES:

- ⊙ 24" FLOOR TRUSSES
- ⊙ 2" POURED CONCRETE ON A CONCRETE FOOTING

NOTE: ALL DRILLING FOOTING TO BE 1000 PSI CONCRETE

FOUNDATION
SCALE: 1/8" = 1'-0"



Exterior Walls:
(1) 2x6" stud grade 16" O.C. w/ sill fastened w/ (2) rows (staggered) of 16d nails 12" O.C. w/ 7/16" OSB fastened to studs/sills/plates w/ 8d nails 6" O.C. at all edges and 12" in the field.

SW1:
Shear wall w/ (1) 2x6" stud grade 16" O.C. w/ sills fastened w/ (2) rows of 16d nails 12" O.C. w/ 5/8" gypsum board panels fastened w/ #9 x 1-1/4" screws 7" O.C. at all edges and 7" in the field w/ truss blocking per figure 2/S3

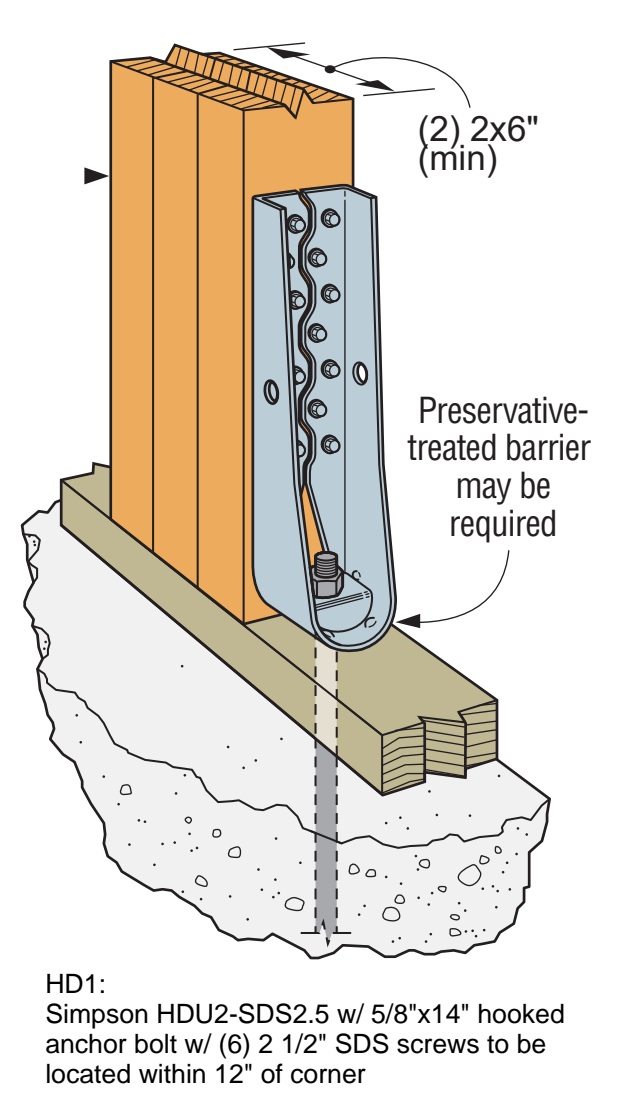
SW1.1:
Shear wall w/ (1) 2x6" stud grade 16" O.C. w/ (1) row of blocking at top of door headers w/ sills fastened w/ 1/2" anchor bolts 48" O.C. and within 12" of ends and openings w/ 5/8" gypsum board panels fastened w/ #9 x 1-1/4" screws 7" O.C. at all edges and 7" in the field w/ (1) floor truss directly above fastened to top plate w/ (2) rows of 10d nails 6" O.C.

Fasten floor sheathing to trusses w/ (1) row of 10d nails (or #10x2-1/2" screws) 6" O.C. at all edges and 12" in the field w/ all seams blocked solid w/ (1) flat 2x4"

Structural Wind Bracing Details Provided by: Becklin & Whitney Consulting Engineers, Inc
139 1st Ave. E. Ste 200
P.O. Box 471
Cambridge, MN 55008
763.689.5631

1. MNBC 2020 Code
2. All hardware by Simpson Strong Tie
3. LVLs to be 1.75" thick and 1.9E rated
4. Stamped truss plans to be submitted to EOR prior to manufacturing
5. Truss manufacturer to supply holdowns adequate to resist uplift at each truss
6. Hilti HIT-HY 200 series epoxy

BRACED WALLS ONLY



I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

William A. Becklin
WILLIAM A. BECKLIN, P.E.
DATE: AUGUST 22, 2023 LIC. NO. 18494

WINDOW UNIT SIZE SHOWN (SUPPLIER TO VERIFY COMPLIANCE W/ ALL LOCAL CODES)

648 SF

MAIN FLOOR
SCALE: 1/8" = 1'-0"

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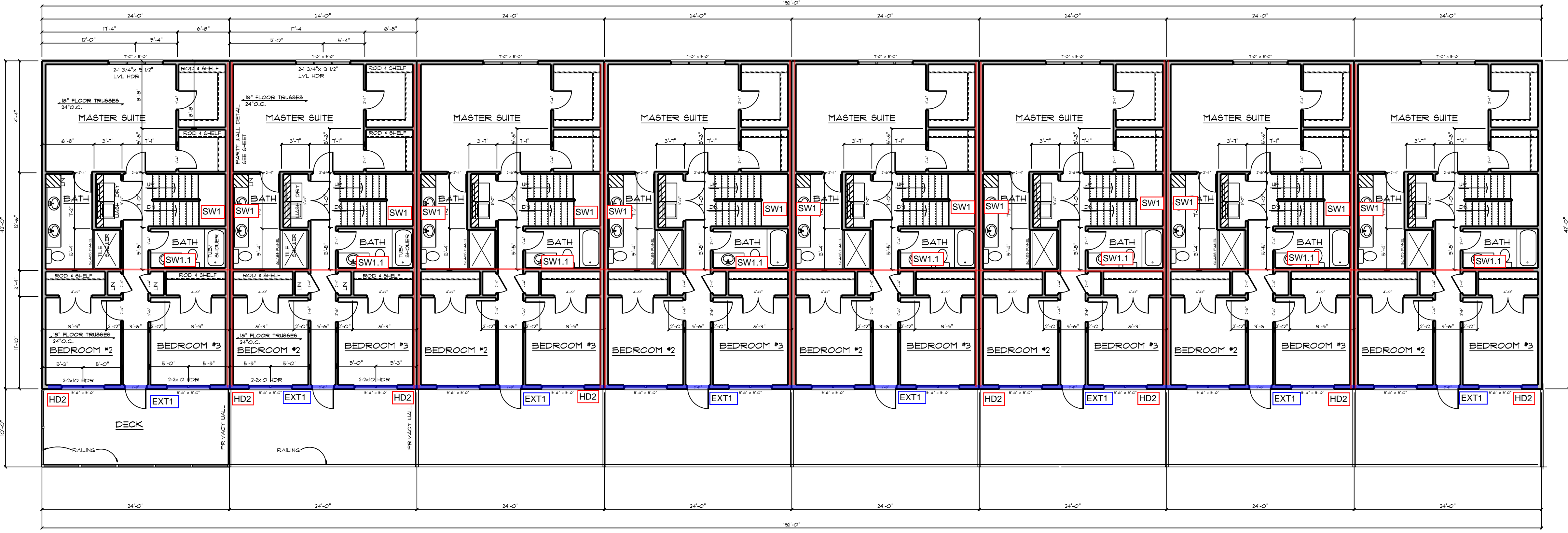
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NO.	REVISIONS

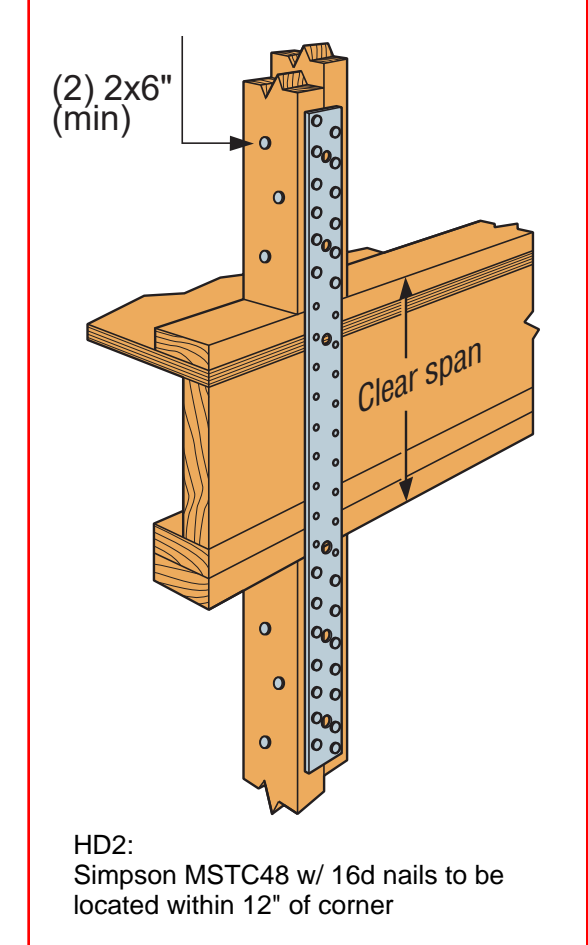
Exterior Walls UNO:
 (1) 2x6" stud grade 16" O.C. w/ sill fastened w/ (2) rows (staggered) of 16d nails 12" O.C. w/ 7/16" OSB fastened to studs/sills/plates w/ 8d nails 6" O.C. at all edges and 12" in the field.

SW1.1:
 Shear wall w/ (1) 2x6" stud grade 16" O.C. w/ sills fastened w/ (2) rows of 16d nails 12" O.C. w/ 5/8" gypsum board panels fastened w/ #9 x 1-1/4" screws 7" O.C. at all edges and 7" in the field w/ truss blocking per figure 2/S3

Exterior Walls 1 (EXT1):
 (1) 2x6" stud grade 16" O.C. w/ sill fastened w/ (2) rows (staggered) of 16d nails 12" O.C. w/ 7/16" OSB fastened to studs/sills/plates w/ 8d nails 4" O.C. at all edges and 8" in the field w/ solid blocking at all seams and blocking at top of headers w/ (1) CS16 fastened to blocking at top of headers w/ 0.148x2-1/2" nails extending 24" (min) beyond each side of opening.

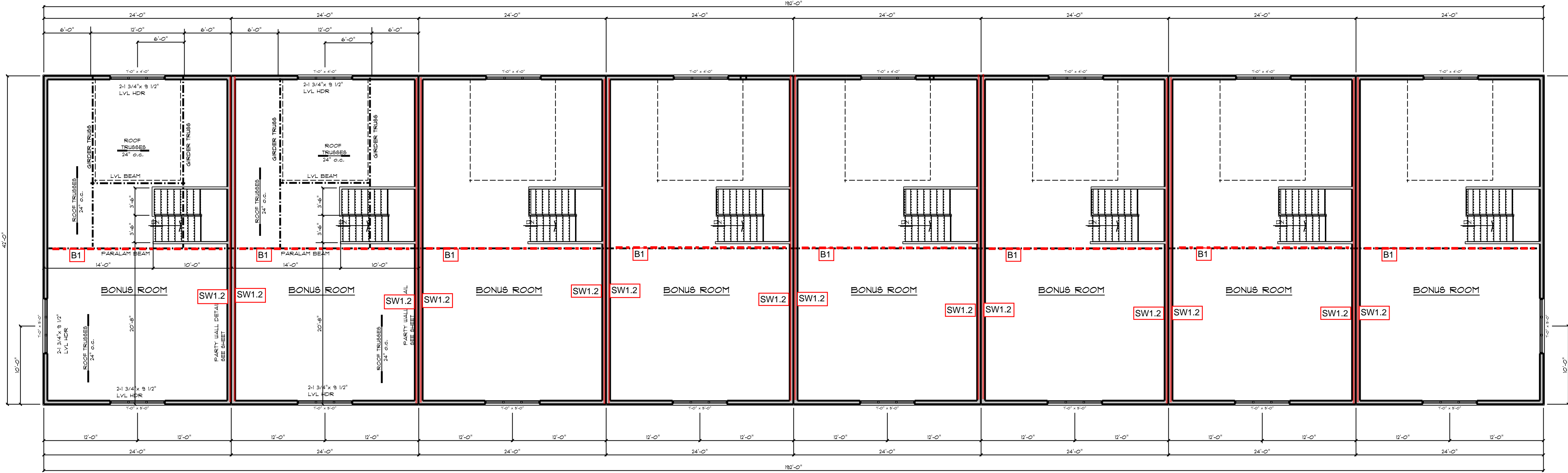


SECOND FLOOR
 SCALE: 1/8" = 1'-0"
 1008 SF



B1:
 (4) 1.75x20" 1.9E LVL w/ (1) 5.25x7" 1.8E Parallam columns w/ solid blocking to foundation

SW1.2:
 Shear wall w/ (1) 2x6" stud grade 16" O.C. w/ sills fastened w/ (2) rows of 16d nails 12" O.C. w/ 5/8" gypsum board panels fastened w/ #9 x 1-1/4" screws 7" O.C. at all edges and 7" in the field w/ (1) roof truss fastened to top plate w/ (1) row of 10d nails (toenailed) 6" O.C.



3RD FLOOR
 SCALE: 1/8" = 1'-0"
 1036 SF

Structural Wind Bracing Details Provided by:
 Becklin & Whitney Consulting Engineers, Inc.
 139 1st Ave. E, Ste 200
 P.O. Box 471
 Cambridge, MN 55008
 763.689.5631

- MNBC 2020 Code
- All hardware by Simpson Strong Tie
- LVLs to be 1.75" thick and 1.9E rated
- Stamped truss plans to be submitted to EOR prior to manufacturing
- Truss manufacturer to supply holdowns adequate to resist uplift at each truss
- Hilti HIT-HY 200 series epoxy

BRACED WALLS ONLY

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William A. Becklin
 WILLIAM A. BECKLIN, P.E.
 DATE: AUGUST 22, 2023 LIC. NO. 18494

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REVISIONS

Certificate Of Completion

Envelope Id: 415559CFF99B4DD99DA832574D7C965D	Status: Delivered
Subject: Kyle Pedersen - ROW Permit Application	
Source Envelope:	
Document Pages: 16	Signatures: 1
Certificate Pages: 4	Initials: 1
AutoNav: Enabled	Envelope Originator:
Envelopeld Stamping: Enabled	Highway Permits
Time Zone: (UTC-06:00) Central Time (US & Canada)	Anoka County Government Center
	2100 3rd Avenue
	Anoka, MN 55303
	HighwayPermits@co.anoka.mn.us
	IP Address: 184.83.22.203

Record Tracking

Status: Original	Holder: Highway Permits	Location: DocuSign
10/30/2023 11:41:17 AM	HighwayPermits@co.anoka.mn.us	
Security Appliance Status: Connected	Pool: StateLocal	
Storage Appliance Status: Connected	Pool: Anoka County	Location: DocuSign

Signer Events

Kyle Pedersen
 kyle@wruckcompanymn.com
 Security Level:
 DocuSign.email
 ID: 1
 10/30/2023 11:41:19 AM

Signature

DocuSigned by:

 727D4EC46AF449A...
 Signature Adoption: Pre-selected Style
 Using IP Address: 184.83.22.203

Timestamp

Sent: 10/30/2023 11:41:18 AM
 Viewed: 10/30/2023 11:42:21 AM
 Signed: 10/30/2023 12:32:28 PM

Electronic Record and Signature Disclosure:

Accepted: 10/26/2023 1:46:51 PM
 ID: 71512f4e-d22e-4ec6-a1bc-5fc915baf485

Susan Burgmeier
 Susan.Burgmeier@co.anoka.mn.us
 Associate Traffic Technician
 Anoka County
 Signing Group: Highway Permits
 Security Level: Email, Account Authentication (Optional)

Sent: 10/30/2023 12:32:34 PM
 Viewed: 10/30/2023 2:09:56 PM

Electronic Record and Signature Disclosure:

Accepted: 10/18/2023 1:18:14 PM
 ID: ca31c322-b472-4062-905d-23282e0ac2a3

In Person Signer Events	Signature	Timestamp
Editor Delivery Events	Status	Timestamp
Agent Delivery Events	Status	Timestamp
Intermediary Delivery Events	Status	Timestamp
Certified Delivery Events	Status	Timestamp
Carbon Copy Events	Status	Timestamp
Witness Events	Signature	Timestamp
Notary Events	Signature	Timestamp

Envelope Summary Events	Status	Timestamps
Envelope Sent	Hashed/Encrypted	10/30/2023 11:41:18 AM
Envelope Updated	Security Checked	10/30/2023 12:32:28 PM
Certified Delivered	Security Checked	10/30/2023 2:09:56 PM

Payment Events	Status	Timestamps
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Electronic Record and Signature Disclosure

ELECTRONIC RECORD AND SIGNATURE DISCLOSURE

The following information constitutes Anoka County's (we, us or Company) written notices or disclosures relating to your use of DocuSign in relation to Anoka County's license. Described below are the terms and conditions for providing notices and disclosures electronically through the DocuSign system. Please read the information below carefully and thoroughly, and if you agree to this Electronic Record and Signature Disclosure (ERSD), please confirm your agreement by selecting the check-box next to 'I agree to use electronic records and signatures' before clicking 'CONTINUE' within the DocuSign system.

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At any time, you may request from us a paper copy of any record provided or made available electronically to you by us. You will have the ability to download and print documents we send to you through the DocuSign system during and immediately after the signing session and, if you elect to create a DocuSign account, you may access the documents for a limited period of time (30 days) after the documents are first sent to you. At any time, if you request paper copies of any documents, you may be charged a fee. You may request paper copies from us by following the procedure described below.

Withdrawing your consent

If you decide to receive notices, disclosures and documents from us electronically, you may change your mind and tell us that going forward you want to receive documents only in paper format. Please note, processing time will be slowed down dramatically as we will be required to print and send the document through the mail and await your return of the documents. The process to change the method of receipt is described below.

All notices and disclosures will be sent to you electronically

Upon your acceptance to receive electronic notifications, all required notices, disclosures, authorizations, acknowledgements, and other documents that are required to be provided or made available to you during the course of our relationship with you will be provided electronically through the DocuSign system. All of the required documents will be provided to you at the address that you have given us.

How to contact Anoka County:

You may change your preferred method of contact whether electronically, or paper copies, or change your email address. You may also request paper copies of certain information from us, or withdraw your prior consent to receive documents. Please use the contact information below for your request and in the body of your correspondence, identify your desired action. If you are

changing an email address, please include your prior email address as well as your new address. If you no longer wish to receive future documents in electronic format, please include that request in the body of your email.

Email: helpdesk@co.anoka.mn.us

Phone: (763)-324-4110

Address: Anoka County Government Center
Attn: Information Technology, #300
2100 3rd Avenue
Anoka, MN 55303

Required hardware and software

The minimum system requirements for using the DocuSign system may change over time. The current system requirements are found here: <https://support.docusign.com/guides/signer-guide-signing-system-requirements>.

ACKNOWLEDGEMENT

To confirm your access to the electronic notices and disclosures, which will be similar to other electronic notices and disclosures that we may provide to you, please acknowledge that you have read this ERSD by selecting the check-box next to 'I agree to use electronic records and signatures' before clicking 'CONTINUE' within the DocuSign system.

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- You can access and read this Electronic Record and Signature Disclosure; and
- You can print this Electronic Record and Signature Disclosure, or save or send this Electronic Record and Disclosure to a location where you can print it, for future reference and access; and
- Until or unless you notify Anoka County as described above, you consent to receive exclusively through electronic means all notices, disclosures, authorizations, acknowledgements, and other documents that are required to be provided or made available to you by Anoka County during the course of your relationship with Anoka County.